

**Phase 1 Cranford Historic Resources Survey:
Review of Existing Documentation, Baseline Inventory, and Survey Updates**

Cranford Historic Preservation Advisory Board
Cranford Township, NJ
November 2016

Volume 2: Appendices

**Phase 1 Cranford Historic Resources Survey:
Review of Existing Documentation, Baseline Inventory, and Survey Updates**

Cranford Historic Preservation Advisory Board

Cranford Township, NJ

November 2016

Volume 2: Appendices

HPAB Survey Committee:

Kinney Clark

Vic Bary

Maureen McDougall

Maureen Starzdon

Bill King

Cranford Township Committee:

Andis Kalnins, Mayor

Mary O'Connnor, Deputy Mayor

John Mallon

Thomas H. Hannen, Jr.

Patrick Giblin

Historic Preservation Advisory Board:

Maureen Strazdon, Chair

Victor Bary

Kinney Clark

Shirley Gazsi

William King

Michael Mason

Maureen McDougall

Ronald Meeks

Teresa Montani

Julie Murphy

Pria McNeil

Nancy Price

Stephen Price

Loretta Smith

Carolyn Youngs

APPENDIX A: INDIVIDUAL PROPERTY DOCUMENTATION

This appendix contains survey forms and other documentation for all baseline historic resources listed above. These documents are extracted and reproduced from the original sources, and are organized by survey ID number as presented in **Table 4: Baseline Inventory, Individual Properties**. As such, documents from the same source may not appear in the same order as originally organized. Phase 1 resource ID numbers have been appended to the original documents in order to aid navigating this section.

(Page intentionally left blank)

35 Alden Street, Cranford, New Jersey	c. 1900 two-and-a-half story vernacular Victorian residence with asphalt shingle roofing, vinyl siding on upper floors and permastone on 1st floor. Additions include upper story enclosed porch and altered entry doors.	700 feet/W	Not Eligible	n/a	B.5.10
37 Alden Street, Cranford, New Jersey	c. 1900, two-and-a-half-story Victorian-style residence, jerkinhead roof with asphalt shingle roofing, shed dormers, vinyl siding. Alterations include replacement windows and large one story addition to primary facade.	700 feet/W	Not Eligible	n/a	B.5.11
Verizon Building, Alden Street, Cranford, New Jersey	c. 1945, one-story commercial building with flat roof, brick exterior, concrete foundation. Roofline is treated with simple railing and “dentils” just below. Large shutters flank the windows on the primary facade. Windows elsewhere are very large 6/6.	1000 feet/W	Not Eligible	n/a	B.5.13
Saint Michael’s Roman Catholic Church, Alden Street, Cranford, New Jersey	1950, two-story, basilica-style church gable, tiled roof, brick exterior with buttresses, round windows in the clerestory and arched topped windows with heavy drip caps on the lower level. Front facade has slightly punched out center bay with rose window and two portals with tympanum.	1000 feet/W	Not Eligible	n/a	B.5.15
Saint Michael’s School, Alden Street, Cranford, New Jersey	1929, two-story, Neo- Classical school building, flat roofed, red brick exterior with stone water table course and parapet wall course. Central entrance is dominated by stonework with pediment, pilasters and 2 story blind arches. Fenestration is grouped in threes.	800 feet/NW	Not Eligible	n/a	B.5.17

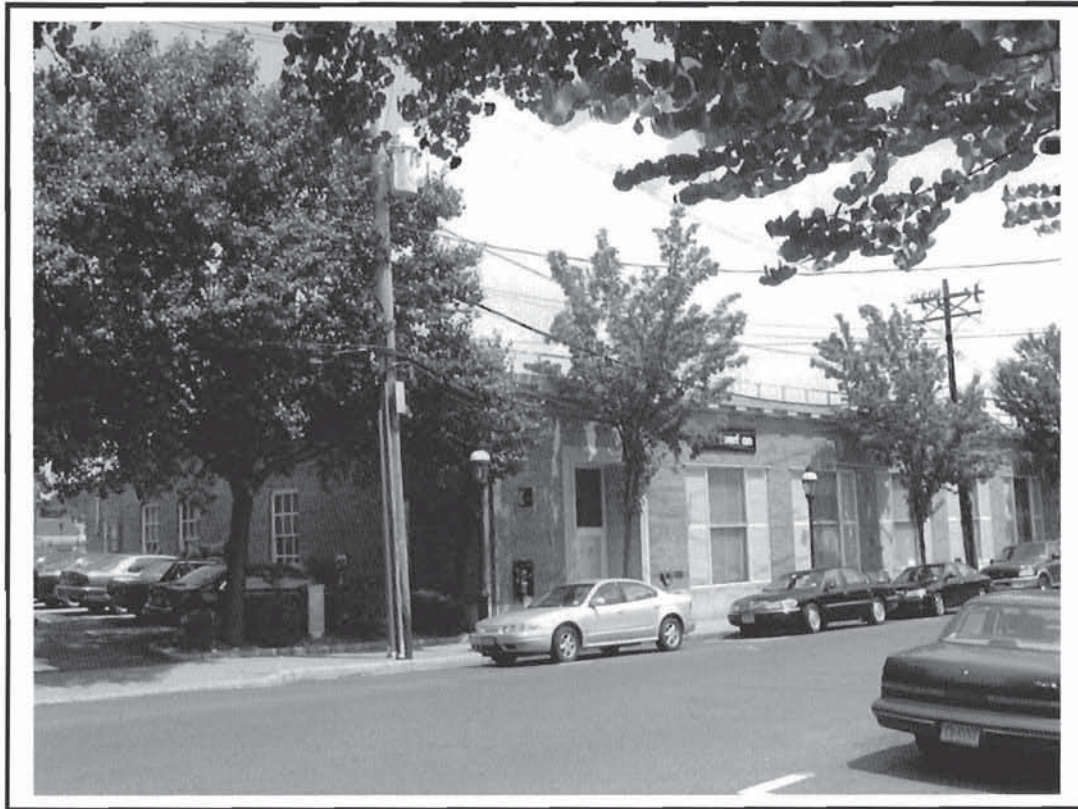


PHOTO LOCATOR MAP

**Plate B.5.13:**

The Verizon Building on Alden Street.
Photo view: East
Photographer: Elaine Kiernan Gold
Date: August 1, 2001

35 Alden Street, Cranford, New Jersey	c. 1900 two-and-a-half story vernacular Victorian residence with asphalt shingle roofing, vinyl siding on upper floors and permastone on 1st floor. Additions include upper story enclosed porch and altered entry doors.	700 feet/W	Not Eligible	n/a	B.5.10
37 Alden Street, Cranford, New Jersey	c. 1900, two-and-a-half-story Victorian-style residence, jerkinhead roof with asphalt shingle roofing, shed dormers, vinyl siding. Alterations include replacement windows and large one story addition to primary facade.	700 feet/W	Not Eligible	n/a	B.5.11
Verizon Building, Alden Street, Cranford, New Jersey	c. 1945, one-story commercial building with flat roof, brick exterior, concrete foundation. Roofline is treated with simple railing and “dentils” just below. Large shutters flank the windows on the primary facade. Windows elsewhere are very large 6/6.	1000 feet/W	Not Eligible	n/a	B.5.13
Saint Michael’s Roman Catholic Church, Alden Street, Cranford, New Jersey	1950, two-story, basilica-style church gable, tiled roof, brick exterior with buttresses, round windows in the clerestory and arched topped windows with heavy drip caps on the lower level. Front facade has slightly punched out center bay with rose window and two portals with tympanum.	1000 feet/W	Not Eligible	n/a	B.5.15
Saint Michael’s School, Alden Street, Cranford, New Jersey	1929, two-story, Neo- Classical school building, flat roofed, red brick exterior with stone water table course and parapet wall course. Central entrance is dominated by stonework with pediment, pilasters and 2 story blind arches. Fenestration is grouped in threes.	800 feet/NW	Not Eligible	n/a	B.5.17



PHOTO LOCATOR MAP

**Plate B.5.10:**

35 Alden Street.
Photo view: South
Photographer: Elaine Kiernan Gold
Date: August 1, 2001

35 Alden Street, Cranford, New Jersey	c. 1900 two-and-a-half story vernacular Victorian residence with asphalt shingle roofing, vinyl siding on upper floors and permastone on 1st floor. Additions include upper story enclosed porch and altered entry doors.	700 feet/W	Not Eligible	n/a	B.5.10
37 Alden Street, Cranford, New Jersey	c. 1900, two-and-a-half-story Victorian-style residence, jerkinhead roof with asphalt shingle roofing, shed dormers, vinyl siding. Alterations include replacement windows and large one story addition to primary facade.	700 feet/W	Not Eligible	n/a	B.5.11
Verizon Building, Alden Street, Cranford, New Jersey	c. 1945, one-story commercial building with flat roof, brick exterior, concrete foundation. Roofline is treated with simple railing and "dentils" just below. Large shutters flank the windows on the primary facade. Windows elsewhere are very large 6/6.	1000 feet/W	Not Eligible	n/a	B.5.13
Saint Michael's Roman Catholic Church, Alden Street, Cranford, New Jersey	1950, two-story, basilica-style church gable, tiled roof, brick exterior with buttresses, round windows in the clerestory and arched topped windows with heavy drip caps on the lower level. Front facade has slightly punched out center bay with rose window and two portals with tympanum.	1000 feet/W	Not Eligible	n/a	B.5.15
Saint Michael's School, Alden Street, Cranford, New Jersey	1929, two-story, Neo- Classical school building, flat roofed, red brick exterior with stone water table course and parapet wall course. Central entrance is dominated by stonework with pediment, pilasters and 2 story blind arches. Fenestration is grouped in threes.	800 feet/NW	Not Eligible	n/a	B.5.17



PHOTO LOCATOR MAP

**Plate B.5.11:**

37 Alden Street.
Photo view: Southwest
Photographer: Elaine Kiernan Gold
Date: August 1, 2001

35 Alden Street, Cranford, New Jersey	c. 1900 two-and-a-half story vernacular Victorian residence with asphalt shingle roofing, vinyl siding on upper floors and permastone on 1st floor. Additions include upper story enclosed porch and altered entry doors.	700 feet/W	Not Eligible	n/a	B.5.10
37 Alden Street, Cranford, New Jersey	c. 1900, two-and-a-half-story Victorian-style residence, jerkinhead roof with asphalt shingle roofing, shed dormers, vinyl siding. Alterations include replacement windows and large one story addition to primary facade.	700 feet/W	Not Eligible	n/a	B.5.11
Verizon Building, Alden Street, Cranford, New Jersey	c. 1945, one-story commercial building with flat roof, brick exterior, concrete foundation. Roofline is treated with simple railing and "dentils" just below. Large shutters flank the windows on the primary facade. Windows elsewhere are very large 6/6.	1000 feet/W	Not Eligible	n/a	B.5.13
Saint Michael's Roman Catholic Church, Alden Street, Cranford, New Jersey	1950, two-story, basilica-style church gable, tiled roof, brick exterior with buttresses, round windows in the clerestory and arched topped windows with heavy drip caps on the lower level. Front facade has slightly punched out center bay with rose window and two portals with tympanum.	1000 feet/W	Not Eligible	n/a	B.5.15
Saint Michael's School, Alden Street, Cranford, New Jersey	1929, two-story, Neo- Classical school building, flat roofed, red brick exterior with stone water table course and parapet wall course. Central entrance is dominated by stonework with pediment, pilasters and 2 story blind arches. Fenestration is grouped in threes.	800 feet/NW	Not Eligible	n/a	B.5.17



PHOTO LOCATOR MAP

**Plate B.5.15:**

Saint Michael's Roman Catholic Church, Alden Street.

Photo view: North

Photographer: Elaine Kiernan Gold

Date: August 1, 2001



35 Alden Street, Cranford, New Jersey	c. 1900 two-and-a-half story vernacular Victorian residence with asphalt shingle roofing, vinyl siding on upper floors and permastone on 1st floor. Additions include upper story enclosed porch and altered entry doors.	700 feet/W	Not Eligible	n/a	B.5.10
37 Alden Street, Cranford, New Jersey	c. 1900, two-and-a-half-story Victorian-style residence, jerkinhead roof with asphalt shingle roofing, shed dormers, vinyl siding. Alterations include replacement windows and large one story addition to primary facade.	700 feet/W	Not Eligible	n/a	B.5.11
Verizon Building, Alden Street, Cranford, New Jersey	c. 1945, one-story commercial building with flat roof, brick exterior, concrete foundation. Roofline is treated with simple railing and "dentils" just below. Large shutters flank the windows on the primary facade. Windows elsewhere are very large 6/6.	1000 feet/W	Not Eligible	n/a	B.5.13
Saint Michael's Roman Catholic Church, Alden Street, Cranford, New Jersey	1950, two-story, basilica-style church gable, tiled roof, brick exterior with buttresses, round windows in the clerestory and arched topped windows with heavy drip caps on the lower level. Front facade has slightly punched out center bay with rose window and two portals with tympanum.	1000 feet/W	Not Eligible	n/a	B.5.15
Saint Michael's School, Alden Street, Cranford, New Jersey	1929, two-story, Neo- Classical school building, flat roofed, red brick exterior with stone water table course and parapet wall course. Central entrance is dominated by stonework with pediment, pilasters and 2 story blind arches. Fenestration is grouped in threes.	800 feet/NW	Not Eligible	n/a	B.5.17





PHOTO LOCATOR MAP

**Plate B.5.17:**

Saint Michael's School.
Photo view: North
Photographer: Elaine Kiernan Gold
Date: August 1, 2001

Inventory of Identified Architectural Resources			
Property	Property Information	Description	NRHP Eligibility
 <p>Photograph Number: 19</p>	<p>Panamerican Resource Number: 19</p> <p>50 Balmiere Parkway</p> <p>Municipality: Cranford, Union County</p> <p>UTM coordinates (1983 Zone 18N): 4501823N, 558229E</p> <p>Date: ca. 1930</p> <p>Other:</p>	<p>Style: Colonial Revival/minimal traditional</p> <p>Stories: 2</p> <p>Plan: rectangular</p> <p>Roof: gable</p> <p>Exterior: vinyl siding</p> <p>Foundation: concrete block</p> <p>Windows: aluminum replacement</p> <p>Details: This house has been significantly altered since its initial construction.</p>	Not eligible, loss of integrity
 <p>Photograph Number: 20</p>	<p>Panamerican Resource Number: 20</p> <p>18 Balmiere Parkway</p> <p>Municipality: Cranford, Union County</p> <p>UTM coordinates (1983 Zone 18N): 4501817N, 558284E</p> <p>Date: ca. 1910</p> <p>Other:</p>	<p>Style: Vernacular, late Victorian</p> <p>Stories: 2</p> <p>Plan: rectangular</p> <p>Roof: gable</p> <p>Exterior: vinyl siding</p> <p>Foundation: concrete</p> <p>Windows: replacement</p> <p>Details: This structure is a much older home that has been significantly altered with porch enclosures, side room additions, and unsympathetic window, roof shingle, shutter, and siding replacements.</p>	Not eligible, loss of integrity

Inventory of Identified Architectural Resources			
Property	Property Information	Description	NRHP Eligibility
 Photograph Number: 21	<p>Panamerican Resource Number: 21</p> <p>19 Balmiere Parkway</p> <p>Municipality: Cranford, Union County</p> <p>UTM coordinates (1983 Zone 18N): 4501795N, 558321E</p> <p>Date: ca. 1930</p> <p>Other: Wood-shingled garage in the rear</p>	<p>Style: Tudor variation</p> <p>Stories: 2.5</p> <p>Plan: rectangular</p> <p>Roof: gable</p> <p>Exterior: vinyl siding</p> <p>Foundation: obscured</p> <p>Windows: 6/1 replacements</p> <p>Details: This is a much changed Tudor-style variation with decorative shutters and a plain front door</p>	<p>Not eligible, loss of integrity</p>
 Photograph Number: 22	<p>Panamerican Resource Number: 22</p> <p>Garage</p> <p>20 Crescent Place</p> <p>Municipality: Cranford, Union County</p> <p>UTM coordinates (1983 Zone 18N): 4501765N, 558349E</p> <p>Date: ca. 2000</p> <p>Other:</p>	<p>Style: Modern</p> <p>Stories: 1</p> <p>Plan: rectangular</p> <p>Roof: gable</p> <p>Exterior: vinyl siding</p> <p>Foundation: concrete</p> <p>Windows: none</p> <p>Details:</p>	<p>Not Eligible, neither old enough nor significant enough to warrant consideration</p>

NEW JERSEY OFFICE OF HISTORIC PRESERVATION HISTORIC SITES INVENTORY # 200311
INDIVIDUAL STRUCTURE SURVEY FORM

HISTORIC NAME:

LOCATION: 35 Balmiere Parkway

COMMON NAME:

BLOCK/LOT 198/52

MUNICIPALITY: Cranford

COUNTY: Union

USGS QUAD:

UTM REFERENCES:

OWNER/ADDRESS: Mr. & Mrs. Berry
35 Balmiere Parkway, Cranford

Zone/Northing/Easting

DESCRIPTION

Construction Date: ca. 1925

Source of Date: visual

Architect:

Builder:

Style: Tudor Revival

Form/Plan Type: Irregular plan

Number of Stories: 2½

Foundation: Brick facing on concrete

Exterior Wall Fabric: Cut stone and brick

Fenestration: Casement windows. Metal frames.

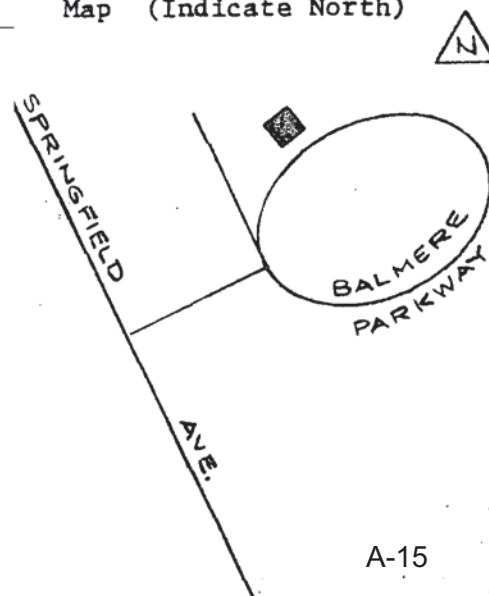
Roof/Chimneys: Steep gable roof. Slate shingles.

Additional Architectural Description:

Large, imposing house with a distinctly medieval feeling.
Brick chimney with five "pots".
Hood moldings of stone around front door.

PHOTO Negative File # Roll 32 frame 11

Map (Indicate North)



SITING, BOUNDARY DESCRIPTION, AND RELATED STRUCTURES:

The house is located on the back line of lot.

SURROUNDING ENVIRONMENT: Urban ☐ Suburban ☒ Scattered Buildings ☐
Open Space ☐ Woodland ☐ Residential ☒ Agricultural ☐
Industrial ☐ Downtown Commercial ☐ Highway Commercial ☐ Other ☐

Balmiere Parkway was opened as an "exclusive" residential neighborhood in the 1920's. There are "Period Houses" of all types in the area.

SIGNIFICANCE:

Good example of Tudor Revival Style of architecture. It is less common in Cranford than in other communities in the area.

ORIGINAL USE: ResidencePHYSICAL CONDITION: Excellent ☒ Good ☐REGISTER ELIGIBILITY: Yes ☐ Possible ☐THREATS TO SITE: Roads ☐ Development ☐No Threat ☒ Other ☐

COMMENTS:

PRESENT USE: Residence

Fair ☐ Poor ☐No ☒ Part of District ☐Zoning ☐ Deterioration ☐

REFERENCES:

Cranford Historical Society

RECORDED BY: JWF
ORGANIZATION: UCCHPAB

DATE: 1981







CRANFORD
Union Co.

* 200311
r. 32/11

35 Balmire Parkway
looking N.

1981

Inventory of Identified Architectural Resources			
Property	Property Information	Description	NRHP Eligibility
 <p>Photograph Number: 17</p>	<p>Panamerican Resource Number: 17</p> <p>Streetscape</p> <p>Route 509/Kenilworth Boulevard from Nomahegan Boulevard to Springfield Avenue</p> <p>Municipality: Cranford, Union County</p> <p>UTM coordinates (1983 Zone 18N): 4502688N, 557751E</p> <p>Date: ca. 1960s</p> <p>Other:</p>	<p>Style: General styles in this neighborhood bordering Lenape Park, but tend toward Colonial Revival</p> <p>Stories: most are 1 although there are some with a second story wing</p> <p>Plan: most are simple plans that have been added to over the years</p> <p>Roof: side and front gables</p> <p>Exterior: almost all are aluminum or vinyl siding</p> <p>Foundation: concrete and concrete block</p> <p>Windows: replacement metal</p> <p>Details: well cared for neighborhood</p>	<p>Not Eligible, does not meet the NRHP criteria for eligibility</p>
 <p>Photograph Number: 18</p>	<p>Panamerican Resource Number: 18</p> <p>47 Balmier Parkway</p> <p>Municipality: Cranford, Union County</p> <p>UTM coordinates (1983 Zone 18N): 4501856N, 558194E</p> <p>Date: ca. 1935</p> <p>Other:</p>	<p>Style: Tudor variation w/Colonial Revival details</p> <p>Stories: 2</p> <p>Plan: rectangular</p> <p>Roof: gable</p> <p>Exterior: brick, shakes, and wood siding</p> <p>Foundation: not visible</p> <p>Windows: 6/6 sash, fixed, and greenhouse</p> <p>Details: house is turned sideways on lot to make the most of the pie-shaped property</p>	<p>Not Eligible, typical area period housing type</p>

Inventory of Identified Architectural Resources			
Property	Property Information	Description	NRHP Eligibility
 <p>Photograph Number: 19</p>	<p>Panamerican Resource Number: 19</p> <p>50 Balmiere Parkway</p> <p>Municipality: Cranford, Union County</p> <p>UTM coordinates (1983 Zone 18N): 4501823N, 558229E</p> <p>Date: ca. 1930</p> <p>Other:</p>	<p>Style: Colonial Revival/minimal traditional</p> <p>Stories: 2</p> <p>Plan: rectangular</p> <p>Roof: gable</p> <p>Exterior: vinyl siding</p> <p>Foundation: concrete block</p> <p>Windows: aluminum replacement</p> <p>Details: This house has been significantly altered since its initial construction.</p>	<p>Not eligible, loss of integrity</p>
 <p>Photograph Number: 20</p>	<p>Panamerican Resource Number: 20</p> <p>18 Balmiere Parkway</p> <p>Municipality: Cranford, Union County</p> <p>UTM coordinates (1983 Zone 18N): 4501817N, 558284E</p> <p>Date: ca. 1910</p> <p>Other:</p>	<p>Style: Vernacular, late Victorian</p> <p>Stories: 2</p> <p>Plan: rectangular</p> <p>Roof: gable</p> <p>Exterior: vinyl siding</p> <p>Foundation: concrete</p> <p>Windows: replacement</p> <p>Details: This structure is a much older home that has been significantly altered with porch enclosures, side room additions, and unsympathetic window, roof shingle, shutter, and siding replacements.</p>	<p>Not eligible, loss of integrity</p>

NEW JERSEY OFFICE OF HISTORIC PRESERVATION HISTORIC SITES INVENTORY # 200315
INDIVIDUAL STRUCTURE SURVEY FORM

HISTORIC NAME:

COMMON NAME:

LOCATION: 17 Berkeley Place

BLOCK/LOT 166/5

MUNICIPALITY: Cranford

COUNTY: Union

USGS QUAD: Roselle

UTM REFERENCES:

OWNER/ADDRESS: Beaman, Arthur, Jr. & Anastasia

Zone/Northing/Easting

17 Berkeley Pl., Cranford

DESCRIPTION

Construction Date: c. 1900

Source of Date: Visual

Architect:

Builder:

Style: Neo-Classical

Form/Plan Type: Center Hall plan

Number of Stories: 2½

Foundation: Not visible

Exterior Wall Fabric: Wooden shingles

Fenestration: 3 x 2 bays; 8/1 sash windows

Roof/Chimneys: Gable Roof

Additional Architectural Description:

Two story, semi-circular portico on facade. Ionic columns, heavy entablature.

Three dormers in roof with wooden keystone accents.

Door with side lights

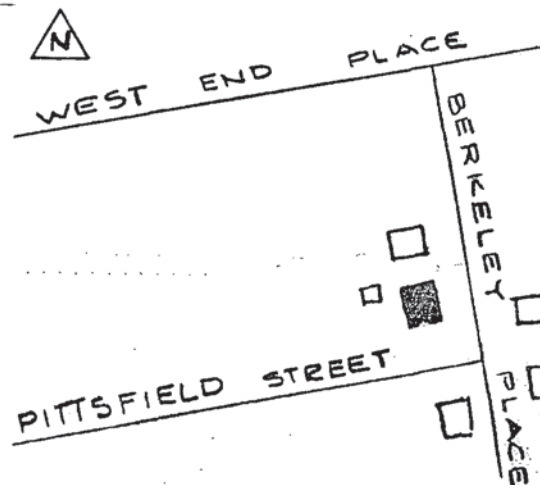
One story rectangular bay on north elevation

PHOTO

Negative File # Roll 32, Frame 11A

R 9, 11

Map (Indicate North)



SITING, BOUNDARY DESCRIPTION, AND RELATED STRUCTURES:

The house, located on a corner lot, is equidistant from Berkeley Place and Pittsfield St., (approx. 35') and stands 20' from the next house on Berkeley. One story garage to rear.

SURROUNDING ENVIRONMENT: Urban ☐ Suburban ☒ Scattered Buildings ☐
Open Space ☐ Woodland ☐ Residential ☒ Agricultural ☐
Industrial ☐ Downtown Commercial ☐ Highway Commercial ☐ Other ☐

SIGNIFICANCE:

Another Colonial Revival Style home, in an area of Cranford which was developed at the turn-of-the-century, during the height of popularity of the Colonial Revival style. The unusual porch treatment of the house makes it noteworthy. The house is listed in Gateways to Architecture, a guide to Union County's architecture by Beverly and Sandy Brown.

ORIGINAL USE: Residence

PHYSICAL CONDITION: Excellent ☐ Good ☒

REGISTER ELIGIBILITY: Yes ☒ Possible ☒

THREATS TO SITE: Roads ☐ Development ☐

No Threat ☒ Other ☐

COMMENTS:

PRESENT USE: Residence

Fair ☐ Poor ☐

No ☐ Part of District ☐

Zoning ☐ Deterioration ☐

REFERENCES:

Brown. Gateways to Architecture. UCCHPAB. 1975

RECORDED BY: JWF
ORGANIZATION: UCCHPAB

DATE: 1981



CRANFORD
Union Co

* 200315
R 32/11A
R 9/11

17 Berkeley Place
looking S.W.

1981

NEW JERSEY OFFICE OF HISTORIC PRESERVATION HISTORIC SITES INVENTORY # 200312
INDIVIDUAL STRUCTURE SURVEY FORM

HISTORIC NAME:

COMMON NAME:

LOCATION: 12 Bloomingdale Ave.

BLOCK/LOT 304/4

MUNICIPALITY: Cranford

COUNTY: Union

USGS QUAD: Roselle

UTM REFERENCES:

OWNER/ADDRESS: Joseph & Jean Sharp

Zone/Northing/Easting

DESCRIPTION

Construction Date: 19th C.

Source of Date: Visual evidence

Architect:

Builder:

Style: Italianate

Form/Plan Type: "L" shape house
with entrance tower

Number of Stories: 2½

Foundation: Brick foundation

Exterior Wall Fabric: Clapboard siding

Fenestration: Three bay facade. Round arched windows.

Roof/Chimneys: Gable roof. Brick chimney.

Additional Architectural Description:

House may have been a simple, planbook type house. It now exhibits a mix of Italianate, Mansard, and Planbood style influences.

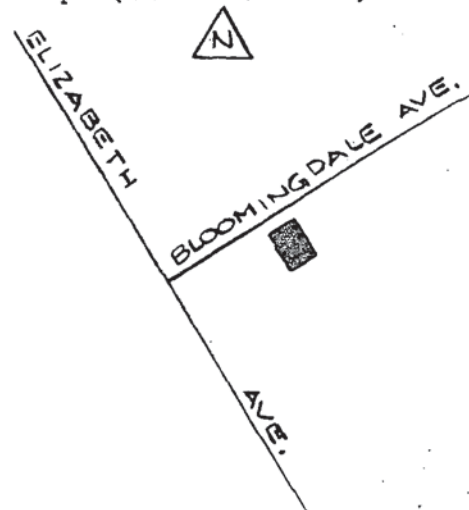
Three story tower in corner of "L" on facade.

Mansard roof on tower. Entry in base.

Brackets under eaves. Ornate wooden trim at porch, tower cornice, and at bay window on first floor.

PHOTO Negative File # Roll 20 frame 3

Map (Indicate North)



Y DESCRIPTION, AND RELATED STRUCTURES:

House sits on east-central portion of an 85' x 200' lot.

SURROUNDING ENVIRONMENT: Urban ☐ Suburban ☒ Scattered Buildings ☐
Open Space ☐ Woodland ☐ Residential ☒ Agricultural ☐
Industrial ☐ Downtown Commercial ☐ Highway Commercial ☐ Other ☐

In a neighborhood of mixed late 19th Century frame houses and smaller, newer single family suburban homes.

SIGNIFICANCE:

The house is one of the oldest in the area of Bloomingdale, Roselle, and Cranford Avenues which were laid out after the Civil War. It is a small house with a catalogue of late 19th Century wooden ornaments of all styles applied to a simple house form.

ORIGINAL USE: Residence

PHYSICAL CONDITION: Excellent ☒ Good ☐

REGISTER ELIGIBILITY: Yes ☒ Possible ☐

THREATS TO SITE: Roads ☐ Development ☐

No Threat ☒ Other ☐

COMMENTS:

PRESENT USE: Residence

Fair ☐ Poor ☐

No ☐ Part of District ☐

Zoning ☐ Deterioration ☐

Recently restored, it is a handsome focal point for the entire street.

REFERENCES:

Cranford Historical Society

RECORDED BY: JWF
ORGANIZATION: UCCHPAB

DATE: 1981



CRANFORD
Union Co.

200312
R. 20/3

12 Bloomingdale
looking S.E.

1981

and Co.
Engineering-Science, Inc.

NJ Transit Historic Railroad Bridge Survey
Structure Inventory Form

LOCATION

NJ TRANSIT Line	Raritan Valley	Milepost	15.99
Town/City	Cranford	Feature Crossed	Centennial Avenue
County	Union	Common Name	(241) Centennial Avenue
USGS Quad	Roselle	UTM Ref.	18.559580.4500100

PHYSICAL SUMMARY

Structure Type	Deck Plate Girder	Deck Type	Ballasted
Overall Length	80'	Width	78'
Spans	2	Span Length	40'
Material(s)	Steel	Design Loading	E-60
Skew	90	Tracks	6
Inspection Report	No	Condition	Good
Bridge Typology Code	XX XX XX XX		

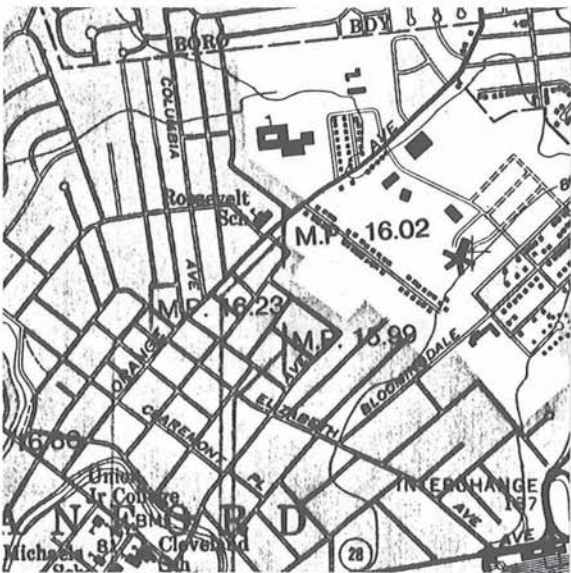
HISTORICAL SUMMARY

Date	1929	Date(s) Rebuilt	
Designer	CNJ	Fabricator	Bethlehem Steel Co.
Patent		Historic Rail Line	CNJ Central Div., Main Line

CULTURAL RESOURCE EVALUATIONS

National Register Status		National Register Date	
State Register Status		State Register Date	
Local Landmark Designation			
HAER Doc. #			
Type and Date of HAER documentation			

GRAPHICS



DeLeuw, Cather and Co.
Engineering-Science, Inc.

NJ Transit Historic Railroad Bridge Survey
Structure Inventory Form

PHYSICAL DESCRIPTION OF BRIDGE AND SETTING

15.99 Centennial Avenue is an open hearth steel deck plate girder span of riveted built up construction with a ballasted deck. The bridge has two spans and concrete abutments and wingwalls. The angular abutment design is characteristic of the CNJ. Originally this bridge carried six tracks and resulted in a large width. A single row of steel roadway columns divide the two spans and are built up members with cross-bracing. There is a steel handrailing across the top of the structure.

STATEMENT OF TECHNOLOGICAL MERIT AND HISTORICAL SIGNIFICANCE

This bridge was constructed as part of a grade crossing elimination in the town of Cranford by the CNJ during 1929. The project included four structures: 15.99 Centennial Avenue, 16.02 Rahway River, 16.23 Walnut Street and 16.66 Lincoln Avenue. This grade crossing elimination project does not have historical significance due to its late date and lack of associational characteristics. These projects, which had become a national issue by 1900, were sometimes characterized by highly publicized negotiations between the towns and railroads. The bridges in this project are also not considered technologically significant as railroad structures. The four bridges in Cranford were built on the Main Line of the CNJ's Central Division which provided passenger and freight service.

RECOMMENDATION OF ELIGIBILITY TO NATIONAL REGISTER OF HISTORIC PLACES

Not eligible

PRIMARY AND SECONDARY SOURCES

See New Jersey Transit Historic Railroad Bridge Survey, Report Bibliography.

Survey Team A. Cottrell, J. Galvin
Survey Date 08/01/90

Reviewer A.C.
Review Date 03/12/91

Photographs P18/00-1
Slides S17/33-35

New Jersey Department of Environmental Protection
Historic Preservation Office

Page 1 of 1

CONTINUATION SHEET

Historic Sites #:

Resource Name: Raritan Valley Line over Centennial Avenue, MP 15.99

Municipality: Cranford

County: Union

Additional Information:

Locator map on 1991 survey form does not show crossing. (See Attached)

Representation in Existing Surveys:

This bridge was surveyed in 1991 as part of NJ Transit's *Historic Railroad Bridge Survey*.

National Register Eligibility:

The 1991 survey only evaluated the bridge for its individual eligibility, not as a component of a historic district. In that context, the bridge was evaluated as not eligible for inclusion in the National Register of Historic Places.

In 1995, the entire Central Railroad of New Jersey Main Line was determined to be eligible for inclusion in the National Register of Historic Places as a linear historic district. Therefore, the purpose of the current study is to determine if individual resources such as bridges contribute to the historic district's overall significance.

As a resource that falls within the historic district's period of significance, the bridge has been determined to contribute to the CRRNJ Main Line Historic District.

Attachments:

- Locator map
- 1991 NJ Transit Historic Railroad Bridge Survey form

Survey Name: NJ Transit Raritan Valley Line Study
Surveyor: Nancy L. Zerbe
Organization: ARCH², Inc.

Date: July, 1999

New Jersey Department of Environmental Protection
Historic Preservation Office

Page 1 of 1

CONTINUATION SHEET

Historic Sites #:

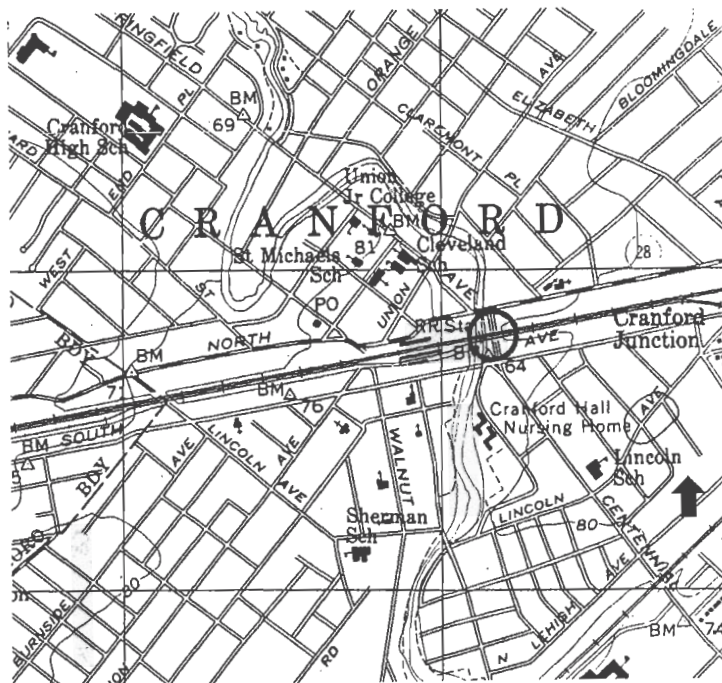
Resource Name: Raritan Valley Line over Centennial Avenue, MP 15.99

Municipality: Cranford

County: Union



Additional Information:



Locator map on 1991 survey form does not show crossing.





Survey Name: NJ Transit Raritan Valley Line Study
Surveyor: Nancy L. Zerbe
Organization: ARCH², Inc.

Date: July, 1999

Inventory of Identified Architectural Resources			
Property	Property Information	Description	NRHP Eligibility
 <p>Photograph Number: 106 (south side crossing Centennial Avenue)</p>	<p>Panamerican Resource Number: 106</p> <p>Central Railroad of New Jersey (CNJ) Bridge at Centennial Avenue (NJDOT #2063150) and crossing Rahway River (NJDOT #1063151)</p> <p>Municipality: Cranford, Union County</p> <p>UTM coordinates (1983 Zone 18N): 4500796N, 559165E</p> <p>Date: 1929 (plaque)</p> <p>Other:</p>	<p>Style: Concrete quadruple arches over river and deck girder over Centennial Avenue</p> <p>Stories: na</p> <p>Plan: na</p> <p>Roof: na</p> <p>Exterior: concrete</p> <p>Foundation: concrete abutments at Centennial Avenue and concrete arches fortified with stone and concrete abutments at river</p> <p>Windows: na</p> <p>Details: Two arches actually span the river while a third springs immediately from the west bank. A fourth arch has been infilled and is used for offices, storage, and bathrooms (see PRN 107).</p>	<p>This structure is eligible as a contributing element to the CNJ Main Line Corridor HD (ID #3500), which was determined eligible for the NRHP by the Secretary of the Interior on November 30, 1995. HD includes associated structures that extend beyond the actual rail bed.</p>
 <p>Photograph Number: 107 (note the infilled fourth arch of the NJRR bridge)</p>	<p>Panamerican Resource Number: 107</p> <p>Storage Building</p> <p>No # Centennial Avenue</p> <p>Municipality: Cranford, Union County</p> <p>UTM coordinates (1983 Zone 18N): 4500775N, 559143E</p> <p>Date: unknown</p> <p>Other: Structure sits right on the Rahway River.</p>	<p>Style: This storage building is associated with the CNJ Bridge (see PRN 106) and has the spare look of the bridge</p> <p>Stories: 1</p> <p>Plan: rectangular</p> <p>Roof: flat</p> <p>Exterior: concrete</p> <p>Foundation: concrete</p> <p>Windows: none—ventilation louver in place of traditional window</p> <p>Details: This storage facility is located immediately outside the "offices" for the CNJ bridge that are located in the infilled fourth arch of the bridge.</p>	<p>This structure is eligible as a contributing element to the CNJ Main Line Corridor HD (ID #3500), which was determined eligible for the NRHP by the Secretary of the Interior on November 30, 1995. HD includes associated structures that extend beyond the actual rail bed.</p>



Inventory of Identified Architectural Resources			
Property	Property Information	Description	NRHP Eligibility
 <p>Photograph Number: 106 (south side crossing Centennial Avenue)</p>	<p>Panamerican Resource Number: 106</p> <p>Central Railroad of New Jersey (CNJ) Bridge at Centennial Avenue (NJDOT #2063150) and crossing Rahway River (NJDOT #1063151)</p> <p>Municipality: Cranford, Union County</p> <p>UTM coordinates (1983 Zone 18N): 4500796N, 559165E</p> <p>Date: 1929 (plaque)</p> <p>Other:</p>	<p>Style: Concrete quadruple arches over river and deck girder over Centennial Avenue</p> <p>Stories: na</p> <p>Plan: na</p> <p>Roof: na</p> <p>Exterior: concrete</p> <p>Foundation: concrete abutments at Centennial Avenue and concrete arches fortified with stone and concrete abutments at river</p> <p>Windows: na</p> <p>Details: Two arches actually span the river while a third springs immediately from the west bank. A fourth arch has been infilled and is used for offices, storage, and bathrooms (see PRN 107).</p>	<p>This structure is eligible as a contributing element to the CNJ Main Line Corridor HD (ID #3500), which was determined eligible for the NRHP by the Secretary of the Interior on November 30, 1995. HD includes associated structures that extend beyond the actual rail bed.</p>
 <p>Photograph Number: 107 (note the infilled fourth arch of the NJRR bridge)</p>	<p>Panamerican Resource Number: 107</p> <p>Storage Building</p> <p>No # Centennial Avenue</p> <p>Municipality: Cranford, Union County</p> <p>UTM coordinates (1983 Zone 18N): 4500775N, 559143E</p> <p>Date: unknown</p> <p>Other: Structure sits right on the Rahway River.</p>	<p>Style: This storage building is associated with the CNJ Bridge (see PRN 106) and has the spare look of the bridge</p> <p>Stories: 1</p> <p>Plan: rectangular</p> <p>Roof: flat</p> <p>Exterior: concrete</p> <p>Foundation: concrete</p> <p>Windows: none—ventilation louver in place of traditional window</p> <p>Details: This storage facility is located immediately outside the "offices" for the CNJ bridge that are located in the infilled fourth arch of the bridge.</p>	<p>This structure is eligible as a contributing element to the CNJ Main Line Corridor HD (ID #3500), which was determined eligible for the NRHP by the Secretary of the Interior on November 30, 1995. HD includes associated structures that extend beyond the actual rail bed.</p>

Inventory of Identified Architectural Resources			
Property	Property Information	Description	NRHP Eligibility
 <p>Photograph Number: 108</p>	<p>Panamerican Resource Number: 108</p> <p>Cranford Chamber of Commerce</p> <p>4 Centennial Avenue</p> <p>Municipality: Cranford, Union County</p> <p>UTM coordinates (1983 Zone 18N): 4500762N, 559155E</p> <p>Date: ca. 1890</p> <p>Other: Building sits right on the Rahway River.</p>	<p>Style: Second Empire</p> <p>Stories: 3</p> <p>Plan: rectangular</p> <p>Roof: Mansard</p> <p>Exterior: vinyl siding</p> <p>Foundation: obscured</p> <p>Windows: replacement vinyl with decorative shutters</p> <p>Details: first and second story contain full width porches; this house has been so altered that it has lost all former decorative elements.</p>	<p>The house has been so altered that it has lost its integrity. Without integrity, it cannot be eligible for the NRHP. It is recommended as not eligible for the NRHP.</p>
 <p>Photograph Number: 109</p>	<p>Panamerican Resource Number: 109</p> <p>Krauszers</p> <p>Centennial Avenue at South Avenue</p> <p>Municipality: Cranford, Union County</p> <p>UTM coordinates (1983 Zone 18N): 4500746N, 559152E</p> <p>Date: ca. 1960</p> <p>Other: Building sits right on the Rahway River.</p>	<p>Style: store front</p> <p>Stories: 1</p> <p>Plan: rectangular</p> <p>Roof: gable with shed roof entry</p> <p>Exterior: concrete block, Drivitt and brick veneer</p> <p>Foundation: concrete</p> <p>Windows: large store front windows removed and covered with Drivitt</p> <p>Details: Convenience store has a large front parking lot which overlooks South Avenue and the entry to Lincoln Park.</p>	<p>This is a typical store front building from the 1960s and does not meet the NRHP eligibility criteria. Panamerican recommends this building as not eligible for the NRHP.</p>

Panamerican Consultants, Inc.

6-56

Rahway River Phase 1A

Inventory of Identified Architectural Resources			
Property	Property Information	Description	NRHP Eligibility
 <p>Photograph Number: 104 (east bound lane balustrade and underside)</p>	<p>Panamerican Resource Number: 104</p> <p>Bridge (NJDOT #2008156) at North Avenue and Centennial Avenue crossing Rahway River</p> <p>Municipality: Cranford, Union County</p> <p>UTM coordinates (1983 Zone 18N): 4500874N, 559117E</p> <p>Date: 1965</p> <p>Other: Plaque indicates this is a State of New Jersey bridge.</p>	<p>Style: steel stringer</p> <p>Stories: na</p> <p>Plan: na</p> <p>Roof: na</p> <p>Exterior: closed concrete balustrade topped with metal guard rails</p> <p>Foundation: concrete abutments</p> <p>Windows: na</p> <p>Details: North Avenue (US 28) is an exceptionally busy thoroughfare and this bridge is a typical state bridge from the period.</p>	<p>Although this bridge is a state-owned property and is not ornamental, it should be considered a contributing element to both the NRE Rahway River Parkway HD (ID #4079) and NRE Union County Park System HD (ID #4424).</p>
 <p>Photograph Number: 105</p>	<p>Panamerican Resource Number: 105</p> <p>Cranford First Aid Squad</p> <p>No # Centennial Avenue</p> <p>Municipality: Cranford, Union County</p> <p>UTM coordinates (1983 Zone 18N): 4500823N, 559136E</p> <p>Date: ca. 1950</p> <p>Other: Building sits on the bank of the Rahway River.</p>	<p>Style: Modern</p> <p>Stories: 1</p> <p>Plan: rectangular</p> <p>Roof: flat</p> <p>Exterior: brick, stone, and concrete block</p> <p>Foundation: concrete</p> <p>Windows: replacement, sliding single pane</p> <p>Details: A low, multi-textured Modern-style building with sympathetically added ambulance bay. A very nice example of 1950s modern.</p>	<p>Not eligible: does not meet eligibility criteria</p>

NEW JERSEY OFFICE OF HISTORIC PRESERVATION HISTORIC SITES INVENTORY # 200318
INDIVIDUAL STRUCTURE SURVEY FORM

HISTORIC NAME: Moore House
LOCATION: 22 Central Ave.

COMMON NAME:
BLOCK/LOT 184/13

MUNICIPALITY: Cranford
USGS QUAD: Roselle
OWNER/ADDRESS: James Lenny
22 Central Ave., Cranford

COUNTY: Union
UTM REFERENCES:
Zone/Northing/Easting

DESCRIPTION

Construction Date: 1862/addition 1868

Source of Date: Cranford Historical Societ

Architect:

Builder:

Style: Second Empire

Form/Plan Type: Side hall plan
Rectangular shape

Number of Stories: 2½

Foundation: Brick

Exterior Wall Fabric: Composition shingles

Fenestration: Three bay facade. Tall 2/2 sash windows.

Roof/Chimneys: Mansard roof. Two brick chimneys.

Additional Architectural Description:

Wide cornice band with floral design.

Projecting lintels over windows.

Facade porch removed.

Transom light above door.

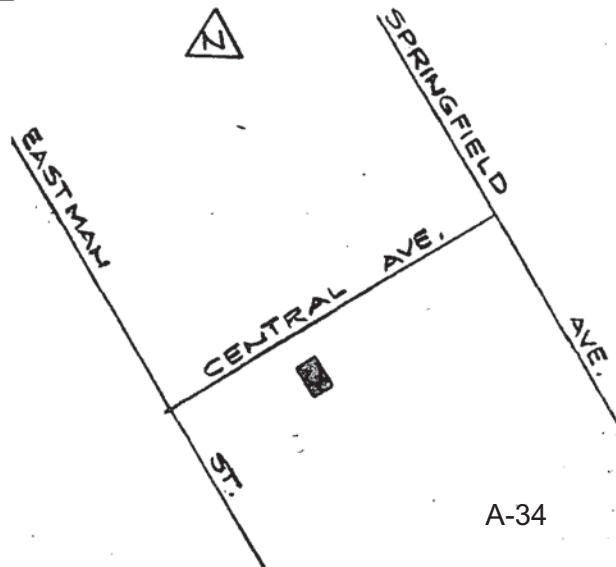
Paired brackets removed

Synthetic siding and roofing material

Large exterior brick chimney added to west elevation

One story addition to rear

PHOTO Negative File # Roll 26 frame 25 R32/13 Map (Indicate North)



NAME, LOCATION, DESCRIPTION, AND RELATED STRUCTURES:

The Rahway River is at the rear of the property.

SURROUNDING ENVIRONMENT: Urban ☐ Suburban ☒ Scattered Buildings ☐
Open Space ☐ Woodland ☐ Residential ☒ Agricultural ☐
Industrial ☐ Downtown Commercial ☐ Highway Commercial ☐ Other ☐

The 19th Century suburban streets were all laid out to afford residents a view of or access to the Rahway River. This is especially true of Central Avenue which intersects a bend in the river allowing residents of both sides of the street a backyard which includes the river.

SIGNIFICANCE:

Central Avenue was a fashionable residential street developed in the late 19th Century. The Lenny house is one of the oldest on Central Avenue, and is reported to have been built in the 1860's. This is a very early date for the Second Empire style to have been used, as it did not generally come into popularity in Union County until the 1870's. The house was first lived in by the Moore family. Mr. Moore was a stone mason.

ORIGINAL USE: Residence

PHYSICAL CONDITION: Excellent ☐ Good ☒

REGISTER ELIGIBILITY: Yes ☒ Possible ☒

THREATS TO SITE: Roads ☐ Development ☐

No Threat ☒ Other ☐

COMMENTS:

PRESENT USE: Residence

Fair ☐ Poor ☐

No ☐ Part of District ☐

Zoning ☐ Deterioration ☐

REFERENCES:

Cranford Historical Society

RECORDED BY: JWF
ORGANIZATION: UCCHPAB

DATE: 1981



CRANFORD
Union Co.
* 200318
R 26/25

Moore Hse.
22 Central Ave
looking S.E.

1981

* 200318
R32/13



CRANFORD
Union Co.

Moore Hse
22 Central Ave
looking E.

1981



Inventory of Identified Architectural Resources



Property	Property Information	Description	NRHP Eligibility
 <p>Photograph Number: 78</p>	<p>Panamerican Resource Number: 78</p> <p>201 Central Avenue</p> <p>Municipality: Cranford, Union County</p> <p>UTM coordinates (1983 Zone 18N): 4501380N, 558599E</p> <p>Date: 1941</p> <p>Other: House overlooks Memorial Park</p>	<p>Style: Colonial Revival</p> <p>Stories: 2</p> <p>Plan: rectangular</p> <p>Roof: gable</p> <p>Exterior: brick and wood</p> <p>Foundation: obscured</p> <p>Windows: 6/6 with storm windows, front door side lights, operable shutters</p> <p>Details: The house has a number of odd architectural details—Spanish-style wrought-iron balconies on the front and side, scalloped bed molding at the front entry—that seem to indicate numerous changes to the house or a truly fanciful interpretation of Colonial Revival.</p>	<p>This house is a non-contributing element to the NRE North Cranford HD (ID #3838; Bary 2012), and is not eligible since it lacks integrity.</p>
 <p>Photograph Number: 79</p>	<p>Panamerican Resource Number: 79</p> <p>127 Riverside Drive</p> <p>Municipality: Cranford, Union County</p> <p>UTM coordinates (1983 Zone 18N): 4501377N, 558629E</p> <p>Date: ca. 1965</p> <p>Other: Backs up to the Rahway River</p>	<p>Style: Split level</p> <p>Stories: 2</p> <p>Plan: rectangular</p> <p>Roof: gable</p> <p>Exterior: brick veneer first floor and wood shingles</p> <p>Foundation: obscured</p> <p>Windows: replacement decorative with decorative shutters</p> <p>Details: 1960s split level wedged between houses primarily from the early twentieth century</p>	<p>Not eligible and non-contributing; it is not yet old enough to be assessed. Further, given its style and the abundance of structures of this type, the house will probably never be eligible for the NRHP.</p>



Panamerican Consultants, Inc.

6-41



Rahway River Phase 1A



Note: Incorrectly cited as 201 Central; Correct address is 104 Central

Inventory of Identified Architectural Resources			
Property	Property Information	Description	NRHP Eligibility
 <p>Photograph Number: 76</p>	<p>Panamerican Resource Number: 76</p> <p>201 Central Avenue</p> <p>Municipality: Cranford, Union County</p> <p>UTM coordinates (1983 Zone 18N): 4501441N, 558580E</p> <p>Date: 1941</p> <p>Other:</p>	<p>Style: Colonial/Federal Revival</p> <p>Stories: 2</p> <p>Plan: rectangular</p> <p>Roof: gable with slate</p> <p>Exterior: brick veneer with hardy board siding</p> <p>Foundation: obscured</p> <p>Windows: 6/6 with storm windows, front door side lights, operable shutters</p> <p>Details: added two-bay garage and enclosed side porch; finely detailed front door with broken pediment</p>	<p>This house is a non-contributing element to the NRE North Cranford HD (ID #3838; Bary 2012), and is not eligible for the NRHP since it does not meet the criteria.</p>
 <p>Photograph Number: 77</p>	<p>Panamerican Resource Number: 77</p> <p>Open corner lot</p> <p>Northeast corner of Central Avenue and Riverside Drive</p> <p>Municipality: Cranford, Union County</p> <p>UTM coordinates (1983 Zone 18N): 4501421N, 558625E</p> <p>Date: na</p> <p>Other:</p>	<p>Style: na</p> <p>Stories: na</p> <p>Plan: na</p> <p>Roof: na</p> <p>Exterior: na</p> <p>Foundation: na</p> <p>Windows: na</p> <p>Details: Judging by the arrangement of the trees, this lot once held a house. Nevertheless, there are no signs of a house or of any structure having been on the property.</p>	<p>This open lot on Central Avenue at Riverside Drive is a contributing element to the NRE North Cranford HD (ID #3838).</p>

Inventory of Identified Architectural Resources			
Property	Property Information	Description	NRHP Eligibility
 <p>Photograph Number: 76</p>	<p>Panamerican Resource Number: 76</p> <p>201 Central Avenue</p> <p>Municipality: Cranford, Union County</p> <p>UTM coordinates (1983 Zone 18N): 4501441N, 558580E</p> <p>Date: 1941</p> <p>Other:</p>	<p>Style: Colonial/Federal Revival</p> <p>Stories: 2</p> <p>Plan: rectangular</p> <p>Roof: gable with slate</p> <p>Exterior: brick veneer with hardy board siding</p> <p>Foundation: obscured</p> <p>Windows: 6/6 with storm windows, front door side lights, operable shutters</p> <p>Details: added two-bay garage and enclosed side porch; finely detailed front door with broken pediment</p>	<p>This house is a non-contributing element to the NRE North Cranford HD (ID #3838; Bary 2012), and is not eligible for the NRHP since it does not meet the criteria.</p>
 <p>Photograph Number: 77</p>	<p>Panamerican Resource Number: 77</p> <p>Open corner lot</p> <p>Northeast corner of Central Avenue and Riverside Drive</p> <p>Municipality: Cranford, Union County</p> <p>UTM coordinates (1983 Zone 18N): 4501421N, 558625E</p> <p>Date: na</p> <p>Other:</p>	<p>Style: na</p> <p>Stories: na</p> <p>Plan: na</p> <p>Roof: na</p> <p>Exterior: na</p> <p>Foundation: na</p> <p>Windows: na</p> <p>Details: Judging by the arrangement of the trees, this lot once held a house. Nevertheless, there are no signs of a house or of any structure having been on the property.</p>	<p>This open lot on Central Avenue at Riverside Drive is a contributing element to the NRE North Cranford HD (ID #3838).</p>

Inventory of Identified Architectural Resources

Property	Property Information	Description	NRHP Eligibility
 <p>Photograph Number: 118 (looking east across the Rahway River from Droscher's Mill to Droscher's Mill Park)</p>	<p>PanamERICAN Resource Number: 118</p> <p>Droscher's Mill Park</p> <p>High Street at Lincoln Avenue</p> <p>Municipality: Cranford, Union County</p> <p>UTM coordinates (1983 Zone 18N): 4500249N, 559050E</p> <p>Date: unknown</p> <p>Other: This resource falls under the Cranford Section (ID #4424.027) of the NRE Rahway River Parkway HD.</p>	<p>Style: informal park</p> <p>Stories: na</p> <p>Plan: a walking path to signage about mill; path and area now almost completely washed out</p> <p>Roof: na</p> <p>Exterior: na</p> <p>Foundation: na</p> <p>Windows: na</p> <p>Details: This small park is designed as a viewing area for the mill and the dam. The whole area is badly eroded and washed away.</p>	<p>Not eligible. This structure is a part of county/municipal infrastructure used to control water flow on the Rahway River. The control of flood waters is an important part of governmental responsibility in this part of New Jersey, but the dam does not represent any new technology or special comment (see PRN 96).</p>
 <p>Photograph Number: 119</p>	<p>PanamERICAN Resource Number: 119</p> <p>2 Chestnut Street</p> <p>Municipality: Cranford, Union County</p> <p>UTM coordinates (1983 Zone 18N): 4500543N, 559026E</p> <p>Date: ca. 1890</p> <p>Other: sits on the Rahway River</p>	<p>Style: Vernacular Victorian with Italianate details</p> <p>Stories: 2 with basement</p> <p>Plan: rectangular</p> <p>Roof: shed</p> <p>Exterior: wood shingles</p> <p>Foundation: concrete and other</p> <p>Windows: replacement</p> <p>Details: This house was once a part of a row of townhouses—the others long missing. The only reminder of those days is the massive Italianate cornice. The house has since had numerous unsympathetic remodels and additions.</p>	<p>The townhouse at 2 Chestnut has been so extensively altered that it now lacks integrity. Without integrity it cannot be eligible for the NRHP. Panamerican recommends it not eligible for the NRHP.</p>

Inventory of Identified Architectural Resources			
Property	Property Information	Description	NRHP Eligibility
 <p>Photograph Number: 15</p>	<p>Panamerican Resource Number: 15</p> <p>Route 509/Kenilworth Boulevard, east and west bound bridges (NJDOT #2003006), crossing a tributary of the Rahway River east of the river between Lenape and Nomahegan Parks</p> <p>Municipality: Kenilworth, Union County</p> <p>UTM coordinates (1983 Zone 18N): 4502676N, 557929E</p> <p>Date: 1992 (sign on bridge)</p> <p>Other:</p>	<p>Style: rustic, Adirondack-style type</p> <p>Stories: na</p> <p>Plan: 1 bay</p> <p>Roof: na</p> <p>Exterior: concrete with stone veneer enclosed balustrade</p> <p>Foundation: concrete abutments</p> <p>Windows: na</p> <p>Details: The current bridges replaced earlier bridges at this site. An attempt was made to make these bridges blend in with the park through the use of stone.</p>	<p>Although the east and west bound bridges are not yet 50 years old, they should be considered eligible as contributing elements to both the NRE Rahway River Parkway HD (ID #4079) and the Union County Park System HD (ID #4424).</p>
 <p>Photograph Number: 16</p>	<p>Panamerican Resource Number: 16</p> <p>101 Nomahegan Blvd</p> <p>Municipality: Cranford, Union County</p> <p>UTM coordinates (1983 Zone 18N): 4502691N, 557813E</p> <p>Date: ca. 1960s</p> <p>Other:</p>	<p>Style: Colonial Revival</p> <p>Stories: 2</p> <p>Plan: rectangular</p> <p>Roof: side gable</p> <p>Exterior: aluminum siding</p> <p>Foundation: covered</p> <p>Windows: metal 1/1 sash</p> <p>Details: much altered house that backs onto Lenape Park</p>	<p>Not Eligible, does not meet the NRHP criteria for eligibility</p>

Note: Incorrectly cited as 101 Nomahegan; Correct address is 101 County Park Dr.

NEW JERSEY OFFICE OF HISTORIC PRESERVATION HISTORIC SITES INVENTORY # 200302
INDIVIDUAL STRUCTURE SURVEY FORM

HISTORIC NAME:

COMMON NAME:

LOCATION: 223 Cranford Ave.

BLOCK/LOT 285/1

MUNICIPALITY: Cranford

COUNTY: Union

USGS QUAD: Roselle

UTM REFERENCES:

OWNER/ADDRESS: Anderson, Ernest
225 Cranford Ave.
Cranford, N.J.

Zone/Northing/Easting

DESCRIPTION

Construction Date: mid-19th Century

Source of Date: Visual

Architect:

Builder:

Style: ~~Planbook~~ PlanbookForm/Plan Type: Rectangular shape.
Side hall plan.Number of Stories: 2- $\frac{1}{2}$

Foundation: Fieldstone

Exterior Wall Fabric: Clapboard

Fenestration: Three bay facade. Tall first floor windows. 2/2 sash windows.

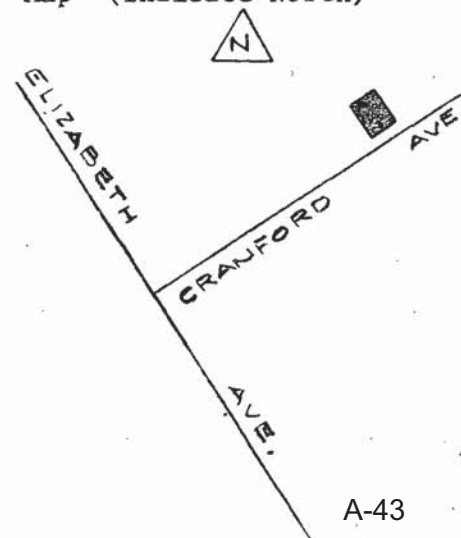
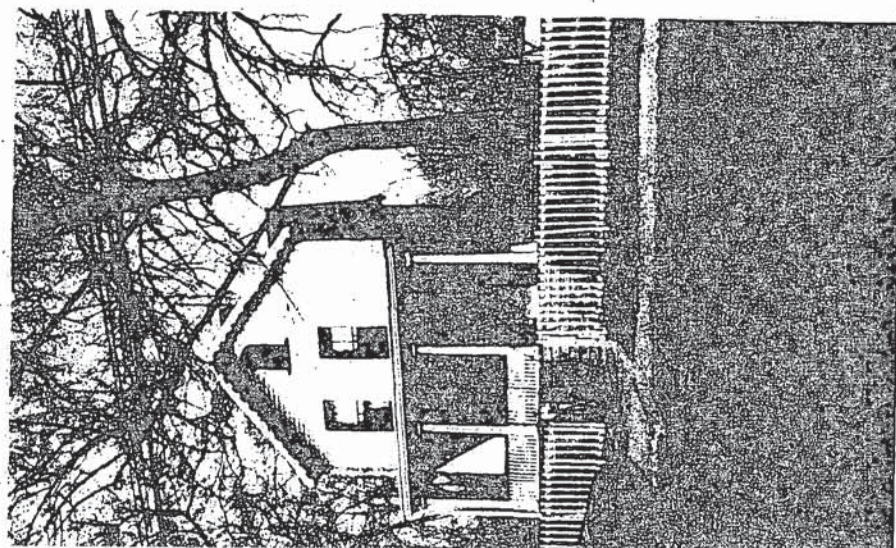
Roof/Chimneys: Gable roof. Central chimney of brick on ridge.

Additional Architectural Description:

Gable end facade.
Bargeboard trim along eaves.
Picket fence around property.
Arched window in gable.
Open porch with turned posts.

PHOTO Negative File # Roll 20 frame 4

Map (Indicate North)



SETTING, BOUNDARY DESCRIPTION, AND RELATED STRUCTURES:

House sits front center on an 85' x 100' lot approximately 20' from Central Ave. Picket fence along front property line. No outbuildings.

SURROUNDING ENVIRONMENT: Urban ☐ Suburban ☒ Scattered Buildings ☐
Open Space ☐ Woodland ☐ Residential ☒ Agricultural ☐
Industrial ☐ Downtown Commercial ☐ Highway Commercial ☐ Other ☐

Out-of-the-way neighborhood of small, frame houses. This house appears to be the oldest and the best maintained.

SIGNIFICANCE:

This neat, well kept house is a good example of a "Planbook" house, of the type common in the mid-19th Century. The Planbook house came from pattern books circulated among 19th Century builders. They provided simple, affordable housing, and the fairly plain house could be decorated with any number of stylistic features popular from the time such as cornice brackets, porches, and bargeboard.

ORIGINAL USE:

PHYSICAL CONDITION: Excellent ☐ Good ☒

REGISTER ELIGIBILITY: Yes ☒ Possible ☐

THREATS TO SITE: Roads ☐ Development ☐

No Threat ☒ Other ☐

COMMENTS:

PRESENT USE:

Fair ☐ Poor ☐

No ☐ Part of District ☐

Zoning ☐ Deterioration ☐

REFERENCES:

Cranford Historical Society

Gateways to Architecture

RECORDED BY: JWF
ORGANIZATION: UCCHPAB

DATE: 1981

CRANFORD
Union Co.,
200302
r. 20/4

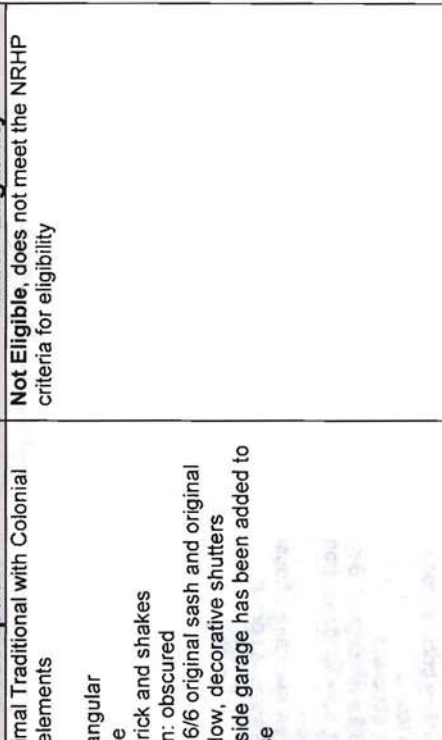

223 Cranford Ave.



looking N.W.

1981



Inventory of Identified Architectural Resources

Property	Property Information	Description	NRHP Eligibility
 <p>Photograph Number: 35</p>	<p>Panamerican Resource Number: 35</p> <p>2 Crescent Place</p> <p>Municipality: Cranford, Union County</p> <p>UTM coordinates (1983 Zone 18N): 4501650N, 558428E</p> <p>Date: ca. 1960</p> <p>Other: House backs onto Rahway River Parkway</p>	<p>Style: Minimal Traditional with Colonial Revival elements</p> <p>Stories: 2</p> <p>Plan: rectangular</p> <p>Roof: gable</p> <p>Exterior: brick and shakes</p> <p>Foundation: obscured</p> <p>Windows: 6/6 original sash and original bay window, decorative shutters</p> <p>Details: a side garage has been added to the house</p>	<p>Not Eligible, does not meet the NRHP criteria for eligibility</p>
 <p>Photograph Number: 36</p>	<p>Panamerican Resource Number: 36</p> <p>20 Hampton Road</p> <p>Municipality: Cranford, Union County</p> <p>UTM coordinates (1983 Zone 18N): 4501599N, 558425E</p> <p>Date: ca. 1920</p> <p>Other: House backs onto Rahway River Parkway</p>	<p>Style: Eclectic mix</p> <p>Stories: 2.5</p> <p>Plan: rectangular</p> <p>Roof: hipped</p> <p>Exterior: concrete</p> <p>Foundation: obscured</p> <p>Windows: original lozenge upper pane/ single lower pane front facade windows, highly decorative fan and side lights on front door</p> <p>Details: This is a highly intact, poured concrete house.</p>	<p>It is not clear why Hampton Road north of Springfield Avenue is not part of the NRE North Cranford HD. This part of the street should be considered as part of an expansion of the HD with this building as a potential contributing element.</p>

Inventory of Identified Architectural Resources			
Property	Property Information	Description	NRHP Eligibility
 <p>Photograph Number: 21</p>	<p>Panamerican Resource Number: 21</p> <p>19 Balmiere Parkway</p> <p>Municipality: Cranford, Union County</p> <p>UTM coordinates (1983 Zone 18N): 4501795N, 558321E</p> <p>Date: ca. 1930</p> <p>Other: Wood-shingled garage in the rear</p>	<p>Style: Tudor variation</p> <p>Stories: 2.5</p> <p>Plan: rectangular</p> <p>Roof: gable</p> <p>Exterior: vinyl siding</p> <p>Foundation: obscured</p> <p>Windows: 6/1 replacements</p> <p>Details: This is a much changed Tudor-style variation with decorative shutters and a plain front door</p>	Not eligible, loss of integrity
 <p>Photograph Number: 22</p>	<p>Panamerican Resource Number: 22</p> <p>Garage</p> <p>20 Crescent Place</p> <p>Municipality: Cranford, Union County</p> <p>UTM coordinates (1983 Zone 18N): 4501765N, 558349E</p> <p>Date: ca. 2000</p> <p>Other:</p>	<p>Style: Modern</p> <p>Stories: 1</p> <p>Plan: rectangular</p> <p>Roof: gable</p> <p>Exterior: vinyl siding</p> <p>Foundation: concrete</p> <p>Windows: none</p> <p>Details:</p>	Not Eligible, neither old enough nor significant enough to warrant consideration

Note: Incorrectly cited as 20 Crescent; Correct address is 18 Crescent

NEW JERSEY OFFICE OF HISTORIC PRESERVATION HISTORIC SITES INVENTORY # 200338
INDIVIDUAL STRUCTURE SURVEY FORM

HISTORIC NAME: COMMON NAME:
 LOCATION: 160 Denman Road BLOCK/LOT 489/1
 MUNICIPALITY: Cranford COUNTY: Union
 USGS QUAD: Roselle UTM REFERENCES:
 OWNER/ADDRESS: Aagaard, Geraldine Zone/Northing/Easting
 160 Denman Road, Cranford, N.J.

DESCRIPTION

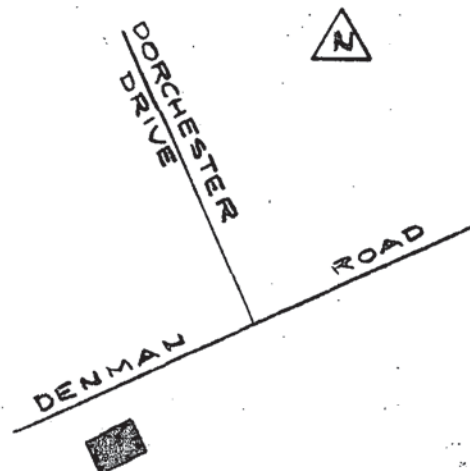
Construction Date: 19th Century Source of Date: Visual
 Architect: Builder:
 Style: Farmhouse Form/Plan Type: "L" shape plan.
 Number of Stories: 2½
 Foundation: Stucco over original foundation materials.
 Exterior Wall Fabric: Wooden shingle siding.
 Fenestration: Three bay facade. 1/1 sash windows.
 Roof/Chimneys: Gable roof. Deep eaves. End brick chimney.

Additional Architectural Description:

Enclosed facade porch with shed roof. Projecting pediment marks entry.
 Rear kitchen addition at right angles to house. Two story - two bay.
 "Stickwork" under eaves.
 Pointed arch attic window.

PHOTO Negative File # Roll 25, Frame 6

Map (Indicate North)



SITING, BOUNDARY DESCRIPTION, AND RELATED STRUCTURES:

1½ story barn in rear of property. Board and batten siding.
6/6 sash windows. Gable roof, Fair to good condition.

SURROUNDING ENVIRONMENT: Urban ☐ Suburban ☒ Scattered Buildings ☐
Open Space ☐ Woodland ☐ Residential ☒ Agricultural ☐
Industrial ☐ Downtown Commercial ☐ Highway Commercial ☐ Other ☐

SIGNIFICANCE:

Was a Ditzel House in 1906 (see 200339). The house may have been built for a member of the family who owned the present Ditzel farm in the 19th Century.

ORIGINAL USE: Farmhouse	PRESENT USE: Residence
PHYSICAL CONDITION: Excellent <input type="checkbox"/> Good <input checked="" type="checkbox"/>	Fair <input type="checkbox"/> Poor <input type="checkbox"/>
REGISTER ELIGIBILITY: Yes <input type="checkbox"/> Possible <input checked="" type="checkbox"/>	No <input checked="" type="checkbox"/> Part of District <input type="checkbox"/>
THREATS TO SITE: Roads <input type="checkbox"/> Development <input type="checkbox"/>	Zoning <input type="checkbox"/> Deterioration <input type="checkbox"/>
No Threat <input checked="" type="checkbox"/> Other <input type="checkbox"/>	
COMMENTS:	

REFERENCES:

1906 Atlas of Union County

RECORDED BY: JWF
ORGANIZATION: UCCHPAB

DATE: 1981



CRANFORD
Union Co.

* 200338
R25/6

160 Denman Rd
looking S.E.

1981

NEW JERSEY OFFICE OF HISTORIC PRESERVATION HISTORIC SITES INVENTORY # 200339
INDIVIDUAL STRUCTURE SURVEY FORM

HISTORIC NAME:	Denman Farm	COMMON NAME:	Ditzel's Farm
LOCATION:	299 Denman Road	BLOCK/LOT	464/8
MUNICIPALITY:	Cranford	COUNTY:	Union
USGS QUAD:	Roselle	UTM REFERENCES:	
OWNER/ADDRESS:	Ditzel, Harry 299 Denman Road, Cranford, N.J.		Zone/Northing/Easting

DESCRIPTION

Construction Date: Early 19th C.

Source of Date: Visual

Architect:

Builder:

Style: Vernacular farmhouse.

Form/Plan Type: Central hall plan.

Number of Stories: Two

Foundation: Cut stone foundation on facade, fieldstone foundation in rear.

Exterior Wall Fabric: Composition shingle siding.

Fenestration: Five bay facade. Some 2/2 sash windows, others replacements.

Roof/Chimneys: Gable roof. End brick chimney.

Additional Architectural Description:

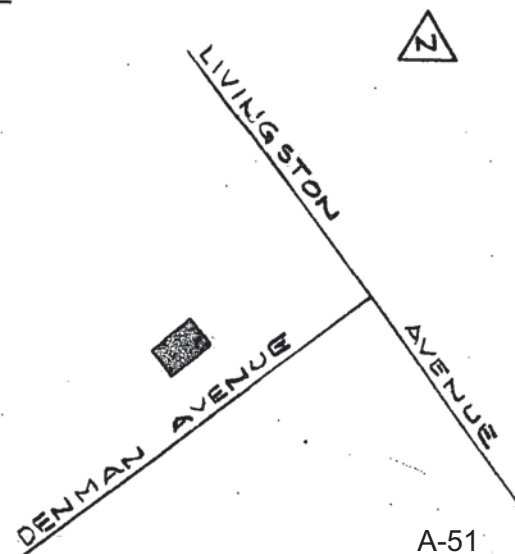
Two dormers on facade.

Wide space above first floor windows on facade.

Second floor windows at rear of house.

PHOTO Negative File # Roll 25, Frame 5.

Map (Indicate North)



SITING, BOUNDARY DESCRIPTION, AND RELATED STRUCTURES:

1½ story barn on property. Natural shingle siding. Lean-to additions.
Barn is in very poor condition.
Five acre property, used until 1980 as a nursery and truck farm.

SURROUNDING ENVIRONMENT: Urban ☐ Suburban ☒ Scattered Buildings ☐
Open Space ☐ Woodland ☐ Residential ☒ Agricultural ☐
Industrial ☐ Downtown Commercial ☐ Highway Commercial ☐ Other ☐

Elementary school nearby, and small, suburban homes surround the property.

SIGNIFICANCE:

In the Ditzel family since 1906. Earlier history of the farm unknown, but it appears to have been one of the early farms in the area now known as Cranford.

The Ditzels had a truck farm here, and through most of the 20th Century the family name was synonymous with good, fresh produce.

The farm was probably originally a farm of the Denman family. The Denmans descend from one John Denman who bought property here in 1688.

ORIGINAL USE: Farmhouse

PHYSICAL CONDITION: Excellent ☐ Good ☐

REGISTER ELIGIBILITY: Yes ☐ Possible ☐

THREATS TO SITE: Roads ☐ Development ☒

No Threat ☐ Other ☐

COMMENTS:

The house and barn are on one of the few large undeveloped lots in Cranford.

PRESENT USE: Residence

Fair ☒ Poor ☐

No ☒ Part of District ☐

Zoning ☐ Deterioration ☐

REFERENCES:

Union County Landmarks Inventory, 1976.

RECORDED BY: JWF

DATE: 1981

ORGANIZATION: UCCHPAB



CRANFORD

DENMAN FARM

Union Co.

* 200339
R 25/5

299 Denman Road
looking W.

1981



CRANFORD

Anton Co



* 200339

R25/4

Denman Farm

299 Denman Road
looking N.W.

1981

Inventory of Identified Architectural Resources			
Property	Property Information	Description	NRHP Eligibility
 <p>Photograph Number: 25</p>	<p>Panamerican Resource Number: 25</p> <p>112 Park Drive</p> <p>Municipality: Cranford, Union County</p> <p>UTM coordinates (1983 Zone 18N): 4502015N, 558074E</p> <p>Date: ca. 1930s</p> <p>Other: House overlooks Nomahegan Park, and is empty and in poor condition.</p>	<p>Style: Colonial Revival</p> <p>Stories: 1.5</p> <p>Plan: rectangular</p> <p>Roof: side gable w/dormers</p> <p>Exterior: brick and shakes</p> <p>Foundation: brick and concrete</p> <p>Windows: 6/6 replacement</p> <p>Details: This small house is made of white brick with a decorative brick door surround. A side porch and garage was added at a later date in vinyl siding. The house sits on an exceptionally large property.</p>	<p>Not Eligible, does not meet the NRHP criteria for eligibility</p>
 <p>Photograph Number: 26</p>	<p>Panamerican Resource Number: 26</p> <p>22 Doering Way</p> <p>Municipality: Cranford, Union County</p> <p>UTM coordinates (1983 Zone 18N): 4501911N, 558118E</p> <p>Date: ca. 1930</p> <p>Other: This structure is on the grounds of what was originally a large estate at the end of Doering Way. It is now subdivided and surrounded by houses of various ages.</p>	<p>Style: Colonial Revival</p> <p>Stories: 2</p> <p>Plan: L shape</p> <p>Roof: gable</p> <p>Exterior: wood siding and hardy board</p> <p>Foundation: concrete</p> <p>Windows: 6/6 sash, 2 bay windows, some fixed—all appear to be replacements; some operational shutters</p> <p>Details: This house has been much altered and added to, but retains much of its original character including the front door surround.</p>	<p>Not Eligible, does not meet the NRHP criteria for eligibility</p>

NEW JERSEY HISTORIC BRIDGE SURVEY

STRUCTURE #: 2003008 COUNTY: UNION OWNER: COUNTY ROUTE: 9020

MILEPOINT: 000000

TOWNSHIP: CRANFORD TOWNSHIP

FACILITY CARRIED: EASTMAN STREET

NAME/FEATURE INTERSECTED: EASTMAN STREET OVER RAHWAY RIVER

TYPE: THRU GIRDER

DESIGN: PARTIALLY ENCASED

MATERIAL: STEEL

SPANS: 001

LENGTH: 000085

WIDTH: 0300

DATE OF CONSTRUCTION: 1918 ALTERATION:

SOURCE: PLAQUE

DESIGNER/PATENT: JACOB L. BAUER, COUNTY ENGINEER BUILDER: AMER. BRIDGE, DVR. BLR. WRKS

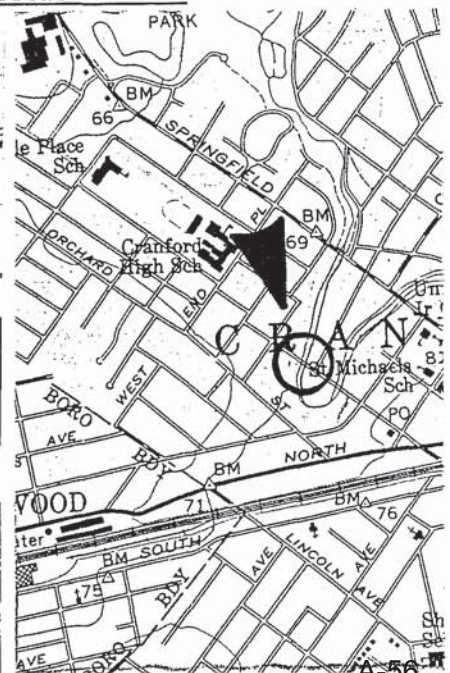
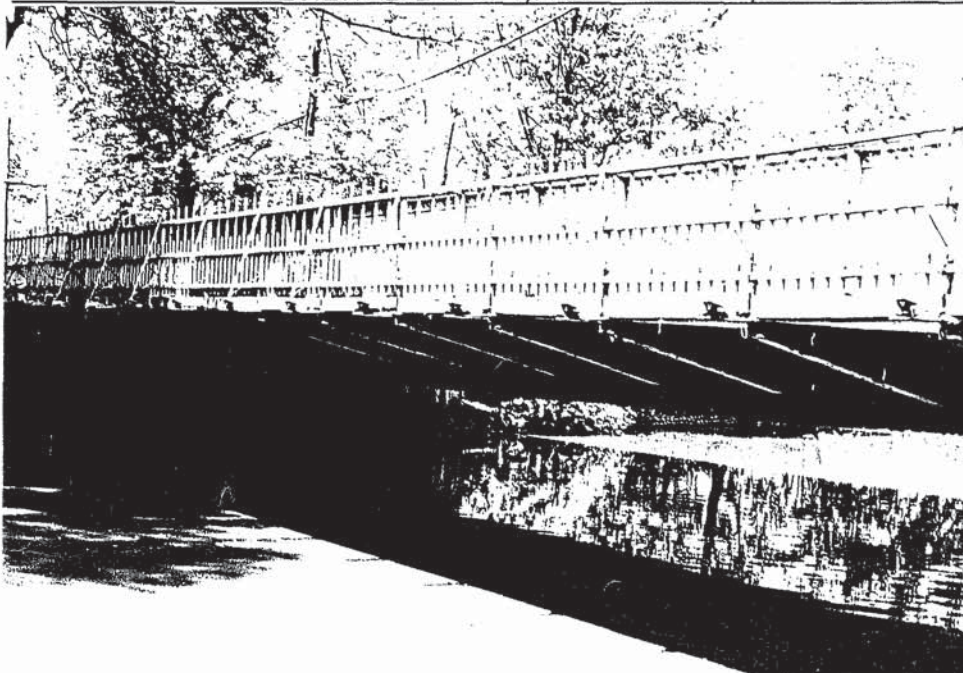
SETTING/CONTEXT: The bridge is located in a potential large historic district of early-20th century residences. The bridge carries a two-lane street and two sidewalks over the Rahway River, a winding waterway that is bounded by parklands for much of its length. The homes in the district are well-preserved examples of picturesque and academic Colonial Revival styles. Bridges 2003014, 2003072, both thru-girder spans, and 2003025, a reinforced concrete arch, are also located in the potential district.

CURRENT NATIONAL REGISTER STATUS: Not Previously Evaluated

NATIONAL REGISTER RECOMMENDATION: Eligible

SUMMARY: The one-span thru-girder bridge sits on concrete abutments. The rolled floorbeams are encased except for the bottom flanges. The cantilevered sidewalks are enclosed with fence-like metal railings. The bridge is a representative example of a common early-20th century bridge type, and is not technologically or historically distinguished by itself. It is significant because it was constructed during the period of significance of the potential district.

PHOTO: 159:6-7 REVIEWED BY: EPV/AGL DATE: 05/92 QUAD: Roselle



DRAFT



DRAFT

2003008

Bibliography: Cranford Board of Trade. Cranford, New Jersey. 1913. Central Railroad of New Jersey. "Why Not Own Your Own Home on the Line of the Central Railroad of New Jersey." 1891.

Physical Description: The 1918 one-span built-up thru girder with floor beams bridge is supported on a concrete substructure. The floor beams are rolled, and both sides of the girders have knee braces. The cantilevered sidewalks are enclosed with the original decorative iron fence-like railings. The girders themselves appear to be unaltered.

Historical and Technological Significance: The 1916 thru girder bridge was designed by County Engineer Jacob L. Bauer. The plaque indicates that the girders were fabricated by the American Bridge Company, but the bridge was erected by the Dover Boiler Works, a Morris County fabricator that was active in Northern New Jersey until at least 1919. The bridge is a representative example of a popular early-20th century bridge type, and it is not technologically distinguished. It is, however, historically significant because of its location in a locally identified National Register-eligible historic district made up of well-preserved late-19th and early-20th century neighborhoods developed as part of the suburbanization of the greater New York area (criterion C).

Cranford, a township established in 1871, is located on the Central Railroad of New Jersey (CNJ) main line to its terminal in Jersey City and New York City connections. Because of its location on the rail line, the CNJ and local interests promoted Cranford as an ideal situation for successful New York City businessmen who were desirous of a comfortable home in a country setting within commuting distance to the city. With the absence of industry and the advantage of the scenic Rahway River meandering through the town, Cranford was consciously developed as a residential community geared to wealthy New York professionals. Promoted as "The Venice of New Jersey," the town is characterized by large well-preserved picturesque and academic Colonial Revival homes on generous, casually landscaped lots. The river played a large factor in both the physical development of the town community, and its image, as recreational use of the Rahway has been highlighted throughout the town's postbellum history. The river was treated as an amenity to enhance the residential development of the community. In many places the riverbank is lined with stone walls and other ornamentation, and both streets and houses are oriented toward the most dominant physical feature in the potential historic district.

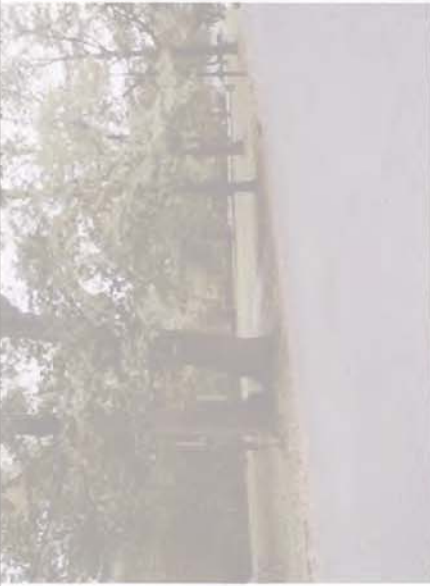

The North Cranford Historic District is a large residential enclave that retains its pre-1930 character. The well-preserved neighborhood ranks as one of the premier railroad-stimulated suburbs in New Jersey. Architecturally it reflects the architectural preferences of the pre-1929 Depression era, and historically it illustrates the strong influence of the railroad on postbellum and early-20th century development in northern New Jersey. Cranford is located about 17 miles west of New York City, and in 1913, the community was serviced by 93 daily trains. The trip, including the ferry connection, was 45 minutes. The initial stimulus for Cranford becoming a "railroad suburb" came in 1864 when the CNJ opened its first bridge over Newark Bay and a new passenger station in the center of what was a crossroads community known as Cranville. Shortly thereafter farms were converted into subdivisions.

The area was identified as a potential National Register historic district after a community wide review by the Cranford Historical Society in 1989. The formal application for National Register listing has not been completed, but the area clearly meets the criteria for inclusion in the

Register. This bridge is one of four contributing spans identified in the potential historic district that has a period of significance from 1838, when train service was introduced to the town, through 1929. This bridge is one of four identified in the potential historic district that has a period of significance from 1838, when train service was introduced to the town, through 1929 (2003025, 2003008, 2003014, 2003072).

Boundary Description and Justification: The bridge is not individually significant, but it is located within and is completely surrounded by an potential architecturally significant late-19th and early-20th century residential historic district with a period of significance through until 1929. Since the 1918 bridge was built within that period of significance and it is not significantly altered, it is a contributing resource. Therefore, the bridge and its surroundings are significant. The limit of the district appears to be beyond the houses adjacent to the west side of the bridge.

DRAFT

Inventory of Identified Architectural Resources			
Property	Property Information	Description	NRHP Eligibility
 <p>Photograph Number: 49</p>	<p>Panamerican Resource Number: 49</p> <p>Hampton Park</p> <p>Hampton Street at Eastman Avenue</p> <p>Municipality: Cranford, Union County</p> <p>UTM coordinates (1983 Zone 18N): 4501139N, 558311E</p> <p>Date: after 1969</p> <p>Other: Cranford municipal park; sits at the edge of the Rahway River</p>	<p>Style: informal</p> <p>Stories: na</p> <p>Plan: informal with a few walking paths</p> <p>Roof: na</p> <p>Exterior: na</p> <p>Foundation: na</p> <p>Windows: na</p> <p>Details: This is a vest pocket park on the edge of the County McConnell Park.</p>	<p>Eligible as Contributing element to the NRE North Cranford HD (ID #3838) and contributing to the Rahway River Parkway HD (ID #4079)</p>
 <p>Photograph Number: 50 (southbound balustrade)</p>	<p>Panamerican Resource Number: 50</p> <p>Eastman Street Bridge (NJDOT #2003008) at Hampton Street crossing Rahway River</p> <p>Municipality: Cranford, Union County</p> <p>UTM coordinates (1983 Zone 18N): 4501070N, 558329E</p> <p>Date: 2004</p> <p>Other: Lies directly in the 100 foot APE; the bridge is owned by Union County.</p>	<p>Style: rustic w/ classical detailing</p> <p>Stories: na</p> <p>Plan: single arch</p> <p>Roof: na</p> <p>Exterior: balustrades with lower rubble course and upper open work metal railings</p> <p>Foundation: concrete abutments</p> <p>Windows: na</p> <p>Details: The bridge was constructed on the site of an earlier bridge. Its rustic styling reflects the neighborhood and it blends with McConnell Park, which is immediately to the southeast. According to plaque, designed by Schoor Depalma, Inc., and constructed by Marbro, Inc.</p>	<p>Although this bridge is not yet 50 years old, it is located directly within the NRE Rahway River Parkway HD (ID #4079), NRE Union County Park System HD (ID #4424), and NRE North Cranford HD (ID #3838), and should be considered a contributing element to all of the HD.</p>

Panamerican Consultants, Inc.

6-26

Rahway River Phase 1A

NEW JERSEY HISTORIC BRIDGE SURVEY

Phase 1 ID: 30

STRUCTURE #: 2003025 COUNTY: UNION OWNER: COUNTY ROUTE: 9020

MILEPOINT: 000000

TOWNSHIP: CRANFORD TOWNSHIP

FACILITY CARRIED: EASTMAN STREET

NAME/FEATURE INTERSECTED: EASTMAN STREET OVER RAHWAY RIVER

DRAFT

TYPE: DECK ARCH

DESIGN: ELLIPTICAL

MATERIAL: REINFORCED CONCRETE

SPANS: 001

LENGTH: 000105 WIDTH: 0298

DATE OF CONSTRUCTION: 1913 ALTERATION: 1985ca.

SOURCE: NJDOT/STYLE

DESIGNER/PATENT:

BUILDER:

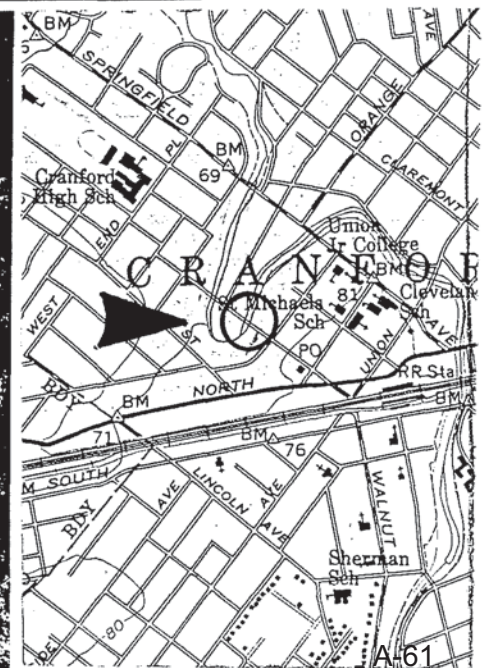
SETTING/CONTEXT: The bridge carries two lanes and two sidewalks over the Rahway River. A greenway borders the winding river through the county, and this bridge is located next to McConnell Park. The neighborhood surrounding the park and bridge is early-20th century residences predominantly in the picturesque and academic Colonial Revival style. The area has historic district potential. The district includes three other bridges, all thru-girder spans from the period of significance of the district.

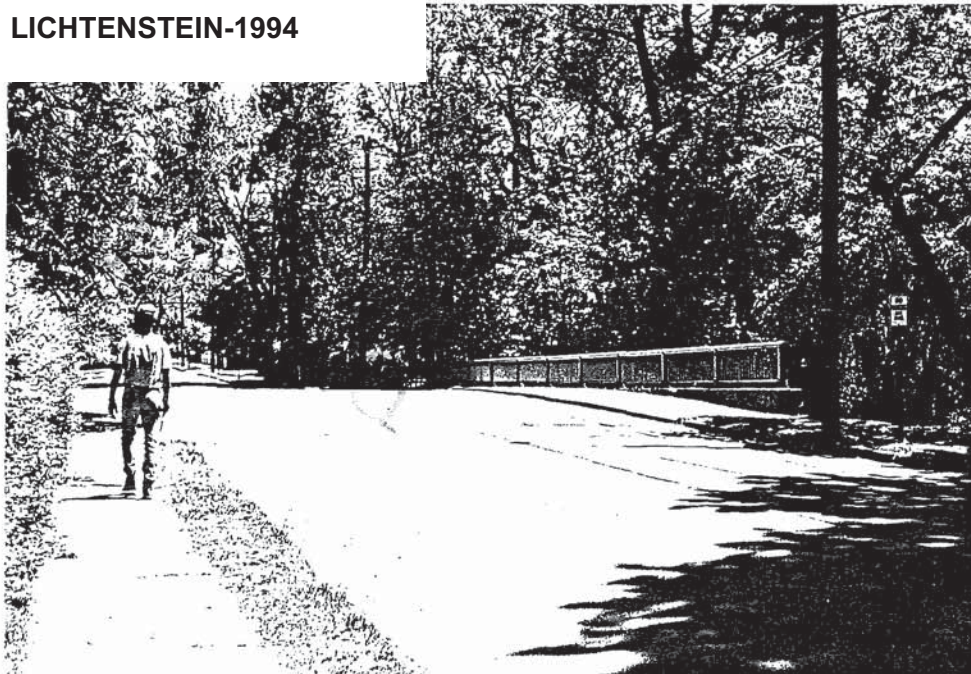
CURRENT NATIONAL REGISTER STATUS: Not Previously Evaluated

NATIONAL REGISTER RECOMMENDATION: Eligible

SUMMARY: The reinforced concrete deck arch bridge has paneled spandrels and wingwalls. The arch creates a vertical crest curve over the bridge, which is skewed. A metal railing is bolted to the curb at the spandrel wall. Though the railing is not original, it is not intrusive into the character of the bridge or its surroundings. The bridge is significant because it was built during the period of significance of a potential historic district but is not technologically distinguished in its own right.

PHOTO: 159:4-5 REVIEWED BY: EPV/AGL DATE: 05/92 QUAD: Roselle





DRAFT

2003025

Bibliography: Cranford Board of Trade. Cranford, New Jersey. 1913.

Physical Description: The 105'-long reinforced concrete deck arch bridge has the traditional elliptical shape and scored spandrel and wingwall panels. The roadway has a slight vertical profile. The original railing has been removed and replaced with an ca. 1985 metal railing that is compatible in styling. It is set into the concrete capstone.

Historical and Technological Significance: The 1913 reinforced concrete deck arch bridge over the Rahway River is significant because of its location in a locally identified National Register-eligible historic district made up of well-preserved late-19th and early-20th century neighborhoods developed as part of the suburbanization of the greater New York area (criterion A). While the span is not individually distinguished because it is not technologically innovative and has a replacement railing, and is one of nine deck arch spans built in the country between 1907 and 1917, it was built within the pre-1929 period of significance of the potential historic district and retains its integrity as an arch span. It is therefore a contributing resource to the potential historic district.

Cranford, a township established in 1871, is located on the Central Railroad of New Jersey (CNJ) main line to its terminal in Jersey City and New York City connections. Because of its location on the rail line, the CNJ and local interests promoted Cranford as an ideal situation for successful New York City businessmen who were desirous of a comfortable home in a country setting within commuting distance to the city. With the absence of industry and the advantage of the scenic Rahway River meandering through the town, Cranford was consciously developed as a residential community geared to wealthy New York professionals. Promoted as "The Venice of New Jersey," the town is characterized by large well-preserved picturesque and academic Colonial Revival homes on generous, casually landscaped lots. The river played a large factor in both the physical development of the comfortable community, and its image, as recreational use of the Rahway has been highlighted throughout the town's postbellum history. The river was treated as an amenity to enhance the residential development of the community. In many places the riverbank is lined with stone walls and other ornamentation, and both streets and houses are oriented toward the most dominant physical feature in the potential historic district.

The North Cranford Historic District is a large residential enclave that retains its pre-1930 character. The well-preserved neighborhood ranks as one of the premier railroad-stimulated suburbs in New Jersey. Architecturally it reflects the architectural preferences of the pre-1929 Depression era, and historically it illustrates the strong influence of the railroad on postbellum and early-20th century development in northern New Jersey. Cranford is located about 17 miles west of New York City, and in 1913, the community was serviced by 93 daily trains. The trip, including the ferry connection, was 45 minutes. The initial stimulus for Cranford becoming a "railroad suburb" came in 1864 when the CNJ opened its first bridge over Newark Bay and a new passenger station in the center of what was a crossroads community known as Cranville. Shortly thereafter farms were converted into subdivisions.



The area was identified as a potential National Register historic district after a community wide review by the Cranford Historical Society in 1989. The formal application for National Register listing has not been completed, but the area clearly meets the criteria for inclusion in the Register. This bridge is one of four identified in the potential historic district that has a period of significance from 1838, when train service was introduced to the town, through 1929 (2003025,





DRAFT

2003008, 2003014, 2003072).

Boundary Description and Justification: The bridge is not individually significant, but it is located within and is completely surrounded by an potential architecturally significant late-19th and early-20th century residential historic district with a period of significance through until 1929. Since the 1913 bridge was built within that period of significance and it is not significantly altered, it is a contributing resource. Therefore, the bridge and its surroundings are significant.

Inventory of Identified Architectural Resources			
Property	Property Information	Description	NRHP Eligibility
 <p>Photograph Number: 61</p>	<p>Panamerican Resource Number: 61</p> <p>122 Eastman Avenue</p> <p>Municipality: Cranford, Union County</p> <p>UTM coordinates (1983 Zone 18N): 4501021N, 558427E</p> <p>Date: 1923</p> <p>Other: House sits on the bank of the Rahway River; it overlooks McConnell Park.</p>	<p>Style: Colonial Revival</p> <p>Stories: 2 with basement</p> <p>Plan: rectangular</p> <p>Roof: gambrel with expanding sides</p> <p>Exterior: vinyl siding</p> <p>Foundation: concrete</p> <p>Windows: 6/1, door has side lights and a fan light; decorative shutters</p> <p>Details: highly decorative doorway</p>	<p>The house at 126 Eastman Avenue is eligible as a contributing element to the NRE North Cranford HD (ID #3338; Bary 2012).</p>
 <p>Photograph Number: 62 (west balustrade in McConnell Park)</p>	<p>Panamerican Resource Number: 62</p> <p>Bridge (NJDOT #2003025) on Eastman Avenue at Holly Street crossing Rahway River</p> <p>Municipality: Cranford, Union County</p> <p>UTM coordinates (1983 Zone 18N): 4500975N, 558451E</p> <p>Date: ca. 1970</p> <p>Other: Bridge was surveyed because it lies directly within the 100' APE; bridge is owned by Union County.</p>	<p>Style: deck arch</p> <p>Stories: na</p> <p>Plan: na</p> <p>Roof: na</p> <p>Exterior: metal open work balustrade, paneled spandrels and wingwalls</p> <p>Foundation: concrete abutments</p> <p>Windows: na</p> <p>Details: Bridge is in the location of an earlier bridge. Although a graceful single arch was used when the bridge was replaced, the metal balustrade detracts from the beauty of the bridge. The bridge can be seen from McConnell Park.</p>	<p>Although the bridge is not yet 50 years old, it is located directly within the NRE Rahway River Parkway HD (ID #4079), NRE Union County Park System HD (ID #4424), and NRE North Cranford HD (ID #3338) and should be considered a contributing element to all of them. The A.G. Lichtenstein & Associates survey (1994) recommended it as NRE.</p>

Inventory of Identified Architectural Resources			
Property	Property Information	Description	NRHP Eligibility
 <p>Photograph Number: 61</p>	<p>Panamerican Resource Number: 61</p> <p>122 Eastman Avenue</p> <p>Municipality: Cranford, Union County</p> <p>UTM coordinates (1983 Zone 18N): 4501021N, 558427E</p> <p>Date: 1923</p> <p>Other: House sits on the bank of the Rahway River, it overlooks McConnell Park.</p>	<p>Style: Colonial Revival</p> <p>Stories: 2 with basement</p> <p>Plan: rectangular</p> <p>Roof: gambrel with expanding sides</p> <p>Exterior: vinyl siding</p> <p>Foundation: concrete</p> <p>Windows: 6/1, door has side lights and a fan light; decorative shutters</p> <p>Details: highly decorative doorway</p>	<p>The house at 126 Eastman Avenue is eligible as a contributing element to the NRE North Cranford HD (ID #3838; Bary 2012).</p>
 <p>Photograph Number: 62 (west balustrade in McConnell Park)</p>	<p>Panamerican Resource Number: 62</p> <p>Bridge (NJDOT #2003025) on Eastman Avenue at Holly Street crossing Rahway River</p> <p>Municipality: Cranford, Union County</p> <p>UTM coordinates (1983 Zone 18N): 4500975N, 558451E</p> <p>Date: ca. 1970</p> <p>Other: Bridge was surveyed because it lies directly within the 100' APE; bridge is owned by Union County.</p>	<p>Style: deck arch</p> <p>Stories: na</p> <p>Plan: na</p> <p>Roof: na</p> <p>Exterior: metal open work balustrade, paneled spandrels and wingwalls</p> <p>Foundation: concrete abutments</p> <p>Windows: na</p> <p>Details: Bridge is in the location of an earlier bridge. Although a graceful single arch was used when the bridge was replaced, the metal balustrade detracts from the beauty of the bridge. The bridge can be seen from McConnell Park.</p>	<p>Although the bridge is not yet 50 years old, it is located directly within the NRE Rahway River Parkway HD (ID #4079), NRE Union County Park System HD (ID #4424), and NRE North Cranford HD (ID #3838) and should be considered a contributing element to all of them. The A.G. Lichtenstein & Associates survey (1994) recommended it as NRE.</p>

Panamerican Consultants, Inc.

6-32

Rahway River Phase IA

According to the Lichtenstein survey, the DVB was a Morris County fabricator that was active in northern New Jersey until at least 1919.

The Lichtenstein survey concluded that the bridge is a representative example of a common early 20th-century bridge type, but is not technologically or historically distinguished by itself. Thus the bridge is significant as a contributing resource to the North Cranford Historic District (Lichtenstein 1994).

4.3 McConnell Park

McConnell Park is located along the Rahway River on Eastman Street, near the Eastman Street Bridge. The Rahway River bends in a U-shape near Eastman Street, and the street actually crosses the river twice. The land that is bounded by this U-shape is the present-day McConnell Park. Historically, this land area was known as the peninsula (Kraft 1977:14).

Dr. Joseph Kerr MacConnell, who was Cranford's first physician, owned the property where the park is located. It was reported that MacConnell sold the property to the township for use as a park at a price below its actual value (Cranford Citizen August 21, 1919). Background research did not indicate any association between this park and the Olmsted Brothers.

The park consists of an open area of grass and trees that is bordered by the Rahway River. The park does not feature any landscape design features, such as a formal design, plantings or formal landscaping along the river.



Plate 22: Photo showing McConnell Park, as seen from the foot of the Eastman Street Bridge. View facing southeast.

DATE: 4/10/01
PHOTOGRAPHER:
ANDREA LODATO
ROLL 4, NEG. 27, EXP. 27

**Phase II Cultural Resource Investigation
Eastman Street Bridge
Cranford Township, Union County, New Jersey**

**CULTURAL
RESOURCE
CONSULTING
GROUP**

Union County Parks Survey

Park Name **Rahway River Parkway: McConnell Park**
 Township/City **Cranford**

photo



Type

☐ recreational ☐ pastoral ☐ picturesque ☐ natural ☐ scenic ☐ other:

Existing Topography

☐ flat ☐ sloped ☐ hilly ☐ mountainous ☐ other:

Spatial Relationships

☐ symmetrical ☐ asymmetrical ☐ axial

Circulation

☐ trails

☐ cycling paths

☐ bridle paths

☐ parking lots

☐ handicapped ramps

☐ other:

Parkways

☐ Border roads ☐ Connecting pleasure roads ☐ Other:
☐ Internal park roads ☐ River parkways
☐ Vernacular roads ☐ Estate roads
☐ Summit roads ☐ Ocean parkways

Surveyor Gregory Dietrich

Date 10/2/03

Union County Parks Survey

Drainage & Engineering

<input type="checkbox"/> Bridge (road)	(See Bridge info)
<input type="checkbox"/> Bridge (pedestrian)	
<input type="checkbox"/> Channel	
<input type="checkbox"/> Culvert	
<input type="checkbox"/> Spillway	
<input type="checkbox"/> Millrace	
<input type="checkbox"/> Retaining wall	rubble stone along Rahway River
<input type="checkbox"/> Storm sewer	
<input type="checkbox"/> Swale	
<input type="checkbox"/> Other:	

Bridge

carries	bridge date	info source	bridge type/design	bridge material
Eastman Street	c.2000	Visual evidence	stringer encased	steel & concrete
Eastman Street	c.2000	Visual evidence	stringer encased	steel & concrete

Landscape Dividers

Land Uses

<input type="checkbox"/> archery	<input type="checkbox"/> skating--sidewalk
<input type="checkbox"/> baseball	<input type="checkbox"/> soccer
<input type="checkbox"/> basketball	<input type="checkbox"/> softball
<input type="checkbox"/> boating/canoeing	<input type="checkbox"/> swimming
<input type="checkbox"/> biking	<input type="checkbox"/> tennis
<input type="checkbox"/> camping	<input type="checkbox"/> theater/music
<input type="checkbox"/> concession/dining	<input type="checkbox"/> track & field
<input type="checkbox"/> cricket	<input type="checkbox"/> trap/skeet shooting
<input type="checkbox"/> driving	<input type="checkbox"/> trail
<input type="checkbox"/> fishing	<input type="checkbox"/> wading
<input type="checkbox"/> football	<input type="checkbox"/> other:
<input type="checkbox"/> golf	
<input type="checkbox"/> gymnastics	
<input type="checkbox"/> handball court	
<input type="checkbox"/> hockey	
<input type="checkbox"/> horseback riding	
<input type="checkbox"/> horticulture/gardening	
<input type="checkbox"/> lawn bowling	
<input type="checkbox"/> model boating	
<input type="checkbox"/> picnicking	
<input type="checkbox"/> playground	
<input type="checkbox"/> shuffleboard	
<input type="checkbox"/> skating--rink	

Water Elements

<input type="checkbox"/> rivers	<input type="checkbox"/> creeks	<input type="checkbox"/> pools
<input type="checkbox"/> lakes	<input type="checkbox"/> ponds	<input type="checkbox"/> fountains
<input type="checkbox"/> brooks	<input type="checkbox"/> waterfalls	<input type="checkbox"/> other :

Buildings

<input type="checkbox"/> maintenance	<input type="checkbox"/> stable	<input type="checkbox"/> other:
<input type="checkbox"/> recreational	<input type="checkbox"/> house	
<input type="checkbox"/> concession	<input type="checkbox"/> rest rooms	
<input type="checkbox"/> picnic shelter	<input type="checkbox"/> mill	
<input type="checkbox"/> gazebo	<input type="checkbox"/> museum/visitor center	
<input type="checkbox"/> bandstand	<input type="checkbox"/> public utility	

Landscape Furniture

<input type="checkbox"/> benches	<input type="checkbox"/> planters
<input type="checkbox"/> bleachers	<input type="checkbox"/> trash cans
<input type="checkbox"/> bike racks	<input type="checkbox"/> other:
<input type="checkbox"/> bollards	
<input type="checkbox"/> drinking fountains	
<input type="checkbox"/> fitness trail stations	
<input type="checkbox"/> grills	
<input type="checkbox"/> lighting fixtures	
<input type="checkbox"/> picnic tables	

Surveyor Gregory Dietrich

Date 10/2/03

Union County Parks Survey

Description

The McConnell Park section of the Rahway River Parkway is a small pastoral landscape bordered on two sides by the Rahway River, and characterized by scattered mature trees within a flat meadow.

Setting

McConnell Park is bordered by Eastman Street on its northeast side, and the Rahway River on its other sides. 2½-story single-family dwellings (c.1915/c.1990) are located along Easton Street and along the Rahway River.

Surveyor

Gregory Dietrich

Date

10/2/03

Union County Parks Survey

Markers/ documents/ sculpture	
Signage	metal
Utilities	
Plantings	mature oaks, apple, sycamores, pines, multi-floral rose, tulips, poplars
CRM issues	NJ-HPO ID No. 4079: Rahway River Parkway Historic District; Survey Form reference number (40) NJ-HPO ID No. 3838: North Cranford Historic District; Survey Form reference number (38) NJ-HPO ID No. 3874: Garden State Parkway Historic District; Survey Form reference number (31) NJ-HPO ID No. 4053: Central Railroad of NJ Bridge over Rahway River; Survey Form reference number (35)

Character Defining Features/Resources	Contributing Features	Non-Contributing Features
Overall configuration of park	Meadow Scattered mature trees	

Union County Parks Survey

Contributing Resources

Non-Contributing Resources

c.2000 bridge carrying Eastman Street
c.2000 bridge carrying Eastman Street
Picnic tables

Boundary of eligible portion of park

All of the property contained within Block 180, Lot 1 of the Township of Cranford, Union County, New Jersey.

Eligible

☐ yes
☐ no

Rating

☐ Key Contributing ☐ Contributing ☐ Non-Contributing

Areas of Significance

<input type="checkbox"/> Agriculture	<input type="checkbox"/> Economics	<input type="checkbox"/> Industry	<input type="checkbox"/> Performing Arts
<input type="checkbox"/> Architecture	<input type="checkbox"/> Education	<input type="checkbox"/> Invention	<input type="checkbox"/> Philosophy
<input type="checkbox"/> Art	<input type="checkbox"/> Engineering	<input type="checkbox"/> Landscape Architecture	<input type="checkbox"/> Politics
<input type="checkbox"/> Commerce	<input type="checkbox"/> Entertainment/Recreation	<input type="checkbox"/> Law	<input type="checkbox"/> Religion
<input type="checkbox"/> Communications	<input type="checkbox"/> Ethnic Heritage	<input type="checkbox"/> Literature	<input type="checkbox"/> Science
<input type="checkbox"/> Community Planning	<input type="checkbox"/> Exploration/Settlement	<input type="checkbox"/> Maritime History	<input type="checkbox"/> Social History
<input type="checkbox"/> Conservation	<input type="checkbox"/> Health/Medicine	<input type="checkbox"/> Military	<input type="checkbox"/> Transportation

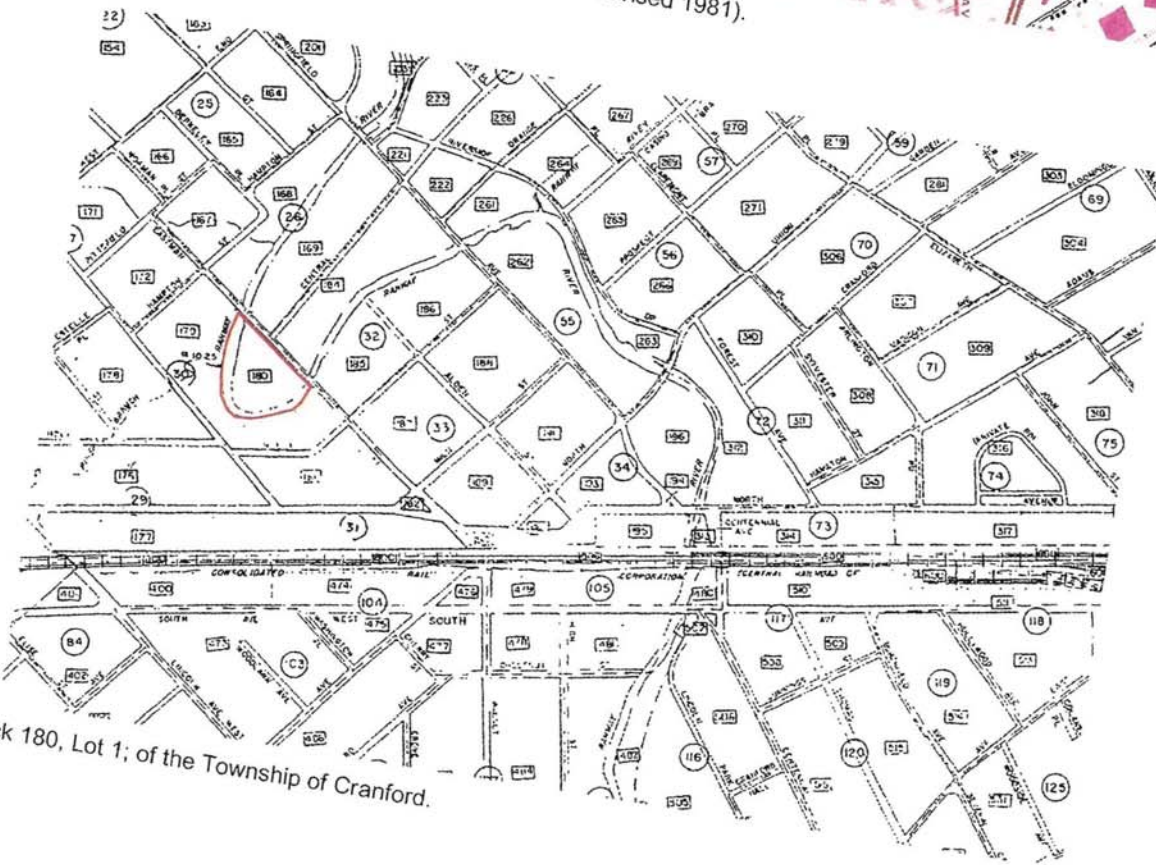
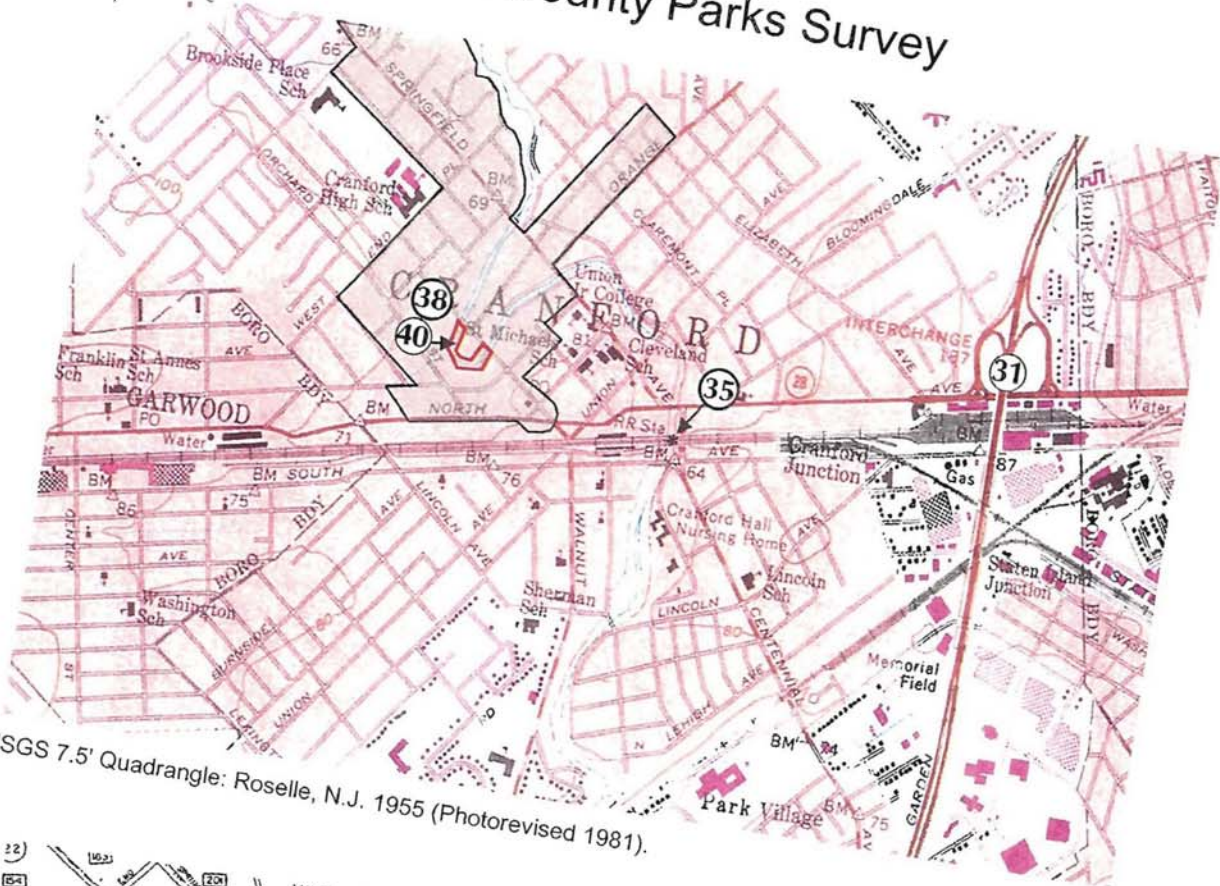
Justification of Eligibility

Acquired and developed within the period of significance (1921-1964), Rahway River Parkway: McConnell Park is eligible under Criterion A for having made contributions to the broad patterns of history in the areas of Community Planning, Entertainment/Recreation, and Landscape Architecture on the national level; and in the area of Conservation on the state level. Under Criterion C, it is significant for its association with the nationally renowned Olmsted Brothers Landscape Architecture Firm of Brookline, Massachusetts. Under Criterion D, it is eligible for having the capacity to yield important information related to history. With regard to historic integrity, in spite of numerous bridge replacements along its waterway, Rahway River Parkway has retained most of its original location, design, feeling and association.



Surveyor Gregory Dietrich

Date 10/2/03

Union County Parks Survey



Inventory of Identified Architectural Resources

Property	Property Information	Description	NRHP Eligibility
 <p>Photograph Number: 122</p>	<p>Panamerican Resource Number: 122</p> <p>Girl Scout Park</p> <p>Springfield Avenue</p> <p>Municipality: Cranford, Union County</p> <p>UTM coordinates (1983 Zone 18N): 4501252N, 558604E</p> <p>Date: unknown</p> <p>Other: to the west of Girl Scout Park are Cranford Clay Tennis Courts, a Cranford Municipal Park</p>	<p>Style: informal, vest pocket park</p> <p>Stories: na</p> <p>Plan: open, small area with scattered picnic tables and benches</p> <p>Roof: na</p> <p>Exterior: na</p> <p>Foundation: na</p> <p>Windows: na</p> <p>Details:</p>	<p>Girl Scout Park is eligible for the NRHP as a contributing element to both the NRE North Cranford HD (ID #3838) and NRE Rahway River Parkway HD (ID #4079).</p>
 <p>Photograph Number: 123</p>	<p>Panamerican Resource Number: 123</p> <p>McConnell Park</p> <p>Eastman Avenue at Central Avenue</p> <p>Municipality: Cranford, Union County</p> <p>UTM coordinates (1983 Zone 18N): 4500961N, 558376E</p> <p>Date: ca. 1919</p> <p>Other: McConnell Park (ID #4424, 025) is part of NRE Rahway River Parkway HD.</p>	<p>Style: informal, small park</p> <p>Stories: na</p> <p>Plan: open area, no paths</p> <p>Roof: na</p> <p>Exterior: na</p> <p>Foundation: na</p> <p>Windows: na</p> <p>Details: This park is located in a U-shaped bend in the Rahway River at the end of Central Avenue.</p>	<p>McConnell Park is eligible for the NRHP as a contributing element to the NRE North Cranford HD (ID #3838), NRE Rahway River Parkway HD (ID #4079), and NRE Union County Park System HD (ID #4424).</p>

NEW JERSEY OFFICE OF HISTORIC PRESERVATION HISTORIC SITES INVENTORY # 200303
INDIVIDUAL STRUCTURE SURVEY FORM

HISTORIC NAME:
LOCATION: 34 Elizabeth Ave.

COMMON NAME:
BLOCK/LOT 320/10

MUNICIPALITY: Cranford
USGS QUAD: Roselle
OWNER/ADDRESS: Roloff, John
408 Margherita Pl.
Bricktown, N.J.

COUNTY: Union
UTM REFERENCES: Zone/Northing/Easting

DESCRIPTION

Construction Date: Early 19th Century

Source of Date: Visual evidence

Architect:

Builder:

Style: Vernacular Federal

Form/Plan Type: Side hall plan

Number of Stories: 2½

Foundation: Fieldstone and cut sandstone, partly stuccoed over.

Exterior Wall Fabric: Wooden shingles.

Fenestration: Three bay facade.

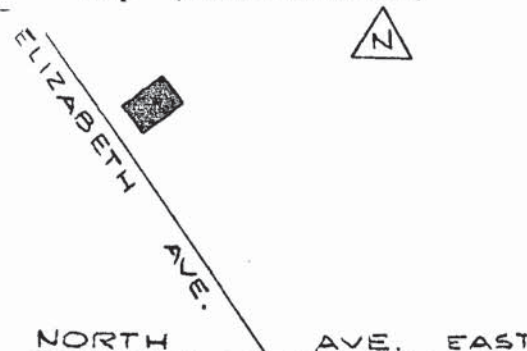
Roof/Chimneys: Gambrel roof. Chimney removed.

Additional Architectural Description:

Brick nogging in interior walls.
Rear lean-to added to house.

PHOTO Negative File # Roll 26 Frame 14, 15

Map (Indicate North)



Y DESCRIPTION, AND RELATED STRUCTURES:

House is along Elizabeth Avenue, an old road through Cranford.

SURROUNDING ENVIRONMENT: Urban ☐ Suburban ☒ Scattered Buildings ☐
Open Space ☐ Woodland ☐ Residential ☒ Agricultural ☐
Industrial ☐ Downtown Commercial ☐ Highway Commercial ☐ Other ☐

Surrounded by smaller, more modern single family houses.

SIGNIFICANCE:

The house appears on early maps of the area, but at this time, nothing is known about the builder or early inhabitants of the house.

It is the classical "Federal" style dwelling for this area. (see St. John's Parsonage, Elizabeth, 2004-29)) The gambrel roof was typical for early 19th Century buildings, as was the three bay, side hall plan. The use of cut stone on the foundation indicates an interest in style for the building. The house has been much altered inside, and many original features stripped from it. Still, federal era architecture is a rarity in Cranford, which would have been strictly an agricultural area when this was built.

ORIGINAL USE: Farmhouse

PHYSICAL CONDITION: Excellent ☐ Good ☐

REGISTER ELIGIBILITY: Yes ☐ Possible ☒

THREATS TO SITE: Roads ☐ Development ☐

No Threat ☒ Other ☐

COMMENTS:

PRESENT USE: Two family residence

Fair ☒ Poor ☐

No ☒ Part of District ☐

Zoning ☐ Deterioration ☐

REFERENCES:

Cranford Historical Society

RECORDED BY: JWF
ORGANIZATION: UCCHPAB

DATE: 1981



CRANFORD
Union Co.



310
* 200303
r. 26/14

34 Elizabeth Ave.
looking N.



1901

Inventory of Identified Architectural Resources

Property	Property Information	Description	NRHP Eligibility
 <p>Photograph Number: 98</p>	<p>Panamerican Resource Number: 98</p> <p>12 Forest Street</p> <p>Municipality: Cranford, Union County</p> <p>UTM coordinates (1983 Zone 18N): 4501082N, 559100E</p> <p>Date: 1915</p> <p>Other: This house backs up to the Rahway River.</p>	<p>Style: none</p> <p>Stories: 2</p> <p>Plan: rectangular</p> <p>Roof: gable</p> <p>Exterior: rusticated concrete block and vinyl siding</p> <p>Foundation: obscured</p> <p>Windows: metal with decorative shutters</p> <p>Details: This house was initially some type of a single story outbuilding along the river. The main building along the road is now gone. The outbuilding had a second story added to it, and a porch that was later enclosed. A real adaptive reuse.</p>	<p>Although extremely altered, this building is considered eligible as a contributing element to the NRE North Cranford HD (ID #3838; Bary 2012).</p>
 <p>Photograph Number: 99</p>	<p>Panamerican Resource Number: 99</p> <p>Garage</p> <p>18 Forest Street</p> <p>Municipality: Cranford, Union County</p> <p>UTM coordinates (1983 Zone 18N): 4501026N, 559150E</p> <p>Date: ca. 1930</p> <p>Other: This garage backs up to the Rahway River.</p>	<p>Style: shingled to match the house, single-bay garage</p> <p>Stories: 1</p> <p>Plan: rectangular</p> <p>Roof: gable</p> <p>Exterior: wood shakes, vinyl trim and door</p> <p>Foundation: obscured</p> <p>Windows: none</p> <p>Details: This small garage appears to be used for storage and not for a car.</p>	<p>The garage is a non-contributing element to the NRE North Cranford HD and is not eligible for the NRHP. The house at 18 Forest Street (1860), however, is eligible as a contributing element to the NRE North Cranford HD (ID #3838; Bary 2012).</p>

Inventory of Identified Architectural Resources			
Property	Property Information	Description	NRHP Eligibility
 <p>Photograph Number: 41</p>	<p>Panamerican Resource Number: 41</p> <p>Garage and Pool House</p> <p>10 Hampton Court</p> <p>Municipality: Cranford, Union County</p> <p>UTM coordinates (1983 Zone 18N): 4501498N, 558478E</p> <p>Date: ca. 1950</p> <p>Other: Both back onto the Rahway River Parkway</p>	<p>Style: Colonial Revival garage, vernacular pool house</p> <p>Stories: 1 both</p> <p>Plan: rectangular both</p> <p>Roof: front gable—garage; flat—pool house</p> <p>Exterior: brick—garage; vinyl siding—pool house</p> <p>Foundation: obscured both</p> <p>Windows: fixed, decorative light over garage door; garage door has upper row of panes; pool house has large single fixed sheets of glass overlooking the river</p> <p>Details: the garage detailing matches the house</p>	<p>Not Eligible, does not meet the NRHP criteria for eligibility</p>
 <p>Photograph Number: 42</p>	<p>Panamerican Resource Number: 42</p> <p>10 Hampton Road</p> <p>Municipality: Cranford, Union County</p> <p>UTM coordinates (1983 Zone 18N): 4501487N, 558464E</p> <p>Date: ca. 1930</p> <p>Other: Has pool house and garage (see Panamerican Resource 41)</p>	<p>Style: Colonial Revival</p> <p>Stories: 2</p> <p>Plan: rectangular</p> <p>Roof: gable with slate</p> <p>Exterior: brick</p> <p>Foundation: obscured</p> <p>Windows: 6/6 wood covered with storm windows; operational shutters, fan and sidelights around front door; key stones on first story window surrounds</p> <p>Details: highly intact Colonial Revival house with historically detailed front porch, enclosed side porch</p>	<p>Not Eligible, does not meet the NRHP criteria for eligibility</p>

Inventory of Identified Architectural Resources

Property	Property Information	Description	NRHP Eligibility
 Photograph Number: 39	Panamerican Resource Number: 39 Garage for 12 Hampton Road Municipality: Cranford, Union County UTM coordinates (1983 Zone 18N): 4501535N, 558469E Date: 1920 Other: Backs onto the Rahway River Parkway	Style: Italian Renaissance Stories: 2 Plan: rectangular Roof: pyramidal, barrel tile Exterior: stucco Foundation: obscured Windows: none Details: matches the house	It is not clear why Hampton Road north of Springfield Avenue is not part of the NRE North Cranford HD. This part of the street should be considered as part of an expansion of the HD with this structure as a potential contributing element.
 Photograph Number: 40	Panamerican Resource Number: 40 12 Hampton Road Municipality: Cranford, Union County UTM coordinates (1983 Zone 18N): 4501521N, 558462E Date: ca. 1920 Other: House backs onto the Rahway River Parkway (see Panamerican Resource 39).	Style: Italian Renaissance Stories: 2 Plan: rectangular Roof: hipped, green barrel tiles Exterior: stucco Foundation: obscured Windows: multiplicity of original window types covered with storm windows Details: This is an exceptionally intact house with original windows, lights, roofing material, and sun porch.	It is not clear why Hampton Road north of Springfield Avenue is not part of the NRE North Cranford HD. This part of the street should be considered as part of an expansion of the HD with this structure as a potential contributing element.

Page 1

Historic Sites #:

Photograph:



Other Designation Date:

Organization: Panamerican Consultants, Inc.

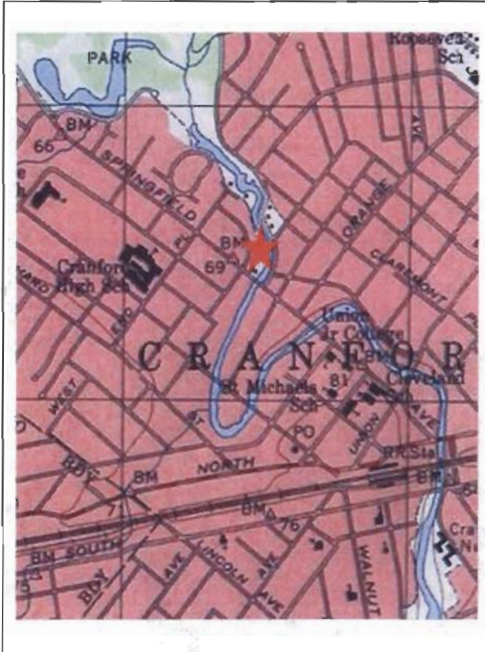
New Jersey Department of Environmental Protection
Historic Preservation Office

Page 2

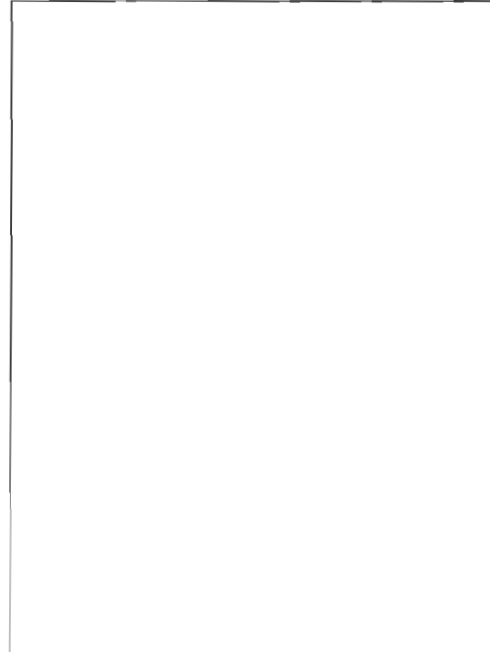
BASE FORM

Historic Sites #:

Location Map:



Site Map:



Bibliography/Sources:

Additional Information:

More Research Needed? ☒ Yes ☐ No

INTENSIVE LEVEL USE ONLY

Attachments Included: ☐ Building ☐ Structure ☐ Object ☐ Bridge
☐ Landscape ☐ Industry

Within Historic District? ☐ Yes ☐ No

Status: ☐ Key-Contributing ☐ Contributing ☐ Non-Contributing

Associated Archaeological Site/Deposit? ☐ Yes
(Known or potential Sites – if yes, please describe briefly)

New Jersey Department of Environmental Protection
Historic Preservation Office

Page 1

BASE FORM

Historic Sites #:

Property Name: 12 Hampton Road, Garage

Street Address: Street #: 12 Apartment #: _____
(Low) (High) (Low) (High)

Prefix: _____ Street Name: Hampton Road Suffix: _____ Type: RD

County(s): Union **Zip Code:** 07016-1807

Municipality(s): Cranford **Block(s):** _____

Local Place Name(s): _____ **Lot(s):** _____

Ownership: _____ **USGS Quad(s):** Roselle 1981

Photograph:



Description: Circa 1920 Italian Renaissance style, 1 story, rectangular plan. Pyramidal roof, green barrel tiles. Stucco exterior, matches the house. Backs up to the Rahway River Parkway Historic District. It is not clear why Hampton Road north of Springfield Avenue is not part of the NRE North Cranford HD. This part of the street should be considered as part of an expansion of the HD with this structure as a potential contributing element.

Registration and Status Dates:

National Historic Landmark: _____

SHPO Opinion: _____

National Register: _____

Local Designation: _____

New Jersey Register: _____

Other Designation: _____

Determination of Eligibility: _____

Other Designation Date: _____

Survey Name: Phase IA Cultural Resources Investigation of the Rahway River Flood Risk Management and Ecosystem Restoration Project, Westfield, Townships of Springfield, Cranford, and Union, and Borough of Kenilworth, Union County, New Jersey

Date: May 13, 2013

Surveyor: Nolte, Kelly, Donald Smith, Mark Steinback, and Michael Cinquino.

Organization: PanamERICAN Consultants, Inc.

A-84

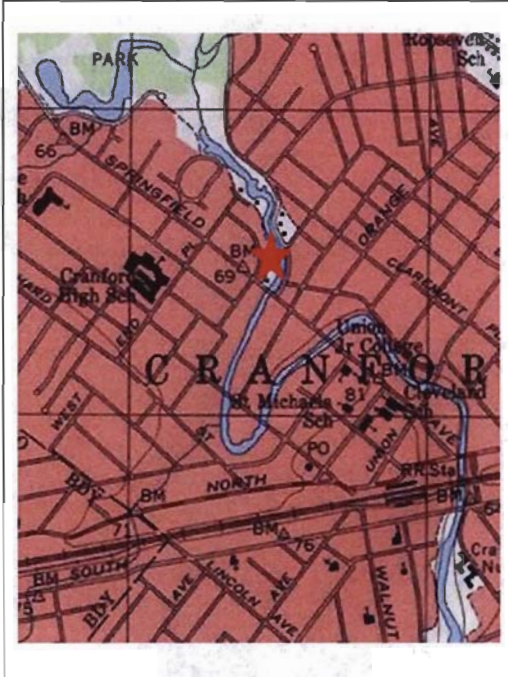
New Jersey Department of Environmental Protection
Historic Preservation Office

Page 2

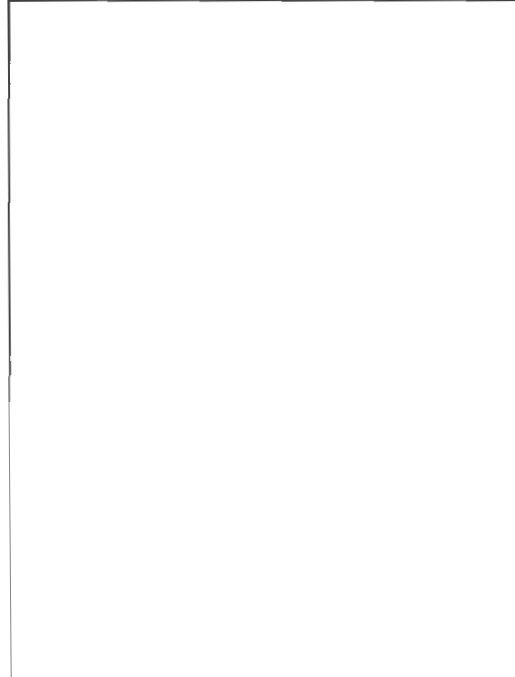
BASE FORM

Historic Sites #:

Location Map:



Site Map:



Bibliography/Sources:

Additional Information:

More Research Needed? ☒ Yes ☐ No



INTENSIVE LEVEL USE ONLY



Attachments Included: ☐ Building ☐ Structure ☐ Object ☐ Bridge
☐ Landscape ☐ Industry

Within Historic District? ☐ Yes ☐ No



Status: ☐ Key-Contributing ☐ Contributing ☐ Non-Contributing

Associated Archaeological Site/Deposit? ☐ Yes
(Known or potential Sites – if yes, please describe briefly)

Inventory of Identified Architectural Resources			
Property	Property Information	Description	NRHP Eligibility
 <p>Photograph Number: 37</p>	<p>Panamerican Resource Number: 37</p> <p>18 Hampton Road</p> <p>Municipality: Cranford, Union County</p> <p>UTM coordinates (1983 Zone 18N): 4501577N, 558443E</p> <p>Date: ca. 1960</p> <p>Other: House backs onto the Rahway River Parkway</p>	<p>Style: Minimal Traditional</p> <p>Stories: 2</p> <p>Plan: rectangular</p> <p>Roof: gable, shed</p> <p>Exterior: vinyl siding</p> <p>Foundation: obscured</p> <p>Windows: vinyl replacements, bay window</p> <p>Details: This house has been much altered and added onto. The porch has been enclosed and elongated to form a garage. The bay window has simply replaced an existing older window.</p>	<p>Not Eligible, does not meet the NRHP criteria for eligibility</p>
 <p>Photograph Number: 38</p>	<p>Panamerican Resource Number: 38</p> <p>14 Hampton Road</p> <p>Municipality: Cranford, Union County</p> <p>UTM coordinates (1983 Zone 18N): 4501554N, 558453E</p> <p>Date: ca. 1950</p> <p>Other: House backs onto the Rahway River Parkway</p>	<p>Style: Minimal Traditional with Colonial elements</p> <p>Stories: 2</p> <p>Plan: rectangular</p> <p>Roof: gable/shed</p> <p>Exterior: 1st story brick, second story wood shakes</p> <p>Foundation: obscured</p> <p>Windows: 6/6 sash original, 6 pane casement, back windows, front door sidelights</p> <p>Details: Nicely maintained house with large side screened porch</p>	<p>Not Eligible, does not meet the NRHP criteria for eligibility</p>

Inventory of Identified Architectural Resources			
Property	Property Information	Description	NRHP Eligibility
 <p>Photograph Number: 37</p>	<p>Panamerican Resource Number: 37</p> <p>18 Hampton Road</p> <p>Municipality: Cranford, Union County</p> <p>UTM coordinates (1983 Zone 18N): 4501577N, 558443E</p> <p>Date: ca. 1960</p> <p>Other: House backs onto the Rahway River Parkway</p>	<p>Style: Minimal Traditional</p> <p>Stories: 2</p> <p>Plan: rectangular</p> <p>Roof: gable, shed</p> <p>Exterior: vinyl siding</p> <p>Foundation: obscured</p> <p>Windows: vinyl replacements, bay window</p> <p>Details: This house has been much altered and added onto. The porch has been enclosed and elongated to form a garage. The bay window has simply replaced an existing older window.</p>	<p>Not Eligible, does not meet the NRHP criteria for eligibility</p>
 <p>Photograph Number: 38</p>	<p>Panamerican Resource Number: 38</p> <p>14 Hampton Road</p> <p>Municipality: Cranford, Union County</p> <p>UTM coordinates (1983 Zone 18N): 4501554N, 558453E</p> <p>Date: ca. 1950</p> <p>Other: House backs onto the Rahway River Parkway</p>	<p>Style: Minimal Traditional with Colonial elements</p> <p>Stories: 2</p> <p>Plan: rectangular</p> <p>Roof: gable/shed</p> <p>Exterior: 1st story brick, second story wood shakes</p> <p>Foundation: obscured</p> <p>Windows: 6/6 sash original, 6 pane casement, back windows, front door sidelights</p> <p>Details: Nicely maintained house with large side screened porch</p>	<p>Not Eligible, does not meet the NRHP criteria for eligibility</p>

Inventory of Identified Architectural Resources

Property	Property Information	Description	NRHP Eligibility
 <p>Photograph Number: 35</p>	<p>Panamerican Resource Number: 35</p> <p>2 Crescent Place</p> <p>Municipality: Cranford, Union County</p> <p>UTM coordinates (1983 Zone 18N): 4501650N, 558428E</p> <p>Date: ca. 1960</p> <p>Other: House backs onto Rahway River Parkway</p>	<p>Style: Minimal Traditional with Colonial Revival elements</p> <p>Stories: 2</p> <p>Plan: rectangular</p> <p>Roof: gable</p> <p>Exterior: brick and shakes</p> <p>Foundation: obscured</p> <p>Windows: 6/6 original sash and original bay window, decorative shutters</p> <p>Details: a side garage has been added to the house</p>	<p>Not Eligible, does not meet the NRHP criteria for eligibility</p>
 <p>Photograph Number: 36</p>	<p>Panamerican Resource Number: 36</p> <p>20 Hampton Road</p> <p>Municipality: Cranford, Union County</p> <p>UTM coordinates (1983 Zone 18N): 4501599N, 558425E</p> <p>Date: ca. 1920</p> <p>Other: House backs onto Rahway River Parkway</p>	<p>Style: Eclectic mix</p> <p>Stories: 2.5</p> <p>Plan: rectangular</p> <p>Roof: hipped</p> <p>Exterior: concrete</p> <p>Foundation: obscured</p> <p>Windows: original lozenge upper panel/ single lower pane front facade windows, highly decorative fan and side lights on front door</p> <p>Details: This is a highly intact, poured concrete house.</p>	<p>It is not clear why Hampton Road north of Springfield Avenue is not part of the NRE North Cranford HD. This part of the street should be considered as part of an expansion of the HD with this building as a potential contributing element.</p>

New Jersey Department of Environmental Protection
Historic Preservation Office

Page 1

BASE FORM

Historic Sites #:

Property Name: 20 Hampton Road

Street Address: Street #: 20 (Low) (High) Apartment #: _____ (Low) (High)

Prefix: _____ Street Name: Hampton Road Suffix: _____ Type: RD

County(s): Union Zip Code: 07016-1807

Municipality(s): Cranford Block(s): _____

Local Place Name(s): _____ Lot(s): _____

Ownership: _____ USGS Quad(s) Roselle 1981

Photograph:



Description: Eclectic mix, 2.5 story, rectangular plan house. Hipped roof, concrete exterior, highly intact poured concrete single residence. Windows are original lozenge, upper pane/single lower pane front façade windows. Highly decorative fan and side lights on front door. It is not clear why Hampton Road north of Springfield Avenue is not part of the NRE North Cranford HD. This part of the street should be considered as part of an expansion of the HD with this building as a **potential contributing element**.

Registration and Status Dates:

National Historic Landmark: _____

SHPO Opinion: _____

National Register: _____

Local Designation: _____

New Jersey Register: _____

Other Designation: _____

Determination of Eligibility: _____

Other Designation Date: _____

Phase IA Cultural Resources Investigation of the Rahway River Flood Risk Management and Ecosystem Restoration Project, Westfield, Townships of Springfield, Cranford, and Union, and Borough of Kenilworth, Union County, New Jersey

Survey Name: _____

Date: May 13, 2013

Surveyor: Nolte, Kelly, Donald Smith, Mark Steinback, and Michael Cinquino.

A-89

Organization: Panamerican Consultants, Inc.

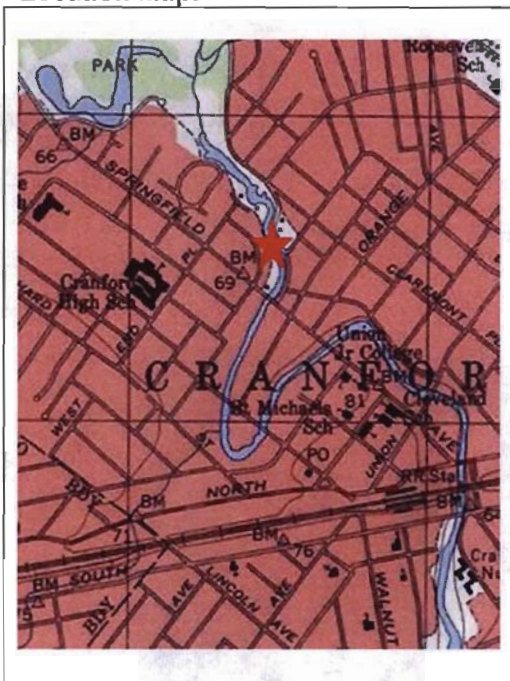
New Jersey Department of Environmental Protection
Historic Preservation Office

Page 2

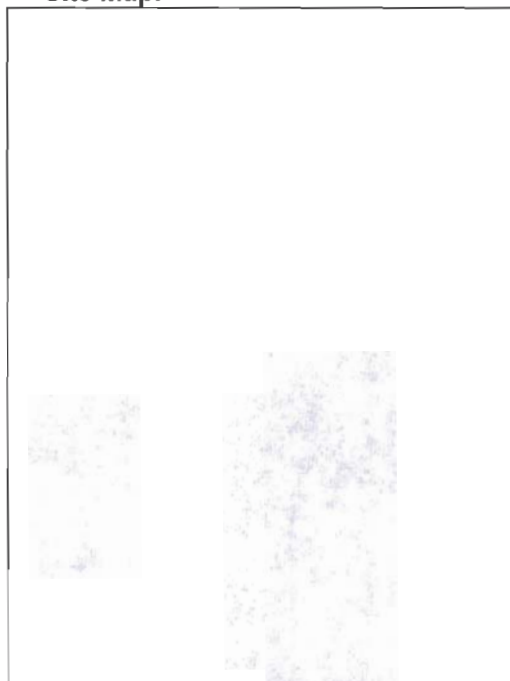
BASE FORM

Historic Sites #:

Location Map:



Site Map:



Bibliography/Sources:

Additional Information:

More Research Needed? ☒ Yes ☐ No



INTENSIVE LEVEL USE ONLY



Attachments Included: ☐ Building ☐ Structure ☐ Object ☐ Bridge
☐ Landscape ☐ Industry

Within Historic District? ☐ Yes ☐ No

Status: ☐ Key-Contributing ☐ Contributing ☐ Non-Contributing

Associated Archaeological Site/Deposit? ☐ Yes
(Known or potential Sites – if yes, please describe briefly)

Inventory of Identified Architectural Resources			
Property	Property Information	Description	NRHP Eligibility
 Photograph Number: 47	Panamerican Resource Number: 47 Garage 204 Hampton Street Municipality: Cranford, Union County UTM coordinates (1983 Zone 18N): 4501296N, 558387E Date: ca. 1920 Other: Backs onto the edge of the Rahway River	Style: Vernacular Stories: 1 Plan: rectangular Roof: hipped Exterior: wood Foundation: obscured Windows: obscured, row of lights in garage door Details: This is a typical small garage added to 204 Hampton Street after automobiles became popular.	Eligible as a contributing element to the North Cranford Historic District (ID #3838) (Bary 2012)
 Photograph Number: 48 (east wall)	Panamerican Resource Number: 48 Culvert crossing Rahway River at Hampton Street Municipality: Cranford, Union County UTM coordinates (1983 Zone 18N): 4501195N, 558307E Date: 1980 Other: Union County Bridge STR. No. CR24; photograph taken of culvert bridge since it's directly in the 100 feet APE	Style: rustic Stories: na Plan: rough stacked and mortared balustrades Roof: na Exterior: rustic cut Foundation: na Windows: na Details: Bridge plaque indicates the bridge was constructed by Union County in 1980. An attempt was made to make the "bridge" blend into the neighborhood by using a rusticated look.	Eligible as a contributing element to the NRE North Cranford HD (ID #3838), and as a contributing resource to the Rahway River Parkway HD (ID #4079; Tingey 2002).



Inventory of Identified Architectural Resources			
Property	Property Information	Description	NRHP Eligibility
 <p>Photograph Number: 49</p>	<p>Panamerican Resource Number: 49</p> <p>Hampton Park</p> <p>Hampton Street at Eastman Avenue</p> <p>Municipality: Cranford, Union County</p> <p>UTM coordinates (1983 Zone 18N): 4501139N, 558311E</p> <p>Date: after 1969</p> <p>Other: Cranford municipal park; sits at the edge of the Rahway River</p>	<p>Style: informal</p> <p>Stories: na</p> <p>Plan: informal with a few walking paths</p> <p>Roof: na</p> <p>Exterior: na</p> <p>Foundation: na</p> <p>Windows: na</p> <p>Details: This is a vest pocket park on the edge of the County McConnell Park.</p>	<p>Eligible as Contributing element to the NRE North Cranford HD (ID #3838) and contributing to the Rahway River Parkway HD (ID #4079)</p>
 <p>Photograph Number: 50 (southbound balustrade)</p>	<p>Panamerican Resource Number: 50</p> <p>Eastman Street Bridge (NJDOT #2003008) at Hampton Street crossing Rahway River</p> <p>Municipality: Cranford, Union County</p> <p>UTM coordinates (1983 Zone 18N): 4501070N, 558329E</p> <p>Date: 2004</p> <p>Other: Lies directly in the 100 foot APE; the bridge is owned by Union County.</p>	<p>Style: rustic w/ classical detailing</p> <p>Stories: na</p> <p>Plan: single arch</p> <p>Roof: na</p> <p>Exterior: balustrades with lower rubble course and upper open work metal railings</p> <p>Foundation: concrete abutments</p> <p>Windows: na</p> <p>Details: The bridge was constructed on the site of an earlier bridge. Its rustic styling reflects the neighborhood and it blends with McConnell Park, which is immediately to the southeast. According to plaque, designed by Schoor Depalma, Inc., and constructed by Marbro, Inc.</p>	<p>Although this bridge is not yet 50 years old, it is located directly within the NRE Rahway River Parkway HD (ID #4079), NRE Union County Park System HD (ID #4424), and NRE North Cranford HD (ID #3838), and should be considered a contributing element to all of the HD.</p>

Panamerican Consultants, Inc.



6-26



Rahway River Phase 1A



Inventory of Identified Architectural Resources

Property	Property Information	Description	NRHP Eligibility
 <p>Photograph Number: 51</p>	<p>Panamerican Resource Number: 51</p> <p>108 Hampton Street</p> <p>Municipality: Cranford, Union County</p> <p>UTM coordinates (1983 Zone 18N): 4501211N, 558343E</p> <p>Date: 1952</p> <p>Other: House backs up to the Rahway River</p>	<p>Style: Traditional Minimal</p> <p>Stories: 1.5</p> <p>Plan: rectangular</p> <p>Roof: gable</p> <p>Exterior: brick</p> <p>Foundation: concrete</p> <p>Windows: a number of different original wooden types including "Picture" window, all covered by storm windows, decorative shutters, window side lights have been replaced with glass block</p> <p>Details: This is a small, unassuming brick "rancher" stuck on a lot between large late Victorian and early twentieth-century houses.</p>	<p>Not eligible, does not meet the NRHP criteria, and is non-contributing to the NRE North Cranford HD (Bary 2012).</p>
 <p>Photograph Number: 52</p>	<p>Panamerican Resource Number: 52</p> <p>Garage</p> <p>3 Central Avenue</p> <p>Municipality: Cranford, Union County</p> <p>UTM coordinates (1983 Zone 18N): 4501085N, 558352E</p> <p>Date: ca. 1930</p> <p>Other: Garage backs up to the Rahway River</p>	<p>Style: matches the house (Dutch Colonial); 2-bay garage</p> <p>Stories: 1</p> <p>Plan: rectangular</p> <p>Roof: gambrel, composite shingle</p> <p>Exterior: wood siding</p> <p>Foundation: obscured</p> <p>Windows: na</p> <p>Details: garage doors are replacements</p>	<p>The house and its outbuilding, the two-bay garage, at 3 Central Avenue are eligible as contributing elements to the NRE North Cranford HD (ID #3838; Bary 2012).</p>

Inventory of Identified Architectural Resources

Property	Property Information	Description	NRHP Eligibility
 <p>Photograph Number: 47</p>	<p>Panamerican Resource Number: 47</p> <p>Garage</p> <p>204 Hampton Street</p> <p>Municipality: Cranford, Union County</p> <p>UTM coordinates (1983 Zone 18N): 4501296N, 558387E</p> <p>Date: ca. 1920</p> <p>Other: Backs onto the edge of the Rahway River</p>	<p>Style: Vernacular</p> <p>Stories: 1</p> <p>Plan: rectangular</p> <p>Roof: hipped</p> <p>Exterior: wood</p> <p>Foundation: obscured</p> <p>Windows: obscured, row of lights in garage door</p> <p>Details: This is a typical small garage added to 204 Hampton Street after automobiles became popular.</p>	<p>Eligible as a contributing element to the North Cranford Historic District (ID #3838) (Bary 2012)</p>
 <p>Photograph Number: 48 (east wall)</p>	<p>Panamerican Resource Number: 48</p> <p>Culvert crossing Rahway River at Hampton Street</p> <p>Municipality: Cranford, Union County</p> <p>UTM coordinates (1983 Zone 18N): 4501195N, 558307E</p> <p>Date: 1980</p> <p>Other: Union County Bridge STR. No. CR24; photograph taken of culvert bridge since it's directly in the 100 feet APE</p>	<p>Style: rustic</p> <p>Stories: na</p> <p>Plan: rough stacked and mortared balustrades</p> <p>Roof: na</p> <p>Exterior: rustic cut</p> <p>Foundation: na</p> <p>Windows: na</p> <p>Details: Bridge plaque indicates the bridge was constructed by Union County in 1980. An attempt was made to make the "bridge" blend into the neighborhood by using a rusticated look.</p>	<p>Eligible as a contributing element to the NRE North Cranford HD (ID #3838), and as a contributing resource to the Rahway River Parkway HD (ID #4079; Tingey 2002).</p>

Inventory of Identified Architectural Resources			
Property	Property Information	Description	NRHP Eligibility
 <p>Photograph Number: 45</p>	<p>Panamerican Resource Number: 45</p> <p>20 Hampton Street</p> <p>Municipality: Cranford, Union County</p> <p>UTM coordinates (1983 Zone 18N): 4501363N, 558404E</p> <p>Date: ca. 1935</p> <p>Other: Right on the edge of the Rahway River</p>	<p>Style: Dutch Colonial</p> <p>Stories: 2.5</p> <p>Plan: rectangular</p> <p>Roof: gambrel w/expanded sides</p> <p>Exterior: aluminum siding</p> <p>Foundation: concrete block</p> <p>Windows: 6/1 with storm windows, decorative shutters</p> <p>Details: very simple Dutch Colonial turned sideways on the lot</p>	<p>Not eligible, does not meet NRHP criteria</p>
 <p>Photograph Number: 46</p>	<p>Panamerican Resource Number: 46</p> <p>208 Hampton Street</p> <p>Municipality: Cranford, Union County</p> <p>UTM coordinates (1983 Zone 18N): 4501347N, 558390E</p> <p>Date: 1914</p> <p>Other: Backs onto the edge of the Rahway River</p>	<p>Style: Dutch Colonial</p> <p>Stories: 2</p> <p>Plan: rectangular</p> <p>Roof: gambrel with expanded sides</p> <p>Exterior: wooden shingles</p> <p>Foundation: obscured</p> <p>Windows: 1/1 with storm windows, operational shutters, front extended window and triple decorative window w/ original window box</p> <p>Details: Highly intact Dutch Colonial with multiple special decorative touches not normally seen on this style such as the small triple window with window box and an extended front window</p>	<p>Eligible as a contributing element to the NRE North Cranford HD (ID #3838) (Bary 2012)</p>

Inventory of Identified Architectural Resources			
Property	Property Information	Description	NRHP Eligibility
 <p>Photograph Number: 45</p>	<p>Panamerican Resource Number: 45</p> <p>20 Hampton Street</p> <p>Municipality: Cranford, Union County</p> <p>UTM coordinates (1983 Zone 18N): 4501363N, 558404E</p> <p>Date: ca. 1935</p> <p>Other: Right on the edge of the Rahway River</p>	<p>Style: Dutch Colonial</p> <p>Stories: 2.5</p> <p>Plan: rectangular</p> <p>Roof: gambrel w/expanded sides</p> <p>Exterior: aluminum siding</p> <p>Foundation: concrete block</p> <p>Windows: 6/1 with storm windows, decorative shutters</p> <p>Details: very simple Dutch Colonial turned sideways on the lot</p>	<p>Not eligible, does not meet NRHP criteria</p>
 <p>Photograph Number: 46</p>	<p>Panamerican Resource Number: 46</p> <p>208 Hampton Street</p> <p>Municipality: Cranford, Union County</p> <p>UTM coordinates (1983 Zone 18N): 4501347N, 558390E</p> <p>Date: 1914</p> <p>Other: Backs onto the edge of the Rahway River</p>	<p>Style: Dutch Colonial</p> <p>Stories: 2</p> <p>Plan: rectangular</p> <p>Roof: gambrel with expanded sides</p> <p>Exterior: wooden shingles</p> <p>Foundation: obscured</p> <p>Windows: 1/1 with storm windows, operational shutters, front extended window and triple decorative window w/ original window box</p> <p>Details: Highly intact Dutch Colonial with multiple special decorative touches not normally seen on this style such as the small triple window with window box and an extended front window</p>	<p>Eligible as a contributing element to the NRE North Cranford HD (ID #3838) (Bary 2012)</p>

Union County Parks Survey

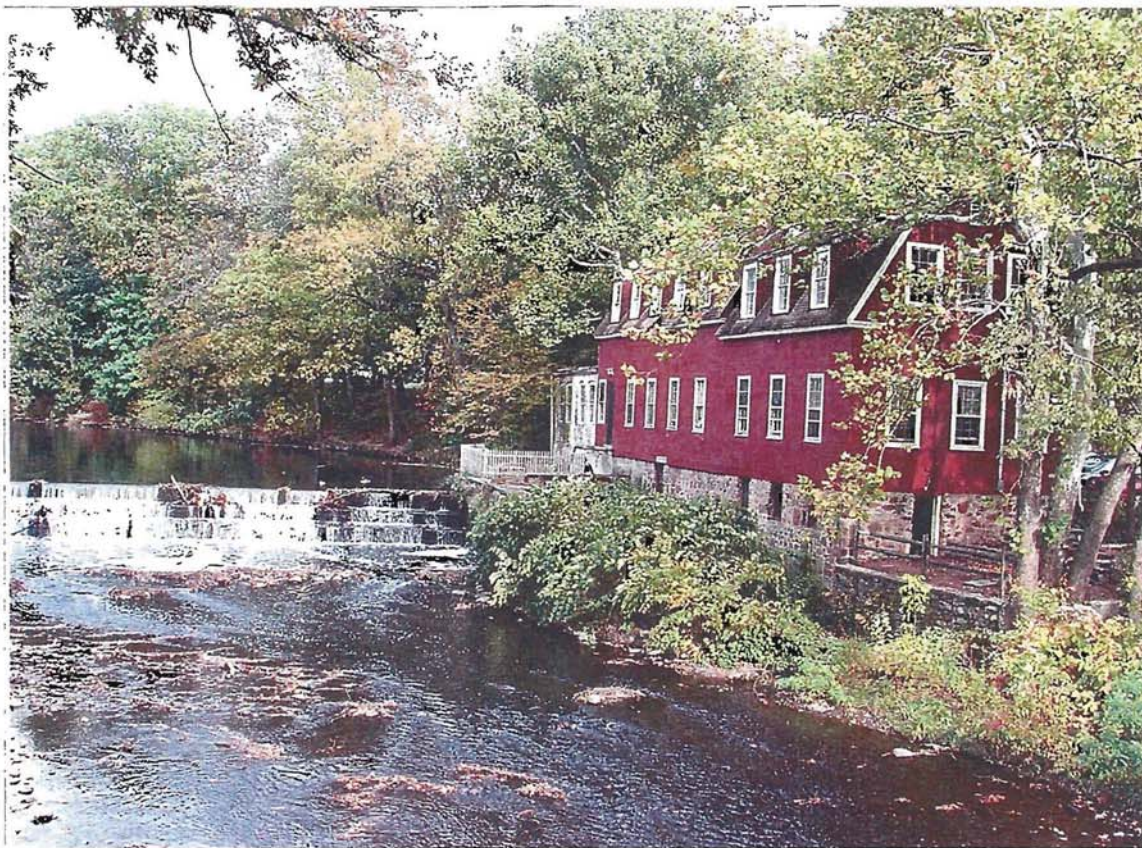
Park Name

Rahway River Parkway: Driescher's Mill Park

Township/City

Cranford

photo



Type

☐ recreational ☐ pastoral ☐ picturesque ☐ natural ☐ scenic ☐ other:

Existing Topography

☐ flat ☐ sloped ☐ hilly ☐ mountainous ☐ other:

Spatial Relationships

☐ symmetrical ☐ asymmetrical ☐ axial

Circulation

trails

dirt

cycling paths

bridle paths

parking lots

handicapped ramps

other:

Parkways

Border roads

Internal park roads

Vernacular roads

Summit roads

Connecting pleasure roads

River parkways

Estate roads

Ocean parkways

Other:

Surveyor

Gregory Dietrich

Date

10/21/03

Union County Parks Survey

Drainage & Engineering

<input type="checkbox"/> Bridge (road)	(See bridge info)
<input type="checkbox"/> Bridge (pedestrian)	
<input type="checkbox"/> Channel	
<input type="checkbox"/> Culvert	
<input type="checkbox"/> Spillway	stone aggregate w/ crank shafts
<input type="checkbox"/> Millrace	
<input type="checkbox"/> Retaining wall	
<input type="checkbox"/> Storm sewer	
<input type="checkbox"/> Swale	
<input type="checkbox"/> Other:	rubble-stone
bridge entry remnant	

Bridge

carries	bridge date	info source	bridge type/design	bridge material
South Avenue	c. 1985	Visual evidence	stringer encased	steel & concrete w/ steel railings
Lincoln Avenue	c. 2003	Visual evidence	stringer encased	steel & concrete w/ rubble-face veneer and metal railings

Land Uses

<input type="checkbox"/> archery	<input type="checkbox"/> skating--sidewalk
<input type="checkbox"/> baseball	<input type="checkbox"/> soccer
<input type="checkbox"/> basketball	<input type="checkbox"/> softball
<input type="checkbox"/> boating/canoeing	<input type="checkbox"/> swimming
<input type="checkbox"/> biking	<input type="checkbox"/> tennis
<input type="checkbox"/> camping	<input type="checkbox"/> theater/music
<input type="checkbox"/> concession/dining	<input type="checkbox"/> track & field
<input type="checkbox"/> cricket	<input type="checkbox"/> trap/skeet shooting
<input type="checkbox"/> driving	<input type="checkbox"/> trail
<input type="checkbox"/> fishing	<input type="checkbox"/> wading
<input type="checkbox"/> football	<input type="checkbox"/> other:
<input type="checkbox"/> golf	
<input type="checkbox"/> gymnastics	
<input type="checkbox"/> handball court	
<input type="checkbox"/> hockey	
<input type="checkbox"/> horseback riding	
<input type="checkbox"/> horticulture/gardening	
<input type="checkbox"/> lawn bowling	
<input type="checkbox"/> model boating	
<input type="checkbox"/> picnicking	
<input type="checkbox"/> playground	
<input type="checkbox"/> shuffleboard	
<input type="checkbox"/> skating--rink	

Landscape Dividers

Formal entrance into Lincoln Parkway at South Avenue, consisting of pairs of rubble-stone piers, and curved rubble-stone walls

Water Elements

<input type="checkbox"/> rivers	<input type="checkbox"/> creeks	<input type="checkbox"/> pools
<input type="checkbox"/> lakes	<input type="checkbox"/> ponds	<input type="checkbox"/> fountains
<input type="checkbox"/> brooks	<input type="checkbox"/> waterfalls	<input type="checkbox"/> other :

Buildings

<input type="checkbox"/> maintenance	<input type="checkbox"/> stable	<input type="checkbox"/> other:
<input type="checkbox"/> recreational	<input type="checkbox"/> house	
<input type="checkbox"/> concession	<input type="checkbox"/> rest rooms	
<input type="checkbox"/> picnic shelter	<input type="checkbox"/> mill	
<input type="checkbox"/> gazebo	<input type="checkbox"/> museum/visitor center	
<input type="checkbox"/> bandstand	<input type="checkbox"/> public utility	

Landscape Furniture

<input type="checkbox"/> benches	<input type="checkbox"/> planters
<input type="checkbox"/> bleachers	<input type="checkbox"/> trash cans
<input type="checkbox"/> bike racks	<input type="checkbox"/> other:
<input type="checkbox"/> bollards	
<input type="checkbox"/> drinking fountains	
<input type="checkbox"/> fitness trail stations	
<input type="checkbox"/> grills	
<input type="checkbox"/> lighting fixtures	
<input type="checkbox"/> picnic tables	

Surveyor Gregory Dietrich

Date 10/21/03

Union County Parks Survey

Description Droescher's Mill Park is a picturesque park occupying a segment of the Rahway River Parkway between South and Lincoln avenues. It features dirt trails on both sides, as well as a mini-river parkway known as Lincoln Avenue running along its west side. A formal entrance, comprised of rubble-stone piers and walls, demarcates the park's entry at South Avenue. The area along the river contains mature trees and wild vegetation. Remnants of piers that once supported Droescher's Mill Bridge (1875) are located at the river's edge about midway along the park trail. Droescher's Mill and an early-20th-century spillway are located within the park's southern portion, just north of the Lincoln Avenue Bridge.

Setting Droescher's Mill Park is bordered by South Avenue to the north, Lincoln Park East to the east, Lincoln Avenue to the south, and Blake and Walnut avenues to the west. The Sperry Park section of the Rahway River Parkway is located north of South Avenue. 2-story multi-family dwellings (c.1945) are located along Lincoln Park East. 1 - 1½-story single-family dwellings (c.1960) are located along Lincoln and Blake avenues.

Surveyor Gregory Dietrich
Date 10/21/03

Union County Parks Survey

Markers/
uments/
sculpture

Signage

metal

Utilities

Plantings

mature oaks and sweetgums

CRM issues

NJ-HPO ID No. 4079: Rahway River Parkway Historic District; Survey Form reference number (40)
NJ-HPO ID No. 2653: Droescher's Mill; Survey Form reference number (43)
NJ-HPO ID No. 2654: Droescher's Mill Bridge (Demolished); Survey Form reference number (37)
NJ-HPO ID No. 4053: Central Railroad of NJ Bridge over Rahway River; Survey Form reference number (35)

Character Defining
Features/Resources

Rahway River
Entrance at South Avenue and Lincoln
Parkway
Configuration of trails thru park
Droescher's Mill
Spillway

Contributing Features

Wooded areas flanking the Rahway River

Non-Contributing Features

Surveyor Gregory Dietrich
Date 10/21/03

Union County Parks Survey

Contributing Resources

Droescher's Mill bridge remnants

Non-Contributing Resources

c.1985 bridge carrying South Avenue
c.2003 bridge carrying Lincoln Avenue

Boundary of
eligible portion
of park

All of the property contained within Block 200, Lot 32; Block 261, Lot 8;
Block 262, Lot 1; Block 263, Lot 1; Block 435, Lot 1; Block 481, Lot 1; Block 482 Lot 1; and Block 483,
Lot 1 of the Township of Cranford, Union County, New Jersey.

Eligible

☐ yes
☐ no

Rating

☐ Key Contributing ☐ Contributing ☐ Non-Contributing

Areas of Significance

<input type="checkbox"/> Agriculture	<input type="checkbox"/> Economics	<input type="checkbox"/> Industry	<input type="checkbox"/> Performing Arts
<input type="checkbox"/> Architecture	<input type="checkbox"/> Education	<input type="checkbox"/> Invention	<input type="checkbox"/> Philosophy
<input type="checkbox"/> Art	<input type="checkbox"/> Engineering	<input type="checkbox"/> Landscape Architecture	<input type="checkbox"/> Politics
<input type="checkbox"/> Commerce	<input type="checkbox"/> Entertainment/Recreation	<input type="checkbox"/> Law	<input type="checkbox"/> Religion
<input type="checkbox"/> Communications	<input type="checkbox"/> Ethnic Heritage	<input type="checkbox"/> Literature	<input type="checkbox"/> Science
<input type="checkbox"/> Community Planning	<input type="checkbox"/> Exploration/Settlement	<input type="checkbox"/> Maritime History	<input type="checkbox"/> Social History
<input type="checkbox"/> Conservation	<input type="checkbox"/> Health/Medicine	<input type="checkbox"/> Military	<input type="checkbox"/> Transportation

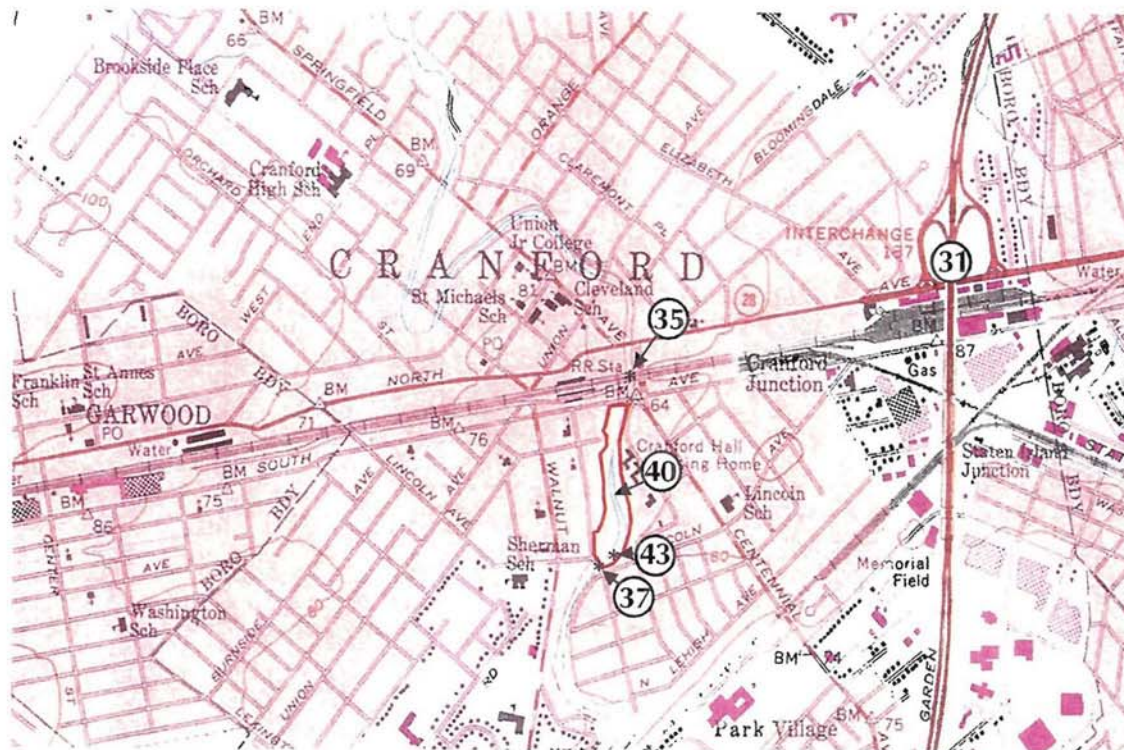
Justification of Eligibility

Acquired and developed within the period of significance (1921-1964), Rahway River Parkway: Droescher's Mill Park is eligible under Criterion A for having made contributions to the broad patterns of history in the areas of Community Planning, Entertainment/Recreation, and Landscape Architecture on the national level; and in the area of Conservation on the state level. Under Criterion C, it is significant for its association with the nationally renowned Olmsted Brothers Landscape Architecture Firm of Brookline, Massachusetts. Under Criterion D, it is eligible for having the capacity to yield important information related to history. With regard to historic integrity, in spite of numerous bridge replacements along its waterway, Rahway River Parkway has retained most of its original location, design, feeling and association.

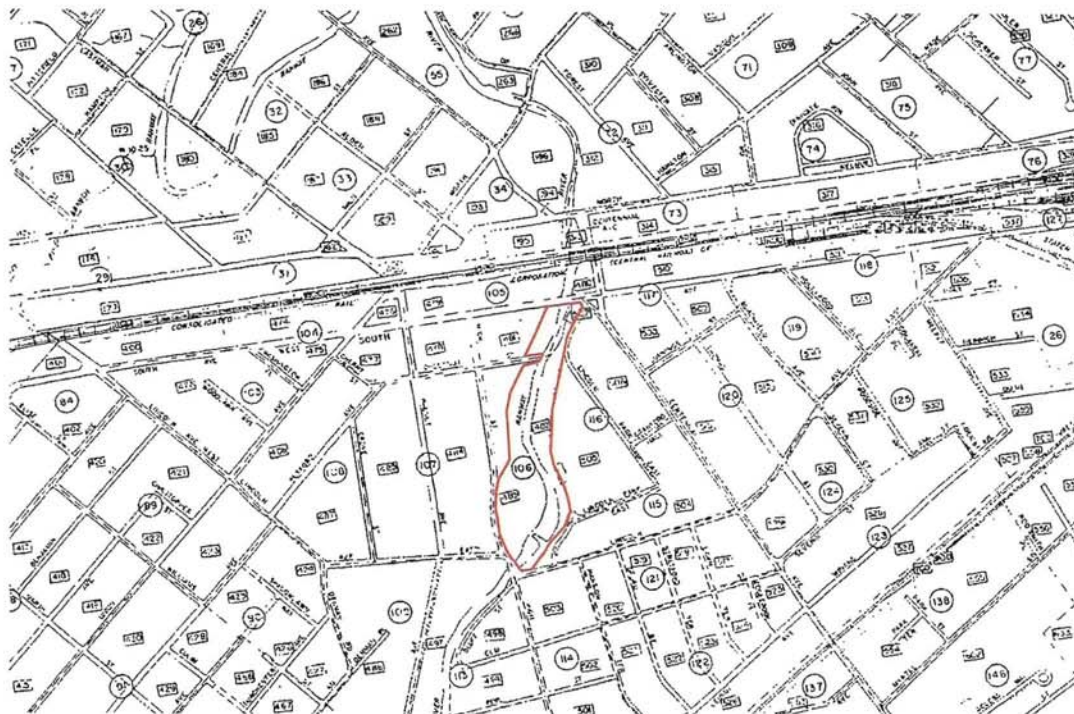
Surveyor Gregory Dietrich

Date 10/21/03

Union County Parks Survey



USGS 7.5' Quadrangle: Roselle, N.J. 1955 (Photorevised 1981).





Block 103, Lot 1; Block 200, Lot 32; Block 261, Lot 6; Blocks 262,263,435,481,482 & 483, Lots 1; of the Township of Cranford.

Surveyor Gregory Dietrich

Date 10/21/03

Inventory of Identified Architectural Resources

Property	Property Information	Description	NRHP Eligibility
 <p>Photograph Number: 118 (looking east across the Rahway River from Droscher's Mill to Droscher's Mill Park)</p>	<p>Panamerican Resource Number: 118</p> <p>Droscher's Mill Park</p> <p>High Street at Lincoln Avenue</p> <p>Municipality: Cranford, Union County</p> <p>UTM coordinates (1983 Zone 18N): 4500249N, 559050E</p> <p>Date: unknown</p> <p>Other: This resource falls under the Cranford Section (ID #4424.027) of the NRE Rahway River Parkway HD.</p>	<p>Style: informal park</p> <p>Stories: na</p> <p>Plan: a walking path to signage about mill; path and area now almost completely washed out</p> <p>Roof: na</p> <p>Exterior: na</p> <p>Foundation: na</p> <p>Windows: na</p> <p>Details: This small park is designed as a viewing area for the mill and the dam. The whole area is badly eroded and washed away.</p>	<p>Not eligible. This structure is a part of county/municipal infrastructure used to control water flow on the Rahway River. The control of flood waters is an important part of governmental responsibility in this part of New Jersey, but the dam does not represent any new technology or special comment (see PRN 96).</p>
 <p>Photograph Number: 119</p>	<p>Panamerican Resource Number: 119</p> <p>2 Chestnut Street</p> <p>Municipality: Cranford, Union County</p> <p>UTM coordinates (1983 Zone 18N): 4500543N, 559026E</p> <p>Date: ca. 1890</p> <p>Other: sits on the Rahway River</p>	<p>Style: Vernacular Victorian with Italianate details</p> <p>Stories: 2 with basement</p> <p>Plan: rectangular</p> <p>Roof: shed</p> <p>Exterior: wood shingles</p> <p>Foundation: concrete and other</p> <p>Windows: replacement</p> <p>Details: This house was once a part of a row of townhouses—the others long missing. The only reminder of those days is the massive Italianate cornice. The house has since had numerous unsympathetic remodels and additions.</p>	<p>The townhouse at 2 Chestnut has been so extensively altered that it now lacks integrity. Without integrity it cannot be eligible for the NRHP. Panamerican recommends it not eligible for the NRHP.</p>

Panamerican Consultants, Inc.

6-61

Rahway River Phase IA

Union County Parks Survey

Park Name **Rahway River Parkway: Cranford Section**

Township/City **Cranford**

photo



Type

☒ recreational ☐ pastoral ☐ picturesque ☐ natural ☐ scenic ☐ other:

Existing Topography

☒ flat ☐ sloped ☐ hilly ☐ mountainous ☐ other:

Spatial Relationships

☒ symmetrical ☐ asymmetrical ☐ axial

Circulation

☒ trails ☐ dirt

☐ cycling paths

☐ bridle paths

☐ parking lots

☐ crushed stone

☐ handicapped ramps

☐ other:

Parkways

☐ Border roads ☐ Connecting pleasure roads ☐ Other:
☐ Internal park roads ☐ River parkways
☐ Vernacular roads ☐ Estate roads
☐ Summit roads ☐ Ocean parkways

Surveyor Gregory Dietrich

Date 10/21/03

Union County Parks Survey

Drainage & Engineering

<input type="checkbox"/> Bridge (road)	
<input type="checkbox"/> Bridge (pedestrian)	wood w/ wood railings; wood w/ steel railings and concrete abutments
<input type="checkbox"/> Channel	stone cladding
<input type="checkbox"/> Culvert	
<input type="checkbox"/> Spillway	stone cladding
<input type="checkbox"/> Millrace	
<input type="checkbox"/> Retaining wall	stone cladding
<input type="checkbox"/> Storm sewer	
<input type="checkbox"/> Swale	
<input type="checkbox"/> Other:	

Bridge

carries	bridge date	info source	bridge type/design	bridge material

Land Uses

<input type="checkbox"/> archery	<input type="checkbox"/> skating--sidewalk
<input type="checkbox"/> baseball	<input type="checkbox"/> soccer
<input type="checkbox"/> basketball	<input type="checkbox"/> softball
<input type="checkbox"/> boating/canoeing	<input type="checkbox"/> swimming
<input type="checkbox"/> biking	<input type="checkbox"/> tennis
<input type="checkbox"/> camping	<input type="checkbox"/> theater/music
<input type="checkbox"/> concession/dining	<input type="checkbox"/> track & field
<input type="checkbox"/> cricket	<input type="checkbox"/> trap/skeet shooting
<input type="checkbox"/> driving	<input type="checkbox"/> trail
<input type="checkbox"/> fishing	<input type="checkbox"/> wading
<input type="checkbox"/> football	<input type="checkbox"/> other:
<input type="checkbox"/> golf	
<input type="checkbox"/> gymnastics	
<input type="checkbox"/> handball court	
<input type="checkbox"/> hockey	
<input type="checkbox"/> horseback riding	
<input type="checkbox"/> horticulture/gardening	
<input type="checkbox"/> lawn bowling	
<input type="checkbox"/> model boating	
<input type="checkbox"/> picnicking	
<input type="checkbox"/> playground	
<input type="checkbox"/> shuffleboard	
<input type="checkbox"/> skating--rink	

Landscape Dividers

low concrete fence

Water Elements

<input type="checkbox"/> rivers	<input type="checkbox"/> creeks	<input type="checkbox"/> pools
<input type="checkbox"/> lakes	<input type="checkbox"/> ponds	<input type="checkbox"/> fountains
<input type="checkbox"/> brooks	<input type="checkbox"/> waterfalls	<input type="checkbox"/> other :

Buildings

<input type="checkbox"/> maintenance	<input type="checkbox"/> stable	<input type="checkbox"/> other:
<input type="checkbox"/> recreational	<input type="checkbox"/> house	
<input type="checkbox"/> concession	<input type="checkbox"/> rest rooms	
<input type="checkbox"/> picnic shelter	<input type="checkbox"/> mill	
<input type="checkbox"/> gazebo	<input type="checkbox"/> museum/visitor center	
<input type="checkbox"/> bandstand	<input type="checkbox"/> public utility	

Landscape Furniture

<input type="checkbox"/> benches	<input type="checkbox"/> planters
<input type="checkbox"/> bleachers	<input type="checkbox"/> trash cans
<input type="checkbox"/> bike racks	<input type="checkbox"/> other:
<input type="checkbox"/> bollards	
<input type="checkbox"/> drinking fountains	
<input type="checkbox"/> fitness trail stations	
<input type="checkbox"/> grills	
<input type="checkbox"/> lighting fixtures	
<input type="checkbox"/> picnic tables	

Surveyor Gregory Dietrich

Date 10/21/03

Union County Parks Survey

Description

The Cranford Section of the Rahway River Parkway consists of a pastoral landscape, two recreational components, and undeveloped areas. A pastoral landscape is located within the park's eastern portion that contains a lake formed by the Rahway River. The lake crosses a modern stone-clad spillway before being submerged underneath the overpass of Stiles Street and the Garden State Parkway to continue to the Clark Section of the riverine parkway. Meadows with scattered mature trees surround this lake, and a landscaped median with a trail divides the lake lengthwise. A basketball court is located southwest of the lake, and a local playground and picnic grove are located on the south side of a footbridge crossing the Rahway River, along Mohawk Avenue. The western portion of the park surrounding the Rahway River is mostly undeveloped, featuring wild trees and vegetation, except for a large, open meadow with scattered mature trees along Crane Parkway.

Setting

The Cranford Section is bordered by Crane Parkway to the north, the Garden State Parkway and Raritan Road to the east, Mohawk Drive to the south, and Walnut Avenue to the west. 1½ - 2-story single-family dwellings (c.1945-1990) are located along Crane Parkway. 1½ - 2-story single-family dwellings (c.1920-1960) are located along Mohawk Drive.

Surveyor Gregory Dietrich

Date 10/21/03

Union County Parks Survey

Markers/
uments/
apture

Signage

metal

Utilities

Plantings

mature oaks, maples, sweetgum

CRM issues

NJ-HPO ID No. 4079: Rahway River Parkway Historic District; Survey Form reference number (40)
NJ-HPO ID No. 3874: Garden State Parkway Historic District; Survey Form reference number (31)Character Defining
Features/ResourcesRahway River
Lake
Median
Wooded area along Rahway River
Open meadow w/ scattered mature
trees at Crane Parkway south of Hillside
Avenue

Contributing Features

Dirt trails

Non-Contributing Features

Union County Parks Survey

Contributing Resources

Wooden footbridge w/ wooden railings
Basketball court

Non-Contributing Resources

Stone-clad spillway
Wood footbridge w/ steel railings
Playground apparatus
Picnic tables
Benches
Parking lot

Boundary of eligible portion of park

All of the property contained within Block 493, Lot 12; Block 500, Lot 19; Block 505, Lot 8; Block 543, Lot 39; Block 544, Lot 1; Block 585, Lot 1; Block 586, Lot 17; and Block 588, Lot 1 of the Township of Cranford, Union County, New Jersey.

Eligible

☐ yes
☐ no

Rating

☐ Key Contributing ☐ Contributing ☐ Non-Contributing

Areas of Significance

<input type="checkbox"/> Agriculture	<input type="checkbox"/> Economics	<input type="checkbox"/> Industry	<input type="checkbox"/> Performing Arts
<input type="checkbox"/> Architecture	<input type="checkbox"/> Education	<input type="checkbox"/> Invention	<input type="checkbox"/> Philosophy
<input type="checkbox"/> Art	<input type="checkbox"/> Engineering	<input type="checkbox"/> Landscape Architecture	<input type="checkbox"/> Politics
<input type="checkbox"/> Commerce	<input type="checkbox"/> Entertainment/Recreation	<input type="checkbox"/> Law	<input type="checkbox"/> Religion
<input type="checkbox"/> Communications	<input type="checkbox"/> Ethnic Heritage	<input type="checkbox"/> Literature	<input type="checkbox"/> Science
<input type="checkbox"/> Community Planning	<input type="checkbox"/> Exploration/Settlement	<input type="checkbox"/> Maritime History	<input type="checkbox"/> Social History
<input type="checkbox"/> Conservation	<input type="checkbox"/> Health/Medicine	<input type="checkbox"/> Military	<input type="checkbox"/> Transportation

Justification of Eligibility

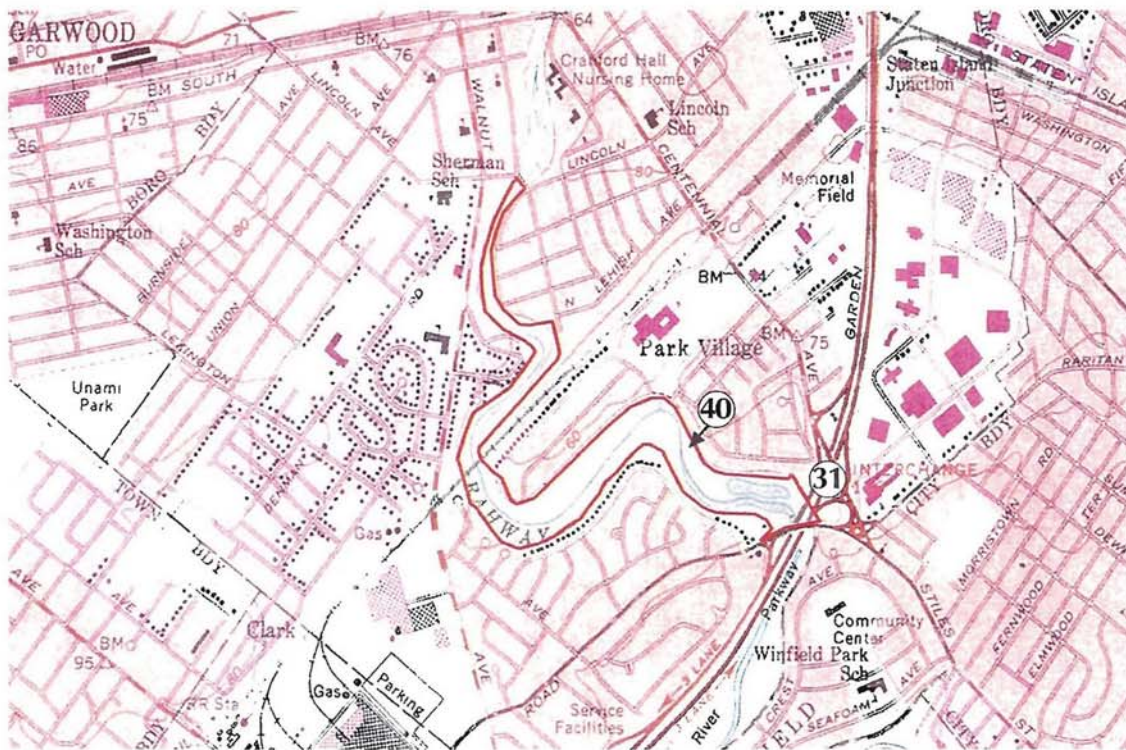
Acquired and developed within the period of significance (1921-1964), Rahway River Parkway: Cranford Section is eligible under Criterion A for having made contributions to the broad patterns of history in the areas of Community Planning, Entertainment/Recreation, and Landscape Architecture on the national level; and in the area of Conservation on the state level. Under Criterion C, it is significant for its association with the nationally renowned Olmsted Brothers Landscape Architecture Firm of Brookline, Massachusetts. Under Criterion D, it is eligible for having the capacity to yield important information related to history. With regard to historic integrity, in spite of numerous bridge replacements along its waterway, Rahway River Parkway has retained most of its original location, design, feeling and association.

Surveyor Gregory Dietrich

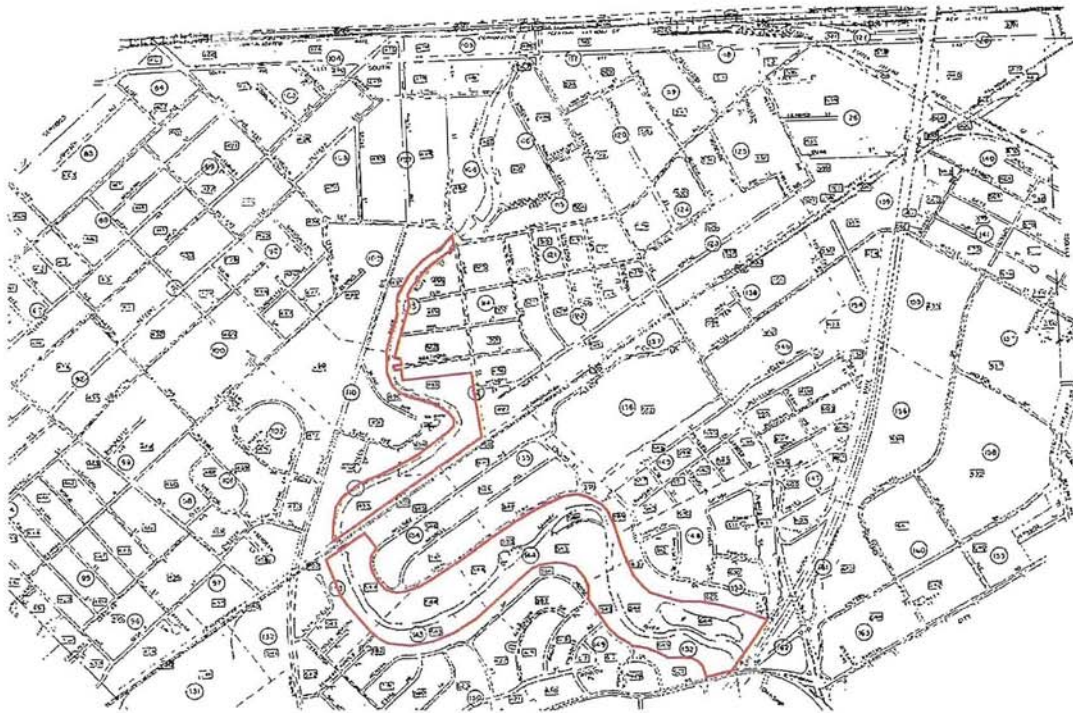
Date 10/21/03

A-108

Union County Parks Survey



USGS 7.5' Quadrangle: Roselle, N.J. 1955 (Photorevised 1981).



Block 493, Lot 12; Block 500, Lot 19; Block 505, Lot 8; Block 543, Lot 39; Block 544 Lot 1; Block 585, Lot 1; Block 586, Lot 17; Block 588, Lot 1; of the Township of Cranford.

UNION COUNTY (16)

CRANFORD:

First Baptist Church
100 High Street

E?

This, Cranford's first black Baptist church, was organized in 1887.

Saint Mark's AME Church
High Street, between South Avenue and Chestnut Street

E?

This brick buiding, about twenty-five feet by sixty feet, may be Cranford's earliest black church. It was built prior to 1915.

ELIZABETH:

Shiloh Baptist Church
87-89 Murray Street

2004-BS-1
E

Organized in 1879 by members who withdrew from Elizabeth's Fourth Baptist Church, Shiloh Baptist Church acquired this site in 1894 and remodeled a large house into its first sanctuary. The present structure was built in 1923 (See Union Baptist Church).

Siloam Presbyterian Church
1133 Washington Street

2004-BS-2
E

Siloam was the third black Presbyterian church to be founded in New Jersey, after those in Newark and Princeton. As early as 1844, the Siloam Presbyterian Mission sponsored for blacks by the First Presbyterian Church, stood on Washington Street (now Dickinson). The mission was formally elevated to a church in 1866. Though the architecture of this building suggests a construction date in the 1840s, our account states that it was built in 1871. The building was remodeled and a fellowship hall added at the rear in 1927 (see Plane Street Presbyterian Church site, Newark, Essex County; Witherspoon Street Presbyterian Church, Princeton, Mercer County).

PLAINFIELD:

Mount Olive Baptist Church
216 Liberty Street

E

This, the oldest of Plainfield's black congregations, was organized in the 1880s. It is also the "mother church" of Shiloh Baptist and Union Baptist churches.

UNION COUNTY (16)**CRANFORD:****First Baptist Church
100 High Street****E?**

This, Cranford's first black Baptist church, was organized in 1887.

Saint Mark's AME Church**High Street, between South Avenue and Chestnut Street****E?**

This brick buiding, about twenty-five feet by sixty feet, may be Cranford's earliest black church. It was built prior to 1915.

ELIZABETH:**Shiloh Baptist Church
87-89 Murray Street****2004-BS-1
E**

Organized in 1879 by members who withdrew from Elizabeth's Fourth Baptist Church, Shiloh Baptist Church acquired this site in 1894 and remodeled a large house into its first sanctuary. The present structure was built in 1923 (See Union Baptist Church).



**Siloam Presbyterian Church
1133 Washington Street****2004-BS-2
E**

Siloam was the third black Presbyterian church to be founded in New Jersey, after those in Newark and Princeton. As early as 1844, the Siloam Presbyterian Mission sponsored for blacks by the First Presbyterian Church, stood on Washington Street (now Dickinson). The mission was formally elevated to a church in 1866. Though the architecture of this building suggests a construction date in the 1840s, our account states that it was built in 1871. The building was remodeled and a fellowship hall added at the rear in 1927 (see Plane Street Presbyterian Church site, Newark, Essex County; Witherspoon Street Presbyterian Church, Princeton, Mercer County).

PLAINFIELD:**Mount Olive Baptist Church
216 Liberty Street****E**

This, the oldest of Plainfield's black congregations, was organized in the 1880s. It is also the "mother church" of Shiloh Baptist and Union Baptist churches.

Inventory of Identified Architectural Resources

Property	Property Information	Description	NRHP Eligibility
 <p>Photograph Number: 63</p>	<p>Panamerican Resource Number: 63</p> <p>Outbuilding</p> <p>23 Holly Street</p> <p>Municipality: Cranford, Union County</p> <p>UTM coordinates (1983 Zone 18N): 4500924N, 558466E</p> <p>Date: ca. 1930s</p> <p>Other: The outbuilding sits at the edge of the Rahway River and the house has a backyard view of McConnell Park.</p>	<p>Style: Vernacular</p> <p>Stories: 1</p> <p>Plan: rectangular</p> <p>Roof: gable with slate</p> <p>Exterior: wood novelty siding</p> <p>Foundation: obscured</p> <p>Windows: missing</p> <p>Details: A small outbuilding, possibly a playhouse or shed, it hugs the bank of the river.</p>	<p>The small outbuilding is non-contributing to the district and is not eligible for the NRHP.</p>
 <p>Photograph Number: 64</p>	<p>Panamerican Resource Number: 64</p> <p>Medical Offices</p> <p>19 Holly Street</p> <p>Municipality: Cranford, Union County</p> <p>UTM coordinates (1983 Zone 18N): 4500886N, 558454E</p> <p>Date: ca. 1936</p> <p>Other: A portion of this large complex has a backyard view of McConnell Park; it backs up to the Rahway River.</p>	<p>Style: Colonial Revival</p> <p>Stories: 2.5</p> <p>Plan: original-rectangular; present – irregular</p> <p>Roof: gable and flat</p> <p>Exterior: brick and siding</p> <p>Foundation: obscured</p> <p>Windows: 6/6 original wood and fixed wood original</p> <p>Details: This is an exceptionally large medical complex with multiple attached wings at the rear. However, from the front the house reads as an excellent example of a fully intact Colonial Revival-style building with multiple stylistic flourishes.</p>	<p>Non-contributing to the NRE North Cranford HD since it falls outside of the period of significance. It is also not eligible since it does not meet the NRHP criteria.</p>

New Jersey Department of Environmental Protection
Historic Preservation Office

BASE FORM**Historic Sites #:**Property Name: 219 Holly StreetStreet Address: Street # 219Apartment # Prefix: Street Name: Holly Suffix: Type: STCounty(s): UnionZip Code: 07016Municipality(s): Cranford Twp. 2003Block(s): 186Local Place Name(s): Lot(s): 7Ownership: PrivateUSGS QUAD: Roselle**Description:**

219 Holly Street is a 2½-story, 3-bay, Queen Anne-style dwelling constructed circa 1890. (See Building Attachment)

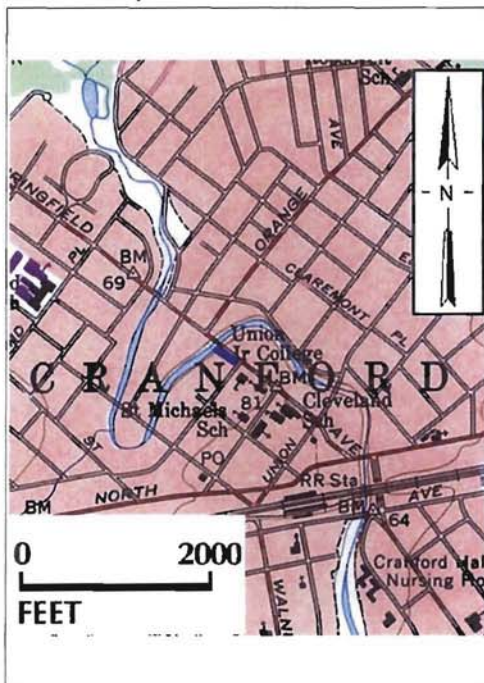
**Registration and
Status Dates:**National Historic
Landmark: SHPO Opinion: 8/23/01National Register: Local Designation: New Jersey Register: Other Designation: Determination of Eligibility: Other Designation Date: COE: 5/19/05**Photograph:**Survey Name: Vanessa ZeoliDate: September 2008Surveyor: Phase II Springfield Ave. Bridge, Cranford, NJOrganization: Cultural Resource Consulting Group

New Jersey Department of Environmental Protection
Historic Preservation Office

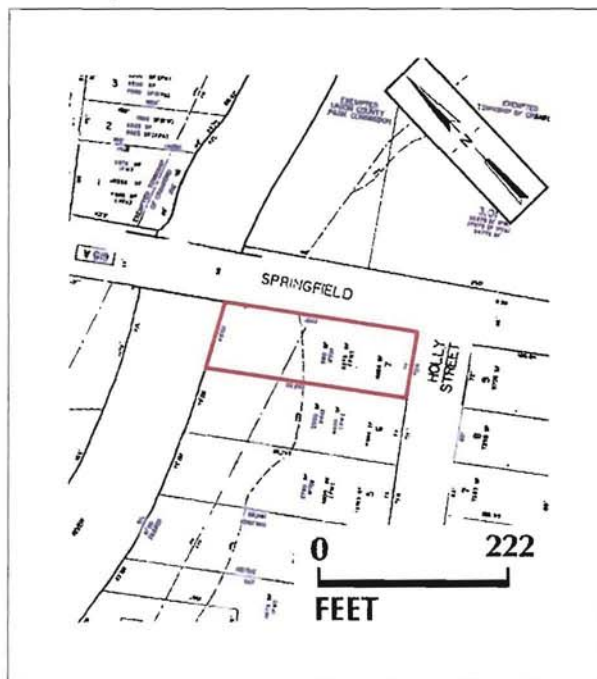
BASE SURVEY FORM

Historic Sites #

Location Map:



Site Map:



Bibliography/Sources:

USGS 7.5' Quadrangle: Roselle, N. J. 1955 (Photorevised 1981).
Tax Maps Township of Cranford, Union County New Jersey, revised 1993.

Additional Information:

More Research Needed? ☐ Yes ☒ No

INTENSIVE-LEVEL USE ONLY:

Attachments Included: ☒ Building/Element ☐ Landscape ☐ Farm
☐ Bridge ☐ Industry

Historic District: Yes Historic District Name: North Cranford Historic District

Status: ☐ Key Contributing ☒ Contributing ☐ Non Contributing

Associated Archaeological Site/Deposite? ☐ Yes ☒ No

(Known or potential sites - if yes, please describe briefly)

Survey Name: Vanessa Zeoli

Date: September 2008

Surveyor: Phase II Springfield Ave. Bridge, Cranford, NJ

Organization: Cultural Resource Consulting Group

New Jersey Department of Environmental Protection
Historic Preservation Office

BUILDING ATTACHMENT**Historic Sites #****Common Name:** 219 Holly Street**Historic Name:** _____**Present Use:** Residential Activity, Permanent**Historic Use:** Residential Activity, Permanent**Construction Date:** 1890**Source:** 1903 Sanborn map; visual evidence**Alteration Date(s):** late-20th-century**Source:** Visual Evidence**Designer:** N/A**Physical Condition:** Excellent**Builder:** Unknown**Remaining Historic Fabric:** High**Style:** Queen Anne**Form:** Irregular**Stories(B):** 2.5**Type:** _____**Bays(B):** 3**Roof Finish Materials (B,S):** Slate**Exterior Finish Materials:** Wood, Clapboard; Wood fishscale shingles**Exterior Description:**

219 Holly Street is a 2½-story, 3-bay, Queen Anne-style dwelling constructed circa 1890. The cross-gable roof is covered with asphalt shingles. The gable has a decorative bargeboard and the slightly overhanging eave is supported by brackets. The brick corbelled chimney is situated on the rear roof peak. The upper stories of the exterior walls are clad in wood fishscale shingles and the first floor is clapboard. The wall surface flares between the 1st and 2nd stories. Window openings contain mostly 1/1 wood sash units, but also have 2/2 wood sash units on the side and rear elevations. The 1-story entry porch wraps around to both side elevations and has a low-pitched hipped roof with a pediment above the porch steps. The porch is supported by turned posts and adorned with a turned balustrade and spindlework in the frieze. The porch deck and steps are wood and access an offset main entrance that has paired wood panel doors. On the northeast elevation is an open, 2nd-story porch that exhibits the same details as the front porch. The lot also contains a 1-story, 2-car garage with a front-gable roof. The roof is clad in asphalt shingles and the walls are clad in clapboard. The automobile entrance contains two sets of paired, wood panel doors set on hinges. Windows on the garage contain 6/1 wood sash units.

Interior Description:N/A**Setting:**

219 Holly Street is located immediately southeast of the bridge on the southwest side of the street. The lot is surrounded on three sides by a modern wood fence, and adorned with many young and mature trees, a driveway and walkways of concrete pavers, and decoratively landscaped beds in the front and rear of the dwelling. It is located at the south corner of Springfield Avenue and Holly Street and situated on Block 186, Lot, 7.

Survey Name: Vanessa Zeoli**Date:** September 2008**Surveyor:** Phase II Springfield Ave. Bridge, Cranford, NJ**Organization:** Cultural Resource Consulting Group

New Jersey Department of Environmental Protection
Historic Preservation Office

ELIGIBILITY WORKSHEET

Historic Sites #

History:

Comparison between the 1882 E. Robinson map and the 1903 Sanborn map combined with visual evidence suggest that 219 Holly Street was constructed circa 1890. Visual evidence also suggests the building was restored within the last few years.

Statement of Significance:

219 Holly Street is individually eligible under Criterion A for its contribution to Cranford's residential growth and development during the late 19th century as the township transformed from a rural farming community into a suburban neighborhood for wealthy businessmen. It is also eligible under Criterion C as a unique surviving example of intact Queen Anne-style domestic architecture in Cranford. Since it retains its integrity and its ability to convey its significance, it is eligible for listing on the National Register of Historic Places. (See Continuation Sheet)

Eligibility for New Jersey
and National Registers:

☒ Yes ☐ No

National

Register Criteria: ☒ A ☐ B ☒ C ☐ D

Level of Significance: ☒ Local ☐ State ☐ National

Justification of Eligibility/Ineligibility:

219 Holly Street retains its integrity of design, materials, location, feeling, workmanship, setting, and association. The dwelling appears to have undergone a major restoration within the last five years. While the extent of alterations is not known, the building appears to retain the majority of its character-defining features. As a result, 219 Holly Street is both individually eligible for the National Register of Historic Places and eligible as a contributing resource to the North Cranford Historic District.

For Historic Districts Only:

Property Count: _____ Key Contributing: _____ Contributing: _____ Non Contributing: _____

For Individual Properties Only:

List the Completed Attachments related to the property's significance:

Narrative Boundary Description

All of the property contained in Block 186, Lot, 7 in the Township of Cranford, Union County, New Jersey.

Survey Name: Vanessa Zeoli

Date: September 2008

Surveyor: Phase II Springfield Ave. Bridge, Cranford, NJ

Organization: Cultural Resource Consulting Group

CONTINUATION SHEET**Historic Sites #****Bibliography/Sources (cont.)**

Bauer, Jacob L.

1906 Atlas of Union County, New Jersey. E. Robinson & Co., New York, N.Y.

Hendrickson, Louie E.

1904 Cranford, New Jersey, Illustrated. Cranford, N.J.

Robinson, E. & R. H. Pidgeon

1882 Atlas of Union County, New Jersey. E. Robinson, New York, N.Y.

Sanborn Map Company

1903 Cranford, Union County, New Jersey. Sheet 4. On file at Rutgers University Libraries, New Brunswick, N.J.

1922 Cranford, Union County, New Jersey. Sheet 3. On file at Rutgers University Libraries, New Brunswick, N.J.

1959 Cranford, Union County, New Jersey. Sheet 3. On file at Rutgers University Libraries, New Brunswick, N.J.

1929 Cranford, Union County, New Jersey. Sheet 3. On file at Rutgers University Libraries, New Brunswick, N.J.

Sidney, J. C.

1850 Map of Essex County, New Jersey. Hiram A. Belding, Newark, N.J.

Statement of Significance (cont.)

In addition, 219 Holly Street is eligible as a contributing resource to the North Cranford Historic District, which was determined eligible for listing on the National Register of Historic Places by the NJDEP-HPO on 5/19/05. The historic district meets Criteria A for its contribution to Cranford's residential growth and development during the late 19th and early 20th century as the township transformed from a rural farming community into a suburban neighborhood for wealthy businessmen. The Rahway River and the Central Railroad of New Jersey were key factors in the success of Cranford's development. The river was a strong influence in the community, forming a part of picturesque neighborhoods and providing recreational activities for residents. The railway enabled Cranford to attract New York professional who could live in the community and commute to work in the city. The historic district is also eligible for listing in the National Register under Criterion C as a historic district with representative examples of numerous styles and types of architecture that include Queen Anne and Colonial Revival, with lesser numbers of Tudor Revival, Shingle style and Craftsman. The historic district's period of significance is 1860 to 1930 (CRCG 2001: 24). No evidence was found to suggest that the district is eligible under Criterion B for association with significant individuals, or under Criterion D for having the ability to yield information important to history and/or pre-history. Since 219 Holly Street was constructed within the historic district's period of significance (1860-1930), retains the majority of its character-defining features, and falls within the district's boundaries, it is a contributing element to the North Cranford Historic District.

Survey Name: Vanessa ZeoliDate: September 2008Surveyor: Phase II Springfield Ave. Bridge, Cranford, NJOrganization: Cultural Resource Consulting Group



CONTINUATION SHEET

Historic Sites #



Garage

Survey Name: Vanessa ZeoliDate: September 2008Surveyor: Phase II Springfield Ave. Bridge, Cranford, NJOrganization: Cultural Resource Consulting Group

Inventory of Identified Architectural Resources			
Property	Property Information	Description	NRHP Eligibility
 <p>Photograph Number 11</p>	<p>Panamerican Resource Number: 11</p> <p>Belting Industries Inc.</p> <p>20 Boright Avenue</p> <p>Municipality: Kenilworth, Union County</p> <p>UTM coordinates (1983 Zone 18N): 4503898N, 558988E</p> <p>Date: ca. 1960s</p> <p>Other: located in industrial park</p>	<p>Style: contemporary</p> <p>Stories: 1</p> <p>Plan: rectangular</p> <p>Roof: flat</p> <p>Exterior: brick</p> <p>Foundation: concrete</p> <p>Windows: single and triple 1/1 awning metal windows</p> <p>Details: sandy red brick building with rectangular projecting office area</p>	<p>Not Eligible, does not meet the NRHP criteria for eligibility</p>
 <p>Photograph Number: 12</p>	<p>Panamerican Resource Number: 12</p> <p>Snack Shop</p> <p>Lenape Park</p> <p>Municipality: Kenilworth, Union County</p> <p>UTM coordinates (1983 Zone 18N): 4502852N, 557872E</p> <p>Date: unknown</p> <p>Other: appears to be in fair condition</p>	<p>Style: originally this was probably in an Adirondack-style type</p> <p>Stories: 1</p> <p>Plan: wing on gable</p> <p>Roof: steep gable</p> <p>Exterior: plaster</p> <p>Foundation: concrete</p> <p>Windows: all boarded</p> <p>Details: the building has been much changed although the original form and stone chimney provide a glimpse at its antecedents.</p>	<p>Not Eligible, lost integrity, and non-contributing to the park</p>

BUREAU OF ENVIRONMENTAL SERVICES



NEW JERSEY HISTORIC BRIDGE DATA

STRUCTURE #		2003006		CO		UNION		OWNER		COUNTY		MILEPOINT		0.0	
NAME & FEATURE INTERSECTED		KENILWORTH BOULEVARD OVER RAHWAY RIVER						FACILITY		KENILWORTH BOULEVARD					
TOWNSHIP		CRANFORD TOWNSHIP													
TYPE		BOX BEAM				DESIGN				MATERIAL				Prestressed Concrete	
# SPANS		2		LENGTH		72 ft		WIDTH		46 ft					
CONSTRUCTION DT		1925		ALTERATION DT		1992				SOURCE		NJDOT			
DESIGNER/PATENT										BUILDER					

SETTING / CONTEXT The bridge is located in a Union County Park, and carries a two-lane road across the Rahway River. The bridge was under construction at the time it was inspected.

1995 SURVEY RECOMMENDATION Not Eligible

HISTORIC BRIDGE MANAGEMENT PLAN (EVALUATED) No

CONSULT STATUS Not Individually Eligible.

CONSULT DOCUMENTS SHPO Finding 02/08/90

SUMMARY The superstructure of the two-span stringer bridge is being completely replaced with prestressed concrete box beams. The original concrete substructure remains, with new concrete caps on top of the pier and abutments. The new structure will have stone parapets. The bridge has not retained the integrity of its original design. The original structure was a common bridge type that was evaluated as not eligible, and the new bridge is not old enough to be evaluated as historically significant.



INFORMATION



PHOTO: None (05/92)

REVISED BY (DATE):

QUAD: Roselle

Inventory of Identified Architectural Resources

Property	Property Information	Description	NRHP Eligibility
 <p>Photograph Number: 15</p>	<p>Panamerican Resource Number: 15</p> <p>Route 509/Kenilworth Boulevard, east and west bound bridges (NJDOT #2003006), crossing a tributary of the Rahway River east of the river between Lenape and Nomahegan Parks</p> <p>Municipality: Kenilworth, Union County</p> <p>UTM coordinates (1983 Zone 18N): 4502676N, 557929E</p> <p>Date: 1992 (sign on bridge)</p> <p>Other:</p>	<p>Style: rustic, Adirondack-style type</p> <p>Stories: na</p> <p>Plan: 1 bay</p> <p>Roof: na</p> <p>Exterior: concrete with stone veneer enclosed balustrade</p> <p>Foundation: concrete abutments</p> <p>Windows: na</p> <p>Details: The current bridges replaced earlier bridges at this site. An attempt was made to make these bridges blend in with the park through the use of stone.</p>	<p>Although the east and west bound bridges are not yet 50 years old, they should be considered eligible as contributing elements to both the NRE Rahway River Parkway HD (ID #4079) and the Union County Park System HD (ID #4424).</p>
 <p>Photograph Number: 16</p>	<p>Panamerican Resource Number: 16</p> <p>101 Nomahegan Blvd</p> <p>Municipality: Cranford, Union County</p> <p>UTM coordinates (1983 Zone 18N): 4502691N, 557813E</p> <p>Date: ca. 1960s</p> <p>Other:</p>	<p>Style: Colonial Revival</p> <p>Stories: 2</p> <p>Plan: rectangular</p> <p>Roof: side gable</p> <p>Exterior: aluminum siding</p> <p>Foundation: covered</p> <p>Windows: metal 1/1 sash</p> <p>Details: much altered house that backs onto Lenape Park</p>	<p>Not Eligible, does not meet the NRHP criteria for eligibility</p>

Inventory of Identified Architectural Resources			
Property	Property Information	Description	NRHP Eligibility
 <p>Photograph Number : 13</p>	<p>Panamerican Resource Number: 13</p> <p>Dam/spillway on Rahway River</p> <p>Lenape Park</p> <p>Municipality: Kenilworth, Union County</p> <p>UTM coordinates (1983 Zone 18N): 4502777N, 558037E</p> <p>Date: unknown</p> <p>Other:</p>	<p>Style: na</p> <p>Stories: na</p> <p>Plan: na</p> <p>Roof: na</p> <p>Exterior: concrete</p> <p>Foundation: concrete abutments</p> <p>Windows: na</p> <p>Details: an open flood-control structure</p>	<p>Not eligible and non-contributing; one of many later additions to the Rahway River made in an attempt to control flooding in the area.</p>
 <p>Photograph Number: 14</p>	<p>Panamerican Resource Number: 14</p> <p>Route 509/Kenilworth Boulevard, east and west bound bridges (NJDOT #2003018), crossing Rahway River between Lenape and Nomahegan parks</p> <p>Municipality: Kenilworth, Union County</p> <p>UTM coordinates (1983 Zone 18N): 4502712N, 558068E</p> <p>Date: 1992 (sign on bridge)</p> <p>Other:</p>	<p>Style: rustic, Adirondack-style type</p> <p>Stories: na</p> <p>Plan: 2-bay</p> <p>Roof: na</p> <p>Exterior: concrete with stone veneer enclosed balustrade</p> <p>Foundation: concrete abutments</p> <p>Windows: na</p> <p>Details: the current bridges replaced earlier bridges at this site. An attempt was made to make these bridges blend in with the park through the use of stone.</p>	<p>Although the east and west bound bridges are not yet 50 years old, they should be considered eligible as contributing elements to both the NRE Rahway River Parkway HD (ID #4079) and the Union County Park System HD (ID #4424).</p>

NEW JERSEY OFFICE OF HISTORIC PRESERVATION HISTORIC SITES INVENTORY # 200340
INDIVIDUAL STRUCTURE SURVEY FORM

HISTORIC NAME: Williams House
LOCATION: 506 Lexington Ave.

COMMON NAME:
BLOCK/LOT 460/16

MUNICIPALITY: Cranford
USGS QUAD: Roselle
OWNER/ADDRESS: Murphy, Donald
478 Adams Ave., Elizabeth, NJ

COUNTY: Union
UTM REFERENCES:
Zone/Northing/Easting

DESCRIPTION

Construction Date: 18th C.

Source of Data: Documentation

Architect:

Builder:

Style: Colonial farmhouse

Form/Plan Type: Rectangular

Number of Stories: Two

Foundation: Fieldstone

Exterior Wall Fabric: Clapboard siding

Fenestration: Four bay facade. Two bay side.

Roof/Chimneys: Gable roof. End chimney - covered with stucco.

Additional Architectural Description:

Kneehigh windows on second floor facade.

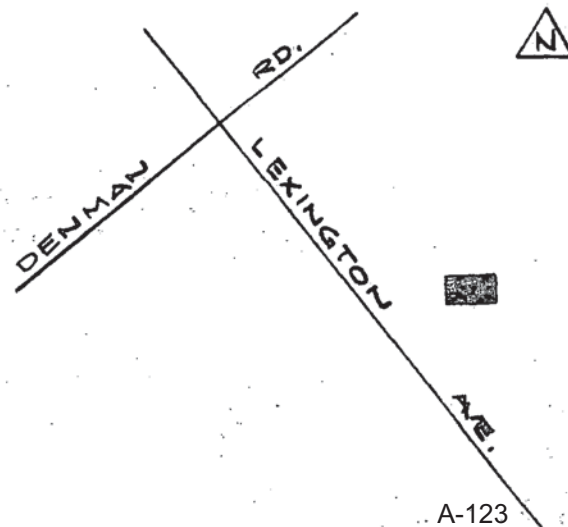
Lean-to additions on west side and at entry on south side.

House angled to street - faces south.

Carriage house at rear of property - 1½ story.

PHOTO Negative File # Roll 25 Frame 3

Map (Indicate North)



RY DESCRIPTION, AND RELATED STRUCTURES:

House faces south, a siting typical of 18th Century homes. It does not have any outbuildings remaining.

SURROUNDING ENVIRONMENT: Urban ☐ Suburban ☒ Scattered Buildings ☐
Open Space ☐ Woodland ☐ Residential ☒ Agricultural ☐
Industrial ☐ Downtown Commercial ☐ Highway Commercial ☐ Other ☐

Adjacent homes are modern, single family, suburban ranch houses.

SIGNIFICANCE:

The earliest documentation for this house dates to 1828, when "Squire Williams" lived here. The Williams family were among the early settlers of Cranford. The house is probably older than that, for it has features common to 18th Century homes in the area.

ORIGINAL USE: Farmhouse

PHYSICAL CONDITION: Excellent ☐ Good ☒

REGISTER ELIGIBILITY: Yes ☒ Possible ☐

THREATS TO SITE: Roads ☐ Development ☐

No Threat ☒ Other ☐

COMMENTS:

PRESENT USE: Residence

Fair ☐ Poor ☐

No ☐ Part of District ☐

Zoning ☐ Deterioration ☐

House has been well-maintained, although small additions to the house currently obscure some of its original features.

REFERENCES:

Cranford Historical Society
Union County Landmark Inventory. 1976.

RECORDED BY: JWF
ORGANIZATION: UCCHPAB

DATE: 1981



CRANFORD
Union Co.

* 200340
R 25/3

Williams Hse
506 Lexington Ave
looking N.E.

1981

document (EXHIBIT 2). Initial coordination with the SHPO staff focused on additional information concerning the mill and site boundaries. Further coordination with the SHPO resulted in a determination that the project would affect two significant cultural resources, both the Droescher's Mill site and the bridge itself.

IV. DESCRIPTION OF THE LINCOLN AVE./HIGH ST. BRIDGE

The original river crossing, once part of the northern "Old York Road", predates the known use of the mill seat. The current bridge that spans the Rahway River just downstream of Droescher's Mill is at least the third structure built in that general location. Although an 1873 construction date is assumed for the bridge, no documentation has been found to confirm that date. By the time of the 1888 A. Lockett map of Cranford, a different bridge had replaced the previous one, as shown on the 1870 Foster map of Cranford. The 1888 location and orientation exactly match that of the current bridge and it is most likely that they are the same. The then-new bridge represents a radical departure from the previous design, in that its orientation is shifted to north-south and the bridge sides are not parallel. Although undocumented, the new configuration appears to be an attempt to favor High Street traffic over Lincoln Avenue or the Westfield Road traffic. High Street, south of the Rahway River, is shown as a paper street on

the 1888 map and it is supposed that the new bridge was designed to accommodate expected development along this new thoroughfare. The non-parallel configuration of the bridge sides was probably instrumental in the choice of construction material and method.

Although built during a period when metal truss construction was very popular, the Lincoln Avenue/High Street bridge was constructed using brick arches. The use of independent, brick-arch spandrels allowed the bridge to be built on a non-standard design resulting in a non-rectangular, non-parallelogram plan view structure. The overall width of the bridge increases as one proceeds from the northern end to the southern approach. This "funnel" shaped plan may be the result of attempting to accommodate traffic from two roads at the northern end and three roads at the southern end. Again, the use of such a non-standard design plan to accommodate an anticipated traffic flow may have precluded the use of standard bridge construction technology and forced the adoption of an alternative construction technique. However, the absence of records renders the assignment of specific goals or reasons to the bridge design as constructed, very tenuous.

The end result was a triple-arch bridge, each arch composed of multiple brick arch-rib construction. Each rib component is slightly skewed in relationship to its adjoining rib(s), as well as being skewed in cross section. This results in the southern side of each arch being several feet longer than the northern side. This technique is continued across all three arches. The space

above the arches was in-filled with rubble, and a stone fascia was used for exterior treatment. About 20 years later, the then-current owner, Crossman Lyons, drastically altered the mill setting by the construction of various stone rubble retaining walls along the river's edge, creating a park-like environment (FIGURE 6, EXHIBIT 2).

Although the various bridges and mills that existed in the project area were functionally unrelated, it was the opinion of the SHPO and the HSC that the existing bridge did contribute to the current mill's "historic setting". Therefore, any alteration of the existing bridge or its abutments would affect the setting of the mill itself (EXHIBITS 2 and 3).

V. EFFECTS OF THE UNDERTAKING

The FHWA and NJDOT have determined in consultation with the SHPO that replacement of the Lincoln Avenue/High Street Bridge will result in an adverse effect to two historic properties, the bridge itself and the Driescher's Mill property. The design is the product of the "Feasibility Study" process begun in 1985 and updated in 1989 and summarized in EXHIBIT 4. Originally, in 1981, the possibility of rehabilitating and widening the existing structure was considered. But two years later, in 1983, this was found to be unfeasible due to the advanced state of deterioration



Figure 2 Photograph of collapsed bridge facia of Lincoln Avenue/High Street



Figure 3 Photographic Detail of Acrow Bridge

NEW JERSEY HISTORIC BRIDGE SURVEY

STRUCTURE #: 2003045 COUNTY: UNION OWNER: COUNTY ROUTE: 9020

MILEPOINT: 000000

TOWNSHIP: CRANFORD TOWNSHIP

FACILITY CARRIED: LINCOLN AVENUE

NAME/FEATURE INTERSECTED: LINCOLN AVENUE OVER RAHWAY RIVER

DRAFT

TYPE: BRICK ARCH

DESIGN: ELLIPTICAL

MATERIAL: BRICK

SPANS: 003 LENGTH: 000073 WIDTH: 0480

DATE OF CONSTRUCTION: 1875 ALTERATION: 1926, 85

SOURCE: COUNTY ENGINEER

DESIGNER/PATENT: BUILDER:

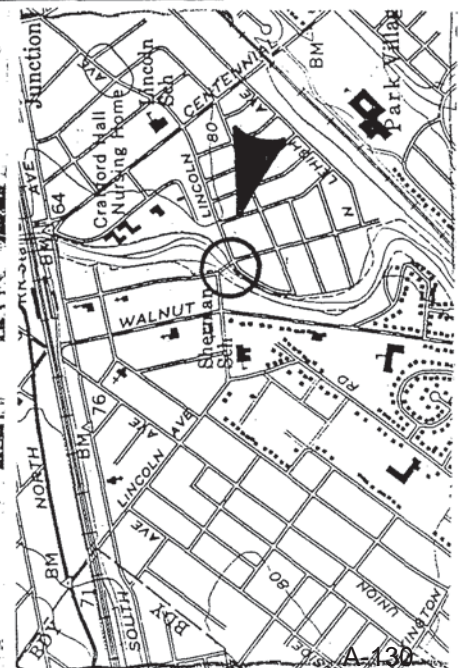
SETTING/CONTEXT: The bridge is located in a wooded greenway along the Rahway River with some late-20th century residences nearby. The bridge carries a two-lane street with two sidewalks over the Rahway River downstream from National Register-listed Droescher's Mill. A dam approximately 300' upstream creates a mill pond and adds to the park-like atmosphere of the mill and bridge.

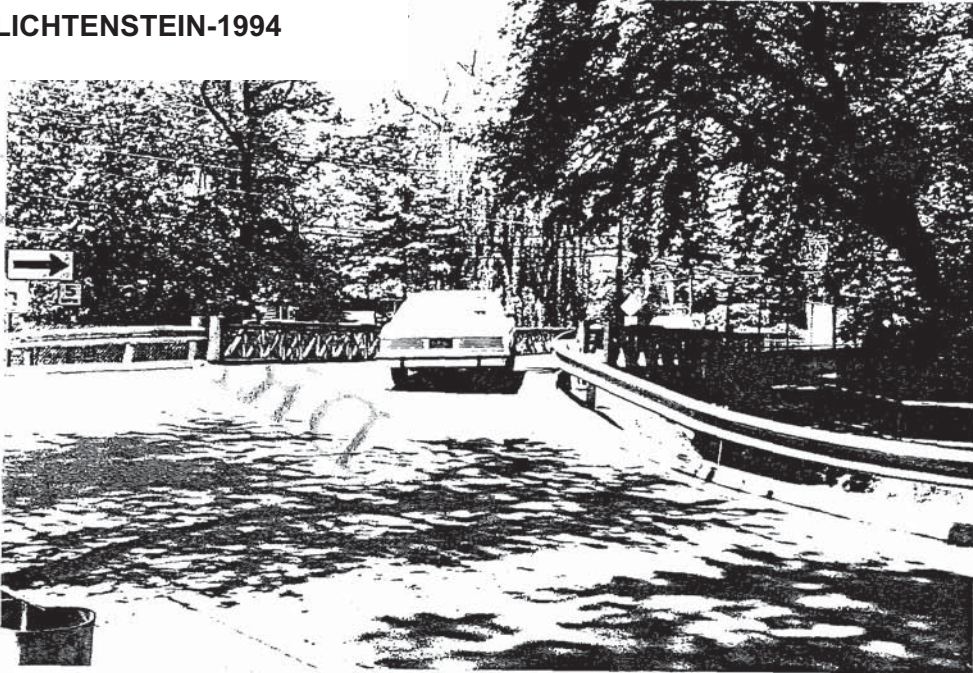
CURRENT NATIONAL REGISTER STATUS: Eligible. SHPO Finding. 7/18/90.

NATIONAL REGISTER RECOMMENDATION: Not Eligible

SUMMARY: The 1875 three-span brick arch bridge no longer carries the load of traffic. A Bailey truss was placed over the arches in 1985 due to the crumbling state of the southern arch. The arches are skewed using a ribbed pattern consisting of approximately 2' wide ribs stepped along the intrados. The arches have a stone fascia, and a metal railing that was placed in 1926. The ribbed brick intrados is a unique feature, but the state of deterioration has destroyed the structural integrity of the arches.

PHOTO: 157:28-33 REVIEWED BY: EPV/AGL DATE: 05/92 QUAD: Roselle





HISTORIC AMERICAN ENGINEERING RECORD

INDEX TO PHOTOGRAPHS

LINCOLN AVENUE/HIGH STREET BRIDGE
(Union County Bridge No. 45/2003045)

HAER No. NJ-84

Spanning the Rahway River at Lincoln Avenue and High Street
Cranford

Union County

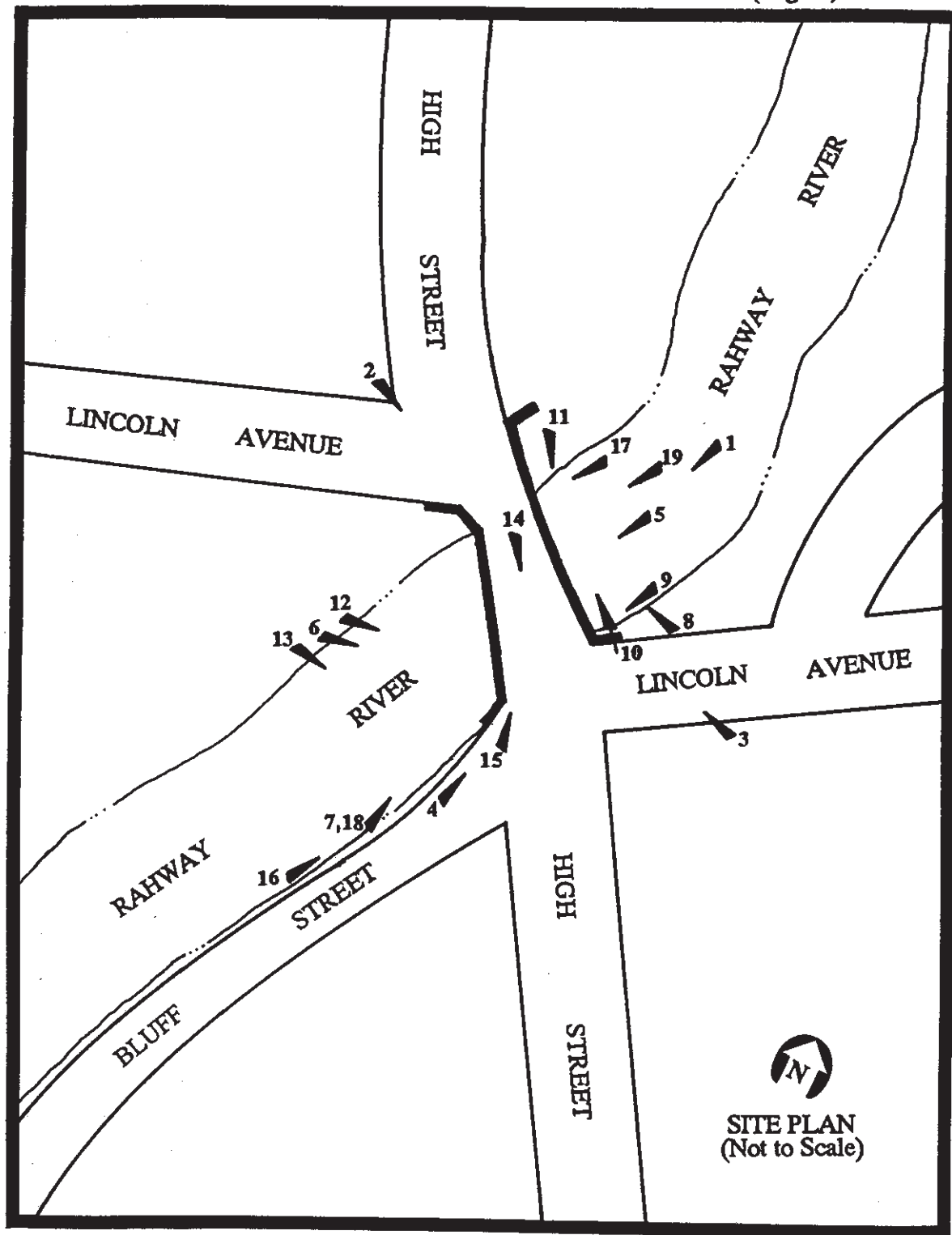
New Jersey

Photographer: Rob Tucher

March and April 1993

- NJ-84-1 GENERAL VIEW SOUTHWEST SHOWING MILL AND BRIDGE
- NJ-84-2 VIEW SOUTHEAST, APPROACH FROM NORTHWEST SIDE
- NJ-84-3 VIEW WEST, APPROACH FROM HIGH STREET
- NJ-84-4 VIEW NORTH, APPROACH FROM BLUFF STREET
- NJ-84-5 VIEW SOUTHWEST OF EAST ELEVATION
- NJ-84-6 VIEW EAST OF SOUTHWEST ELEVATION
- NJ-84-7 OBLIQUE VIEW NORTH OF SOUTHWEST ELEVATION
- NJ-84-8 OBLIQUE VIEW WEST OF NORTHEAST ELEVATION, WESTERNMOST
TWO ARCHES AND CONSTRUCTION DETAIL
- NJ-84-9 VIEW SOUTHWEST, DETAIL OF EASTERNMOST ARCH ON
NORTHEAST ELEVATION
- NJ-84-10 VIEW NORTHWEST BEHIND NORTHEAST ELEVATION SHOWING
CONSTRUCTION DETAILS
- NJ-84-11 VIEW SOUTHEAST BEHIND NORTHEAST ELEVATION SHOWING
CONSTRUCTION DETAILS
- NJ-84-12 VIEW EAST, DETAIL OF EASTERNMOST ARCH ON SOUTHWEST
ELEVATION, ALSO SHOWS KEYSTONE, TURNBUCKLE AND
RAILING
- NJ-84-13 VIEW EAST OF SOUTHWEST ELEVATION/SOUTHEAST SIDE
ABUTMENT
- NJ-84-14 VIEW SOUTHEAST ACROSS BRIDGE OF FORMER ROADWAY
- NJ-84-15 VIEW NORTH, DETAIL OF GUARD RAIL AND END POST

Key to Photographs
Lincoln Avenue/High Street Bridge
(Union County Bridge No. 45/2003045)
HAER No. NJ-84 (Page 3) 2



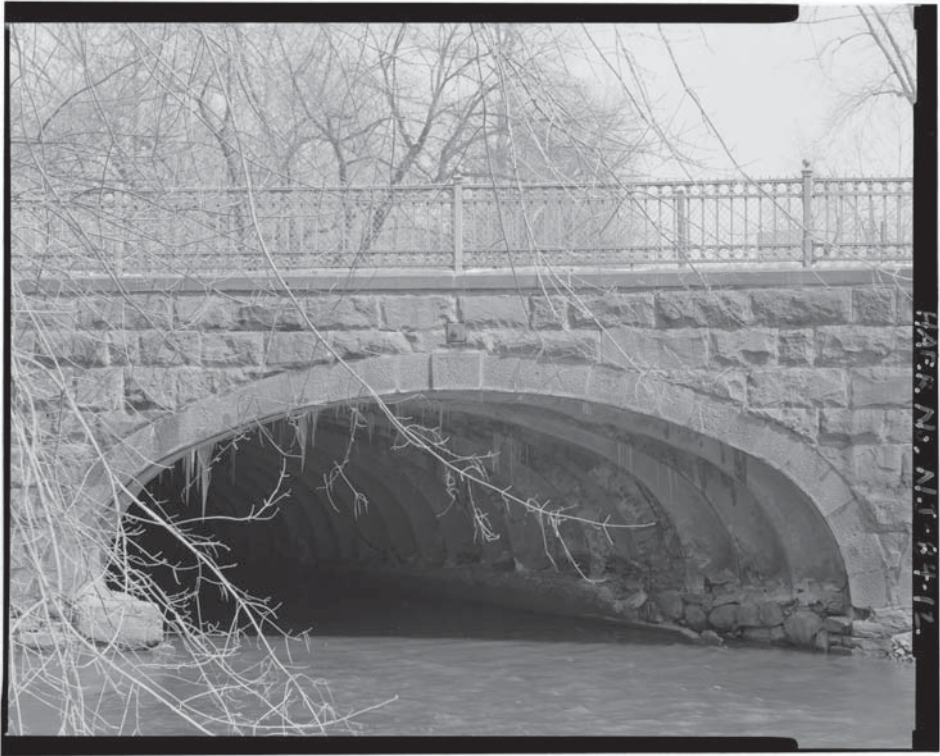


















HAER N. N. J. 84-15

Inventory of Identified Architectural Resources			
Property	Property Information	Description	NRHP Eligibility
 <p>Photograph Number: 116</p>	<p>Panamerican Resource Number: 116</p> <p>Droescher's Mill and Dam</p> <p>347 Lincoln Avenue</p> <p>Municipality: Cranford, Union County</p> <p>UTM coordinates (1983 Zone 18N): 4500184N, 559080E</p> <p>Date: 1821 (mill); 1985 (dam)</p> <p>Other: This resource falls under the Cranford Section (ID #4424,027) of the NRE Rahway River Parkway HD.</p>	<p>Style: Stories: 2 with basement Plan: rectangular Roof: gambrel Exterior: wood siding Foundation: rock Windows: 6/6 wood with storm windows Details: The dam was rebuilt by Union County in 1985; the mill building itself has undergone a number of changes and additions.</p>	<p>The mill and millrace are listed on the NRHP (NR #74001192). The dam appears to be a complete rebuild of dams that were traditionally at this site. Panamerican recommends the dam as not eligible (see PRN 96).</p>
 <p>Photograph Number: 117</p>	<p>Panamerican Resource Number: 117</p> <p>Bridge (NJDOT #2003045) at Lincoln Avenue crossing the Rahway River at Droescher's Mill</p> <p>Municipality: Cranford, Union County</p> <p>UTM coordinates (1983 Zone 18N): 4500159N, 559035E</p> <p>Date: ca. 2010</p> <p>Other: This bridge was recorded because it was directly inside the 100' APE across the river.</p>	<p>Style: concrete stringer Stories: na Plan: na Roof: na Exterior: enclosed concrete balustrade with rubble rock veneer topped by decorative open work metal railings and lanterns Foundation: concrete abutments Windows: na Details: The current bridge replaced an earlier bridge in this location. The current bridge design blends with the park and residential neighborhood.</p>	<p>Although this is a county bridge and not yet 50 years old, it should be considered a contributing element to both the NRE Rahway River Parkway HD (ID #4079) and Union County Park System HD (ID #4424).</p>



CRANFORD
Union Co.

* 200330
R 33/23

Vreeland Home
306 Lincoln Ave. E.
looking S.

1981

Form 10-300
(Rev. 6-72)UNITED STATES DEPARTMENT OF THE INTERIOR
NATIONAL PARK SERVICENATIONAL REGISTER OF HISTORIC PLACES
INVENTORY - NOMINATION FORM

(Type all entries - complete applicable sections)

STATE:	New Jersey
COUNTY:	Union
FOR NPS USE ONLY	
ENTRY DATE	JAN 8 1974

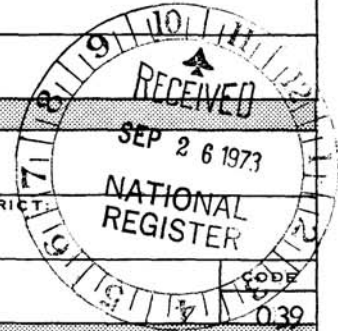
1. NAME			
COMMON:			
Droeschers Mill (Rahway River Mill)			
AND/OR HISTORIC:			
Williams Mill			
2. LOCATION			
STREET AND NUMBER:			
347 Lincoln Ave. East			
CITY OR TOWN:		CONGRESSIONAL DISTRICT:	
Cranford		Twelveth	
STATE	CODE	COUNTY:	CODE
New Jersey	34	Union	039

3. CLASSIFICATION			
CATEGORY (Check One)	OWNERSHIP	STATUS	ACCESSIBLE TO THE PUBLIC
<input type="checkbox"/> District <input type="checkbox"/> Site <input type="checkbox"/> Object	<input checked="" type="checkbox"/> Building <input type="checkbox"/> Structure <input type="checkbox"/> Both	<input type="checkbox"/> Public <input checked="" type="checkbox"/> Private <input type="checkbox"/> Both	Public Acquisition: <input type="checkbox"/> In Process <input type="checkbox"/> Being Considered
		<input checked="" type="checkbox"/> Occupied <input type="checkbox"/> Unoccupied <input type="checkbox"/> Preservation work in progress	Yes: <input checked="" type="checkbox"/> Restricted <input type="checkbox"/> Unrestricted <input type="checkbox"/> No
PRESENT USE (Check One or More as Appropriate)			
<input type="checkbox"/> Agricultural <input checked="" type="checkbox"/> Commercial <input type="checkbox"/> Educational <input type="checkbox"/> Entertainment	<input type="checkbox"/> Government <input type="checkbox"/> Industrial <input type="checkbox"/> Military <input type="checkbox"/> Museum	<input type="checkbox"/> Park <input type="checkbox"/> Private Residence <input type="checkbox"/> Religious <input type="checkbox"/> Scientific	<input type="checkbox"/> Transportation <input type="checkbox"/> Other (Specify) _____ _____ _____

4. OWNER OF PROPERTY			
OWNER'S NAME:			
Robert and Nancy Brooks			
STREET AND NUMBER:			
105 Columbia Ave.			
CITY OR TOWN:		STATE:	CODE
Cranford		New Jersey	34

5. LOCATION OF LEGAL DESCRIPTION			
COURTHOUSE, REGISTRY OF DEEDS, ETC:			
Union County Court House			
STREET AND NUMBER:			
Broad St.			
CITY OR TOWN:		STATE:	CODE
Elizabeth		New Jersey	34

6. REPRESENTATION IN EXISTING SURVEYS			
TITLE OF SURVEY:			
New Jersey Historic Sites Inventory 813.5			
DATE OF SURVEY: 1972			
<input type="checkbox"/> Federal <input checked="" type="checkbox"/> State <input type="checkbox"/> County <input type="checkbox"/> Local			
DEPOSITORY FOR SURVEY RECORDS:			
Historic Sites Section of Department of Environmental Protection			
STREET AND NUMBER:			
Box 1420			
CITY OR TOWN:		STATE:	CODE
Trenton		New Jersey	34



SEE INSTRUCTIONS

H12 3-1-1000 2-15-10000000

STATE:	New Jersey
COUNTY:	Union
ENTRY NUMBER	JAN 8 1974
DATE	A-144

FOR NPS USE ONLY

7. DESCRIPTION

CONDITION

(Check One)
☐ Excellent ☒ Good ☐ Fair ☐ Deteriorated ☐ Ruins ☐ Unexposed

(Check One)

☒ Altered ☐ Unaltered

(Check One)

☐ Moved ☒ Original Site

DESCRIBE THE PRESENT AND ORIGINAL (if known) PHYSICAL APPEARANCE

Present Appearance:Exterior

The siding is horizontal novelty siding, painted white laid over 2" thick tongue and groove siding, and installed at a 45° angle. Windows are double hung 6 over 6 with plain trim. Glass appears quite old with many imperfections and is slightly green in color. Two large windows approximately 12 ft. wide, 10 ft. high occur at the ends in the front of the building. They consist 2' x 2' panes set in 5/4 specially cut continuous frames. The window at the south end of the building has been in place for some time: probably early 20th Century. The north end window was installed in 1971 to match the other and to replace a horizontal, double, sliding barn type door.

The main entrance at the front consists of a separate enclosure with double doors. Two doors at the building wall line provide access to the first floor area and to the stairs to the second floor. Siding is the same as the rest of the building. Interior walls of the entrance porch are recently installed walnut panelling over plaster walls.

Another front subentrance is located at the North end front, in the brick wall addition. In 1971 the opening formerly covered with a horizontal metal clad sliding door was recently replaced with a colonial period door, complete with trim.

At the second floor level there is a set of double doors complete with pivoted windows. Construction is typical "barn door" type.

The North (up - river) structure is single story brick walled with a sloping roof, pitched toward the river.

The windows at the second floor level are dormer type with 6 over 6 double hung sash.

The roofing is now asphalt shingles over cedar shakes. Under the shakes are roofing planks.

Structural Frame

Vertical 6" x 6" timbers on an approximate 2' - 0" center form the main structural frame. They rest on a 4" x 6" sill plate and are cross-braced by the 2" tongue and groove sub-siding applied directly to the 6" x 6".

First floor joists are 3' x 16' members approximately 16" on center, with support from a beam at the half - way position. Rough flooring is 2" tongue and groove.

SEE INSTRUCTIONS

B. SIGNIFICANCE**PERIOD (Check One or More as Appropriate)**

- ☐ Pre-Columbian | ☐ 16th Century | ☒ 18th Century | ☐ 20th Century
☐ 15th Century | ☐ 17th Century | ☐ 19th Century

SPECIFIC DATE(S) (If Applicable and Known)**AREAS OF SIGNIFICANCE (Check One or More as Appropriate)**

- | | | | |
|--|---------------------------------------|--|--|
| <input type="checkbox"/> Aboriginal | <input type="checkbox"/> Education | <input type="checkbox"/> Political | <input type="checkbox"/> Urban Planning |
| <input type="checkbox"/> Prehistoric | <input type="checkbox"/> Engineering | <input type="checkbox"/> Religion/Philosophy | <input type="checkbox"/> Other (Specify) _____ |
| <input type="checkbox"/> Historic | <input type="checkbox"/> Industry | <input type="checkbox"/> Science | _____ |
| <input type="checkbox"/> Agriculture | <input type="checkbox"/> Invention | <input type="checkbox"/> Sculpture | _____ |
| <input type="checkbox"/> Architecture | <input type="checkbox"/> Landscape | <input type="checkbox"/> Social/Humanitarian | _____ |
| <input type="checkbox"/> Art | <input type="checkbox"/> Architecture | <input type="checkbox"/> Theater | _____ |
| <input checked="" type="checkbox"/> Commerce | <input type="checkbox"/> Literature | <input type="checkbox"/> Transportation | _____ |
| <input type="checkbox"/> Communications | <input type="checkbox"/> Military | | |
| <input type="checkbox"/> Conservation | <input type="checkbox"/> Music | | |

STATEMENT OF SIGNIFICANCE

The documented history of what is now Cranford contains reports of many mills on the Rahway River. In early times prior to 1801, there were two portions: Craneville and Williams Farms. This continued until 1864 when the name of Cranford was adopted.

G. W. Thomas wrote in 1920 in the "Cranford Citizen and Chronicle" that in 1710 Benjamin Williams built a dam across the Rahway River near the present High Street Bridge and started a primitive saw mill, and later established a woolen mill. Other mills are suggested, but not mentioned. This was along an old stage coach road now known as Lincoln Avenue. The present mill stands on this same site although the age of the present structure is not fully documented, except to place it somewhere before the Revolution.

The present "Droescher's Mill" (formerly Williams Mill) is the last of eleven mills along the Raritan River, extending from Cranford to Clark and then to Rahway. It is a part of the Master Plan for the development of a Heritage Corridor in Cranford along the Rahway River. It now contains the start of a museum of milling and water powered machines, as well as artifacts obtained in the area.

The history of the Mill, its use and occupancy is as follows.

The site of the Mill had a crude saw mill somewhere near the present buildings structure. This was operated by a Squire Williams, formerly of Elizabethtown somewhere before 1737. Further, it is reported that the first structure was erected in 1737 by Squire Williams. At that time, he maintained a farm to the east of the Mill and towards Elizabethtown.

A pre-revolutionary map of Elizabethtown indicates the road to "Squire Williams Farm". Lincoln Avenue was once known also as "Kings Highway" and extended from the East, what is now known as North Avenue through Cranford and again hitting North Avenue toward the Garwood line and thence going to Lincoln and Philadelphia. It is reported by Dr. Homer Hall that the Swift Sure Railroad operated a stage coach on "Kings Highway."

BIBLIOGRAPHICAL REFERENCES						
<p>G. W. Thomas, "Regarding Cranford's Naming", <u>Cranford Citizen and Chronicle</u>, July 28th, 1921</p> <p>Bragdon, William B, Cranford Historical Society, <u>Cranford Citizen and Chronicle</u>, 1937</p> <p>Hall, Dr. Homer, "300 Years at Crane's Ford" <u>Cranford Citizen and Chronicle</u>, 1937</p> <p>Mendell, W.W. <u>Sketches of Cranford's Early Days</u>, after 1871.</p> <p>Hatfield, Reverend Edwin. <u>History of Elizabeth</u>. 1868.</p> <p>Architecture. New Jersey. March, 1972.</p>						
10. GEOGRAPHICAL DATA						
LATITUDE AND LONGITUDE COORDINATES DEFINING A RECTANGLE LOCATING THE PROPERTY			O R	LATITUDE AND LONGITUDE COORDINATES DEFINING THE CENTER POINT OF A PROPERTY OF LESS THAN TEN ACRES		
CORNER	LATITUDE	LONGITUDE		LATITUDE	LONGITUDE	
	Degrees Minutes Seconds	Degrees Minutes Seconds		Degrees Minutes Seconds	Degrees Minutes Seconds	
NW	° ' "	° ' "		40° 39' 01"	74° 18' 06"	
NE	° ' "	° ' "				
SE	° ' "	° ' "				
SW	° ' "	° ' "				
APPROXIMATE ACREAGE OF NOMINATED PROPERTY: <u>one half acre</u>						
LIST ALL STATES AND COUNTIES FOR PROPERTIES OVERLAPPING STATE OR COUNTY BOUNDARIES						
STATE:	CODE	COUNTY	CODE			
STATE:	CODE	COUNTY:	CODE			
STATE:	CODE	COUNTY:	CODE			
STATE:	CODE	COUNTY:	CODE			

RECEIVED
 SEP 26 1973
 NATIONAL
 REGISTER

11. FORM PREPARED BY			
NAME AND TITLE:			
<u>(Robert Brooks, Consulting Engineer) Historic Sites Staff (Lois Richman)</u>			
ORGANIZATION			DATE
<u>Department of Environmental Protection Historic Sites</u>			<u>July 13, 1973</u>
STREET AND NUMBER:			
<u>John Fitch Plaza, Labor and Industry Building</u>			
CITY OR TOWN:		STATE	CODE
<u>Trenton</u>		<u>New Jersey</u>	<u>34</u>

12. STATE LIAISON OFFICER CERTIFICATION	NATIONAL REGISTER VERIFICATION
<p>As the designated State Liaison Officer for the National Historic Preservation Act of 1966 (Public Law 89-665), I hereby nominate this property for inclusion in the National Register and certify that it has been evaluated according to the criteria and procedures set forth by the National Park Service. The recommended level of significance of this nomination is:</p> <p style="text-align: center;">National <input type="checkbox"/> State <input type="checkbox"/> Local <input checked="" type="checkbox"/></p> <p>Name <u>Richard J. Sullivan</u></p> <p style="text-align: center;">Richard J. Sullivan</p> <p>Title <u>Commissioner</u></p> <p style="text-align: center;">Department of Environmental Protection</p> <p>Date <u>September 18, 1973</u></p>	<p>I hereby certify that this property is included in the National Register.</p> <p style="text-align: center;"><u>W. R. Mortenson</u></p> <p style="text-align: center;">Director, Office of Archeology and Historic Preservation</p> <p>Date <u>1/8/74</u></p> <p>ATTEST:</p> <p style="text-align: center;"><u>Charles H. Herring</u></p> <p style="text-align: center;">Acting Keeper of The National Register</p> <p>Date <u>1-7-74</u></p>

SEE INSTRUCTIONS

UNITED STATES DEPARTMENT OF THE INTERIOR
NATIONAL PARK SERVICE

STATE	New Jersey
COUNTY	Union
FOR NPS USE ONLY	
ENTRY NUMBER	DATE
	JAN 8 1974



NATIONAL REGISTER OF HISTORIC PLACES
INVENTORY - NOMINATION FORM

(Continuation Sheet)

7. Condition
New Jersey 34
Union County 039
Droescher's Mill (Rahway River Mill)

Second floor joists are 2" x 16" with two together. A portion near the center of the building has a center support beam with supporting 8" x 8" columns. Rough flooring is 2" tongue and groove. A portion of the second floor has a 2" layer of concrete as a finished floor.

Attic floor joists are 2" x 16" with 1" rough pine flooring. Roof rafters are 2" x 12" arranged with a dutch roof style. Part of the roof system contains a 12" x 12" horizontal member for the exterior hoist to the second floor double door.

Interior Finishes

Upon acquiring the Mill, the sheet rock covering the 6" x 6" vertical main chambers was removed. 2" x 3" nailers were installed against the siding at each column, 3" insulation put in place and sheet rock was applied, thus exposing the columns which were hand finished. The sheet rock was painted with a sand coat paint, giving the appearance of a sand base plaster. Interior partitions are a mixture of walnut vertical grain panelling, painted plaster board, and burlap covered fiberboard. Several partitions are old pine wide board flooring and have been retained intact.

Foundation

The foundation is composed largely of stone, with some areas of brickwork apparently added to close up original openings. Portions of the foundation vary, in that different types were used in different areas. The South end is largely composed of field sandstone which could have been found in the local area. The center portion contains cut sandstone, a hard blue stone, and a hard stone with mica infusion. The latter two types are not found in this area and would have been imported from Morris or some of the Northern Counties in New Jersey.

The basement floor is at two levels. The Southern end is about 2'-6" higher than the northern end and is not hard surface. The lower portion is a hard surface concrete type material that apparently was laid over previous surfaces. It appears quite old itself, but excavation for a sump pump revealed a brick floor followed by field stone. Various machine foundations with mounting bolts are still present.

UNITED STATES DEPARTMENT OF THE INTERIOR
NATIONAL PARK SERVICE

NATIONAL REGISTER OF HISTORIC PLACES
INVENTORY - NOMINATION FORM

(Continuation Sheet)

STATE New Jersey	
COUNTY Union	
FOR NPS USE ONLY	
ENTRY NUMBER JAN 8 1974	DATE

(Number all entries)

7. Condition (2)
New Jersey 34
Union County 039

Millrace

The Millrace is composed largely of stone containing a mica schist. The mortar gives the appearance of a lime mortar, quite old. A poured concrete section provides a well for the present horizontal turbine which replaced the original undershot wheel. The race for a wheel has been covered over with concrete, but did exist.

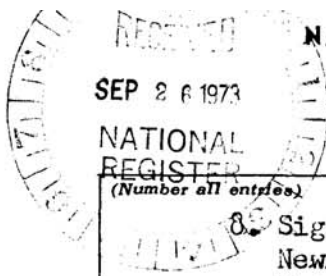
The tail race foundation is exposed at low river water, but the wall has been destroyed. An aerial photograph taken in 1925 shows the tailrace intact.

The horizontal turbine, while not operative now is to be restored and put into operation to operate machinery being assembled in the basement.

Original Appearance

There is a photograph and a free hand pencil sketch which shows that a shed was constructed over the Millrace and Tailrace in the late 1890's. This is no longer a part of the Mill. The photograph indicates that there may have been other building programs since the mill was first built.



NATIONAL REGISTER OF HISTORIC PLACES
INVENTORY - NOMINATION FORM

(Continuation Sheet)

STATE New Jersey	
COUNTY Union	
FOR NPS USE ONLY	
ENTRY NUMBER	DATE
	JAN 8 1974

NATIONAL
REGISTER

(Number all entries)

8. Significance (continued)

New Jersey 34

Union County 039

It is not definitely known what use Squire Williams made of the Mill however; it is known that during the Revolutionary War blankets were produced at the Mill.

Further, the British tried to burn down the Mill as it was supplying blankets to the Revolutionaries. It is presumed that the blanket manufacturing continued during and to the Civil War Era.

During the Civil War mini-balls were produced at the Mill. Evidence indicates that the Calvary spurs were also produced at the Mill as there are a large number of spur blanks which have been found not only around the Mill but also in the river. H.E. Sostman, a tenant prior to 1971, uncovered a finished calvary spur within the wall. *CHALKY.*

After the Civil War and until 1900 or thereabouts a Crosson Lyons operated a saw mill in the basement of the Mill. This has been verified by a nephew by personal conversation with him during 1972.

Around 1900 Severn Droescher purchased the Mill and it is believed to have been from a man named Vreeland.

Severn Droescher operated an oil stone manufacturing works and a cutlery importing and processing business.

A number of the oil stones are found in the foundation and throughout the area of the Mill. He transferred title to the Suburban Trust Company. The Suburban Trust Company sold the building to Lett Organ Company who held it for a few short years and it was then sold to Dr. Neil Castaldo. Dr. Castaldo operated the Mill as a rental property until purchased by the writer in 1971.

Congressional Representatives

Senators - Harrison Williams

Clifford Case

Congreeman - Mathew S. Renaldo

NEW JERSEY OFFICE OF HISTORIC PRESERVATION HISTORIC SITES INVENTORY # 200329
INDIVIDUAL STRUCTURE SURVEY FORM

HISTORIC NAME: Williams-Droescher Mill
LOCATION: 347 Lincoln Ave., East

COMMON NAME:
BLOCK/LOT 482/2

MUNICIPALITY: Cranford

COUNTY: Union

USGS QUAD: Roselle

UTM REFERENCES:

OWNER/ADDRESS: Kwonick, Harold & Judith
904 Boulevard, Westfield, N.J.

Zone/Northing/Easting

DESCRIPTION

Construction Date: 1720/1830

Source of Date: Documentation

Architect:

Builder:

Style: Mill - Vernacular

Form/Plan Type: Rectangular Shape

Number of Stories: 2½

Foundation: Fieldstone and brick

Exterior Wall Fabric: Ship-lap wooden siding

Fenestration: Eight bays. 6/6 sash windows

Roof/Chimneys: Gambrel roof

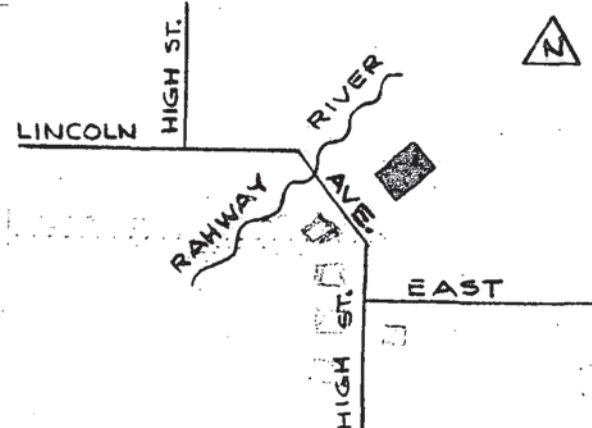
Additional Architectural Description:

Now adapted to office space.

On the National Register of Historic Places.

PHOTO Negative File # Roll 9 Frame 13

Map (Indicate North)



SETTING, BOUNDARY DESCRIPTION, AND RELATED STRUCTURES:

The mill sits on a small triangular lot surrounded by a chain link fence.

SURROUNDING ENVIRONMENT: Urban ☐ Suburban ☒ Scattered Buildings ☐
Open Space ☐ Woodland ☐ Residential ☒ Agricultural ☐
Industrial ☐ Downtown Commercial ☐ Highway Commercial ☐ Other ☐

Surrounded by early 20th Century suburban homes.

SIGNIFICANCE:

Only remaining mill on the Rahway River, which was once lined with mills of this type.

During the Revolutionary War, the mill was known as Williams Factory. Woolen blankets were manufactured here for the American Army. Cider was also pressed at the mill. During the Civil War, the mill again supplied woolen blankets to the army, as well as cannon balls and riding stirrups.

The mill ceased operations in the late 19th Century. It was rehabilitated and remodelled inside to serve as offices in the 1960's.

Droescher's Mill has long been a landmark and symbol of Cranford.

ORIGINAL USE: Mill	PRESENT USE: Offices
PHYSICAL CONDITION: Excellent <input type="checkbox"/> Good <input checked="" type="checkbox"/>	Fair <input type="checkbox"/> Poor <input type="checkbox"/>
REGISTER ELIGIBILITY: Yes <input checked="" type="checkbox"/> Possible <input type="checkbox"/>	No <input type="checkbox"/> Part of District <input type="checkbox"/>
THREATS TO SITE: Roads <input type="checkbox"/> Development <input type="checkbox"/>	Zoning <input type="checkbox"/> Deterioration <input type="checkbox"/>
No Threat <input checked="" type="checkbox"/> Other <input type="checkbox"/>	
COMMENTS:	

Placed on the National Register of Historic Places, 1/8/74.

REFERENCES:

Union County Landmarks Inventory, 1976.
Cranford Historical Society

RECORDED BY: JWF
ORGANIZATION: UCCHPAB

DATE: 1981



347 LINCOLN AVE.

CRANFORD

Williams or Proescher's Mill

19th C.



* 200329

R9/13

347 Lincoln Ave.
Cranford, Union Co.

Roll 9
1980

Inventory of Identified Architectural Resources

Property	Property Information	Description	NRHP Eligibility
 <p>Photograph Number: 116</p>	<p>Panamerican Resource Number: 116</p> <p>Droescher's Mill and Dam</p> <p>347 Lincoln Avenue</p> <p>Municipality: Cranford, Union County</p> <p>UTM coordinates (1983 Zone 18N): 4500184N, 559080E</p> <p>Date: 1821 (mill); 1985 (dam)</p> <p>Other: This resource falls under the Cranford Section (ID #4424.027) of the NRE Rahway River Parkway HD.</p>	<p>Style: Stories: 2 with basement Plan: rectangular Roof: gambrel Exterior: wood siding Foundation: rock Windows: 6/6 wood with storm windows Details: The dam was rebuilt by Union County in 1985; the mill building itself has undergone a number of changes and additions.</p>	<p>The mill and millrace are listed on the NRHP (NR #74001192). The dam appears to be a complete rebuild of dams that were traditionally at this site. Panamerican recommends the dam as not eligible (see PRN 96).</p>
 <p>Photograph Number: 117</p>	<p>Panamerican Resource Number: 117</p> <p>Bridge (NJDOT #2003045) at Lincoln Avenue crossing the Rahway River at Droescher's Mill</p> <p>Municipality: Cranford, Union County</p> <p>UTM coordinates (1983 Zone 18N): 4500159N, 559035E</p> <p>Date: ca. 2010</p> <p>Other: This bridge was recorded because it was directly inside the 100' APE across the river.</p>	<p>Style: concrete stringer Stories: na Plan: na Roof: na Exterior: enclosed concrete balustrade with rubble rock veneer topped by decorative open work metal railings and lanterns Foundation: concrete abutments Windows: na Details: The current bridge replaced an earlier bridge in this location. The current bridge design blends with the park and residential neighborhood.</p>	<p>Although this is a county bridge and not yet 50 years old, it should be considered a contributing element to both the NRE Rahway River Parkway HD (ID #4079) and Union County Park System HD (ID #4424).</p>

FEDERAL BUREAU OF HISTORIC PRESERVATION HISTORIC SITES INVENTORY # 200327
INDIVIDUAL STRUCTURE SURVEY FORM

HISTORIC NAME: Cranford Hall
LOCATION: 600 Lincoln Park East

COMMON NAME: Cranford Hall Nursing Home
BLOCK/LOT 505/2

MUNICIPALITY: Cranford

COUNTY: Union

USGS QUAD: Roselle

UTM REFERENCES:

OWNER/ADDRESS: Cranford Hall Nursing Home
600 Lincoln Park E., Cranford, N.J.

Zone/Northing/Easting

DESCRIPTION

Construction Date: ca. 1912

Source of Date: Documentation

Architect:

Builder:

Style: "Norman Castle"

Form/Plan Type: Irregular

Number of Stories: Three

Foundation: Stone

Exterior Wall Fabric: Rough cut hexagonal shaped stone, laid to create "honey comb" effect.

Fenestration: Irregular bays. Various types of bay windows, arched windows, and square windows used.

Roof/Chimneys: Gable and hipped roof. Red tile. Four stone chimneys.

Additional Architectural Description:

Stone on first floor and in crenellated tower.

Main entry in base of tower, through a porch of massive timber supports and Gothic carvings.

Half-timber effect on second floor.

Driveway leads through an arch to main "courtyard."

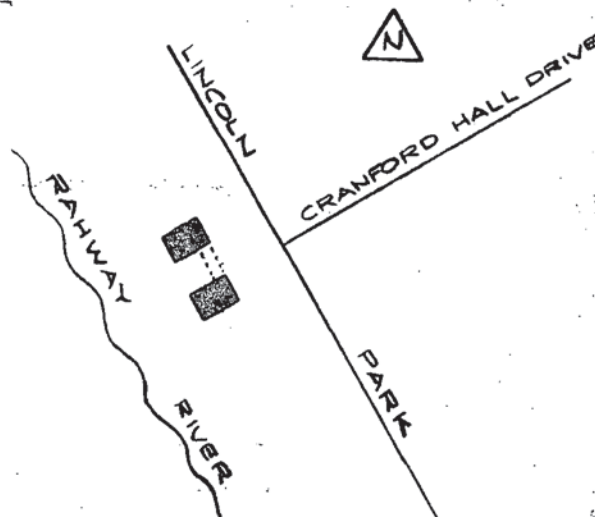
Stables and service areas are divided from main house by the arch.

Modern enlargements of the nursing home have not significantly altered the main structure, but the service wing of the house has been changed more to accommodate services for the nursing home.

Additions have respected the courtyard form and the sweep of lawn from the house to the river.

PHOTO Negative File # Roll 9 Frame 14

Map (Indicate North)



A-155

Located along the Rahway River. The original grounds of Cranford Hall included all the property along the present Lincoln Park roadway.

SURROUNDING ENVIRONMENT: Urban ☐ Suburban ☒ Scattered Buildings ☐
Open Space ☐ Woodland ☐ Residential ☒ Agricultural ☐
Industrial ☐ Downtown Commercial ☐ Highway Commercial ☐ Other ☐

Most of the original property for Cranford Hall was sold off, and garden apartments were erected along Lincoln Park. South of Cranford Hall is an area of large, single family, modern residences.

SIGNIFICANCE:

Cranford Hall was built for the Kaltenbach family before World War I, and probably around 1912. The Kaltenbach's were wealthy New Yorkers, who had made their money in silk and other textiles.

Cranford Hall was the largest and most elaborate of any house in Cranford. The stone and half-timbered building effectively evokes the presence of an English castle, but on a charming residential scale.

The Kaltenbach's lost a great deal of money during the Depression, and were forced to sell the house. It was purchased for use as a nursing home, and has been used as such ever since.

ORIGINAL USE: Residence

PRESENT USE: Nursing Home

PHYSICAL CONDITION: Excellent ☐ Good ☒

Fair ☐ Poor ☐

REGISTER ELIGIBILITY: Yes ☒ Possible ☐

No ☐ Part of District ☐

THREATS TO SITE: Roads ☐ Development ☐

Zoning ☐ Deterioration ☐

No Threat ☒ Other ☐

COMMENTS:

Well maintained, and successfully adapted to a new use at an early date, which undoubtedly preserved this especially large residence.

REFERENCES:

Cranford Historical Society

RECORDED BY: JWF
ORGANIZATION: UCCHPAB

DATE: 1981



LINCOLN PARK E.

CRANFORD

Nursing Home

* 200327
Roll 9/14

680 Lincoln Park E.
Cranford, Union Co.
looking N.W.

Roll 9
1981

ICE OF HISTORIC PRESERVATION HISTORIC SITES INVENTORY # 200328

INDIVIDUAL STRUCTURE SURVEY FORM

HISTORIC NAME: Droescher's Bridge COMMON NAME:
 LOCATION: Along banks of Rahway River, north of intersection of Lincoln Park E. and Lincoln Ave. BLOCK/LOT 482/1
 MUNICIPALITY: Cranford COUNTY: Union
 USGS QUAD: Roselle UTM REFERENCES:
 OWNER/ADDRESS: Union County Park Commission land. Zone/Northing/Easting

DESCRIPTION Bridge Bases
 Construction Date: ca. 1912

Source of Date: Documentation

Architect:

Builder:

Style:

Form/Plan Type:

Number of Stories:

Foundation:

Exterior Wall Fabric:

Fenestration:

Roof/Chimneys:

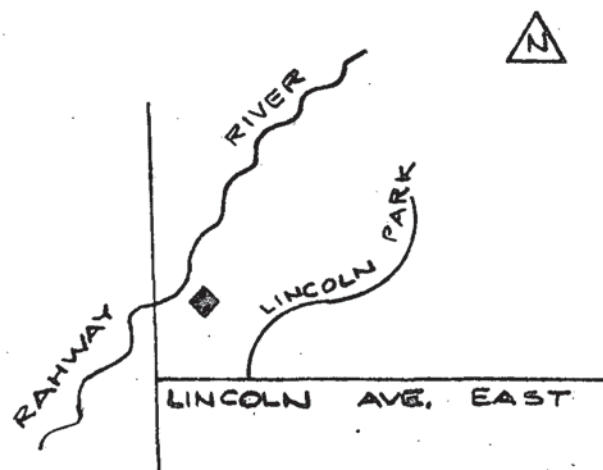
Additional Architectural Description:

Cobblestone bridge base. Four cobblestone pillars mark corners of approximately 8'x8' concrete and stone platform above the Rahway River. Part of the S.R. Droescher estate at the turn-of-the-century.

The four pillars were the base of a foot bridge over the mill race of Droescher's mill. The bridge was erected in an era when the annual river carnivals on the Rahway River were a major social event, and all kinds of water-related structures were erected to enhance the beauty of the river.

PHOTO Negative File # Roll 33 frame 17A, 18A

Map (Indicate North)



SURROUNDING ENVIRONMENT: Urban ☐ Suburban ☒ Scattered Buildings ☐
Open Space ☒ Woodland ☐ Residential ☐ Agricultural ☐
Industrial ☐ Downtown Commercial ☐ Highway Commercial ☐ Other ☐

Located in the open space corridor along the Rahway River Parkway.

SIGNIFICANCE:

The Rahway River was the focus of social activity in the late 19th and early 20th Centuries in Cranford. River Carnivals were festive summer events, and everyone would participate, or promenade the banks. The local historical society (and probably many residents) have photographs of the decorated boats, the picnics, the shady, romantic coves enjoyed by Cranford residents. The riverbank has been preserved as open space, primarily for flood control reasons. However, very few people use the Rahway River today. The ruins of the footbridge are now a mysterious ruin, but noted by many residents as marking a "favorite spot" for contemplation, and imagining the "Water Carnivals" of the past.

ORIGINAL USE: Bridge base

PHYSICAL CONDITION: Excellent ☐ Good ☐

REGISTER ELIGIBILITY: Yes ☐ Possible ☐

THREATS TO SITE: Roads ☐ Development ☐

No Threat ☐ Other ☐

COMMENTS:

PRESENT USE: Ruins

Fair ☒ Poor ☐

No ☒ Part of District ☐

Zoning ☐ Deterioration ☒

REFERENCES:

Cranford Historical Society

Central Board of Trade. Cranford, New Jersey, 1913.

RECORDED BY: JWF
ORGANIZATION: UCCHPAB

DATE: 1981



CRANFORD

Union Co.

* 2003 28

33/17

looking N.E.

Droescher's Bridge
Btw. Lincoln Park & Lincoln Ave
+ Rahway River

1981

#200328
 CRANFORD, Union Co. R 33/18
 WATER CARNIVAL
 BRIDGE BASC



Droescher's Bridge
 Btw. Lincoln Park E., Lincoln
 Ave., & Rahway River

looking S.



1981





Inventory of Identified Architectural Resources

Property	Property Information	Description	NRHP Eligibility
 <p>Photograph Number: 114</p>	<p>Panamerican Resource Number: 114</p> <p>Stone Overlook Walls</p> <p>Lincoln Park</p> <p>Municipality: Cranford, Union County</p> <p>UTM coordinates (1983 Zone 18N): 4500315N, 559130E</p> <p>Date: ca. 1917</p> <p>Other: South of the skating pool is a series of stone overlook walls and paths, most of which are in poor condition.</p>	<p>Style: Rustic</p> <p>Stories: na</p> <p>Plan: na</p> <p>Roof: na</p> <p>Exterior: Rusticated cut stone in rubble course</p> <p>Foundation: stone</p> <p>Windows: na</p> <p>Details: These overlooks and knee walls more or less line the river between the skating pond and the mill. Residents have claimed a few of the more stable overlooks and outfitted with benches and tables.</p>	<p>Lincoln Park is a contributing element to the both NRE Railway River Parkway HD (ID #4079) and NRE Union County Park System HD (ID #4424). The stone overlooks and walls are eligible as contributing elements to NRE Lincoln Park.</p>
 <p>Photograph Number: 115</p>	<p>Panamerican Resource Number: 115</p> <p>Remnants of Pavilion</p> <p>Lincoln Park</p> <p>Municipality: Cranford, Union County</p> <p>UTM coordinates (1983 Zone 18N): 4500217N, 559108E</p> <p>Date: ca. 1917</p> <p>Other: This resource falls under the Cranford Section (ID #4424.027) of the NRE Railway River Parkway HD.</p>	<p>Style: Rustic</p> <p>Stories: 1</p> <p>Plan: Square</p> <p>Roof: missing</p> <p>Exterior: open pavilion with stone rubble columns</p> <p>Foundation: concrete</p> <p>Windows: na</p> <p>Details: This small pavilion, missing its roof, leads right to the Railway River. Its use is unclear.</p>	<p>Lincoln Park is a contributing element to the both NRE Railway River Parkway HD (ID #4079) and NRE Union County Park System HD (ID #4424). The remnants of the small pavilion are eligible as contributing elements to NRE Lincoln Park.</p>

Inventory of Identified Architectural Resources

Property	Property Information	Description	NRHP Eligibility
 <p>Photograph Number: 114</p>	<p>Panamerican Resource Number: 114</p> <p>Stone Overlook Walls</p> <p>Lincoln Park</p> <p>Municipality: Cranford, Union County</p> <p>UTM coordinates (1983 Zone 18N): 4500315N, 559130E</p> <p>Date: ca. 1917</p> <p>Other: South of the skating pool is a series of stone overlook walls and paths, most of which are in poor condition.</p>	<p>Style: Rustic</p> <p>Stories: na</p> <p>Plan: na</p> <p>Roof: na</p> <p>Exterior: Rusticated cut stone in rubble course</p> <p>Foundation: stone</p> <p>Windows: na</p> <p>Details: These overlooks and knee walls more or less line the river between the skating pond and the mill. Residents have claimed a few of the more stable overlooks and outfitted with benches and tables.</p>	<p>Lincoln Park is a contributing element to the both NRE Railway River Parkway HD (ID #4079) and NRE Union County Park System HD (ID #4424). The stone overlooks and walls are eligible as contributing elements to NRE Lincoln Park.</p>
 <p>Photograph Number: 115</p>	<p>Panamerican Resource Number: 115</p> <p>Remnants of Pavilion</p> <p>Lincoln Park</p> <p>Municipality: Cranford, Union County</p> <p>UTM coordinates (1983 Zone 18N): 4500217N, 559108E</p> <p>Date: ca. 1917</p> <p>Other: This resource falls under the Cranford Section (ID #4424.027) of the NRE Railway River Parkway HD.</p>	<p>Style: Rustic</p> <p>Stories: 1</p> <p>Plan: Square</p> <p>Roof: missing</p> <p>Exterior: open pavilion with stone rubble columns</p> <p>Foundation: concrete</p> <p>Windows: na</p> <p>Details: This small pavilion, missing its roof, leads right to the Railway River. Its use is unclear.</p>	<p>Lincoln Park is a contributing element to the both NRE Railway River Parkway HD (ID #4079) and NRE Union County Park System HD (ID #4424). The remnants of the small pavilion are eligible as contributing elements to NRE Lincoln Park.</p>

Inventory of Identified Architectural Resources			
Property	Property Information	Description	NRHP Eligibility
 Photograph Number: 112	Panamerican Resource Number: 112 Riverside Inn No # North Avenue Municipality: Cranford, Union County UTM coordinates (1983 Zone 18N): 4500852N, 559096E Date: ca. 1920 Other: located at west end of bridge (see PRN 104)	Style: store front Stories: 3 Plan: rectangular Roof: flat Exterior: brick with Drivit facade Foundation: brick Windows: replacements of various types and sizes Details: The entire front façade of this building has been remodeled and any stylistic elements it once had are now gone.	This building has undergone a complete remodel that has obliterated any original stylistic elements, thereby destroying its integrity. Without integrity it cannot be eligible for the NRHP. Panamerican recommends it is not eligible for the NRHP.
 Photograph Number: 113	Panamerican Resource Number: 113 Former Pond Lincoln Park Municipality: Cranford, Union County UTM coordinates (1983 Zone 18N): 4500357N, 559112E Date: ca. 1917 Other: This resource is in the Cranford Section (ID #4424.027) of the NRE Rahway River Parkway HD.	Style: Skating/reflecting pond Stories: na Plan: kidney-shaped, shallow pool Roof: na Exterior: rimmed with stones and concrete Foundation: concrete and dirt Windows: na Details: Shallow pools were an important part of park design during the late nineteenth and early twentieth centuries. This pond is the remnants of Lincoln Park's pool, located right off the walking path.	Lincoln Park is a contributing element to the both NRE Rahway River Parkway HD (ID #4079) and NRE Union County Park System HD (ID #4424). The skating /reflecting pool is a contributing element to NRE Lincoln Park

DeLeuw, Cather and Co.
Engineering-Science, Inc.

NJ Transit Historic Railroad Bridge Survey
Structure Inventory Form

LOCATION

NJ TRANSIT Line	Raritan Valley	Milepost	16.66
Town/City	Cranford	Feature Crossed	Lincoln Avenue
County	Union	Common Name	(244) Lincoln Avenue
USGS Quad	Roselle	UTM Ref.	18.558380.4500090

PHYSICAL SUMMARY

Structure Type	Through Plate Girder	Deck Type	Ballasted
Overall Length	102'	Width	78'
Spans	2	Span Length	51'
Material(s)	Steel	Design Loading	E-60
Skew	46	Tracks	6
Inspection Report	No	Condition	Good
Bridge Typology Code	XX XX XX XX		

HISTORICAL SUMMARY

Date	1929	Date(s) Rebuilt	
Designer	CNJ	Fabricator	Bethlehem Steel Co.
Patent		Historic Rail Line	CNJ Central Div., Main Line

CULTURAL RESOURCE EVALUATIONS

National Register Status
State Register Status
Local Landmark Designation
HAER Doc. #
Type and Date of HAER documentation

National Register Date
State Register Date

GRAPHICS



and Co.
Engineering-Science, Inc.

NJ Transit Historic Railroad Bridge Survey
Structure Inventory Form

PHYSICAL DESCRIPTION OF BRIDGE AND SETTING

16.66 Lincoln Avenue is a two span steel through plate girder bridge supported by steel columns of built up cross braced construction. The abutments are concrete. The deck is a ballasted form.

STATEMENT OF TECHNOLOGICAL MERIT AND HISTORICAL SIGNIFICANCE

16.66 Lincoln Avenue was constructed as part of a grade crossing elimination in the town of Cranford by the CNJ during 1929. The project included four structures: 15.99 Centennial Avenue, 16.02 Rahway River, 16.23 Walnut Street and 16.66 Lincoln Avenue. This grade crossing elimination project does not have historical significance due to its late date and lack of associational characteristics. These projects, which had become a national issue by 1900, were sometimes characterized by highly publicized negotiations between the towns and railroads. The bridges in this project are also not considered technologically significant as railroad structures. The four bridges in Cranford were built on the Main Line of the CNJ's Central Division which provided passenger and freight service.

RECOMMENDATION OF ELIGIBILITY TO NATIONAL REGISTER OF HISTORIC PLACES

Not eligible

PRIMARY AND SECONDARY SOURCES

See New Jersey Transit Historic Railroad Bridge Survey, Report Bibliography.

Survey Team A. Cottrell, J. Galvin
Survey Date 08/01/90

Reviewer A.C.
Review Date 03/12/91

Photographs P18/6,7
Slides S18/5,6

New Jersey Department of Environmental Protection
Historic Preservation Office

Page 1 of 1

CONTINUATION SHEET

Historic Sites #:

Resource Name: Raritan Valley Line over Lincoln Avenue, MP 16.66

Municipality: Cranford

County: Union

Additional Information:

Representation in Existing Surveys:

This bridge was surveyed in 1991 as part of NJ Transit's *Historic Railroad Bridge Survey*.

National Register Eligibility:

The 1991 survey only evaluated the bridge for its individual eligibility, not as a component of a historic district. In that context, the bridge was evaluated as not eligible for inclusion in the National Register of Historic Places.

In 1995, the entire Central Railroad of New Jersey Main Line was determined to be eligible for inclusion in the National Register of Historic Places as a linear historic district. Therefore, the purpose of the current study is to determine if individual resources such as bridges contribute to the historic district's overall significance.

As a resource that falls within the historic district's period of significance, the bridge has been determined to contribute to the CRRNJ Main Line Historic District.

Attachments:

- 1991 NJ Transit Historic Railroad Bridge Survey form

Survey Name: NJ Transit Raritan Valley Line Study
Surveyor: Nancy L. Zerbe
Organization: ARCH², Inc.

Date: July, 1999

NEW JERSEY OFFICE OF HISTORIC PRESERVATION HISTORIC SITES INVENTORY # 200335
INDIVIDUAL STRUCTURE SURVEY FORM

HISTORIC NAME: _____ COMMON NAME: _____
 LOCATION: 18 Lincoln Avenue BLOCK/LOT 423/3
 MUNICIPALITY: Cranford COUNTY: Union
 USGS QUAD: Roselle UTM REFERENCES: _____
 OWNER/ADDRESS: Wright, Katherine Zone/Northing/Easting
 18 Lincoln Ave. E., Cranford, N.J.

DESCRIPTION

Construction Date: Mid 19th C. Source of Date: Visual
 Architect: _____ Builder: _____
 Style: Planbook type. Form/Plan Type: Side hall plan.
 Rectangular shape.

Number of Stories: 2½

Foundation: Stucco over original material.

Exterior Wall Fabric: Aluminum siding.

Fenestration: Three bay facade.

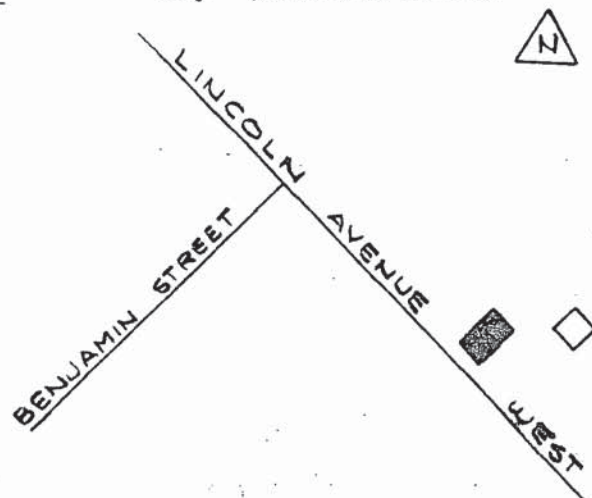
Roof/Chimneys: Gable roof. Cross gable centered on facade.

Additional Architectural Description:

Porch across first floor facade and side with pier supports.
 Tall sash windows - originally 2/2 sash, but now altered with
 Queen-Anne type multi-paned windows in top sash.

PHOTO Negative File # Roll 25, Frame 1

Map (Indicate North)



NAME, LOCATION, DESCRIPTION, AND RELATED STRUCTURES:

Barn in rear of property. Wooden plank siding.
Gable roof single dormer - now boarded shut.

SURROUNDING ENVIRONMENT: Urban ☐ Suburban ☒ Scattered Buildings ☐
Open Space ☐ Woodland ☐ Residential ☒ Agricultural ☐
Industrial ☐ Downtown Commercial ☐ Highway Commercial ☐ Other ☐

SIGNIFICANCE:

This small house fits into the mid 19th Century of Cranford's development. At that time, Cranford was more than just an agricultural area. The train came through, and a village sprang up around the tracks and main roads. Simple, neat homes like this one were built by merchants and employees of local businesses. It was a fairly large lot, and undoubtedly the family had a garden, but it was not the farmhouse of old; nor was it the suburban home of a New York commuter, the type of home which came to predominate in Cranford after the Civil War.

ORIGINAL USE: Residence

PHYSICAL CONDITION: Excellent ☐ Good ☒

REGISTER ELIGIBILITY: Yes ☐ Possible ☒

THREATS TO SITE: Roads ☐ Development ☐

No Threat ☒ Other ☐

COMMENTS:

PRESENT USE: Residence

Fair ☐ Poor ☐

No ☒ Part of District ☐

Zoning ☐ Deterioration ☐

REFERENCES:

Cranford Historical Society

RECORDED BY: JWF
ORGANIZATION: UCCHPAB

DATE: 1981



18 Lincoln Ave.
Cranford,
Union Co.
N.J.

#200335
R25/1

FEDERAL BUREAU OF HISTORIC PRESERVATION HISTORIC SITES INVENTORY #200336
INDIVIDUAL STRUCTURE SURVEY FORM

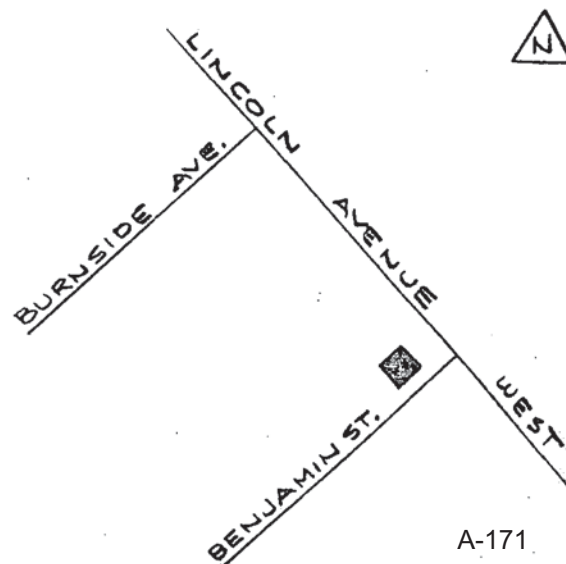
HISTORIC NAME:		COMMON NAME:	
LOCATION:	29 Lincoln Ave. W	BLOCK/LOT	420/2
MUNICIPALITY:	Cranford	COUNTY:	Union
USGS QUAD:	Roselle	UTM REFERENCES:	
OWNER/ADDRESS:	Gallant, Raymond 115 Ledden Terr., So. Plainfield, N.J.		Zone/Northing/Easting

DESCRIPTION

Construction Date:	18th C.	Source of Date:	Visual
Architect:		Builder:	
Style:	Colonial N.J. Farmhouse	Form/Plan Type:	Square plan much altered.
Number of Stories:	2½		
Foundation:	Rough cut sandstone.		
Exterior Wall Fabric:	Composition shingle siding.		
Fenestration:	Window placement much altered. Original two-bay rear remains.		
Roof/Chimneys:	Gable roof. Wide brick chimney - west side. Hexagonal chimney pots added above.		
Additional Architectural Description:	Simple brackets under front and rear eaves. 2½ story additions to west side. Composition shingle siding. Shed roof.		

PHOTO Negative File # Roll 33 frame 15A | 16A

Map (Indicate North)



RY DESCRIPTION, AND RELATED STRUCTURES:

House faces south, a feature typical of 18th Century homes. It is along Lincoln Avenue, which was part of the Old York Road, the 18th Century route from Philadelphia to Elizabethtown.

SURROUNDING ENVIRONMENT: Urban ☐ Suburban ☒ Scattered Buildings ☐
Open Space ☐ Woodland ☐ Residential ☒ Agricultural ☐
Industrial ☐ Downtown Commercial ☐ Highway Commercial ☐ Other ☐

The house is on a small, standard suburban lot, surrounded by frame houses from the early 20th Century.

SIGNIFICANCE:

Research is needed to confirm the 18th Century date for this house, but its form and its location along the Old York Road make a good case for this designation. It was originally of the three bay, side hall type common to early dwellings of this area. It probably even had a kitchen wing to the side. The present additions to the west side of the house may enclose or replace the early wing. The house has been altered over time, but it is one of the few survivors of Cranford's earliest recorded history.

ORIGINAL USE: Residence	PRESENT USE: Residence
PHYSICAL CONDITION: Excellent <input type="checkbox"/> Good <input type="checkbox"/>	Fair <input checked="" type="checkbox"/> Poor <input type="checkbox"/>
REGISTER ELIGIBILITY: Yes <input type="checkbox"/> Possible <input type="checkbox"/>	No <input checked="" type="checkbox"/> Part of District <input type="checkbox"/>
THREATS TO SITE: Roads <input type="checkbox"/> Development <input type="checkbox"/>	Zoning <input type="checkbox"/> Deterioration <input checked="" type="checkbox"/>
No Threat <input type="checkbox"/> Other <input checked="" type="checkbox"/>	
COMMENTS:	

REFERENCES:

Cranford Historical Society

RECORDED BY: JWF
ORGANIZATION: UCCHPAB

DATE: 1981



CRANFORD
Union Co.

* 209336
R33/15

29 Lincoln Ave. W.
looking W.

1981



CRANFORD, Union Co.
29 LINCOLN AV. WEST
looking W.

#200336
R33/16

1981

Historic, Architectural & Archeological Significance Survey

1. General Information

a. Facility Name United States Post Office, Cranford		b. Historic/Original Name United States Post Office, Cranford	
c. Finance/Subloc. No. 331905G01	d. Site Size (Sq. Ft.) 45,951	e. Site Size (Dimension) 93.0', 36.72', 316.50', 204.75'	
f. Property Address 3 Miln Street, Cranford, N.J. 07016	g. Building Size (Sq. Ft.) 35,600	h. Building Size (Dimensions) 191'-5" x 115'-7"	
	i. Is Building Open to Public? Yes		
j. Address of Office with Building Records (name and address of field office, region, etc. with official file.) Field Real Estate and Buildings Office, 90 Church St., New York, N.Y. 10007		k. Original Use Post Office	
		l. Present Use Post Office	

2. Property Appearance (see attached)

a. Description of General Area (Describe neighborhood, historic district, land use & direct or indirect effect upon other building of historic interest. If more space is needed, attach additional sheets.)	b. General Condition of Property (Site and Building)
	d. Description of Building Material (Roofs, walls, foundation, interior features, floor and ceiling, etc.)
c. Description of Floorplan (Attach drawings if available.)	
e. Description of unusual or unique subterranean features	

3. Architectural History

a. Date Designed 1934		b. Date Constructed 1936		c. Date Restored/Renovated 1971		d. Date Moved to Present Site N/A
e. Original Owner (Name & Biographical Data) Department of Treasury United States Post Office				f. Original Builder (Name & Biographical Data) Arthur Verner Company		
g. Original Architect (Name & Biographical Data) Louis A. Simon Wesley Sherwood Bessell (see attached)				h. Other Significant Participants (Names, relation to building, biographical data) Gerald Foster (see attached)		
i. Restoration Architect or Designer (if building has been restored/renovated) Firme, Lyman, Firme and Reese, 60 Prince St., Elizabeth, N.J. 07208						

4. Architectural Significance (see attached)

Identify type, period, method of construction, artistic value, significant and distinguishable entity (See National Register criteria, 36 CFR 60.6, Criteria for Evaluation).

5. Historical Significance (see attached)

Identify the broad patterns of American history (National, state or local level) or historic persons with whom the property is associated. (See National Register Criteria.)

6. Conclusion and Recommendation (see attached)

Should this property be nominated to the National Register? ☒ Yes ☐ No

Give a brief statement to support your recommendation.

Signature Uiana D. Zakalak		Signature Robert F. Meadows	
red by		Final Recommendation by	
Title Senior Researcher		Title Architect	
Date 9/30/82		Date 9/30/82	
A-176			

2. PROPERTY APPEARANCE

2a. Description of general area

The Post Office is located in the central business district of Cranford. Across the street is a small triangular park which gives the area an appearance of a town square. On the other two sides of the triangle are small shops and the local theatre. There are no architecturally or historically significant buildings in the area of the Post Office.

The town of Cranford is mostly residential with two and three story older wood frame houses and some garden apartment complexes.

The Cranford Post Office is not in a historic district.

2b. General condition of property

Both the site and building are in generally good condition; no obvious repairs are necessary.

2c. Description of floorplan

The Cranford Post Office is a one story plus basement building. The main entrance is located in the original (southeast) portion of the building. Another entrance was added to the front of the building when it was expanded to the west. Both lead into the public lobby, a large rectangular space extending along the front facade of the building. The lobby has been divided in two; to the right is the lock box lobby and to the left is the service lobby. The work room is located behind the postal screen. In the southwest corner of the building and along the western facade are the current offices including the Post Master's, the Assistant Post Master's, the lunch room and the toilets.

The original first floor plan drawings show a small entrance vestibule leading into a public lobby at either end. The Post Master's office was on the right of the lobby in the southeast corner. The basement contained the swing room, boiler and fuel rooms, and storage area. The basement is now used primarily for storage. According to the Assistant Post Master, an eighty bed Civil Defense hospital is located in the front section of the basement.

The original drawings are located at the Field Real Estate and Buildings Office, 90 church Street, New York, New York.

2d. Description of building material

The Cranford Post Office is a one story skeletal frame building with masonry cladding. The exterior is finished in rough-face hand made brick in colors ranging from sherry red to salmon pink laid in Flemish bond with raked 1/2" joints. The terra cotta Roman brick-shaped window surrounds, quoins and coping were manufactured to match the brick in color and texture. The base of the building is of reddish brown to brown rough sawed face brownstone. The double hollow metal and glass pane entrance doors are approached by a concrete platform and stairs. The original doors were wood with a leaded glass transom above. The roof is slate, and the cornice is of wood.

The new addition was constructed to match the originals in materials. Pigmented cast concrete was used to simulate the red stone foundation; parapets were covered with slate to match the existing roof.

The interior of the public lobby is finished in a marble wainscot with plaster walls and ceiling above. The work areas have plaster walls and ceilings, and wood floors. The original interior of the lobby was finished in inlaid veneers with a marble floor.

2e. Description of unique or unusual subterranean features

The pre-construction site survey plans show a two and one half story wood frame building built according to the traditions of the Italian Renaissance with a belvedere on top. This house was destroyed in order to build the Post Office on the site.

3. ARCHITECTURAL HISTORY

3g. Original architect

The Cranford Post Office was designed during the administration of Louis A. Simon, Supervising Architect of the Department of Treasury from 1933 to 1939, best known for his conception of the Triangle, a major complex of federal buildings in Washington, D.C. Born in 1867, Simon graduated from the Massachusetts Institute of Technology in 1891 and entered the Supervising Architect's Office in 1896. From 1905 to 1933, Simon served as the Superintendent of Architecture under the leadership of "Judge" James A. Wetmore and was responsible for much of the Office's architectural direction during this time.

The consulting architect was Wesley Sherwood Bessell, who also designed the United States Post Office in Perth Amboy (1937), Christ Church Parish House, Hackensack (1922), and several private residences.

3h. Other significant participants

Gerald Foster, a local artist employed by the Treasury Department Section of Fine Arts during the Depression, painted the murals on the

walls of the public lobby. His three foot high mural, extending the length above the lock box walls, shows British redcoats running for cover from the fire of the charging American Revolutionaries. Foster described his historical work this way: "A British foraging party consisting partly of Hessian mercenaries is seen crossing Crane's Ford on the way back to British headquarters on Staten Island. They are surprised by a detachment of American soldiers who were characteristically on the lookout for these raids on New Jersey farms."

Foster painted the mural with the assistance of John Poehler and Henry Paulsen.

As a young man, Foster lived in Westfield, New Jersey, where he had his studio. He graduated from Princeton in 1923, and between 1923 and 1928 he studied at the National Academy, the Art Students League and the Tiffany Foundation. He is a member of the American Artists Professional League, the Princeton Architectural Society, the Westfield Art Association and the Modern Artists of New Jersey.

3i. Restoration architect or designer

1971 - Extension and modernization by Finne, Lyman, Finne and Reese, 60 Prince St., Elizabeth, New Jersey 07208.

4. ARCHITECTURAL SIGNIFICANCE

During the Depression, as a result of legislation passed to stimulate local economies and employ community members, some local architects were awarded contracts to design government sponsored buildings. Standardization continued to be a priority, with the Treasury Department providing standard floor plans for post offices of different sizes. Where practicable, individual treatment was given to exterior details. Such is the case with the Cranford Post Office. The local architect provided the exterior details for an otherwise very common post office type. The building facade, with its usual two bays on each side of the entrance doors, is embellished by a projecting central pavilion surmounted by a steeply pitched roof, reminiscent of a medieval turret. Perhaps the architect had in mind a simplified approach to modernism, especially to that in northern Europe in the first decades of the twentieth century, where such motifs were prevalent.

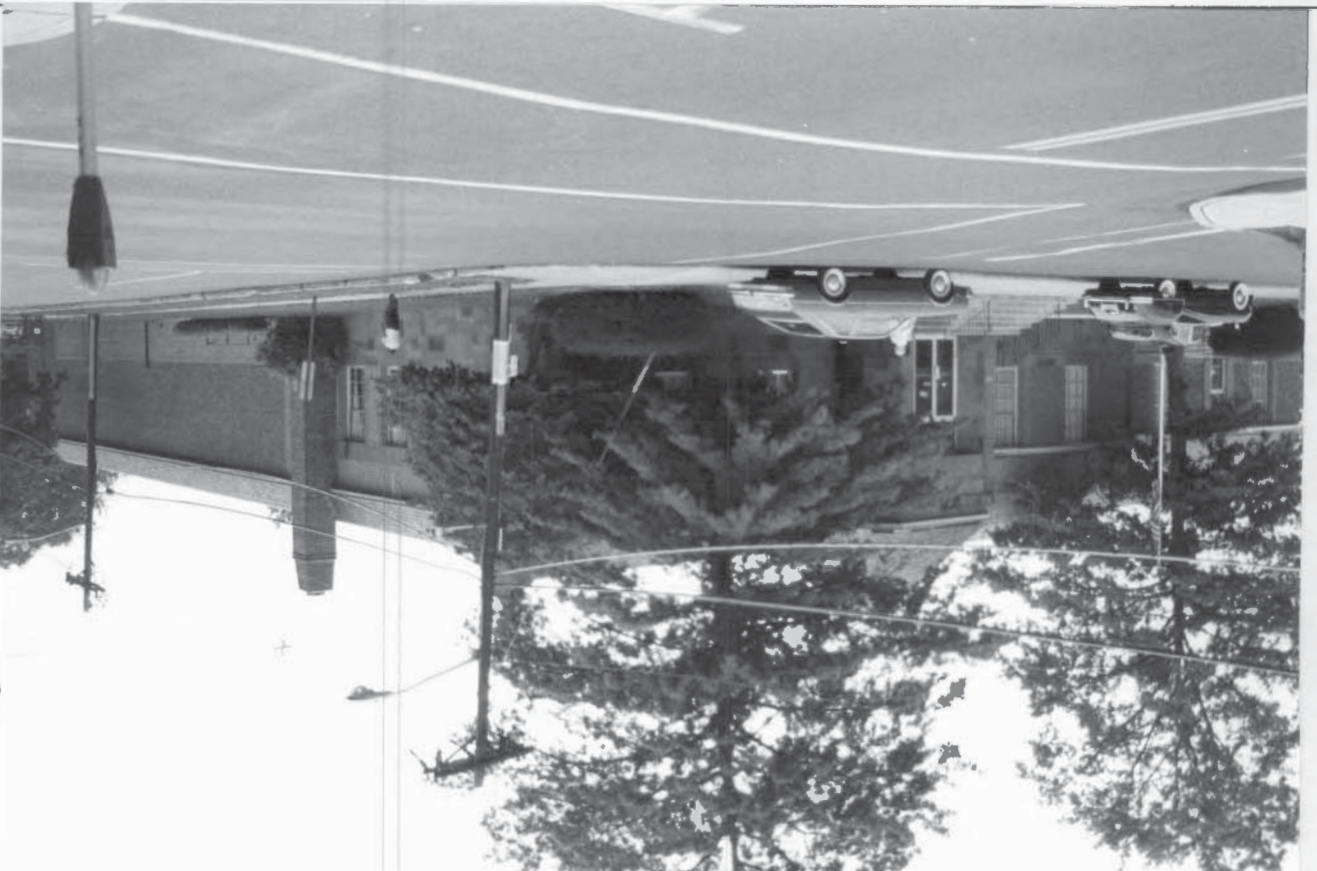
5. HISTORICAL SIGNIFICANCE

This Post Office was built under New Deal Legislation promoted by Franklin D. Roosevelt as a result of the Depression. Under the New Deal's Public Works Administration over \$700 million bought some 1,300 new buildings, the most important building program undertaken since the first few decades under the Constitution.

6. CONCLUSION AND RECOMMENDATION

No. The Cranford Post Office possesses minor architectural or historical significance and therefore, should not be nominated to the National Register.





Attach



115 Miln Street, Cranford, New Jersey	c. 1910, two-story, Dutch Colonial Revival-style house with asphalt shingle roofing, aluminum siding, paired windows, enclosed porch, pedimented entry with simple posts,	900 feet/NW	Not Eligible	n/a	B.5.18
211 Miln Street, Cranford, New Jersey	c. 1890, two-and-a-half story, vernacular Victorian style house with cross gable, asphalt shingle roof, aluminum siding, 1/1 windows, brick foundation, vergeboard at gable end of attic window.	300 feet/N	Not Eligible	n/a	B.5.20
215 Miln Street, Cranford, New Jersey	c.1890, two-and-a-half-story, vernacular Queen Anne-style residence, green asphalt shingle roofing, clapboard siding, cloche domed tower, wrap around porch with decorative woodwork.	300 feet/N	Not Eligible	n/a	B.5.22
12 Springfield Avenue, Cranford, New Jersey	c.1940, two-story, Colonial Revival-style residence, asphalt shingle roofing, shed roof dormers with casement windows, shingle siding, 6/6 windows at 2nd story, 6/9 windows at 1st story.	600 feet E	Not Eligible	n/a	B.5.23
First Church of Christ Scientist, 21 Springfield Street, Cranford, New Jersey	1908, Neo-Classical style, two-story, green tile hipped roof, stucco walls and foundation. Five bays with three central bays projecting, pedimented entry with Ionic columns, windows grouped in threes.	700 feet/NE	Eligible NJSHPO Site Inventory #200308	No Adverse Effect	B.5.25
24 Springfield Avenue, Cranford, New Jersey	c. 1940, three-story, Tudor Revival-style apartment building, slate roofing, brick exterior on lower two stories, upper 1/1/2 stories half-timbering. Replacement 1/1 paired windows.	800 feet/NE	not eligible	n/a	B.5.27

* Distances are rounded to the nearest 100 feet.



PHOTO LOCATOR MAP

**Plate B.5.18:**

115 Miln Street.
 Photo view: West
 Photographer: Elaine Kiernan Gold
 Date: August 1, 2001

115 Miln Street, Cranford, New Jersey	c. 1910, two-story, Dutch Colonial Revival-style house with asphalt shingle roofing, aluminum siding, paired windows, enclosed porch, pedimented entry with simple posts,	900 feet/NW	Not Eligible	n/a	B.5.18
211 Miln Street, Cranford, New Jersey	c. 1890, two-and-a-half story, vernacular Victorian style house with cross gable, asphalt shingle roof, aluminum siding, 1/1 windows, brick foundation, vergeboard at gable end of attic window.	300 feet/N	Not Eligible	n/a	B.5.20
215 Miln Street, Cranford, New Jersey	c.1890, two-and-a-half-story, vernacular Queen Anne-style residence, green asphalt shingle roofing, clapboard siding, cloche domed tower, wrap around porch with decorative woodwork.	300 feet/N	Not Eligible	n/a	B.5.22
12 Springfield Avenue, Cranford, New Jersey	c.1940, two-story, Colonial Revival-style residence, asphalt shingle roofing, shed roof dormers with casement windows, shingle siding, 6/6 windows at 2nd story, 6/9 windows at 1st story.	600 feet E	Not Eligible	n/a	B.5.23
First Church of Christ Scientist, 21 Springfield Street, Cranford, New Jersey	1908, Neo-Classical style, two-story, green tile hipped roof, stucco walls and foundation. Five bays with three central bays projecting, pedimented entry with Ionic columns, windows grouped in threes.	700 feet/NE	Eligible NJSHPO Site Inventory #200308	No Adverse Effect	B.5.25
24 Springfield Avenue, Cranford, New Jersey	c. 1940, three-story, Tudor Revival-style apartment building, slate roofing, brick exterior on lower two stories, upper 1/1/2 stories half-timbering. Replacement 1/1 paired windows.	800 feet/NE	not eligible	n/a	B.5.27

* Distances are rounded to the nearest 100 feet.



PHOTO LOCATOR MAP

**Plate B.5.20:**

211 Miln Street.
Photo view: Northwest
Photographer: Elaine Kiernan Gold
Date: August 1, 2001

115 Miln Street, Cranford, New Jersey	c. 1910, two-story, Dutch Colonial Revival-style house with asphalt shingle roofing, aluminum siding, paired windows, enclosed porch, pedimented entry with simple posts,	900 feet/NW	Not Eligible	n/a	B.5.18
211 Miln Street, Cranford, New Jersey	c. 1890, two-and-a-half story, vernacular Victorian style house with cross gable, asphalt shingle roof, aluminum siding, 1/1 windows, brick foundation, vergeboard at gable end of attic window.	300 feet/N	Not Eligible	n/a	B.5.20
215 Miln Street, Cranford, New Jersey	c.1890, two-and-a-half-story, vernacular Queen Anne-style residence, green asphalt shingle roofing, clapboard siding, cloche domed tower, wrap around porch with decorative woodwork.	300 feet/N	Not Eligible	n/a	B.5.22
12 Springfield Avenue, Cranford, New Jersey	c.1940, two-story, Colonial Revival-style residence, asphalt shingle roofing, shed roof dormers with casement windows, shingle siding, 6/6 windows at 2nd story, 6/9 windows at 1st story.	600 feet E	Not Eligible	n/a	B.5.23
First Church of Christ Scientist, 21 Springfield Street, Cranford, New Jersey	1908, Neo-Classical style, two-story, green tile hipped roof, stucco walls and foundation. Five bays with three central bays projecting, pedimented entry with Ionic columns, windows grouped in threes.	700 feet/NE	Eligible NJSHPO Site Inventory #200308	No Adverse Effect	B.5.25
24 Springfield Avenue, Cranford, New Jersey	c. 1940, three-story, Tudor Revival-style apartment building, slate roofing, brick exterior on lower two stories, upper 1/1/2 stories half-timbering. Replacement 1/1 paired windows.	800 feet/NE	not eligible	n/a	B.5.27



* Distances are rounded to the nearest 100 feet.





PHOTO LOCATOR MAP

**Plate B.5.22:**

215 Miln Street.
 Photo view: Northwest
 Photographer: Elaine Kiernan Gold
 Date: August 1, 2001

Inventory of Identified Architectural Resources			
Property	Property Information	Description	NRHP Eligibility
 Photograph Number: 74	Panamerican Resource Number: 74 Sewer Outfall Structure Riverside Drive north of Venetia Drive Municipality: Cranford, Union County UTM coordinates (1983 Zone 18N): 4502207N, 558314E Date: unknown Other: Located directly on the levee between Nomahegan and Memorial parks.	Style: na Stories: na Plan: na Roof: na Exterior: within a chain-link fence Foundation: concrete, raised Windows: na Details: This is a sewer outfall into the Rahway River.	Although the outfall structure is located within two historic districts, it is not eligible and not a non-contributing element to either one. If plans are made to upgrade or change the outfall, it should be made less conspicuous.
 Photograph Number: 75 (Richmond Avenue side)	Panamerican Resource Number: 75 Foot Bridge crossing Rahway River between Baltimore Parkway and Richmond Avenue Municipality: Cranford, Union County UTM coordinates (1983 Zone 18N): 4501841N, 558321E Date: unknown Other:	Style: Rustic Stories: na Plan: na Roof: na Exterior: wood balustrade Foundation: concrete abutments Windows: na Details: This bridge connects the levees running alongside the Rahway River from Nomahegan Park to the intersection of Riverside Drive and Springfield Avenue. The levees have walking paths on their tops and are used extensively in this area. This bridge connects the north and south Cranford neighborhoods to the park and its walking paths.	This bridge should be considered a contributing element to the NRE Rahway River Parkway HD (ID #4079) and NRE Union County Park System HD (ID #4424).

Inventory of Identified Architectural Resources

Property	Property Information	Description	NRHP Eligibility
 <p>Photograph Number: 104 (east bound lane balustrade and underside)</p>	<p>Panamerican Resource Number: 104</p> <p>Bridge (NJDOT #2008156) at North Avenue and Centennial Avenue crossing Rahway River</p> <p>Municipality: Cranford, Union County</p> <p>UTM coordinates (1983 Zone 18N): 4500874N, 559117E</p> <p>Date: 1965</p> <p>Other: Plaque indicates this is a State of New Jersey bridge.</p>	<p>Style: steel stringer</p> <p>Stories: na</p> <p>Plan: na</p> <p>Roof: na</p> <p>Exterior: closed concrete balustrade topped with metal guard rails</p> <p>Foundation: concrete abutments</p> <p>Windows: na</p> <p>Details: North Avenue (US 28) is an exceptionally busy thoroughfare and this bridge is a typical state bridge from the period.</p>	<p>Although this bridge is a state-owned property and is not ornamental, it should be considered a contributing element to both the NRE Rahway River Parkway HD (ID #4079) and NRE Union County Park System HD (ID #4424).</p>
 <p>Photograph Number: 105</p>	<p>Panamerican Resource Number: 105</p> <p>Cranford First Aid Squad</p> <p>No # Centennial Avenue</p> <p>Municipality: Cranford, Union County</p> <p>UTM coordinates (1983 Zone 18N): 4500823N, 559136E</p> <p>Date: ca. 1950</p> <p>Other: Building sits on the bank of the Rahway River.</p>	<p>Style: Modern</p> <p>Stories: 1</p> <p>Plan: rectangular</p> <p>Roof: flat</p> <p>Exterior: brick, stone, and concrete block</p> <p>Foundation: concrete</p> <p>Windows: replacement, sliding single pane</p> <p>Details: A low, multi-textured Modern-style building with sympathetically added ambulance bay. A very nice example of 1950s modern.</p>	<p>Not eligible; does not meet eligibility criteria</p>

Panamerican Consultants, Inc.

6-54

Rahway River Phase IA

New Jersey Department of Environmental Protection
Historic Preservation Office

Page 1 of 2

BASE SURVEY FORM

Historic Sites #:

Property Name: Cranford Junction Coach Yard

Street Address: Street #: _____ Apartment #: _____
(Low) (High) (Low) (High)

Prefix: _____ Street Name: _____ Suffix: _____ Type: _____

County(s): Union Zip Code: _____

Municipality(s): Cranford

Local Place Name(s): _____ Block(s): _____ Lot(s): _____

Ownership: Public USGS Quad: Roselle

Description:

The Cranford Coach Yard is located east of Cranford, in the vicinity of milepost marker 15.6. The yard currently contains a single track, portions of the roundhouse, and a small stone building adjacent to the roundhouse.

Registration and Status Dates:	National Historic Landmark: _____	SHPO Opinion: <u>7 / 19 / 91</u>
	National Register: <u> / /</u>	Local Designation: <u> / /</u>
	New Jersey Register: <u> / /</u>	Other Designation: _____
	Determination of Eligibility: <u>11 / 30 / 95</u>	Other Designation Date: <u> / /</u>

Photograph:



Survey Name: NJ Transit Raritan Valley Line Study
Surveyor: Nancy L. Zerbe
Organization: ARCH², Inc.

Date: August, 1999

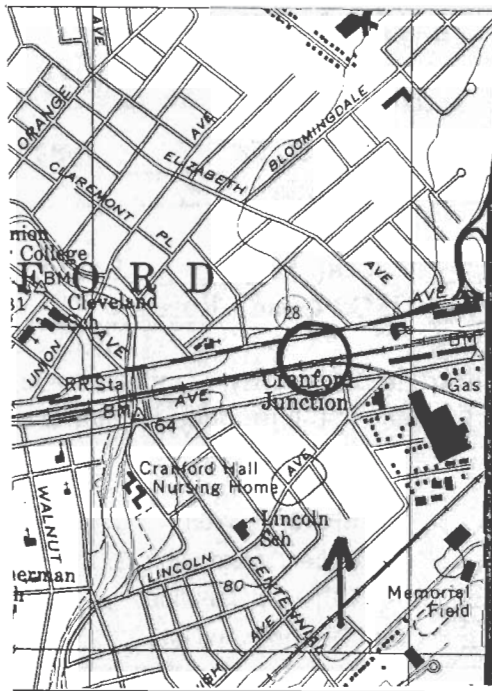
New Jersey Department of Environmental Protection
Historic Preservation Office

Page 2 of 2

BASE SURVEY FORM

Historic Sites #:

Location Map:



Site Map:

INTENSIVE-LEVEL USE ONLY
2.5" x 3.5"
(Include North Arrow and Bar Scale)

Bibliography/Sources:

Baer, Christopher. Unpublished research, 1998.
(Cont'd, See Continuation Sheet)

Additional Information:

The yard was built in 1912-1913 to replace the Roselle yard. In 1914, the CNJ built the 8-stall roundhouse and a coaling station. The facility is shown on 1913 and 1930 plans (copy attached) on file at NJ Transit.

More Research Needed?

☐ Yes

☒ No

INTENSIVE-LEVEL USE ONLY:

Attachments Included: ☒ Building/Element ☐ Landscape ☐ Farm
☐ Bridge ☐ Industry

Historic District: ☒ Yes ☐ No Historic District Name: CRRNJ Main Line

Status: ☐ Key Contributing ☒ Contributing ☐ Non-contributing

Associated Archaeological Site/Deposits? ☐ Yes ☒ No
(Known or potential sites - if yes, please describe briefly)

Survey Name: NJ Transit Raritan Valley Line Study
Surveyor: Nancy L. Zerbe
Organization: ARCH², Inc.

Date: August, 1999

New Jersey Department of Environmental Protection
Historic Preservation Office

Page 1 of 1

BUILDING/ELEMENT ATTACHMENT☒ BUILDING ☐ STRUCTURE ☐ OBJECT

Historic Sites #:

Common Name: Cranford Junction Roundhouse
Historic Name: _____
Present Use: Industrial Activity
Historic Use: Transportation and Movement Activity
Construction Date: 1914 **Source:** Baer, C. Unpublished research
Alteration Date(s): _____ **Source:** _____
Designer: _____ **Physical Condition:** Good
Builder: _____ **Remaining Historic Fabric:** Medium
Style: Colonial Revival
Form (B): _____ **Stories (B):** 1
Type (B,S): _____ **Bays (B):** 8
Roof Finish Materials (B,S): _____
Exterior Finish Materials: Brick

Exterior Description:

Brick roundhouse consisting of original western portion with 8-bay curved façade, and a sloping shed roof. Bays consist of brick foundation with projecting masonry cap and upper level concrete block with small rectangular windows at the top and bottom of the concrete block portion. Each bay is flanked by projecting masonry pilasters capped by medallions.

The south façade facing the main line consists of three brick bays, each with central areas filled in with concrete block and a small 6-light industrial window. Each bay is flanked by projecting brick pilasters and capped by brick corbeling at the eaves line.

(Cont'd, See Continuation Sheet)

Interior Description:

Not Accessible

Setting:

The roundhouse is located to the north of the main line at milepost 15.6. The immediate vicinity is mixed industrial and commercial. To the east is the Garden State Parkway.

Survey Name: NJ Transit Raritan Valley Line Study
Surveyor: Nancy L. Zerbe
Organization: ARCH², Inc.

Date: August, 1999

New Jersey Department of Environmental Protection
Historic Preservation Office

Page 1 of 1

CONTINUATION SHEET

Historic Sites #:

Cranford Junction Roundhouse Building/Element Attachment Form Cont'd...

Exterior Description:

The eastern façade of this portion (now primarily covered) reveals an upper story brick façade with large openings, and a wide overhanging eave with medallions.

To the east of this main section is a smaller brick section with a sloping shed roof which is sloped in the opposite direction of the main portion of the structure described above. This section also contains areas filled in with concrete block and small 6-light industrial windows, and is capped by brick corbeling at the eaves line.

At the extreme eastern end of the structure is a modern concrete block addition.

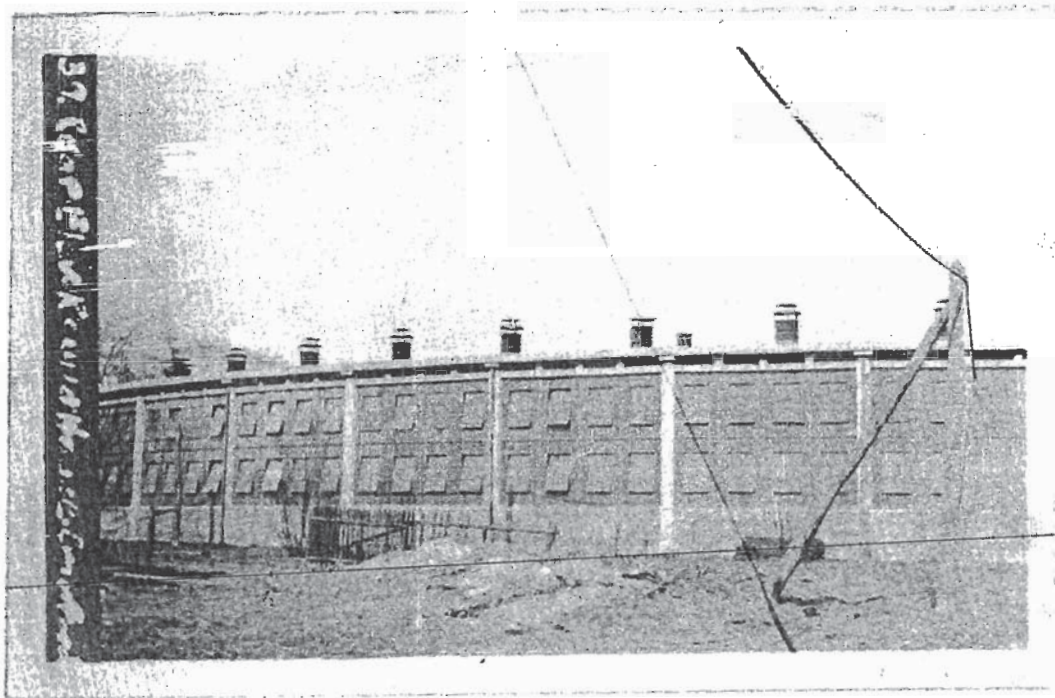
Photograph:



Cranford Roundhouse (West Façade)

Survey Name: NJ Transit Raritan Valley Line Study
Surveyor: Nancy L. Zerbe
Organization: ARCH², Inc.

Date: August, 1999



Engineer House/Boiler House/Machine Shop at Cranford, NJ, 1918. ICC, Division of Valuation. *Engineering Field Notes of Interstate Commerce Commission Parties Surveying the Physical Property of Railroads.*

Survey Name: NJ Transit Raritan Valley Line Study
Surveyor: Nancy L. Zerbe
Organization: ARCH², Inc.

Date: July, 1999

New Jersey Department of Environmental Protection
Historic Preservation Office

Page 1 of 1

BUILDING/ELEMENT ATTACHMENT

☒ BUILDING ☐ STRUCTURE ☐ OBJECT

Historic Sites #:

Common Name: Downtown Cranford Office
Historic Name: Cranford Junction Roundhouse Office
Present Use: Commercial Activity
Historic Use: Transportation and Movement Activity
Construction Date: _____ **Source:** _____
Alteration Date(s): _____ **Source:** _____
Designer: _____ **Physical Condition:** Fair
Builder: _____ **Remaining Historic Fabric:** Medium
Style: Craftsman
Form (B): Bungalow **Stories (B):** 1
Type (B,S): _____ **Bays (B):** 2
Roof Finish Materials (B,S): Asphalt Shingle
Exterior Finish Materials: Stucco

Exterior Description:

This single-story stuccoed structure has a cross-gabled roof covered with composition shingles. Replacement vinyl windows are located throughout the building. The north window is boarded up. The walls are covered with rough stucco. A small, shed-roof addition is attached to the rear of the building.

Interior Description:

Not Accessible

Setting:

The office is located adjacent to the roundhouse at the sidewalk along North Avenue. The roundhouse is located to the north of the main line tracks at milepost 15.6. The immediate vicinity is mixed industrial and commercial. To the east is the Garden State Parkway.

Survey Name: NJ Transit Raritan Valley Line Study
Surveyor: Stacy E. Spies
Organization: ARCH², Inc.

Date: September, 1999

New Jersey Department of Environmental Protection
Historic Preservation Office

Page 1 of 1

CONTINUATION SHEET

Historic Sites #:

Downtown Cranford Office Building/Element Attachement Form Cont'd...

Photograph: Downtown Cranford Office



Survey Name: NJ Transit Raritan Valley Line Study
Surveyor: Nancy L. Zerbe
Organization: ARCH², Inc.

Date: September, 1999

New Jersey Department of Environmental Protection
Historic Preservation Office

Page 1 of 1

CONTINUATION SHEET

Historic Sites #:

Cranford Junction Yard Cont'd...

Bibliography/Sources:

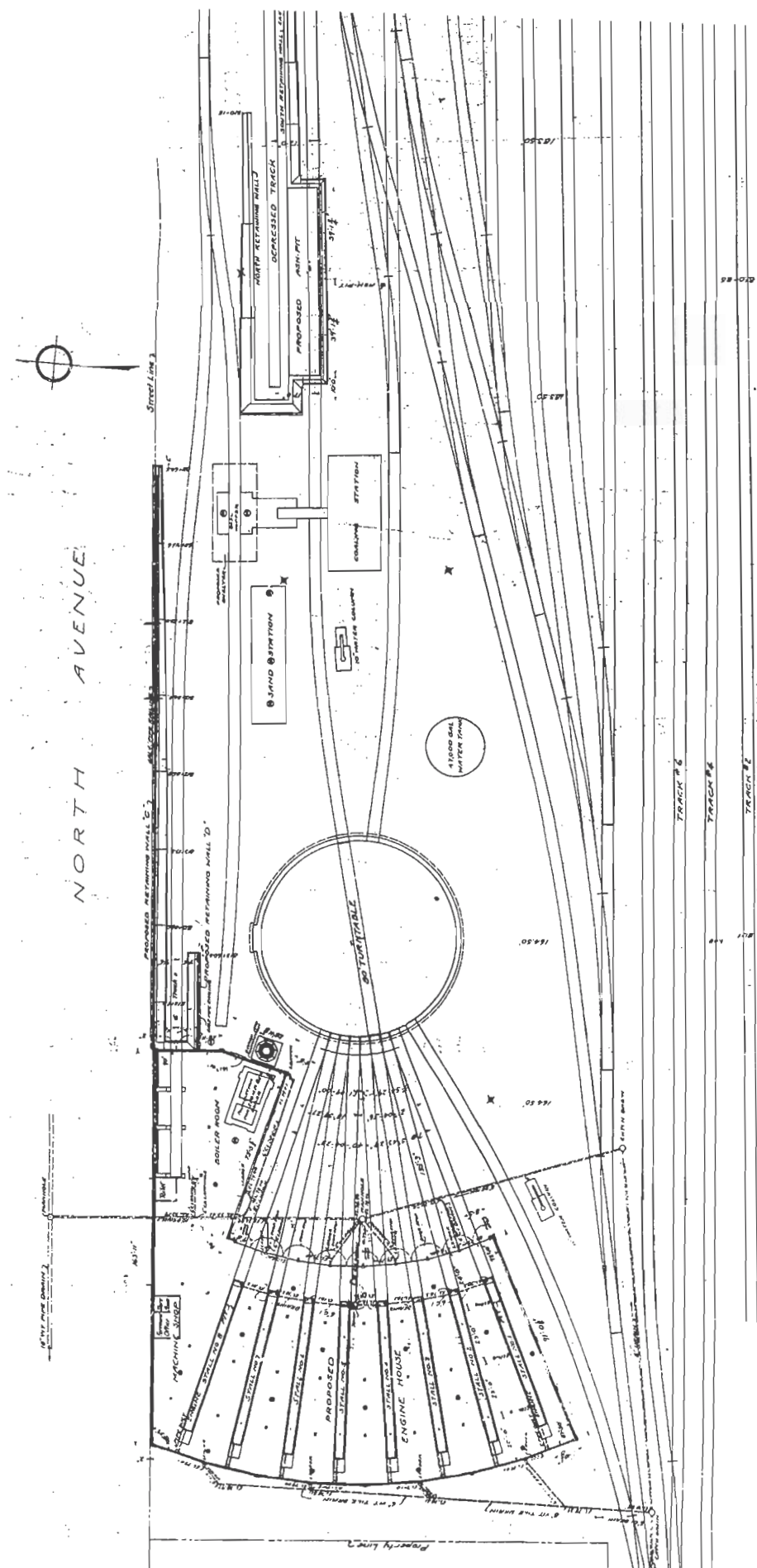
Cranford Yard. Proposed Suburban Terminal Engine House, Boiler Room and Machine Shop. October 6, 1913. Drawing on file at NJ Transit.

Cranford Yard. 1930 Grade Crossing Elimination Drawing Showing Roundhouse, Turntable, Sand Station, Coaling Station, Coal Hopper, and Ash Pit. Drawing on file at NJ Transit.

Interstate Commerce Commission, Division of Valuation. *Engineering Field Notes of Interstate Commrce Commission Parties Surveying the Physical Property of Railroads*, 1918 (revised 1923).

Survey Name: NJ Transit Raritan Valley Line Study
Surveyor: Nancy L. Zerbe
Organization: ARCH², Inc.

Date: August, 1999



Cranford Yard. Proposed Suburban Terminal Engine House, Boiler Room and Machine Shop. October 6, 1913. Drawing on file at NJ Transit.

Survey Name: NJ Transit Raritan Valley Line Study
Surveyor: Nancy L. Zerbe
Organization: ARCH², Inc.

Date: July, 1999

NEW JERSEY OFFICE OF HISTORIC PRESERVATION HISTORIC SITES INVENTORY #
INDIVIDUAL STRUCTURE SURVEY FORM

HISTORIC NAME: William Sperry Building
 LOCATION: Corner North Avenue & Alden St
 15-17 North Ave.
 MUNICIPALITY: Cranford
 USGS QUAD: Roselle
 OWNER/ADDRESS: Evelyn Biach
 10 Indian Spring Rd
 Cranford

COMMON NAME:
 BLOCK/LOT 192/6
 COUNTY: Union
 UTM REFERENCES:
 Zone/Northing/Easting

DESCRIPTION

Construction Date: 1906
 Source of Date: Plaque on building

Architect:
 Builder:

Style: Classical Revival Commercial
 Form/Plan Type: Commercial Building

Number of Stories: Three

Foundation: Brick

Exterior Wall Fabric: White brick with yellow brick stripes (facade). Rear: red brick

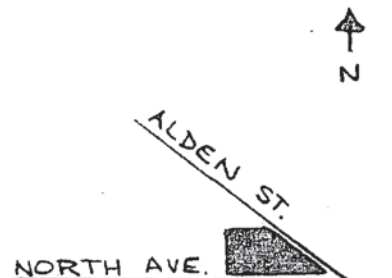
Fenestration: 3 Bay windows - All windows 1/1 sash with diamond pattern in upper sash.

Roof/Chimneys: Flat roof. Brick chimney

Additional Architectural Description: Building shape is irregular, conforming to unusual lot shape.
 Plaque on building facade: "William Sperry Building 1906"
 Rusticated stone lintels and sills.
 Bay windows accented with swag designs.
 Wide cornice.
 First floor storefront altered.

PHOTO Negative File # Roll 33 frame 33A, 32A

Map (Indicate North)



RY DESCRIPTION, AND RELATED STRUCTURES:

Located on a busy intersection in the commercial center of Cranford, and across North Avenue from the railroad station.

SURROUNDING ENVIRONMENT: Urban ☐ Suburban ☐ Scattered Buildings ☐
Open Space ☐ Woodland ☐ Residential ☐ Agricultural ☐
Industrial ☐ Downtown Commercial ☒ Highway Commercial ☐ Other ☐

SIGNIFICANCE:

William Sperry was a prominent figure in Cranford in the late 19th Century. He was a successful businessman, and brother of Thomas Sperry, the co-founder of the Sperry & Hutchison green stamp company. William Sperry was also a great promoter of Cranford, and was involved in many civic organizations. His business building in the center of Cranford has been a well known landmark since it was built in 1906.

The Sperry Building is the best-preserved of the turn-of-the-Century commercial buildings in downtown Cranford.

ORIGINAL USE: Commercial/Apartments

PHYSICAL CONDITION: Excellent ☒ Good ☐

REGISTER ELIGIBILITY: Yes ☒ Possible ☐

THREATS TO SITE: Roads ☐ Development ☐

No Threat ☒ Other ☐

COMMENTS:

PRESENT USE: Commercial/Apts.

Fair ☐ Poor ☐

No ☐ Part of District ☐

Zoning ☐ Deterioration ☐

REFERENCES:

Cranford Historical Society

RECORDED BY: JWF

DATE: 1981

ORGANIZATION: UCCHPAB



CRANFORD
Union Co.

* 200323
R 33/33

William Speery Bldg.
15-17 North Ave
looking N.E

1981





SPERRY BUILDING
CRANFORD
Union Co.

*200323
R 33/32

15-17 North Ave
looking N.

1981

Inventory of Identified Architectural Resources			
Property	Property Information	Description	NRHP Eligibility
 <p>Photograph Number: 112</p>	<p>Panamerican Resource Number: 112</p> <p>Riverside Inn</p> <p>No # North Avenue</p> <p>Municipality: Cranford, Union County</p> <p>UTM coordinates (1983 Zone 18N): 4500852N, 559096E</p> <p>Date: ca. 1920</p> <p>Other: located at west end of bridge (see PRN 104)</p>	<p>Style: store front</p> <p>Stories: 3</p> <p>Plan: rectangular</p> <p>Roof: flat</p> <p>Exterior: brick with Drivit facade</p> <p>Foundation: brick</p> <p>Windows: replacements of various types and sizes</p> <p>Details: The entire front facade of this building has been remodeled and any stylistic elements it once had are now gone.</p>	<p>This building has undergone a complete remodel that has obliterated any original stylistic elements, thereby destroying its integrity. Without integrity it cannot be eligible for the NRHP. Panamerican recommends it is not eligible for the NRHP.</p>
 <p>Photograph Number: 113</p>	<p>Panamerican Resource Number: 113</p> <p>Former Pond</p> <p>Lincoln Park</p> <p>Municipality: Cranford, Union County</p> <p>UTM coordinates (1983 Zone 18N): 4500357N, 559112E</p> <p>Date: ca. 1917</p> <p>Other: This resource is in the Cranford Section (ID #4424.027) of the NRE Rahway River Parkway HD.</p>	<p>Style: Skating/reflecting pond</p> <p>Stories: na</p> <p>Plan: kidney-shaped, shallow pool</p> <p>Roof: na</p> <p>Exterior: rimmed with stones and concrete</p> <p>Foundation: concrete and dirt</p> <p>Windows: na</p> <p>Details: Shallow pools were an important part of park design during the late nineteenth and early twentieth centuries. This pond is the remnants of Lincoln Park's pool, located right off the walking path.</p>	<p>Lincoln Park is a contributing element to the both NRE Rahway River Parkway HD (ID #4079) and NRE Union County Park System HD (ID #4424). The skating /reflecting pool is a contributing element to NRE Lincoln Park</p>

NEW JERSEY OFFICE OF HISTORIC PRESERVATION HISTORIC SITES INVENTORY # 200304
INDIVIDUAL STRUCTURE SURVEY FORM

HISTORIC NAME: Trinity Church
LOCATION: 205 North Ave. East

COMMON NAME: Trinity Episcopal Church
BLOCK/LOT 315/1

MUNICIPALITY: Cranford
USGS QUAD: Roselle
OWNER/ADDRESS: Trinity Episcopal Church
205 North Ave., Cranford

COUNTY: Union
UTM REFERENCES: Zone/Northing/Easting

DESCRIPTION

Construction Date: 1875

Source of Date: Documentation

Architect:

Builder:

Style: Gothic Revival

Form/Plan Type: T shape. Cross-gabled

Number of Stories: 1½

Foundation: Stucco

Exterior Wall Fabric: Stucco

Fenestration: 4 Bay side. Stained glass windows set in threes.

Roof/Chimneys: Gable roof. Stucco chimney.

Additional Architectural Description:

Triangular gabled dormer with bargeboard trim punctuates roof. Rose medallion window. Small buttresses applied to side of building. Entry on gable ends has crenelated porch with gothic label hoods over its windows; entry on the north has a door hood supported by trusses. Present square corner tower was once a pointed bell tower.

Some bargeboard trim has been removed from the gables.

PHOTO Negative File #Roll 20, Frame 2,4



Map (Indicate North)



SITING, BOUNDARY DESCRIPTION, AND RELATED STRUCTURES:

The church is located at the western end of a lot bounded by Hamilton, Forest and North Avenues. The lot is heavily developed with several support buildings: a rectangular, two-story gabled structure (1968) along Hamilton Ave.; two connected one-and-one-half story structures along North Ave.

SURROUNDING ENVIRONMENT: Urban ☐ Suburban ☒ Scattered Buildings ☐
Open Space ☐ Woodland ☐ Residential ☒ Agricultural ☐
Industrial ☐ Downtown Commercial ☐ Highway Commercial ☐ Other ☐

Located in residential neighborhood, largely developed in the late 19th Century.

SIGNIFICANCE:

The Protestant Episcopal congregation in Cranford was organized in 1872. The small group of families held services in their homes until 1875 when they completed the present church building.

It is a good example of the late Victorian Gothic Revival style. The building form is more solid, with more emphasis on shape than on surface decoration, as was true in the early 19th Century version of Gothic Revival.

Trinity Church is now the oldest church building in Cranford. Other congregations in Cranford are older, but have replaced their original buildings.

ORIGINAL USE: Church	PRESENT USE: Church
PHYSICAL CONDITION: Excellent <input checked="" type="checkbox"/> Good <input type="checkbox"/>	Fair <input type="checkbox"/> Poor <input type="checkbox"/>
REGISTER ELIGIBILITY: Yes <input type="checkbox"/> Possible <input type="checkbox"/>	No <input type="checkbox"/> Part of District <input checked="" type="checkbox"/>
THREATS TO SITE: Roads <input type="checkbox"/> Development <input type="checkbox"/>	Zoning <input type="checkbox"/> Deterioration <input type="checkbox"/>
No Threat <input checked="" type="checkbox"/> Other <input type="checkbox"/>	

COMMENTS: Trinity Church has been well maintained over the years, and its historic architecture is appreciated by the congregation and community.

REFERENCES:

Cranford Historical Society

Ricord. History of Union County. 1896.

RECORDED BY: JWF
ORGANIZATION: UCCHPAB

DATE: 1981



CRANFORD

Union Co

* 200304

c. 20/2

295 North Ave. East
looking S.E

TRINITY CHURCH

1981

PROPERTY REPORT

Property ID: -438840167

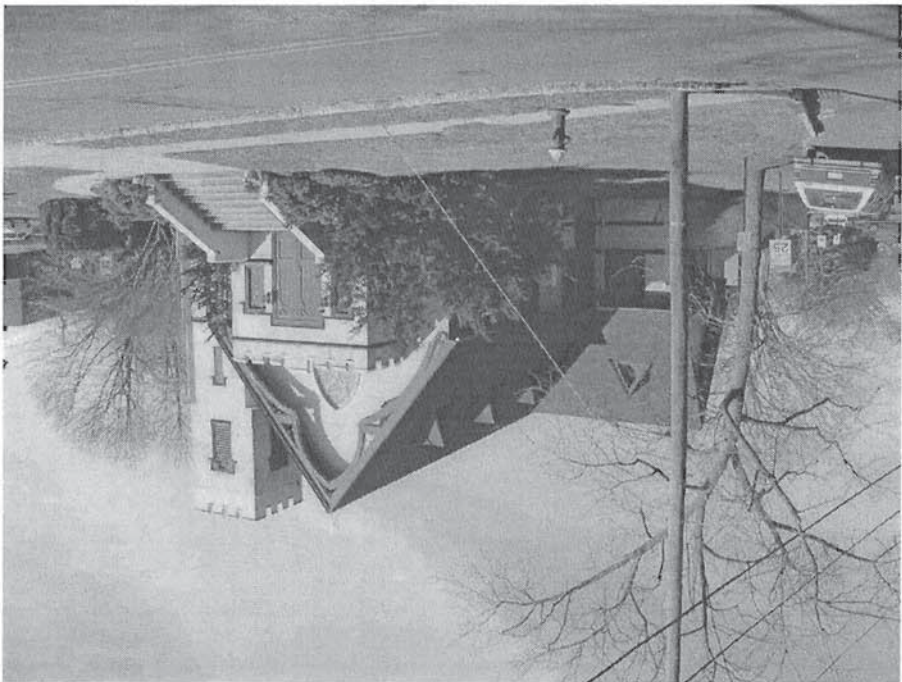
Property Name: Trinity Episcopal Church of Cranford
Address: 215 North Avenue East
Ownership: Non-profit

Apartment #: ZIP: 07016

PROPERTY LOCATION(S):

County: Municipality: Local Place Name: USGS Quad: Block: Lot:
UNION Cranford township Roselle 315 6

Property Photo:



Old HSI Number:

NRIS Number:

HABS/HAER Number:

Description:

Trinity Episcopal church typifies the Gothic Revival form of an English country church often found in American Episcopalian churches of the mid- to late-19th century. Its cruciform, cross-gabled style, executed here in stucco, exemplifies the form with its steeply pitched roofs. It symmetry is interrupted, however, as the transept gable projections vary from side to side. A three-story bell tower, stepped back from the front elevation, furthers this interruption. The trefoil motif bargeboard, applied on all of the gable ends is a prominent feature, as are the triangular dormers piercing the roof. The dormers contain decorative molding on their gable ends, and they house triangular leaded-glass windows. Stepped buttresses enhance the corners of the building, the corners of the tower, and also appear between the windows on the nave.

Setting:

The Church is situated on the north side of the heavily traveled Route 28 or North Avenue East. It is on a small irregular wedge-shaped corner lot bounded to the north by Hamilton Avenue, to the east by Arlington Avenue, to the south by North Avenue East and to the west by Forest Avenue. The church is situated in a residential neighborhood to the north with 100 year old homes in varying stages of restoration, suggesting a renewed interest in revitalization in the surrounding community.

Registration and Status

National Historic Landmark? ☐

National Register:

New Jersey Register:

Determination of Eligibility:

Certification of Eligibility:

☒ Eligibility Worksheet included in present survey?

☐ Is this Property an identifiable farm or former farm?

Dates:

SHPO Opinion:

Local Designation:

Other Designation:

Other Designation Date:

Survey Name: Women's Historic Sites Survey

Principal Investigator: Ellen Schultz

Organization: Preservation Partners

☒ (Primary Contact)

Property ID: -438840167



Site Map:

(Site Map Not Available)

BIBLIOGRAPHY:

Author:	Title:	Year:	HPO Accession #:
Burstyn, editor, Joan	Past and Promise: Lives of New Jersey Women (Syracuse University Press)	1997	
(None Listed)	"Quits as Head of Civic Group" Elizabeth Daily Journal.	1930	
Lakey, Alice	The Cranford Village Improvement Association: A Brief Historical Sketch	1930	
(None Listed)	New York Times Obituary June 19, 1935	1935	
(None Listed)	Postcard announcement for Speaker W.R.M. Wharton on "Why we need a new Food and Drugs Act" at the Trinity Church. Cranford Historical Society Collection.	1933	
Lakey, Alice	"A Few Words to the VIA" Talk	1929	
(None Listed)	Citizen Chronicle, obituary June 20, 1935	1935	
(None Listed)	Cranford Historical Society Clippings File.		
Cooper, Walter E.	History of Trinity Church in Cranford: 1872-1972	1972	
Anderson, Jr., Oscar E.	The Health of A Nation: Harvey W. Wiley and the Fight for Pure Food. (Chicago: University of Chicago Press)	1958	

Additional Information:

More Research Needed? ☐ (checked=Yes)

INTENSIVE-LEVEL USE ONLY:

Attachments Included:

- 1 Building
- 0 Structure
- 0 Object
- 0 Bridge
- 0 Landscape
- 0 Industry

Historic District ? ☐

District Name: not applicable

Status:

Survey Name: Women's Historic Sites Survey

Principal Investigator : Ellen Schultz

Organization: Preservation Partners

Property ID:

-438840167

☒ (Primary Contact)

al Site/Deposits?
sites. If Yes, please describe briefly)

☐

Conversion Problem? ☐ ConversionNote: Null

Date form completed: 11/12/2001

Survey Name: Women's Historic Sites Survey

ncipal Investigator : Ellen Schultz

Organization: Preservation Partners

☒ (Primary Contact)

Property ID:

-438840167

Page 3

BUILDING ATTACHMENT

Property ID:	-438840167
Element ID:	-307758814

Common Name: Trinity Episcopal Church of Cranford
Historic Name: Trinity Church

Present Use: Institutional, religious assembly

Historic Use: Institutional, religious assembly

Construction Date: 1875 Source: History of Trinity Church in Cranford, 1872-1972 by Walter E. Cooper

Construction Start Date: End Date: Construction

Style: Gothic Revival

Form: Irregular

Type: Other

Roof Finish Materials: Asphalt Shingle

Exterior Finish Materials: Stucco

Exterior Description:

Trinity Episcopal church typifies the Gothic Revival form of an English country church often found in American Episcopalian churches of the mid to late 19th century. Its cruciform, cross-gabled style, executed here in stucco, exemplifies the form with its steeply pitched roof. Its symmetry is interrupted, however, as the transept gable projections vary from side to side. A three-story belfry tower, stepped back from the front elevation, furthers this interruption. The trefoil motif bargeboard, applied on all of the gable ends is a prominent feature, as are the triangular dormers piercing the roofs of both gable roofs. The dormers contain decorative molding on their gable ends, and they house triangular, leaded-glass windows. Stepped buttresses enhance the corners of the building, the corners of the tower, and also appear between the windows on the nave.

Two elevations of the tower are fenestrated and it is topped with a crenellated parapet. The ground floor has stained-glass windows with lintels and sills serving as belt courses. Leaded glass windows on the second floor are topped with flat-arched lintels with drip moldings. The towered openings for the bell on the third floor are the largest. They are also embellished with drip moldings. Here the corner buttresses extend to the top of the second floor.

An eight-step stone stairway with stuccoed cheek walls leads to the narthex, which projects out from the gable end of the nave. It has the same crenellated parapet as the tower. Its double-leaf, paneled, wooden doors have trefoil motifs on the upper sections. Leaded-glass windows flank the doorway. The lintel over the doorway drops to top the windows, as it ends with a drip molding. Above the narthex, a pointed-arched opening holds a stained-glass window with three large circles.

The nave contains three sets of tripart stained-glass windows on the (north) elevation and only two sets on the south elevation, as the tower precludes the third set. The top of each window has a trefoil shape, and each set is topped with simple a flat-arched lintel. The sills of these openings form a belt course that extends the length of the nave and onto the adjacent transepts. The north transept projects out enough to contain one set of the tripart windows on the side elevation. Each transept has an entrance, which is topped with a pitched porch roof held by brackets. Molding mimicking that on the gable ends trims the porch. The entrances have paneled doors, and each is flanked by a pair of windows that follow the order of those on the nave. A ramp on the north side, transverse from the edge of the sidewalk to the doorway of the transept.

The church sits on a northeast corner lot that is enhanced with mature trees and shrubbery. An ancillary building at the rear of the church is connected with a series of sidewalks.

Interior Description:

Alteration Dates:

Architect/Designer:

Date form completed: 3/24/2002

Survey Name: Women's Historic Sites Survey

Principal Investigator: Ellen Schultz

Organization: Preservation Partners

☒ (Primary Contact)

Property ID: -438840167

ELIGIBILITY WORKSHEET - Properties

Property ID

-438840167

History: Alice Lakey was instrumental in rousing the public support of American women behind the passage of the Pure Food Act of 1906. It was through her initial alliance with chemist Harvey Wiley in 1903 and her personal role in advocating for this cause that she was able to rally the support of the New Jersey State Federation of Women's Clubs behind this bill. She was a concert singer, who forced by ill health to give up her musical career, in 1895 moved to Cranford to live with her father. Since that time, she was actively involved in civic improvement in Cranford and, as president of the Village Improvement Association, had written Wiley in 1903 to give a talk for the organization. She went on to advocate for the Bill on a national level and was appointed to the newly formed Food Committee of the National Consumers' League in 1905, a position she held until 1913. Here she lobbied for the passage of the bill and studied the adulteration of foods and the conditions under which they were produced. As a leading advocate of pure food legislation, Lakey was one of six, including Wiley, who visited Theodore Roosevelt in February 1905 to directly appeal for support. The federal law only prevented interstate commerce in mislabeled or adulterated food. The fight to advocate for state legislation began, and Lakey mobilized women once again to write letters favoring a food bill and a meat and slaughterhouse inspection bill. Lakey organized a Food Convention in Cranford in 1907 with speakers on safe, clean milk and meat and on detecting food adulteration. In bits and pieces, from 1907 to 1910, the New Jersey legislature passed new food and drug laws. She chaired the Committee of Women's Organizations for Women's Day at the 1918 and 1919 National Milk Expositions in New York.

She was an ardent supporter of the Village Improvement Association and its programs. As noted in a 1920 "Elizabeth Daily Journal" article, the VIA worked to improve garbage and ash collection, the building of a new school, a free emergency hospital facility, better milk inspection, improving the quality of motion pictures and fighting against drug addiction. She helped found the Visiting Nurse Association and the Needlework Guild, which made clothing for the needy. She was part of a pioneering effort to provide school lunches. In 1930, Lakey looks back in her historical sketch of the Village Improvement Association, and sees the women who started it as pioneers in civic betterment. "The women who were the civic pioneers in 1896 blazed a trail that other women have followed, not only in Cranford but in other towns. They had splendid courage for it was not easy to do what these women did in 1896." She acknowledged that they often met with opposition from others in the town or from their own husbands. "When fun was made of them, when husbands forbade their wives joining an organization that spoke of women's rights' that hated bug-a-boo, the pioneers turned a deaf ear."

After the death of her father in 1919, Lakey took over the management of "Insurance", a magazine he had founded in 1883. In 1921, she was appointed the insurance specialist of the General Federation of Women's Clubs and was instrumental in starting the movement to use insurance as a way of saving money for college. In 1924, Secretary of Commerce Herbert Hoover appointed Lakey a member of the Insurance Division of the Street and Highway Safety Conferences in Washington, DC.

Lakey was an active member of the Trinity Church for many years and was president of the Parish Circle for seven years. The Church, no doubt, hosted many meetings of the Village Improvement Association. One such postcard invitation survives inviting members to attend a lecture by someone from the Department of Agriculture on "Why we need a new Food and Drugs Act" in 1933. Lakey died on June 18, 1935 at the age of 77 in Cranford. Mayor George E. Osterheldt of Cranford declared an official ten-day period of mourning.

Statement of Significance:

According to her obituary in "The New York Times," June 18, 1935, "It was largely through her work in organizing women that Dr. Harvey W. Wiley's Pure Food Bill was passed by Congress in 1906." She went on to help the enactment of the Federal Milk and Measur Law and was founder of the American Pure Food League. She became the only woman member of the New York Milk Committee, lecturing on clean milk and foods for the New York Board of Education. In 1914, she was made a member of the National Institute of Social Sciences in recognition of her work. Lakey was also active locally in the Cranford Village Improvement Association. She served as president for 12 years and as honorary president for 5 more. The VIA was active in civic improvement related to quality of life issues in the community from garbage collection to building new schools, to improving public health and safety. At the beginning of the 20th century, women's charitable impulse often found expression in the settlement house movement and in social reform through the large number of women's clubs that proliferated from the 1870s to the 1920s. The earlier pattern of working with needy individuals through local charitable organizations continued, but increasingly women sought legislative solutions for urban problems. In fact, Lakey's rallying of women behind the passage of pure food legislation was significant on a national, state and local level of reform. In 1976, the U.S. Department of Agriculture posthumously honored her for her contribution to the 1906 Food and Drug Act.

Eligibility for New Jersey and National Registers: ☒ Yes ☐ No

National Register Criteria: ☐ A ☒ B ☐ C ☐ D

Level of Significance: ☒ Local ☐ State ☐ National

Justification of Eligibility/Ineligibility:

Alice Lakey had a major influence on the passage of pure food legislation and its enforcement on the local, state and national level. She was recognized by the US Department of Agriculture in 1976 for her contribution to the passage of the 1906 Food and Drugs Act. Unfortunately, her home on Millin Street was demolished and the only extant building with which she was actively associated is the Trinity Church. More research is recommended to find out how much of a role the Church played for meetings and lectures, but it is the only extant site associated with Lakey's advocacy work. The building itself maintains a high degree of integrity as a gothic

Survey Name: Women's Historic Sites Survey

Principal Investigator : Ellen Schultz

Organization: Preservation Partners

☒ (Primary Contact)

Property ID:

-438840167

Total Number of Attachments: 1

List of Element Names: Building

Narrative Boundary Description:

The property is situated on a wedge-shaped parcel comprised of two lots, the church is Block 315, Lot 6 and the Church school building is Block 315, lot 1 in the USGS quadrangle of Roselle

Date Form Completed: 3/18/2002

Survey Name: Women's Historic Sites Survey
Principal Investigator : Ellen Schultz
Organization: Preservation Partners

Property ID:

-438840167

☒ (Primary Contact)

NEW JERSEY OFFICE OF HISTORIC PRESERVATION HISTORIC SITES INVENTORY # 200319
INDIVIDUAL STRUCTURE SURVEY FORMHISTORIC NAME: Cranford Trust Building
LOCATION: 2 North Ave.COMMON NAME: First Fidelity Bank
BLOCK/LOT 189/1MUNICIPALITY: Cranford
USGS QUAD: RoselleCOUNTY: Union
UTM REFERENCES:OWNER/ADDRESS: The National Bank of N.J.
151 Centennial Ave., Piscataway, N.J.

Zone/Northing/Easting

DESCRIPTION

Construction Date: 1913

Source of Data: Documentation

Architect:

Builder: M. Byrnes Building Co.
Elizabeth, N.J.

Style: Neo-Classical Commercial

Form/Plan Type: Irregular shape to fill lot

Number of Stories: Three story

Foundation: Not visible

Exterior Wall Fabric: Brick with stone details

Fenestration: Rows of 1/1 sash windows fill each bay - seven bays.

Roof/Chimneys: Flat roof.

Additional Architectural Description:

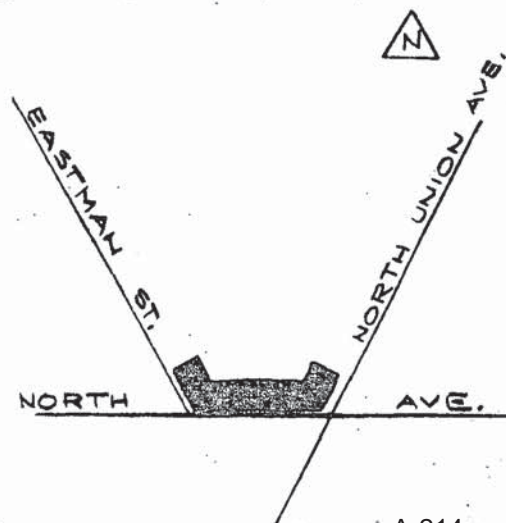
Building extends the full length of the block on the north side of North Avenue between Eastman St. and No. Union Ave. It contains 6 stores plus a bank. The facade is divided into bays by raised brick "pilasters." A pedimented pavillion marks each end of the building; at the east end, the pediment (and building) curve around the corner of North Ave. and No. Union Ave.

Some cornice runs across top of entire building.

Fenestration in bank portion of block altered on all three stories.

PHOTO Negative File # Roll 33 frames 29A, 35A

Map (Indicate North)



SITING, BOUNDARY DESCRIPTION, AND RELATED STRUCTURES:

Building stands on sidewalk line between Eastman St.
and N. Union Ave. on North Ave.

SURROUNDING ENVIRONMENT: Urban ☐ Suburban ☐ Scattered Buildings ☐
Open Space ☐ Woodland ☐ Residential ☐ Agricultural ☐
Industrial ☐ Downtown Commercial ☒ Highway Commercial ☐ Other ☐

Located across North Avenue from the Cranford Railroad Station of the
Central Railroad of New Jersey.

SIGNIFICANCE:

The Cranford Trust Building is on the site of the Opera Block, a notable
commercial building of the turn-of-the-Century which burned in 1912. Both the
Opera Block and the Cranford Trust Building were the subject of numerous post
cards in the early 20th Century, for it was a real landmark building.

The Cranford Trust Building nicely incorporates stock Classical features,
such as pediments, columns, and dentil cornice in an ambitiously large building
designed to fill an unusually shaped lot.

An architect for the building is unknown, but the M. Byrnes Construction
Company who built it erected at least two other large, well-known structures,
both homes now demolished in Cranford.

ORIGINAL USE: Commercial	PRESENT USE: Commercial
PHYSICAL CONDITION: Excellent <input type="checkbox"/> Good <input checked="" type="checkbox"/>	Fair <input type="checkbox"/> Poor <input type="checkbox"/>
REGISTER ELIGIBILITY: Yes <input checked="" type="checkbox"/> Possible <input type="checkbox"/>	No <input type="checkbox"/> Part of District <input type="checkbox"/>
THREATS TO SITE: Roads <input checked="" type="checkbox"/> Development <input type="checkbox"/>	Zoning <input type="checkbox"/> Deterioration <input type="checkbox"/>
No Threat <input type="checkbox"/> Other <input type="checkbox"/>	
COMMENTS:	

Well preserved building, in which the bank has successfully incorporated
modernizations.

REFERENCES:

Cranford Historical Society.
Cranford Board of Trade. Cranford, N.J., 1913

RECORDED BY: JWF
ORGANIZATION: UCCHPAB

DATE: 1981



CRANFORD
Union Co.

200319
B 33/35

Cranford Trust Bldg.
2 North Ave.
looking N.W

1981



CRANFORD

Union Co.

* 200319

R33/29

Cranford Trust Bldg.
2 North Ave.

looking N.

1981

BASE FORM

Historic Sites #:

Property Name: 6 Orange Avenue
Street Address: Street # 6
Apartment #

Prefix: Street Name: Orange
Suffix: Type: AVE

County(s): Union
Municipality(s): Cranford Twp. 2003
Block(s): 261
Zip Code: 07016
Local Place Name(s):
Lot(s): 2
USGS QUAD: Roselle

Description:
6 Orange Avenue is a 1 1/2-story, 2-bay, Colonial Revival-style dwelling constructed circa 1900. (See Building Attachment)

Registration and Status Dates: National Historic Landmark:
National Register:
New Jersey Register:
Determination of Eligibility:
SHPO Opinion:
Local Designation:
Other Designation:
Other Designation Date:

Photograph:



Survey Name: Vanessa Zeoli

Surveyor: Phase II Springfield Ave. Bridge, Cranford, NJ

Organization: Cultural Resource Consulting Group

Date: September 2008

BASE SURVEY FORM

Historic Sites #



Location Map

Site Map

Bibliography/Sources:

USGS 7.5' Quadrangle: Roselle, N. J. 1955 (Photorevised 1981).
Tax Maps Township of Cranford, Union County New Jersey, revised 1993.

Additional Information:

More Research Needed? ☐ Yes ☒ No

INTENSIVE-LEVEL USE ONLY:

Attachments Included: ☒ Building/Element ☐ Landscape ☐ Farm ☐ Bridge ☐ Industry

Historic District: Yes ☐ No ☒ Historic District Name: North Cranford Historic District

Status: ☐ Key Contributing ☒ Contributing ☐ Non Contributing

Associated Archaeological Site/Deposit? ☐ Yes ☒ No

(Known or potential sites - if yes, please describe briefly)

Survey Name: Vanessa Zeoli

Surveyor: Phase II Springfield Ave. Bridge, Cranford, NJ

Organization: Cultural Resource Consulting Group

Date: September 2008

BUILDING ATTACHMENT

Historic Sites #

Common Name: 6 Orange Avenue

Historic Name:

Present Use: Residential Activity, Permanent

Historic Use: Residential Activity, Permanent

Construction Date: 1900 Source: 1882 map, 1903 map, visual evidence

Alteration Date(s): late-20th century Source: Visual Evidence

Designer: N/A Physical Condition: Good

Builder: Unknown Remaining Historic Fabric: Low

Style: Colonial Revival

Form: Gable Front

Type: Stories(B): 1.5 Bays(B): 2

Roof Finish Materials (B,S): Asphalt Shingle

Exterior Finish Materials: Stone veneer, vinyl siding

Exterior Description:

6 Orange Avenue is a 1½-story, 2-bay, Colonial Revival-style dwelling constructed circa 1900. The front-gable roof is clad in asphalt shingles and the cornice displays a slight return at the gable end. The eave is boxed with wood. The gable dormers also have a cornice return. The exterior wall surface is clad in asbestos shingles. Windows contain 1/1 vinyl sash units with synthetic louvered shutters. Windows in the dormers are paired. The northwest gable end has a wood Palladian-style window with a keystone in the arched frame. The front (northwest) facade has a 1-story, enclosed porch with a hip roof. The offset entrance has a wood panel door with fanlight and is accessed by brick steps with stone treads. The foundation is rough-faced concrete block. On the northeast elevation is a 1-story, 3-sided bay window with a hip roof and supported by two wood brackets.

Interior Description:

N/A

Setting:

6 Orange Avenue is located approximately 90 feet from the bridge on the southeast side of the road. The parcel is identified as Block 261, Lot 2 and is bordered on the southeast by the Rahway River. The property has a gravel driveway on the northeast side and is adorned with young and matures trees, and shrubs along the front of the dwelling.

Survey Name: Vanessa Zeoli

Surveyor: Phase II Springfield Ave. Bridge, Cranford, NJ

Date: September 2008

Organization: Cultural Resource Consulting Group

ELIGIBILITY WORKSHEET

Historic Sites #

History:

Comparison between the 1882 E. Robinson map and the 1903 Sanborn map, combined with visual evidence suggest that 6 Orange Avenue was constructed circa 1900. Visual evidence also suggests the building had its window sashes and siding replaced in the late 20th-century.

Statement of Significance:

6 Orange Avenue is not individually eligible for the National Register, but is eligible as a contributing resource to the North Cranford Historic District. The North Cranford Historic District was determined eligible for listing on the National Register of Historic Places by the NJDEP-HPO on 5/19/05. The historic district meets Criteria A for its contribution to Cranford's residential growth and development during the late 19th and early 20th century as the township transformed from a rural farming community into a suburban neighborhood for wealthy businessmen. (See Continuation Sheet)

Eligibility for New Jersey
and National Registers: ☐ Yes ☒ No

National
Register Criteria: ☒ A ☐ B ☒ C ☐ D

Level of Significance: ☒ Local ☐ State ☐ National

Eligibility/Ineligibility:

CRCG-2009
aims its integrity of design, location, feeling, workmanship, setting, and association, Phase 1 ID: 78
s that have compromised its integrity of materials. These changes include the replacement of
its exterior wall material with asbestos siding, installation of synthetic window sashes, the enclosed front porch,
and the possible replacement of the roof cladding. As a result, 6 Orange Avenue is no longer able to convey any
potential historic and/or design significance as an individually eligible resource; however, it is still capable of
conveying its significance as a contributing resource to the North Cranford Historic District.

For Historic Districts Only:

Property Count: _____ Key Contributing: _____ Contributing: _____ Non Contributing: _____
For Individual Properties Only:
List the Completed Attachments related to the property's significance:

Narrative Boundary Description

All of the property contained in Block 261, Lot 2 in the Township of Cranford, Union County, New Jersey.

Survey Name: Vanessa Zeoli

Date: September 2008

Surveyor: Phase II Springfield Ave. Bridge, Cranford, NJ

Organization: Cultural Resource Consulting Group

CONTINUATION SHEET

Historic Sites #

Bibliography/Sources (cont.)

Bauer, Jacob L. 1906 Atlas of Union County, New Jersey. E. Robinson & Co., New York, N.Y.

Hendrickson, Louie E. 1904 Cranford, New Jersey, Illustrated. Cranford, N.J.

Robinson, E. & R. H. Pidgeon 1882 Atlas of Union County, New Jersey. E. Robinson, New York, N.Y.

Sanborn Map Company 1903 Cranford, Union County, New Jersey. Sheet 4. On file at Rutgers University Libraries, New Brunswick, N.J.
1922 Cranford, Union County, New Jersey. Sheet 3. On file at Rutgers University Libraries, New Brunswick, N.J.
1959 Cranford, Union County, New Jersey. Sheet 3. On file at Rutgers University Libraries, New Brunswick, N.J.
1929 Cranford, Union County, New Jersey. Sheet 3. On file at Rutgers University Libraries, New Brunswick, N.J.

Sidney, J. C. 1850 Map of Essex County, New Jersey. Hiram A. Belding, Newark, N.J.

Statement of Significance (cont.)



The Rahway River and the Central Railroad of New Jersey were key factors in the success of Cranford's development. The river was a strong influence in the community, forming a part of picturesque neighborhoods and providing recreational activities for residents. The railway enabled Cranford to attract New York professionals who could live in the community and commute to work in the city. The historic district is also eligible for listing in the National Register under Criterion C as a historic district with representative examples of numerous styles and types of architecture that include Queen Anne and Colonial Revival, with lesser numbers of Tudor Revival, Shingle style and Craftsman. The historic district's period of significance is 1860 to 1930 (CRCG 2001: 24). No evidence was found to suggest that the district is eligible under Criterion B for association with significant individuals, or under Criterion D for having the ability to yield information important to history and/or pre-history. Since 6 Orange Avenue was constructed within the historic district's period of significance (1860-1930), retains the majority of its character-defining features, and falls within the district's boundaries, it is a contributing element to the North Cranford Historic District.

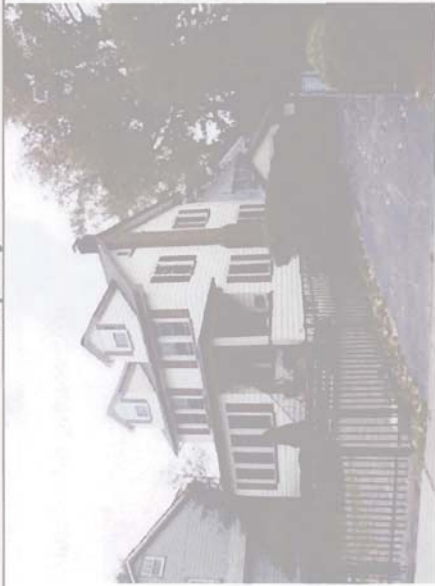

Survey Name: Vanessa Zeoli

Surveyor: Phase II Springfield Ave. Bridge, Cranford, NJ


Organization: Cultural Resource Consulting Group

Date: September 2008

Inventory of Identified Architectural Resources			
Property	Property Information	Description	NRHP Eligibility
 <p>Photograph Number: 84 (north balustrade as seen from the Cranford Canoe Club dock)</p>	<p>Panamerican Resource Number: 84</p> <p>Bridge (NJDOT #2003014) on Springfield Avenue at Orange Street crossing Rahway River</p> <p>Municipality: Cranford, Union County</p> <p>UTM coordinates (1983 Zone 18N): 4501245N, 558662E</p> <p>Date: 2010</p> <p>Other: This bridge was recorded because it lies directly in within the 100' APE. It was designed by CMX; constructed by Marbro, Inc.</p>	<p>Style: Rustic w/classical elements</p> <p>Stories: na</p> <p>Plan: steel stringer</p> <p>Roof: na</p> <p>Exterior: concrete enclosed balustrades with rubble veneer face topped with decorative open work metal railing with a classical feeling</p> <p>Foundation: concrete abutments with rubble veneer on concrete and rock</p> <p>Windows: na</p> <p>Details: This bridge replaced an earlier bridge on this site. It was designed to blend with the adjacent Girl Scout Park and neighborhood.</p>	<p>Although this bridge is not yet 50 years old, it should be considered a contributing element to both the NRE Rahway River Parkway HD (ID #4079) and NRE Union County Park System HD (ID #4424).</p>
 <p>Photograph Number: 85</p>	<p>Panamerican Resource Number: 85</p> <p>16 Orange Street</p> <p>Municipality: Cranford, Union County</p> <p>UTM coordinates (1983 Zone 18N): 4501371N, 558701E</p> <p>Date: ca. 1880</p> <p>Other: This is one of the oldest buildings recorded during this survey.</p>	<p>Style: Stick style with Gothic elements</p> <p>Stories: 2.5</p> <p>Plan: L shape</p> <p>Roof: gable</p> <p>Exterior: wood siding</p> <p>Foundation: obscured</p> <p>Windows: many original wood 2/2 and 4/4 with storm windows</p> <p>Details: House has a deep wrap-around porch. Unfortunately, the house appears to be divided into apartments and it is difficult to find the front entry.</p>	<p>This house is a contributing element to the NRE North Cranford HD (ID #3838; Bary 2012).</p>

Inventory of Identified Architectural Resources			
Property	Property Information	Description	NRHP Eligibility
 <p>Photograph Number: 80</p>	<p>Panamerican Resource Number: 80</p> <p>House and garage</p> <p>123 Riverside Drive</p> <p>Municipality: Cranford, Union County</p> <p>UTM coordinates (1983 Zone 18N): 4501372N, 558651E</p> <p>Date: ca. 1920</p> <p>Other: Backs up to the Rahway River</p>	<p>Style: Four square</p> <p>Stories: 2.5 with basement</p> <p>Plan: square</p> <p>Roof: gable</p> <p>Exterior: wood drop siding</p> <p>Foundation: concrete</p> <p>Windows: replacement metal with storm windows and decorative shutters</p> <p>Details: enclosed one side of the full width porch; small front gable, single-bay garage built to match house is also on the lot.</p>	<p>This house and garage are contributing elements to the NRE North Cranford HD (ID #3838).</p>
 <p>Photograph Number: 81</p>	<p>Panamerican Resource Number: 81</p> <p>101 Orange Street</p> <p>Municipality: Cranford, Union County</p> <p>UTM coordinates (1983 Zone 18N): 4501412N, 558686E</p> <p>Date: unknown</p> <p>Other:</p>	<p>Style: This building has been changed so much that its original style is indiscernible.</p> <p>Stories: 2</p> <p>Plan: rectangular</p> <p>Roof: hipped</p> <p>Exterior: Drivit with faux rock</p> <p>Foundation: obscured</p> <p>Windows: metal replacements</p> <p>Details:</p>	<p>Non-contributing and not eligible, lacks integrity</p>

Inventory of Identified Architectural Resources

Property	Property Information	Description	NRHP Eligibility
 <p>Photograph Number: 86</p>	<p>Panamerican Resource Number: 86</p> <p>House and garage</p> <p>104 Orange Street at corner of Riverside Drive</p> <p>Municipality: Cranford, Union County</p> <p>UTM coordinates (1983 Zone 18N): 4501404N, 558747E</p> <p>Date: ca. 1930</p> <p>Other: House includes a small garage</p>	<p>Style: Colonial Revival</p> <p>Stories: 2.5</p> <p>Plan: rectangular</p> <p>Roof: gable with cornice return</p> <p>Exterior: aluminum siding</p> <p>Foundation: obscured</p> <p>Windows: metal replacements, decorative shutters</p> <p>Details: The front stoop was enclosed as was the side porch. Decorative elements, if present originally, were eliminated when the aluminum siding was applied. The one-bay garage has a gable roof with a cornice return and like the house is covered in aluminum siding.</p>	<p>This house falls outside the boundaries of the North Cranford HD. It is not eligible for the NRHP since it does not meet any of the eligibility criteria.</p>
 <p>Photograph Number: 87</p>	<p>Panamerican Resource Number: 87</p> <p>House and garage</p> <p>104 Riverside Drive</p> <p>Municipality: Cranford, Union County</p> <p>UTM coordinates (1983 Zone 18N): 4501393N, 558762E</p> <p>Date: ca. 1920</p> <p>Other:</p>	<p>Style: Dutch Colonial</p> <p>Stories: 2.5 with basement</p> <p>Plan: rectangular</p> <p>Roof: gambrel</p> <p>Exterior: aluminum siding</p> <p>Foundation: concrete</p> <p>Windows: replacement 6/1 with storm windows.</p> <p>Details: Side porch enclosed. Any decorative detail was eliminated with addition of siding. Rear one-bay garage has a hipped roof that sports a rather fanciful cupola.</p>	<p>This house is a contributing element to the NRE North Cranford HD (ID #3838).</p>

NEW JERSEY OFFICE OF HISTORIC PRESERVATION HISTORIC SITES INVENTORY # 200301
INDIVIDUAL STRUCTURE SURVEY FORMHISTORIC NAME: Norris-Oakey House
LOCATION: 1119 Orange AvenueCOMMON NAME:
BLOCK/LOT 157/13MUNICIPALITY: Cranford
USGS QUAD: Roselle
OWNER/ADDRESS: Township of Cranford
Cranford, N.J.COUNTY: Union
UTM REFERENCES:
Zone/Northing/Easting

DESCRIPTION

Construction Date: ca. 1750, ca. 1820

Source of Date: Visual and documentation

Architect:

Builder:

Style: Vernacular Federal

Form/Plan Type: Side Hall Plan

Number of Stories: 2½

Foundation: Rubble stone

Exterior Wall Fabric: Clapboard

Fenestration: Six-bay facade. Tall multi-paned windows.

Roof/Chimneys: Low pitch gable roof.

Additional Architectural Description:

Original Norris farmhouse was built about 1750. It was a 1½ story frame dwelling with a gambrel roof.

In about 1820, a Federal style addition, much larger than the original house was put on the east side. At the same time, the roof of the original house was raised to create a full second story, kneehigh windows were added to the facade, and the interior was remodelled with 19th Century doors, chair rails, and mantlepieces.

PHOTO

Negative File #

Roll 26 Frame 36

Map (Indicate North)



HEMLOCK CIRCLE

ORANGE AVE.



SITING, BOUNDARY DESCRIPTION, AND RELATED STRUCTURES:

The Norris-Oakey house has no surviving outbuildings, and is surrounded by modern subdivisions. The house is quite close to the road (Orange Ave.), but is raised on a small embankment.

SURROUNDING ENVIRONMENT: Urban ☐ Suburban ☒ Scattered Buildings ☐
Open Space ☐ Woodland ☐ Residential ☒ Agricultural ☐
Industrial ☐ Downtown Commercial ☐ Highway Commercial ☐ Other ☐

SIGNIFICANCE:

The land and first house on this site belonged to Samuel Norris, (died 1951), son of Henry Norris (ca. 1640-1706), one of the early settlers of "Elizabeth-towne." He died in 1751, just as his rights to the land where his house stood were being disputed as part of the prolonged battle to New Jersey's "ownership" between Proprietors and Associates. When title to the Cranford farm was finally cleared, it was inherited by Nathaniel Norris (1732-98), Samuel's younger son. Nathaniel Norris, and later his son, Noah, farmed here until 1836. At that time it was sold to the Oakey family, who retained it until the end of the 19th Century as a working farm.

The Oakey House displays in its architecture the modernizations and alterations that would be expected in a building that served as a busy focus for a farm for over 150 years.

Despite recent alterations, the Norris-Oakey house remains a fine example of vernacular federal architecture in Union County; it is also indicative of the manner in which 3-bay 18th c. houses were incorporated into a larger farmstead.

ORIGINAL USE: Farmhouse
PHYSICAL CONDITION: Excellent ☐ Good ☐ Fair ☒ Poor ☐
REGISTER ELIGIBILITY: Yes ☒ Possible ☐ No ☐ Part of District ☐
THREATS TO SITE: Roads ☐ Development ☐ Zoning ☐ Deterioration ☒
No Threat ☐ Other ☐
COMMENTS:

House was to be restored in 1977-78 using Community Development Funds and using CETA workers. Lack of informed supervision led to severe damaging of the house, and the work was halted. The house is now to be sold to a private individual with covenants to assure preservation of the remaining features.

REFERENCES:

The Broadside, newsletter of the Cranford Heritage Corridor.





CRANFORD
Union Co.



*200301
t. 26/36



Norris-Oakey Hse,
1119 Orange Ave.
looking North

1981



Inventory of Identified Architectural Resources			
Property	Property Information	Description	NRHP Eligibility
 <p>Photograph Number: 72 (north bound balustrade)</p>	<p>Panamerican Resource Number: 72</p> <p>Orchard Street Bridge (NJDOT #2003071) crossing unnamed brook</p> <p>Municipality: Cranford, Union County</p> <p>UTM coordinates (1983 Zone 18N): 4500919N, 558241E</p> <p>Date: 1875/1979</p> <p>Other: Bridge was recorded because it lies directly within the 100' APE.</p>	<p>Style: Rustic</p> <p>Stories: na</p> <p>Plan: culvert</p> <p>Roof: na</p> <p>Exterior: rubble balustrade with jagged top</p> <p>Foundation: concrete abutments</p> <p>Windows: na</p> <p>Details: A plaque on the north bound balustrade indicates that the culvert was constructed in 1979; a plaque on the south bound balustrade, obviously taken from the old balustrade, indicates that an early bridge/culvert at this site was built in 1875 and was designed by Jacob Klein, Builder.</p>	<p>This bridge is eligible as a contributing element to the NRE North Cranford HD (ID #3838).</p>
 <p>Photograph Number: 73</p>	<p>Panamerican Resource Number: 73</p> <p>Site of former Skeet Range, Lenape Park</p> <p>Municipality: Kenilworth, Union County</p> <p>UTM coordinates (1983 Zone 18N): 4502894N, 557943E</p> <p>Date: ca. 1928</p> <p>Other:</p>	<p>Style: Skeet Range</p> <p>Stories: na</p> <p>Plan: na</p> <p>Roof: na</p> <p>Exterior: na</p> <p>Foundation: dirt</p> <p>Windows: na</p> <p>Details: This site once had three firing stalls for skeet.</p>	<p>The site is within Lenape Park which is a contributing element to both the NRE Rahway River Parkway HD and NRE Union County Park System HD. The former Skeet Range has no integrity and, therefore, is not eligible and does not contribute to Lenape Park.</p>

Inventory of Identified Architectural Resources

Property	Property Information	Description	NRHP Eligibility
 <p>Photograph Number: 67</p>	<p>Panamerican Resource Number: 67</p> <p>102 Orchard Street</p> <p>Municipality: Cranford, Union County</p> <p>UTM coordinates (1983 Zone 18N): 4500859N, 558345E</p> <p>Date: 1914</p> <p>Other: House backs up to the Rahway River and has a backyard view of McConnell Park.</p>	<p>Style: Shingle Style</p> <p>Stories: 2.5</p> <p>Plan: irregular</p> <p>Roof: multiple types</p> <p>Exterior: wood shingles</p> <p>Foundation: obscured</p> <p>Windows: 1/1 original wood covered with storm windows, original decorative diamond pane and front round window.</p> <p>Details: The house features a corner tower with a "Witches Cap" roof.</p>	<p>The house is eligible as a contributing element to the NRE North Cranford HD (ID #3838; Bary 2012).</p>
 <p>Photograph Number: 68</p>	<p>Panamerican Resource Number: 68</p> <p>House and Garage</p> <p>104 Orchard Street</p> <p>Municipality: Cranford, Union County</p> <p>UTM coordinates (1983 Zone 18N): 4500872N, 558329E</p> <p>Date: 1900</p> <p>Other: House backs up to the Rahway River and has a backyard view of McConnell Park.</p>	<p>Style: Vernacular Victorian</p> <p>Stories: 2.5 with basement</p> <p>Plan: rectangular</p> <p>Roof: hipped</p> <p>Exterior: aluminum siding</p> <p>Foundation: concrete</p> <p>Windows: 1/1 with storm windows</p> <p>Details: This is a fairly plain vernacular-style house with large decorative style brackets. The decorative elements may have been lost when the siding was applied. A small, one-bay, hipped roof, covered in wood shingles, probably the original house covering. A garage is located in the rear of the house.</p>	<p>The house and garage are eligible as contributing elements to the NRE North Cranford HD (ID #3838; Bary 2012).</p>

Inventory of Identified Architectural Resources			
Property	Property Information	Description	NRHP Eligibility
 <p>Photograph Number: 69 (house only)</p>	<p>Panamerican Resource Number: 69</p> <p>House and Garage (see next row)</p> <p>106 Orchard Street</p> <p>Municipality: Cranford, Union County</p> <p>UTM coordinates (1983 Zone 18N): 4500892N, 558303E</p> <p>Date: 1900</p> <p>Other: This house backs up to the Rahway River and has a backyard view of McConnell Park; House occupies two lots.</p>	<p>Style: Dutch Colonial/Colonial Revival</p> <p>Stories: 2</p> <p>Plan: rectangular</p> <p>Roof: gambrel with expanded sides</p> <p>Exterior: wood shingles</p> <p>Foundation: obscured</p> <p>Windows: 6/1 original with storm windows; original decorative fixed, side lights at front door</p> <p>Details: The front porch has been completely remade and its elements are too large for the scale of the rest of the house. A garage that matches the house is also on the lot – see row below.</p>	<p>The house and garage at 106 Orchard Street are eligible as contributing elements to the NRE North Cranford HD (ID #3838) (Bary 2012).</p>
 <p>Photograph Number: 69 (garage only)</p>	<p>Panamerican Resource Number: 69</p> <p>House and Garage</p> <p>106 Orchard Street</p> <p>Municipality: Cranford, Union County</p> <p>UTM coordinates (1983 Zone 18N): 4500884N, 558314E</p> <p>Date: ca. 1920</p> <p>Other: Garage backs up to the Rahway River.</p>	<p>Style: matches the house – Dutch Colonial/Colonial Revival</p> <p>Stories: 1</p> <p>Plan: rectangular</p> <p>Roof: hipped</p> <p>Exterior: wood shingles</p> <p>Foundation: concrete</p> <p>Windows: none</p> <p>Details: wood batted doors painted to match house</p>	<p>The house and garage at 106 Orchard Street are eligible as contributing elements to the NRE North Cranford HD (ID #3838) (Bary 2012).</p>

Inventory of Identified Architectural Resources

Property	Property Information	Description	NRHP Eligibility
 <p>Photograph Number: 70</p>	<p>Panamerican Resource Number: 70</p> <p>110 Orchard Street</p> <p>Municipality: Cranford, Union County</p> <p>UTM coordinates (1983 Zone 18N): 4500911N, 558291E</p> <p>Date: 1949</p> <p>Other: House backs up to the Rahway River and has a backyard view of McConnell Park.</p>	<p>Style: Minimal Traditional w/Colonial Revival elements</p> <p>Stories: 1</p> <p>Plan: L shape</p> <p>Roof: gable with asphalt shingles</p> <p>Exterior: wood siding and flush board</p> <p>Foundation: concrete</p> <p>Windows: 6/6 wood originals and other original configurations</p> <p>Details: House features the "Early American" decorative elements of dentil molding, front door side lights, and a fanciful "Colonial Cupola" flying a weather vane in a bird shape.</p>	<p>This house is non-contributing to the NRE North Cranford HD (ID #3838; Bary 2012) and is not eligible for the NRHP since it does not meet the criteria.</p>
 <p>Photograph Number: 71</p>	<p>Panamerican Resource Number: 71</p> <p>Garage and house</p> <p>114 Orchard Street</p> <p>Municipality: Cranford, Union County</p> <p>UTM coordinates (1983 Zone 18N): 4500928N, 558271E</p> <p>Date: 1914</p> <p>Other: House backs up to the Rahway River and a small stream runs along its west side. It has a backyard view of McConnell Park.</p>	<p>Style: Four square</p> <p>Stories: 2.5</p> <p>Plan: square; now has an addition</p> <p>Roof: hipped</p> <p>Exterior: stucco</p> <p>Foundation: obscured</p> <p>Windows: various types of originals covered with storms; front windows have upper fan, door has side lights, decorative shutters</p> <p>Details: House has been significantly changed. The porch is new and not sympathetic. The window treatments, the metal balustrades on sidewalks, and the changes to the eaves detract. A flat roofed stucco garage was added at a later date.</p>	<p>The house is eligible as a contributing element to the NRE North Cranford HD (ID #3838; Bary 2012). The garage, a much later addition, is not eligible and non-contributing.</p>

NEW JERSEY OFFICE OF HISTORIC PRESERVATION HISTORIC SITES INVENTORY # 200317
INDIVIDUAL STRUCTURE SURVEY FORM

HISTORIC NAME:

LOCATION: 222 Orchard St.

COMMON NAME:

BLOCK/LOT 179/6

MUNICIPALITY: Cranford

COUNTY: Union

USGS QUAD: Roselle

UTM REFERENCES:

OWNER/ADDRESS: Maxson, Frank Towler, Jr.
222 Orchard St., Cranford

Zone/Northing/Easting

DESCRIPTION

Construction Date: ca. 1920

Source of Date: visual

Architect:

Builder:

Style: Tudor Revival

Form/Plan Type: Center hall

Number of Stories: 2½

Foundation: Brick facing on concrete

Exterior Wall Fabric: Brick first floor, half timber and stucco on second floor.

Fenestration: Five bay facade. Diamond paned casement windows.

Roof/Chimneys: Gable roof. Cross gables on facade. Two brick chimneys.

Additional Architectural Description:

Dark wood trim around doors, windows.

PHOTO Negative File # Roll 32 frame 9

R 9, 12

Map (Indicate North)



SITING, BOUNDARY DESCRIPTION, AND RELATED STRUCTURES:

Carriage house - hipped roof. Brick.

SURROUNDING ENVIRONMENT: Urban ☐ Suburban ☒ Scattered Buildings ☐
Open Space ☐ Woodland ☐ Residential ☒ Agricultural ☐
Industrial ☐ Downtown Commercial ☐ Highway Commercial ☐ Other ☐

SIGNIFICANCE:

Good example of the Tudor Revival style of architecture. It is less common in Cranford than in other communities in the area. Noted in Gateways to Architecture, a book describing Union County Architecture, by Beverly and Sandy Brown.

ORIGINAL USE: Residence

PHYSICAL CONDITION: Excellent ☒ Good ☐

REGISTER ELIGIBILITY: Yes ☒ Possible ☐

THREATS TO SITE: Roads ☐ Development ☐

No Threat ☒ Other ☐

COMMENTS:

PRESENT USE: Residence

Fair ☐ Poor ☐

No ☐ Part of District ☐

Zoning ☐ Deterioration ☐

REFERENCES:

Brown. Gateways to Architecture. UCCHPAB. 1975

RECORDED BY: JWF
ORGANIZATION: UCCHPAB

DATE: 1981



CRANFORD
Union Co.

* 200317
R 32/9

222 Orchard St.
looking N.E.

1981

NEW JERSEY OFFICE OF HISTORIC PRESERVATION HISTORIC SITES INVENTORY # 200314
INDIVIDUAL STRUCTURE SURVEY FORMHISTORIC NAME:
LOCATION: 410 Orchard StreetCOMMON NAME: Brook Lodge
BLOCK/LOT 171/1MUNICIPALITY: Cranford
USGS QUAD: Roselle
OWNER/ADDRESS: Brook Lodge Nursing Home
410 Orchard St., CranfordCOUNTY: Union
UTM REFERENCES: Zone/Northing/Easting

DESCRIPTION

Construction Date: ca. 1928

Source of Date: Cranford Historical Society

Architect: Frank Hollingsworth

Builder:

Style: Colonial Revival

Form/Plan Type: Center hall plan

Number of Stories: 2½

Foundation: Cement-covered

Exterior Wall Fabric: Wooden shingles

Fenestration: Five bay facade. Central door. Huge 8/8 first floor windows.
6/6 sash windows on second floor.

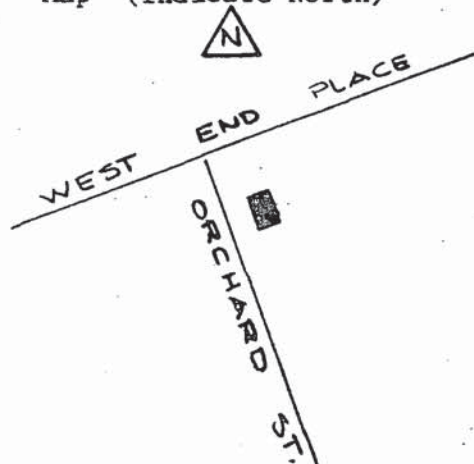
Roof/Chimneys: Gable roof. End brick chimneys.

Additional Architectural Description:

Pediment over front door.
Cornice with dentil and modillion molding.
Modillions on molding over first floor windows.
Wing on south side of house - 2½ story, 3 bay.

PHOTO Negative File # Roll 26 frame 27 29,9

Map (Indicate North)



SITING, BOUNDARY DESCRIPTION, AND RELATED STRUCTURES:

A carriage house which was at the rear of the property is now converted to a residence facing West End Place.

SURROUNDING ENVIRONMENT: Urban ☐ Suburban ☒ Scattered Buildings ☐
Open Space ☐ Woodland ☐ Residential ☒ Agricultural ☐
Industrial ☐ Downtown Commercial ☐ Highway Commercial ☐ Other ☐

SIGNIFICANCE:

The original house on the site was built at the turn of the century (ca. 1898) in the hip-roofed, square shape typical of the time. It was the residence of George Teller, the tax collector of Cranford.

The Brown family acquired the house in the 1920's. They hired the architect, Frank Hollingsworth, to redesign the house in the Colonial Revival style. The remodelling left very little of the earlier house visible, and was, in fact quite "correct" in many of the colonial or Georgian details. The grand house did not make the transition into the Depression years very well, however, and in 1932 it was sold to become a nursing home. It remains a nursing home today, and most of its architectural features are well preserved.

HOUSE DEMOLISHED

ORIGINAL USE: Residence
PHYSICAL CONDITION: Excellent ☐ Good ☒ Fair ☐ Poor ☐
REGISTER ELIGIBILITY: Yes ☐ Possible ☒ No ☒ Part of District ☐
THREATS TO SITE: Roads ☐ Development ☐ Zoning ☐ Deterioration ☐
No Threat ☒ Other ☐
COMMENTS:

House has been well adapted to a nursing home and has been well maintained.

REFERENCES:

Cranford Historical Society.

RECORDED BY: JWF
ORGANIZATION: UCCHPAB

DATE: 1981



CRANFORD
Union Co.

* 200314
R. 26/27

410 Orchard St.
looking N.E.

1981

NEW JERSEY OFFICE OF HISTORIC PRESERVATION HISTORIC SITES INVENTORY # 200313
INDIVIDUAL STRUCTURE SURVEY FORM

HISTORIC NAME: Torbush House
LOCATION: 464 Orchard Street

COMMON NAME:
BLOCK/LOT 160/22

MUNICIPALITY: Cranford
USGS QUAD: Roselle
OWNER/ADDRESS: Robert & Pamela Lowyns
464 Orchard St., Cranford

COUNTY: Union
UTM REFERENCES:
Zone/Northing/Easting

DESCRIPTION

Construction Date: ca. 1870

Source of Date:

Architect:

Builder:

Style: Planbook

Form/Plan Type: Central hall

Number of Stories: 1½

Foundation: Brick and fieldstone

Exterior Wall Fabric: Composition shingle siding

Fenestration: Four bay facade. 2/2 sash windows. Second floor windows much smaller.

Roof/Chimneys: Gable roof. Box cornice. Pediment returns. Central brick chimney.

Additional Architectural Description:

Cross gable centered on facade.
Rear one-story addition - flat roofed, one bay.
Arched window in center gable.

PHOTO Negative File # Roll 32 frame 10

Map (Indicate North)



A-239

SETTING, LOCATION, DESCRIPTION, AND RELATED STRUCTURES:

Located in the middle of a block in an area densely developed in single family houses.

No outbuildings remain.

SURROUNDING ENVIRONMENT: Urban ☐ Suburban ☒ Scattered Buildings ☐
Open Space ☐ Woodland ☐ Residential ☒ Agricultural ☐
Industrial ☐ Downtown Commercial ☐ Highway Commercial ☐ Other ☐

Surrounded by small, Cape-Cod style tract houses.

SIGNIFICANCE:

One of the earliest houses in this area of Cranford. The Torbush house was the center of a thriving farm.

This simple, center-gable, rectangular-massed farmhouse is representative of the vernacular Gothic Revival style; a rare surviving example for Union County, other houses of this type can be found throughout the rural areas of New Jersey.

The Torbush house was always fairly plain, and early photographs show a house quite similar to the present dwelling.

ORIGINAL USE: Farmhouse

PHYSICAL CONDITION: Excellent ☐ Good ☒

REGISTER ELIGIBILITY: Yes ☐ Possible ☒

THREATS TO SITE: Roads ☐ Development ☐

No Threat ☒ Other ☐

COMMENTS:

PRESENT USE: Residence

Fair ☐ Poor ☐

No ☒ Part of District ☐

Zoning ☐ Deterioration ☐

REFERENCES:

Cranford Historical Society

RECORDED BY: JWF
ORGANIZATION: UCCHPAB

DATE: 1981





CRANFORD
Union Co.



* 200313

R. 32/10

Torbush Hse.
464 Orchard St.
looking N.E.

1981

Inventory of Identified Architectural Resources			
Property	Property Information	Description	NRHP Eligibility
 <p>Photograph Number: 23</p>	<p>Panamerican Resource Number: 23</p> <p>12 Park Place</p> <p>Municipality: Cranford, Union County</p> <p>UTM coordinates (1983 Zone 18N): 4501899N, 557915E</p> <p>Date: ca. 2010</p> <p>Other: This building was surveyed since it was in the flood plain.</p>	<p>Style: Post Modern</p> <p>Stories: 2</p> <p>Plan: rectangular</p> <p>Roof: gable</p> <p>Exterior: stone</p> <p>Foundation: concrete</p> <p>Windows: casement and fixed</p> <p>Details: This house looks out on Nomahegan Park.</p>	<p>Not Eligible, neither old enough nor significant enough to warrant consideration</p>
 <p>Photograph Number: 24</p>	<p>Panamerican Resource Number: 24</p> <p>29 Park Drive</p> <p>Municipality: Cranford, Union County</p> <p>UTM coordinates (1983 Zone 18N): 4501905N, 557997E</p> <p>Date: ca. 1960</p> <p>Other: House overlooks Nomahegan Park.</p>	<p>Style: Colonial Revival</p> <p>Stories: 2</p> <p>Plan: rectangular</p> <p>Roof: gable</p> <p>Exterior: vinyl siding with first floor stone veneer</p> <p>Foundation: obscured</p> <p>Windows: replacements, two front bay windows with copper roofs</p> <p>Details: This is a much altered house with a huge side addition.</p>	<p>Not Eligible, does not meet the NRHP criteria for eligibility</p>

Inventory of Identified Architectural Resources			
Property	Property Information	Description	NRHP Eligibility
 <p>Photograph Number: 25</p>	<p>Panamerican Resource Number: 25</p> <p>112 Park Drive</p> <p>Municipality: Cranford, Union County</p> <p>UTM coordinates (1983 Zone 18N): 4502015N, 558074E</p> <p>Date: ca. 1930s</p> <p>Other: House overlooks Nomahegan Park, and is empty and in poor condition.</p>	<p>Style: Colonial Revival</p> <p>Stories: 1.5</p> <p>Plan: rectangular</p> <p>Roof: side gable w/dormers</p> <p>Exterior: brick and shakes</p> <p>Foundation: brick and concrete</p> <p>Windows: 6/6 replacement</p> <p>Details: This small house is made of white brick with a decorative brick door surround. A side porch and garage was added at a later date in vinyl siding. The house sits on an exceptionally large property.</p>	<p>Not Eligible, does not meet the NRHP criteria for eligibility</p>
 <p>Photograph Number: 26</p>	<p>Panamerican Resource Number: 26</p> <p>22 Doering Way</p> <p>Municipality: Cranford, Union County</p> <p>UTM coordinates (1983 Zone 18N): 4501911N, 558118E</p> <p>Date: ca. 1930</p> <p>Other: This structure is on the grounds of what was originally a large estate at the end of Doering Way. It is now subdivided and surrounded by houses of various ages.</p>	<p>Style: Colonial Revival</p> <p>Stories: 2</p> <p>Plan: L shape</p> <p>Roof: gable</p> <p>Exterior: wood siding and hardy board</p> <p>Foundation: concrete</p> <p>Windows: 6/6 sash, 2 bay windows, some fixed—all appear to be replacements, some operational shutters</p> <p>Details: This house has been much altered and added to, but retains much of its original character including the front door surround.</p>	<p>Not Eligible, does not meet the NRHP criteria for eligibility</p>

NEW JERSEY OFFICE OF HISTORIC PRESERVATION HISTORIC SITES INVENTORY # 200316
INDIVIDUAL STRUCTURE SURVEY FORM

HISTORIC NAME:

COMMON NAME:

LOCATION: 20 Pittsfield Street

BLOCK/LOT 167/1

MUNICIPALITY: Cranford

COUNTY: Union

USGS QUAD: Roselle

UTM REFERENCES:

OWNER/ADDRESS: Krause, Frank & Barbara
20 Pittsfield St., Cranford

Zone/Northing/Easting

DESCRIPTION

Construction Date: ca. 1920

Source of Date: Visual

Architect:

Builder:

Style: Federal Revival Style

Form/Plan Type: Center hall plan

Number of Stories: 2½

Foundation: Concrete

Exterior Wall Fabric: Clapboard

Fenestration: Five bay facade. 6/6 sash windows.

Roof/Chimneys: Gambrel roof. End brick chimneys. "Chimneys", exterior.

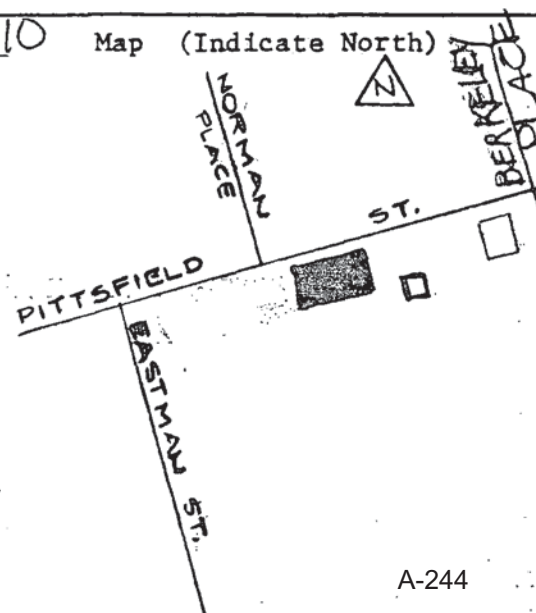
Additional Architectural Description:

Delicate fan light and sidelights surround front door.

Round arched dormers.

Greek-key design in cornice band around house.

PHOTO Negative File # Roll 26 frame 26 29, 10 Map (Indicate North)



SITING, BOUNDARY DESCRIPTION, AND RELATED STRUCTURES:

Large lot at the N.E. corner of Pittsfield St. and Eastment St. The house sits approx. 50' from Pittsfield St. toward the N.E. end of the lot; large open space to S.W. Garage to N.E. of house.

SURROUNDING ENVIRONMENT: Urban ☐ Suburban ☒ Scattered Buildings ☐
Open Space ☐ Woodland ☐ Residential ☒ Agricultural ☐
Industrial ☐ Downtown Commercial ☐ Highway Commercial ☐ Other ☐

SIGNIFICANCE:

Outstanding example of the Georgian Revival Style. Noted for this in Gateways to Architecture, a book on the architecture of Union County by Beverly and Sandy Brown.

ORIGINAL USE: Residence

PHYSICAL CONDITION: Excellent ☐ Good ☒

REGISTER ELIGIBILITY: Yes ☐ Possible ☒

THREATS TO SITE: Roads ☐ Development ☐

No Threat ☒ Other ☐

COMMENTS:

PRESENT USE: Residence

Fair ☐ Poor ☐

No ☒ Part of District ☐

Zoning ☐ Deterioration ☐

REFERENCES:

Brown. Gateways to Architecture. UCCHPAB. 1975.

RECORDED BY: JWF
ORGANIZATION: UCCHPAB

DATE: 1981



CRANFORD
Union Co.

*200316
R 26/26

20 Pittsfield St.
looking S.E.

1981

it can safely be assumed that from a cultural point of view, the proposed channel improvement of the Rahway River will have no adverse impact on this area.

Springfield Avenue to Hansel's Dam (Maps 4,5,8).

This section of the Rahway River was at one time significant both in terms of the prehistory and early history of Cranford. In the late nineteenth century Sylvester Cahill Jr., son of Cranford's first mayor, discovered an Indian site on the corner of what is now Casino Avenue and Riverside Park Drive. It has been stated (Bragdon 1937:7) *many Indian relics were picked up on that site, including flint arrow-heads, pieces of pottery, stone axes, spear-heads, stone knives, and a large corn grinding stone mill. Sylvester Cahill Jr., became an ardent collector, but unfortunately his interesting and valuable relics were lost at the time his home on Riverside Drive was destroyed by fire.* A few aboriginal artifacts are in the collection of the Cranford Historical Society. These allegedly derive from this site, but without adequate provenience they can contribute little to our understanding of the prehistoric peoples who at one time camped here.

Today this site is totally destroyed. Apartment houses, private dwellings, sidewalks and macadamized roads contributed to the destruction of the site. In any event, the proposed channel improvement for the Rahway River will do little to alter the terrain in this area (Fig. 6).

In order to ascertain whether the prehistoric site extended into the area between Riverside Park Drive and the river itself, we employed a mechanical auger with an 8" screw and probed the soil to a depth of 3 feet. Additionally, we scrutinized the exposed banks of the river and other denuded areas along the floodplain. No chips, fire-cracked rock, charcoal or other evidence of possible prehistoric occupation was found (Strat. 3).

Just south of the present-day intersection of Casino Avenue and Riverside Park Drive was the Cranford Casino Clubhouse completed in 1891. This building was destroyed by fire in 1897. Today the Cranford Post #212 of the American Legion occupies the site.



Approximately midway between Prospect Street and North Union Avenue is **Hansel's Dam.** This is the site of John Crane's Grist Mill which, under various owners, was in operation from about 1716 until 1860. It is said that grain for Washington's troops was ground here. In 1807, this mill complex was sold to a Nicholas Moony, and was later known as Gorman's Mill. Among the deeds on file at the Union County Park Commission headquarters is one dated 1804 which relates to this mill complex (Map 10). A map prepared by the surveyor Edward Moore Jr. entitled "A Map of the Roads Leading From Bridgetown and Rahway to Springfield" and dated January 16, 1811 (see Map 9) also shows these mill locations in the vicinity of David Marsh's house. This surveyor's drawing which accompanies the 1804 deed is of particular interest because it shows a grist mill, a saw mill and a mill dam located near what is now Hansel's Dam. Slightly downstream is the bridge and the "Country Road Leading to Elizabethtown". This map takes on a great deal of significance because of the recent excavations that have been carried

out in this area by the Cranford Heritage Corridor, Inc., under the direction of Robert Brooks and Dr. Peter S. Miller, a physical anthropologist from Drew University in Madison, New Jersey. Before commenting on the seriousness of this "archaeological investigation", it is pertinent to note that this 1804 map clearly places this grist mill in what is now Sperrie Park and the saw mill a short distance to the east along what is now Riverside Park Drive. (Map 10).

This area has been totally modified as a result of the creation of Sperrie Park. The mill dam shown on the 1804 map appears to be a few yards below what is now Hansel's Dam and the mill races that attended both the grist mill and the saw mill have been totally filled in, so much so, that the area no longer bears any resemblance to the mill complex shown on the 1804 map. The following article (p.17) appeared in the New York Times on March 28, 1977. It gives some information concerning the ideals and expectations of the sponsors of the Cranford Heritage Corridor group with respect to the excavation of the Crane Mill ruins. The intentions were doubtless good, but it is difficult at this time to evaluate the competence of the excavators and the quality of the work. To date, no report has been issued concerning this historic excavation. Moreover, the field records were unavailable for inspection and the artifacts deposited with the Cranford Public Library, and from some of the artifacts entrusted to Mr. and Mrs. Roger Goodspeed for care and restoration, indicate that most of the artifacts postdate the 1880's and appear to consist of material used in filling in Sperrie Pond and the ruins. The drawings of the excavations prepared by Dr. Miller are inconclusive (see Map 13) and show only portions of the foundation. Interviews with Roger Goodspeed, Larry Fuhro and others associated with the project, indicate that very few artifacts were recovered and that the area was scarcely penetrated to a depth of 4 feet (see Figs. 7-10).

It is noted that Dr. Peter S. Miller, a physical anthropologist, appears to have had no formal training in historic archaeology. His supervisors, students and volunteers were young and inexperienced. Until recently, the site was left open (Figs. 9,10) to the annoyance of neighbors and park-goers, and this condition also contributed to further vandalism and destruction by local children and seekers of historic artifacts and bottles. A recently received communication from Dr. Miller (May 9, 1977, see Appendix A) indicates that he does not want to do any more excavating on this site. He also states, "Perhaps no further mitigation of the remains is warranted, since further excavation would mostly confirm what is already known."

Mr. Brooks and members of the Cranford Heritage Corridor, Inc., submitted this site for nomination to the State and National Registers of Historic Places. However, the nomination was tabled for lack of sufficient information and documentation. Conversations with Mr. Terry Karshner and others of the Office of Historic Sites and members of the State Review Committee for Historic Sites, of which this writer is a member, indicated that it would be unlikely that this nomination would be accepted unless it could be supported with better documentation and reasons for acceptance.

Inventory of Identified Architectural Resources			
Property	Property Information	Description	NRHP Eligibility
 <p>Photograph Number: 96</p>	<p>Panamerican Resource Number: 96</p> <p>Flood Control Dam</p> <p>Rahway River, Sperry Park Riverside Drive</p> <p>Municipality: Cranford, Union County</p> <p>UTM coordinates (1983 Zone 18N): 450118N, 558961E</p> <p>Date: unknown</p> <p>Other:</p>	<p>Style: none</p> <p>Stories: na</p> <p>Plan: na</p> <p>Roof: na</p> <p>Exterior: concrete</p> <p>Foundation: concrete</p> <p>Windows: na</p> <p>Details: Concrete dam that can be opened and closed to regulate the flow of water in this section of the river.</p>	<p>This structure is not eligible. It is a part of county and/or municipal infrastructure used to control water flow of the Rahway River. Control of flood waters is an important part of governmental responsibility in this part of New Jersey, but the dam does not represent any new technology or special comment (see PRN 118).</p>
 <p>Photograph Number: 97 (north balustrade)</p>	<p>Panamerican Resource Number: 97</p> <p>Bridge (NJDOT #2003016) on Union Avenue crossing the Rahway River at Sperry Park</p> <p>Municipality: Cranford, Union County</p> <p>UTM coordinates (1983 Zone 18N): 4501096N, 559030E</p> <p>Date: 1916</p> <p>Other: Portions of this bridge have been replaced or added to.</p>	<p>Style: Concrete single arch</p> <p>Stories: na</p> <p>Plan: na</p> <p>Roof: na</p> <p>Exterior: combination of closed concrete balustrades with sunk panels and open metal work balustrades</p> <p>Foundation: concrete abutments</p> <p>Windows: na</p> <p>Details: Bridge was rehabilitated with new railings added to some portions. The bridge was designed to blend with the park and the neighborhood. A plaque indicates that the 1916 bridge was erected by Weldon Contracting Co. and the County Engineer was Jacob L. Bauer.</p>	<p>This bridge is eligible as a contributing element to the NRE Rahway River Parkway HD (ID #4079), NRE Union County Park System HD (ID #4424), and NRE North Cranford HD (ID #3838). The A.G. Lichtenstein & Associates survey (1994) recommended it as not eligible.</p>

Panamerican Consultants, Inc.

6-50

Rahway River Phase 1A

New Jersey Department of Environmental Protection
Historic Preservation Office

Page 1

BASE FORM

Historic Sites #:

Property Name: Flood Control Dam on Riverside Drive at Sperry Park (ID #4424.026)**Street Address:** Street #: _____ (Low) _____ (High) Apartment #: _____ (Low) _____ (High)Prefix: _____ Street Name: Riverside Drive Suffix: _____ Type: _____**County(s):** Union **Zip Code:** 07016**Municipality(s):** Cranford **Block(s):** _____**Local Place Name(s):** Sperry Park **Lot(s):** _____**Ownership:** _____ **USGS Quad(s)** Roselle 1981**Photograph:**

Description: Concrete dam that can be opened and closed to regulate the flow of water in this section of the river. Date unknown. This structure is **not eligible**. It is a part of county and/or municipal infrastructure used to control water flow of the Rahway River. Control of flood waters is an important part of governmental responsibility in this part of New Jersey, but the dam does not represent any new technology or special comment. This dam is located in Sperry Park (ID #4424.026) but is not a contributing element to that park.

Registration and Status Dates: National Historic Landmark: _____

SHPO Opinion: _____

National Register: _____

Local Designation: _____

New Jersey Register: _____

Other Designation: _____

Determination of Eligibility: _____

Other Designation Date: _____

Survey Name: Phase IA Cultural Resources Investigation of the Rahway River Flood Risk Management and Ecosystem Restoration Project, Westfield, Townships of Springfield, Cranford, and Union, and Borough of Kenilworth, Union County, New Jersey

Date: May 13, 2013Surveyor: Nolte, Kelly, Donald Smith, Mark Steinback, and Michael Cinquino.Organization: Panamerican Consultants, Inc.

A-250

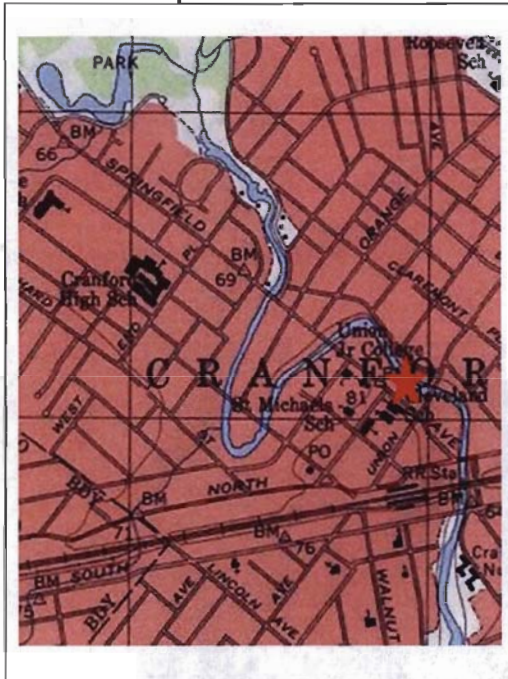
New Jersey Department of Environmental Protection
Historic Preservation Office

Page 2

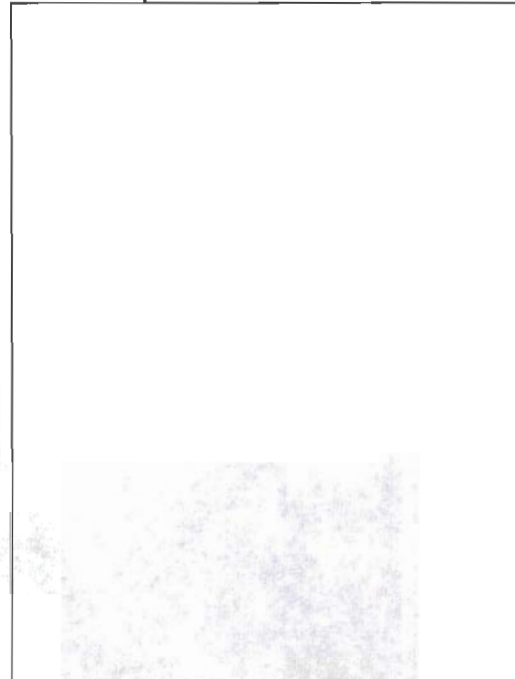
BASE FORM

Historic Sites #:

Location Map:



Site Map:



Bibliography/Sources:

Additional Information:

More Research Needed? ☒ Yes ☐ No**INTENSIVE LEVEL USE ONLY**

Attachments Included:

- ☐ Building ☐ Structure ☐ Object ☐ Bridge
☐ Landscape ☐ Industry

Within Historic District?

- ☐ Yes ☐ No

Status:

- ☐ Key-Contributing ☐ Contributing ☐ Non-Contributing

Associated Archaeological Site/Deposit? ☐ Yes

(Known or potential Sites – if yes, please describe briefly)



JAMES LEFFEL WATER WHEELS

BUILT BY
THE JAMES LEFFEL & CO.

Nearly 30 Years in Business.

Sizes Running: SIX inches to NINETY-SIX inches diameter, under heads ranging from 2 to 350 feet, in many of the finest mills of this country, one company using Fourteen and another Twenty-seven Leffel Wheels.

Many Styles and Varieties on Horizontal Shafts

The Company are now in better condition than ever before to supply these justly celebrated Wheels with dispatch and promptness.

Send for freely illustrated catalogue, and write your wants fully. Address

THE JAMES LEFFEL & CO., SPRINGFIELD, OHIO, U.S.
or 110 LIBERTY STREET, NEW YORK CITY.

hones and barber supplies such as combs, razors, scissors, and razor stones. Because of this merchandise the mill became known as the "Old Stone Works". Among Droesher's customers were the firms of Hammacher-Schlemer, Wilkinson Sword and Montgomery Ward. Droesher employed ten men in the mills. In the 1920's, Droesher was making surgical instrument hones from imported flint, and a decade later, during the depression, he was making curved razor hones which permitted the sharpening and reuse of safety razor blades. Between 1934 and 1935 the turbine ceased to function, but operations continued at the mill for another two years. Droesher died in 1938 and in 1940 or 1941 the equipment was sold to Widen Metal Products in Garwood, N.J. In April 1941, the Leet Organ Company purchased the mill from the Cranford Trust Company. The premises were then used for the manufacture of organ consoles. The building then became known as the "Leet Organ Works". In 1947 Leet sold the building to Dr. Neil Castaldo for use as a rental property. Miniarama, Inc., Ramco Manufacturing Co., and H.E. Sostman & Co. were among the occupants of the mill until it was purchased by Robert A. Brooks in 1971. Brooks renovated the mill and converted it into a professional building. At the time of our survey, Brooks was selling this real estate. An advertising firm "Innoventions" is there now.

The Williams - Droesher Mill has undergone numerous modifications, additions, renovations and replacements over the last century. Nevertheless, it was placed on the State and National Registers of Historic Places on January 8, 1974 as a structure that has been associated with "events that have made a significant contribution to the broad pattern of our heritage".

The Dam at Droesher's Mill.

It has been noted previously (p. 20) that the dam was not specifically included when the Droesher Mill was nominated to the State and National Registers of Historical Places. This structure belongs to the Union County Park Commission. Having been built in 1923, it did not share in the many real estate transactions that characterize the mill. It is not certain whether the original Williams' Mill dam or Vreeland's Mill dam was located at the site of the present dam. Some of the stones in the present dam and along the foot of the concrete dam may have belonged to earlier dams, but this could not be confidently ascertained by Edward J. Lenik, the historic archaeologist, who examined the structure. It is generally agreed that this dam has no historic

value. Documents in the possession of the Union County Park Commission attest to the fact that the present dam was constructed in the 1920's.

In order to improve the flow of the river and alleviate the flooding that has raised havoc in this area in times of heavy rains (Droesher's Mill, for example, has been flooded to the ceilings of the basement), the U.S. Corps of Engineers has recommended that the present dam be replaced by a lower dam. The Union County Park Commission does not object to this proposal, provided that some spillway is maintained to preserve the aesthetic setting of the park. The alternative plan, a bypass channel, has already been mentioned on p. 21.

The Problem of the Leffel Turbine



It has been noted above that the water wheel which once activated the machinery in the Lyon Mill was replaced by the Leffel turbine in the 1890's. A wooden cribbing encompassed the turbine and provided a suitable flow of water to its head. Sometime between 1910 and 1919 this turbine was refurbished and reset by Anthony Della Serra, one of Droesher's foremen. At that time a concrete base and enclosure was constructed to replace the wooden cribbing. Thereafter the turbine continued in use until 1934 or 1935. It has been inactive ever since. Parts of the turbine are now broken and missing, and only a shaft and two drive wheels remain inside the mill itself.



Several thousand dollars have been collected to restore the turbine to active use. However, it is not known how badly the turbine is rusted and ruined. Some estimates of the refurbishing costs have run as high as \$25,000. Additionally it is noted that the concrete housing, sluiceway and gate valve also would require replacement. Moreover, the waterway leading up to the turbine is completely silted in and would require extensive dredging along the eastern embankment of the Rahway River to provide for an adequate flow of water to the turbine.

Since the turbine is privately owned and serves no functional purpose in a building to which the general public has no free access, it would seem to me, and the other historians such as Edward J. Lenik, Larry Fuhiro and Robert Fridlington, that the public funds needed to restore this facility would be better spent on another, truly public enterprise. There certainly is no good historical/cultural reason why this turbine should be reactivated, and it certainly has no aesthetic appeal. Therefore, if the turbine is not to be reconstructed, there would be no reason why the dam could not be lowered to improve the flow of the river. This opinion is shared by the members of the Union County Park Commission which owns the dam. If, however, the Leffel turbine is reactivated, then an adequate head of water would be required, at least along the east bank of the river, to assure the hydrolic power.

The High Street Bridge. (Fig. 15).

The dredging of the river and the replacement of the tripple arch bridge that now spans the Rahway River at High Street, below the Droesher's Mill, will pose no problems, because this bridge is not of historic value. It is generally conceded by the members of the Union County Park Commission, local historians and residents of the area, that this bridge is a major causative factor of flooding in this area since, in fact, it impedes the flow of the river in times of high water.

Inventory of Identified Architectural Resources			
Property	Property Information	Description	NRHP Eligibility
 <p>Photograph Number: 116</p>	<p>Panameric Resource Number: 116</p> <p>Droescher's Mill and Dam</p> <p>347 Lincoln Avenue</p> <p>Municipality: Cranford, Union County</p> <p>UTM coordinates (1983 Zone 18N): 4500184N, 559080E</p> <p>Date: 1821 (mill); 1985 (dam)</p> <p>Other: This resource falls under the Cranford Section (ID #4424.027) of the NRE Rahway River Parkway HD.</p>	<p>Style: na</p> <p>Stories: 2 with basement</p> <p>Plan: rectangular</p> <p>Roof: gambrel</p> <p>Exterior: wood siding</p> <p>Foundation: rock</p> <p>Windows: 6/6 wood with storm windows</p> <p>Details: The dam was rebuilt by Union County in 1985; the mill building itself has undergone a number of changes and additions.</p>	<p>The mill and millrace are listed on the NRHP (NR #74001192). The dam appears to be a complete rebuild of dams that were traditionally at this site. Panamerican recommends the dam as not eligible (see PRN 96).</p>
 <p>Photograph Number: 117</p>	<p>Panameric Resource Number: 117</p> <p>Bridge (NJDOT #2003045) at Lincoln Avenue crossing the Rahway River at Droescher's Mill</p> <p>Municipality: Cranford, Union County</p> <p>UTM coordinates (1983 Zone 18N): 4500159N, 559035E</p> <p>Date: ca. 2010</p> <p>Other: This bridge was recorded because it was directly inside the 100' APE across the river.</p>	<p>Style: concrete stringer</p> <p>Stories: na</p> <p>Plan: na</p> <p>Roof: na</p> <p>Exterior: enclosed concrete balustrade with rubble rock veneer topped by decorative open work metal railings and lanterns</p> <p>Foundation: concrete abutments</p> <p>Windows: na</p> <p>Details: The current bridge replaced an earlier bridge in this location. The current bridge design blends with the park and residential neighborhood.</p>	<p>Although this is a county bridge and not yet 50 years old, it should be considered a contributing element to both the NRE Rahway River Parkway HD (ID #4079) and Union County Park System HD (ID #4424).</p>

Inventory of Identified Architectural Resources			
Property	Property Information	Description	NRHP Eligibility
 <p>Photograph Number : 13</p>	<p>Panamerican Resource Number: 13</p> <p>Dam/spillway on Rahway River</p> <p>Lenape Park</p> <p>Municipality: Kenilworth, Union County</p> <p>UTM coordinates (1983 Zone 18N): 4502777N, 558037E</p> <p>Date: unknown</p> <p>Other:</p>	<p>Style: na</p> <p>Stories: na</p> <p>Plan: na</p> <p>Roof: na</p> <p>Exterior: concrete</p> <p>Foundation: concrete abutments</p> <p>Windows: na</p> <p>Details: an open flood-control structure</p>	<p>Not eligible and non-contributing; one of many later additions to the Rahway River made in an attempt to control flooding in the area.</p>
 <p>Photograph Number: 14</p>	<p>Panamerican Resource Number: 14</p> <p>Route 509/Kenilworth Boulevard, east and west bound bridges (NJDOT #2003018), crossing Rahway River between Lenape and Nomahegan parks</p> <p>Municipality: Kenilworth, Union County</p> <p>UTM coordinates (1983 Zone 18N): 4502712N, 558068E</p> <p>Date: 1992 (sign on bridge)</p> <p>Other:</p>	<p>Style: rustic, Adirondack-style type</p> <p>Stories: na</p> <p>Plan: 2-bay</p> <p>Roof: na</p> <p>Exterior: concrete with stone veneer enclosed balustrade</p> <p>Foundation: concrete abutments</p> <p>Windows: na</p> <p>Details: the current bridges replaced earlier bridges at this site. An attempt was made to make these bridges blend in with the park through the use of stone.</p>	<p>Although the east and west bound bridges are not yet 50 years old, they should be considered eligible as contributing elements to both the NRE Rahway River Parkway HD (ID #4079) and the Union County Park System HD (ID #4424).</p>

DeLeuw, Cather and Co.
Engineering-Science, Inc.

NJ Transit Historic Railroad Bridge Survey
Structure Inventory Form

LOCATION

NJ TRANSIT Line	Raritan Valley
Town/City	Cranford
County	Union
USGS Quad	Roselle

Milepost	16.02
Feature Crossed	Rahway River
Common Name	(242) Rahway River
UTM Ref.	18.590060.4500560

PHYSICAL SUMMARY

Structure Type	Concrete Arch
Overall Length	141'
Spans	3
Material(s)	Reinforced Concrete
Skew	90
Inspection Report	No
Bridge Typology Code	XX XX XX XX

Deck Type	Ballasted
Width	85'
Span Length	38'
Design Loading	E-60
Tracks	6
Condition	Good

HISTORICAL SUMMARY

Date	1929
Designer	CNJ
Patent	

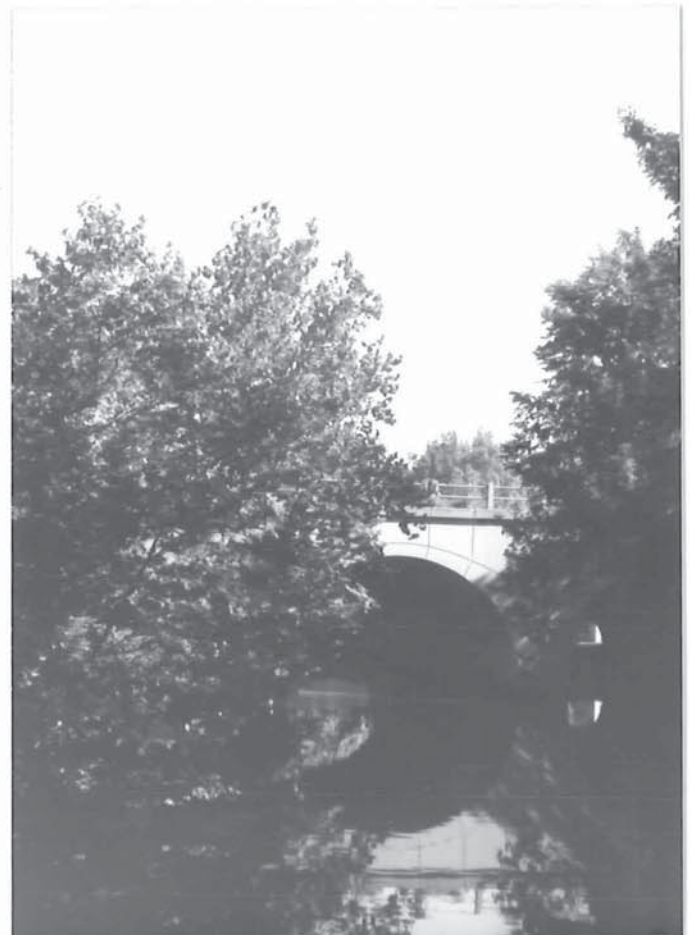
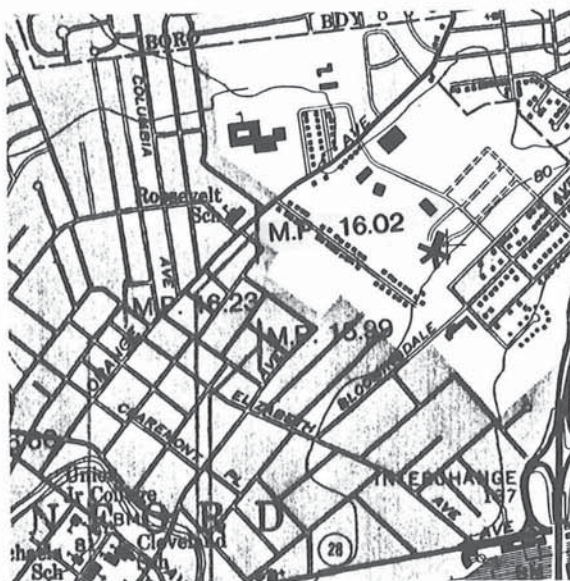
Date(s) Rebuilt	
Fabricator	
Historic Rail Line	CNJ Central Div., Main Line

CULTURAL RESOURCE EVALUATIONS

National Register Status	
State Register Status	
Local Landmark Designation	
HAER Doc. #	
Type and Date of HAER documentation	

National Register Date	
State Register Date	

GRAPHICS



and Co.
Engineering-Science, Inc.

NJ Transit Historic Railroad Bridge Survey
Structure Inventory Form

PHYSICAL DESCRIPTION OF BRIDGE AND SETTING

16.02 Rahway River is a three span reinforced concrete arch bridge. The span length is 40' and piers and abutments are concrete with granite faced ends and corners. The bridge has a concrete parapet that is crowned with galvanized iron pipe handrailings. The bridge has been altered significantly; one span which crosses the road has been completely filled in and is used as storage space. The only two spans remaining are crossing the river.

STATEMENT OF TECHNOLOGICAL MERIT AND HISTORICAL SIGNIFICANCE

16.02 Rahway River was constructed as part of a grade crossing elimination in the town of Cranford by the CNJ during 1929. The project included four structures: 15.99 Centennial Avenue, 16.02 Rahway River, 16.23 Walnut Street and 16.66 Lincoln Avenue. This grade crossing elimination project does not have historical significance due to its late date and lack of associational characteristics. These projects, which had become a national issue by 1900, were sometimes characterized by highly publicized negotiations between the towns and railroads. The bridges in this project are also not considered technologically significant as railroad structures. 16.02 in particular has lost much integrity in design since one of its three concrete arch spans has been completely filled in. The four bridges in Cranford were built on the Main Line of the CNJ's Central Division which provided passenger and freight service.

RECOMMENDATION OF ELIGIBILITY TO NATIONAL REGISTER OF HISTORIC PLACES

Not eligible

PRIMARY AND SECONDARY SOURCES

See New Jersey Transit Historic Railroad Bridge Survey, Report Bibliography.

Survey Team A. Cottrell, J. Galvin
Survey Date 08/01/90

Reviewer A.C.
Review Date 03/12/91

Photographs P18/2
Slides S17/36-38

New Jersey Department of Environmental Protection
Historic Preservation Office

Page 1 of 1

CONTINUATION SHEET

Historic Sites #:

Resource Name: Raritan Valley Line over the Rahway River, MP 16.02

Municipality: Cranford

County: Union

Additional Information:

Locator map on 1991 survey form does not show crossing. (See Attached)

Representation in Existing Surveys:

This bridge was surveyed in 1991 as part of NJ Transit's *Historic Railroad Bridge Survey*.

National Register Eligibility:

The 1991 survey only evaluated the bridge for its individual eligibility, not as a component of a historic district. In that context, the bridge was evaluated as not eligible for inclusion in the National Register of Historic Places.

In 1995, the entire Central Railroad of New Jersey Main Line was determined to be eligible for inclusion in the National Register of Historic Places as a linear historic district. Therefore, the purpose of the current study is to determine if individual resources such as bridges contribute to the historic district's overall significance.

As a resource that falls within the historic district's period of significance, the bridge has been determined to contribute to the CRRNJ Main Line Historic District.

Attachments:

- Locator map
- 1991 NJ Transit Historic Railroad Bridge Survey form

Survey Name: NJ Transit Raritan Valley Line Study
Surveyor: Nancy L. Zerbe
Organization: ARCH², Inc.

Date: July, 1999

New Jersey Department of Environmental Protection
Historic Preservation Office

Page 1 of 1

CONTINUATION SHEET

Historic Sites #:

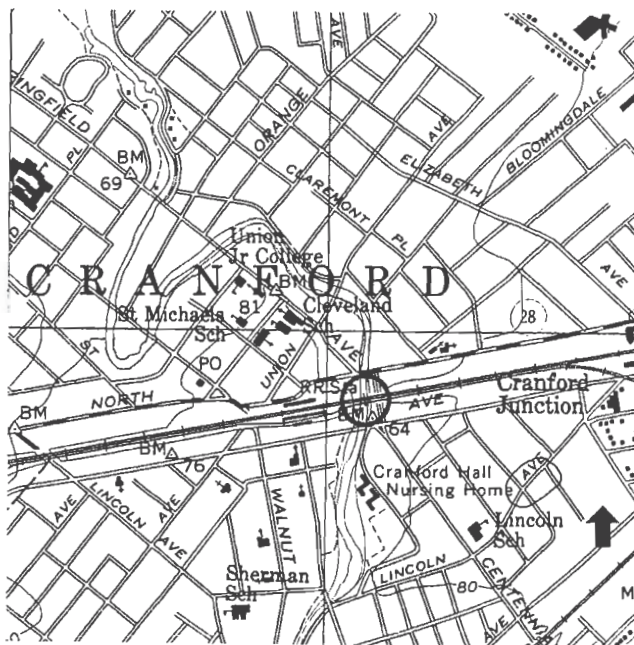
Resource Name: Raritan Valley Line over the Rahway River, MP 16.02

Municipality: Cranford

County: Union



Additional Information:

Locator map on 1991 survey form does not show crossing.


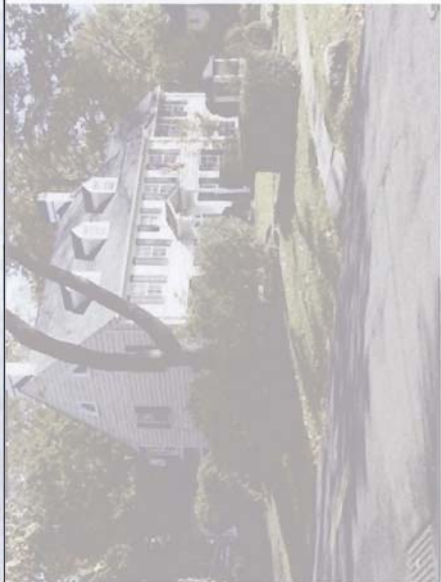




Survey Name: NJ Transit Raritan Valley Line Study
Surveyor: Nancy L. Zerbe
Organization: ARCH², Inc.

Date: July, 1999

Inventory of Identified Architectural Resources			
Property	Property Information	Description	NRHP Eligibility
 <p>Photograph Number: 106 (south side crossing Centennial Avenue)</p>	<p>Panamerican Resource Number: 106</p> <p>Central Railroad of New Jersey (CNJ) Bridge at Centennial Avenue (NJDOT #2063150) and crossing Rahway River (NJDOT #1063151)</p> <p>Municipality: Cranford, Union County</p> <p>UTM coordinates (1983 Zone 18N): 4500796N, 559165E</p> <p>Date: 1929 (plaque)</p> <p>Other:</p>	<p>Style: Concrete quadruple arches over river and deck girder over Centennial Avenue</p> <p>Stories: na</p> <p>Plan: na</p> <p>Roof: na</p> <p>Exterior: concrete</p> <p>Foundation: concrete abutments at Centennial Avenue and concrete arches fortified with stone and concrete abutments at river</p> <p>Windows: na</p> <p>Details: Two arches actually span the river while a third springs immediately from the west bank. A fourth arch has been infilled and is used for offices, storage, and bathrooms (see PRN 107).</p>	<p>This structure is eligible as a contributing element to the CNJ Main Line Corridor HD (ID #3500), which was determined eligible for the NRHP by the Secretary of the Interior on November 30, 1995. HD includes associated structures that extend beyond the actual rail bed.</p>
 <p>Photograph Number: 107 (note the infilled fourth arch of the NJRR bridge)</p>	<p>Panamerican Resource Number: 107</p> <p>Storage Building</p> <p>No # Centennial Avenue</p> <p>Municipality: Cranford, Union County</p> <p>UTM coordinates (1983 Zone 18N): 4500775N, 559143E</p> <p>Date: unknown</p> <p>Other: Structure sits right on the Rahway River.</p>	<p>Style: This storage building is associated with the CNJ Bridge (see PRN 106) and has the spare look of the bridge</p> <p>Stories: 1</p> <p>Plan: rectangular</p> <p>Roof: flat</p> <p>Exterior: concrete</p> <p>Foundation: concrete</p> <p>Windows: none—ventilation louver in place of traditional window</p> <p>Details: This storage facility is located immediately outside the "offices" for the CNJ bridge that are located in the infilled fourth arch of the bridge.</p>	<p>This structure is eligible as a contributing element to the CNJ Main Line Corridor HD (ID #3500), which was determined eligible for the NRHP by the Secretary of the Interior on November 30, 1995. HD includes associated structures that extend beyond the actual rail bed.</p>

Inventory of Identified Architectural Resources

Property	Property Information	Description	NRHP Eligibility
 <p>Photograph Number: 31</p>	<p>Panamerican Resource Number: 31</p> <p>Crane's Ford Monument</p> <p>Riverside Drive at Springfield Avenue, east bank of Rahway River</p> <p>Municipality: Cranford, Union County</p> <p>UTM coordinates (1983 Zone 18N): 4501420N, 558499E</p> <p>Date: 1929</p> <p>Other: The river bank at this point has been built up since Crane's and Washington's occupation.</p>	<p>Style: bronze plaque on boulder</p> <p>Stories: na</p> <p>Plan: na</p> <p>Roof: na</p> <p>Exterior: na</p> <p>Foundation: cement and rock</p> <p>Windows: na</p> <p>Details: The plaque indicates that the monument was erected on July 4, 1929 by the Cranford Historical Society at the site of Crane's Ford, which was guarded during the Revolutionary War by Light Horseman from Washington's Army at Morristown.</p>	<p>This monument should be considered eligible as a contributing element to the NRE Rahway River Parkway HD (ID #4079) and the North Cranford HD (ID #3838).</p>
 <p>Photograph Number: 32</p>	<p>Panamerican Resource Number: 32</p> <p>420 Riverside Drive</p> <p>Municipality: Cranford, Union County</p> <p>UTM coordinates (1983 Zone 18N): 4501446N, 558539E</p> <p>Date: ca. 1920</p> <p>Other: This property overlooks Memorial Park on the front (south) and the Rahway River Parkway on the west side.</p>	<p>Style: Colonial Revival</p> <p>Stories: 2.5</p> <p>Plan: rectangular</p> <p>Roof: gable</p> <p>Exterior: wood</p> <p>Foundation: obscured</p> <p>Windows: 6/6 wood with storm windows, some fixed decorative, some Gibb windows; operational shutters</p> <p>Details: This house is in exceptionally good condition and, although it has a large rear addition, it has retained its original look and feeling.</p>	<p>This house is eligible as a contributing element to the North Cranford HD (ID #3838).</p>

Inventory of Identified Architectural Resources			
Property	Property Information	Description	NRHP Eligibility
 <p>Photograph Number: 33</p>	<p>Panamerican Resource Number: 33</p> <p>Rock Wall</p> <p>Riverside Drive immediately west of 420 Riverside Drive</p> <p>Municipality: Cranford, Union County</p> <p>UTM coordinates (1983 Zone 18N): 4501461N, 558512E</p> <p>Date: ca. 1930s</p> <p>Other: Possibly a WPA or CCC construction project</p>	<p>Style: Rustic</p> <p>Stories: na</p> <p>Plan: na</p> <p>Roof: na</p> <p>Exterior: rusticated cut stone, random pattern, topped with stone ledge</p> <p>Foundation: none</p> <p>Windows: NA</p> <p>Details: This stone wall stands approximately 2.5' and acts both as a levee and a road guard.</p>	<p>This stone wall should be considered eligible as a contributing element to the NRE Rahway River Parkway HD (ID #4079).</p>
 <p>Photograph Number: 34</p>	<p>Panamerican Resource Number: 34</p> <p>652 Riverside Drive</p> <p>Municipality: Kenilworth, Union County</p> <p>UTM coordinates (1983 Zone 18N): 4502573N, 558259E</p> <p>Date: ca. 2005</p> <p>Other: This house was surveyed because it lies directly in the flood plain. The house overlooks Nomahegan Park.</p>	<p>Style: Post Modern</p> <p>Stories: 2.5</p> <p>Plan: irregular</p> <p>Roof: multiple</p> <p>Exterior: drivet</p> <p>Foundation: obscured</p> <p>Windows: multiple metal types</p> <p>Details: modern house with a corner tower and walled yard</p>	<p>Not eligible, does not yet meet criteria</p>

New Jersey Department of Environmental Protection
Historic Preservation Office

Page 1

BASE FORM

Historic Sites #:

Property Name: Rock Wall, Riverside Drive, immediately west of 420 Riverside Drive

Street Address: Street #: _____ (Low) _____ (High) Apartment #: _____ (Low) _____ (High)

Prefix: _____ Street Name: Riverside Drive Suffix: _____ Type: RD

County(s): Union **Zip Code:** 07016

Municipality(s): Cranford

Block(s): _____

Local Place Name(s): _____

Lot(s): _____

Ownership: _____ **USGS Quad(s)** Roselle 1981

Photograph:



Description: Possibly a WPA or CCC construction project. Rusticated cut stone, random patterns, topped with stone ledge. Stands approximately 2.5' and acts both as a levee and a road guard. The stone should be considered eligible as a contributing element to the NRE Rahway River Parkway HD (ID #4079).

**Registration and
Status Dates:**

National Historic
Landmark: _____

SHPO Opinion: _____

National Register: _____

Local Designation: _____

New Jersey Register: _____

Other Designation: _____

Determination of Eligibility: _____

Other Designation Date: _____

Survey Name: Phase IA Cultural Resources Investigation of the Rahway River Flood Risk Management and Ecosystem Restoration Project, Westfield, Townships of Springfield, Cranford, and Union, and Borough of Kenilworth, Union County, New Jersey

Date: May 13, 2013

Surveyor: Nolte, Kelly, Donald Smith, Mark Steinback, and Michael Cinquino.

Organization: Panamerican Consultants, Inc.

A-263

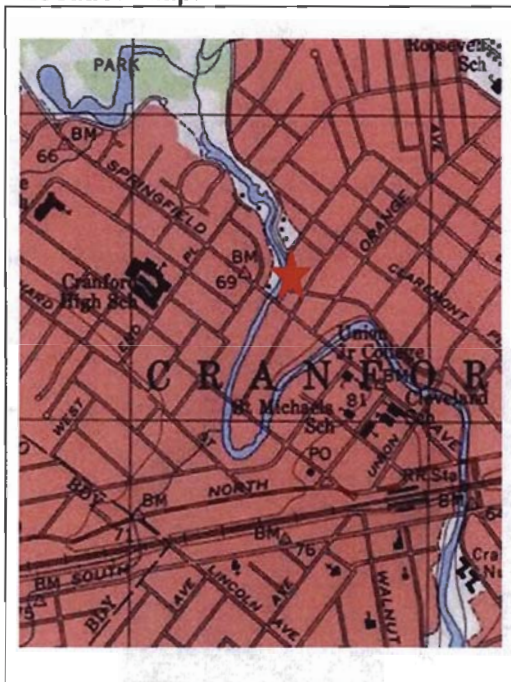
New Jersey Department of Environmental Protection
Historic Preservation Office

Page 2

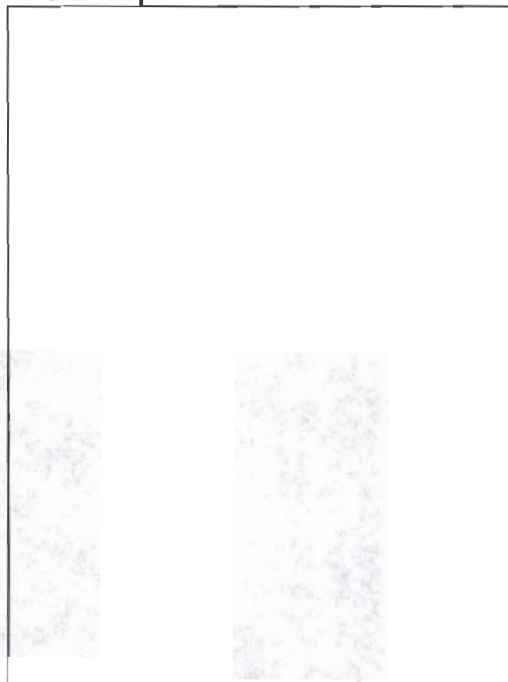
BASE FORM

Historic Sites #: 1000

Location Map:



Site Map:



Bibliography/Sources:

Additional Information:

More Research Needed? ☒ Yes ☐ No**INTENSIVE LEVEL USE ONLY**

Attachments Included:

- ☐ Building ☐ Structure ☐ Object ☐ Bridge
☐ Landscape ☐ Industry

Within Historic District?



- ☐ Yes ☐ No



Status:

- ☐ Key-Contributing ☐ Contributing ☐ Non-Contributing

Associated Archaeological Site/Deposit? ☐ Yes

(Known or potential Sites – if yes, please describe briefly)

Inventory of Identified Architectural Resources			
Property	Property Information	Description	NRHP Eligibility
 <p>Photograph Number: 74</p>	<p>Panamerican Resource Number: 74</p> <p>Sewer Outfall Structure</p> <p>Riverside Drive north of Venetia Drive</p> <p>Municipality: Cranford, Union County</p> <p>UTM coordinates (1983 Zone 18N): 4502207N, 558314E</p> <p>Date: unknown</p> <p>Other: Located directly on the levee between Nomahegan and Memorial parks.</p>	<p>Style: na</p> <p>Stories: na</p> <p>Plan: na</p> <p>Roof: na</p> <p>Exterior: within a chain-link fence</p> <p>Foundation: concrete, raised</p> <p>Windows: na</p> <p>Details: This is a sewer outfall into the Rahway River.</p>	<p>Although the outfall structure is located within two historic districts, it is not eligible and not a non-contributing element to either one. If plans are made to upgrade or change the outfall, it should be made less conspicuous.</p>
 <p>Photograph Number: 75 (Richmond Avenue side)</p>	<p>Panamerican Resource Number: 75</p> <p>Foot Bridge crossing Rahway River between Balmiere Parkway and Richmond Avenue</p> <p>Municipality: Cranford, Union County</p> <p>UTM coordinates (1983 Zone 18N): 4501341N, 558321E</p> <p>Date: unknown</p> <p>Other:</p>	<p>Style: Rustic</p> <p>Stories: na</p> <p>Plan: na</p> <p>Roof: na</p> <p>Exterior: wood balustrade</p> <p>Foundation: concrete abutments</p> <p>Windows: na</p> <p>Details: This bridge connects the levees running alongside the Rahway River from Nomahegan Park to the intersection of Riverside Drive and Springfield Avenue. The levees have walking paths on their tops and are used extensively in this area. This bridge connects the north and south Cranford neighborhoods to the park and its walking paths.</p>	<p>This bridge should be considered a contributing element to the NRE Rahway River Parkway HD (ID #4079) and NRE Union County Park System HD (ID #4424).</p>

Inventory of Identified Architectural Resources			
Property	Property Information	Description	NRHP Eligibility
 <p>Photograph Number: 92 (east side of contemporary house)</p>	<p>Panamerican Resource Number: 92</p> <p>House and Culvert</p> <p>107 Riverside Drive (see PRN 91)</p> <p>Municipality: Cranford, Union County</p> <p>UTM coordinates (1983 Zone 18N): 4501362N, 558768E</p> <p>Date: unknown</p> <p>Other: This contemporary house was photographed only on the side in relationship to the culvert.</p>	<p>Style: concrete culvert</p> <p>Stories: na</p> <p>Plan: na</p> <p>Roof: na</p> <p>Exterior: metal fence blocks immediate access to the culvert</p> <p>Foundation: concrete</p> <p>Windows: na</p> <p>Details: The culvert allows a channelized stream to flow under Riverside Drive and into the Rahway River at 91 and 92 Riverside Drive.</p>	<p>The house is non-contributing to the NRE North Cranford HD since its construction falls outside of the period of significance, and is not eligible for the NRHP since it fails to meet any of the eligibility criteria. The culvert and walls should be considered eligible as contributing elements to the district.</p>
 <p>Photograph Number: 93</p>	<p>Panamerican Resource Number: 93</p> <p>Apartment Building</p> <p>22 Riverside Drive at Casino Avenue</p> <p>Municipality: Cranford, Union County</p> <p>UTM coordinates (1983 Zone 18N): 4501326N, 558864E</p> <p>Date: ca. 1970</p> <p>Other: Overlooks Rahway River and Hanson Park</p>	<p>Style: Contemporary with faux Mansard roof</p> <p>Stories: 4</p> <p>Plan: L shape</p> <p>Roof: flat</p> <p>Exterior: brick, Mansard roof is metal</p> <p>Foundation: concrete</p> <p>Windows: combination of metal types including glass doors that lead onto balconies</p> <p>Details: modern apartment building in an area of apartment buildings</p>	<p>Outside of the North Cranford HD; it is not eligible since it does not yet meet the age criteria.</p>

Panamerican Consultants, Inc.

6-48

Rahway River Phase 1A

Union County Parks Survey

Park Name **Rahway River Parkway: Sperry Park**
 Township/City **Cranford**

photo



Type

☐ recreational ☐ pastoral ☐ picturesque ☐ natural ☐ scenic ☐ other:

Existing Topography

☐ flat ☐ sloped ☐ hilly ☐ mountainous ☐ other:

Spatial Relationships

☐ symmetrical ☐ asymmetrical ☐ axial

Circulation

☐ trails ☐ concrete, asphalt

☐ cycling paths

☐ bridle paths

☐ parking lots

☐ handicapped ramps

☐ other:

Parkways

☐ Border roads ☐ Connecting pleasure roads ☐ Other: transecting
☐ Internal park roads ☐ River parkways
☐ Vernacular roads ☐ Estate roads
☐ Summit roads ☐ Ocean parkways

Surveyor Gregory Dietrich

Date 10/21/03

Union County Parks Survey

Drainage & Engineering

<input type="checkbox"/> Bridge (road)	(See bridge info)
<input type="checkbox"/> Bridge (pedestrian)	
<input type="checkbox"/> Channel	
<input type="checkbox"/> Culvert	concrete
<input type="checkbox"/> Spillway	concrete w/ mechanical remnants (c.1938)
<input type="checkbox"/> Millrace	
<input type="checkbox"/> Retaining wall	concrete
<input type="checkbox"/> Storm sewer	
<input type="checkbox"/> Swale	
<input type="checkbox"/> Other:	

Bridge

carries	bridge date	info source	bridge type/design	bridge material
North Union Avenue	1916	Plaque	elliptical	steel & concrete w/ cast iron railings
Springfield Avenue	1916/1979	A.G. Lichtenstein	thru girder partially	steel w/ cast iron railings
Springfield Avenue	1914	A.G. Lichtenstein	encased thru girder partially encased	steel w/ cantilevered sidewalks & cast iron railings

Landscape Dividers

--

Land Uses

<input type="checkbox"/> archery	<input type="checkbox"/> skating--sidewalk
<input type="checkbox"/> baseball	<input type="checkbox"/> soccer
<input type="checkbox"/> basketball	<input type="checkbox"/> softball
<input type="checkbox"/> boating/canoeing	<input type="checkbox"/> swimming
<input type="checkbox"/> biking	<input type="checkbox"/> tennis
<input type="checkbox"/> camping	<input type="checkbox"/> theater/music
<input type="checkbox"/> concession/dining	<input type="checkbox"/> track & field
<input type="checkbox"/> cricket	<input type="checkbox"/> trap/skeet shooting
<input type="checkbox"/> driving	<input type="checkbox"/> trail
<input type="checkbox"/> fishing	<input type="checkbox"/> wading
<input type="checkbox"/> football	<input type="checkbox"/> other:
<input type="checkbox"/> golf	
<input type="checkbox"/> gymnastics	
<input type="checkbox"/> handball court	
<input type="checkbox"/> hockey	
<input type="checkbox"/> horseback riding	
<input type="checkbox"/> horticulture/gardening	
<input type="checkbox"/> lawn bowling	
<input type="checkbox"/> model boating	
<input type="checkbox"/> picnicking	
<input type="checkbox"/> playground	
<input type="checkbox"/> shuffleboard	
<input type="checkbox"/> skating--rink	

Water Elements

<input type="checkbox"/> rivers	<input type="checkbox"/> creeks	<input type="checkbox"/> pools
<input type="checkbox"/> lakes	<input type="checkbox"/> ponds	<input type="checkbox"/> fountains
<input type="checkbox"/> brooks	<input type="checkbox"/> waterfalls	<input type="checkbox"/> other :

Buildings

<input type="checkbox"/> maintenance	<input type="checkbox"/> stable	<input type="checkbox"/> other:
<input type="checkbox"/> recreational	<input type="checkbox"/> house	
<input type="checkbox"/> concession	<input type="checkbox"/> rest rooms	
<input type="checkbox"/> picnic shelter	<input type="checkbox"/> mill	
<input type="checkbox"/> gazebo	<input type="checkbox"/> museum/visitor center	
<input type="checkbox"/> bandstand	<input type="checkbox"/> public utility	

Landscape Furniture

<input type="checkbox"/> benches	<input type="checkbox"/> planters
<input type="checkbox"/> bleachers	<input type="checkbox"/> trash cans
<input type="checkbox"/> bike racks	<input type="checkbox"/> other:
<input type="checkbox"/> bollards	
<input type="checkbox"/> drinking fountains	
<input type="checkbox"/> fitness trail stations	
<input type="checkbox"/> grills	
<input type="checkbox"/> lighting fixtures	
<input type="checkbox"/> picnic tables	

Union County Parks Survey

Description	<p>The Sperry Park section of the Rahway River Parkway consists of two sections divided by Union Avenue. The northern half contains a small vest pocket park along the east side with scattered mature trees and two concrete paths. One path runs along Riverside Drive while the other diverges toward the Rahway River. A spillway (c.1938) is located in the river within this section with a retaining wall running alongside of it. On the opposite side of the river, there is newer path paved in asphalt that abruptly terminates. There are two memorial markers along this path commemorating the site of Crane's Mill, and the Cranford victims of September 11th, 2001</p> <p>The historic Crane-Phillips House is located on the south side of Union Avenue, and occupies the area to the west of the park along with a covered bench. An asphalt trail links Union and North avenues, running parallel to the river. There is a contemporary gazebo located along this path, which is bordered by municipal parking lots to the west and dense thickets of trees along the river to the east. An elliptical bridge made of steel and concrete with cast iron railings (1916) carries North Union Avenue.</p>
Setting	<p>Sperry Park is bordered by Orange Avenue to the north, Riverside Drive and Forest Avenue to the east, North Avenue to the south, and the rear lots along Springfield Avenue to the west. 3-story multi-family dwellings (c.1915) are located along Riverside Drive to the east, and 2½-story single-family dwellings (c.1920-1950) are located along the east bank of the Rahway River. The Driescher's Mill section of the Rahway River Parkway is located to the south of North Avenue. 6-story multi-family residential buildings (c.1980) and 2-story institutional buildings (Cranford municipal complex, c.1940) and a 1-story commercial building (c.1998) are located along Springfield Avenue.</p>

Surveyor Gregory Dietrich

Date 10/21/03

Union County Parks Survey

Markers/ uments/ apture	boulder plaque commemorating Crane's Mills, dedicated in 1977 boulder plaque commemorating site of Josiah Crane, Sr.'s mill, dedicated in 1971 colonnade memorializing Cranford Twp's victims of September 11th, 2001
Signage	metal
Utilities	
Plantings	mature oaks, maples, catalpas
CRM issues	NJ-HPO ID No. 4079: Rahway River Parkway Historic District; Survey Form reference number (40) NJ-HPO ID No. 246: Springfield Avenue Bridge over the Rahway River; Survey Form reference number (41) NJ-HPO ID No. 3474: Crane-Phillips House; Survey Form reference number (42) NJ-HPO ID No. 4053: Central Railroad of NJ Bridge over Rahway River; Survey Form reference number (35)

Character Defining
Features/Resources

Contributing Features

Non-Contributing Features

Rahway River
Overall configuration of park
Crane-Phillips House
1916 bridge carrying North Union Ave.
1916/1979 bridge carrying Springfield Ave.
1914 bridge carrying Springfield Ave.
Concrete spillway w/ mechanical remnants (c.1938)

Scattered mature trees w/ grassy area
Mature trees along the Rahway River

Union County Parks Survey

Contributing Resources

Non-Contributing Resources

Crane's Mills boulder plaque
 Josiah Crane, Sr.'s mill boulder plaque
 Victims of September 11th, 2001 colonnade memorial
 Gazebo
 Covered bench

Boundary of eligible portion of park

All of the property contained within Block 196, Lot 2 of the Township of Cranford, Union County, New Jersey.

Eligible

☐ yes
☐ no

Rating

☐ Key Contributing ☐ Contributing ☐ Non-Contributing

Areas of Significance

<input type="checkbox"/> Agriculture	<input type="checkbox"/> Economics	<input type="checkbox"/> Industry	<input type="checkbox"/> Performing Arts
<input type="checkbox"/> Architecture	<input type="checkbox"/> Education	<input type="checkbox"/> Invention	<input type="checkbox"/> Philosophy
<input type="checkbox"/> Art	<input type="checkbox"/> Engineering	<input type="checkbox"/> Landscape Architecture	<input type="checkbox"/> Politics
<input type="checkbox"/> Commerce	<input type="checkbox"/> Entertainment/Recreation	<input type="checkbox"/> Law	<input type="checkbox"/> Religion
<input type="checkbox"/> Communications	<input type="checkbox"/> Ethnic Heritage	<input type="checkbox"/> Literature	<input type="checkbox"/> Science
<input type="checkbox"/> Community Planning	<input type="checkbox"/> Exploration/Settlement	<input type="checkbox"/> Maritime History	<input type="checkbox"/> Social History
<input type="checkbox"/> Conservation	<input type="checkbox"/> Health/Medicine	<input type="checkbox"/> Military	<input type="checkbox"/> Transportation

Justification of Eligibility

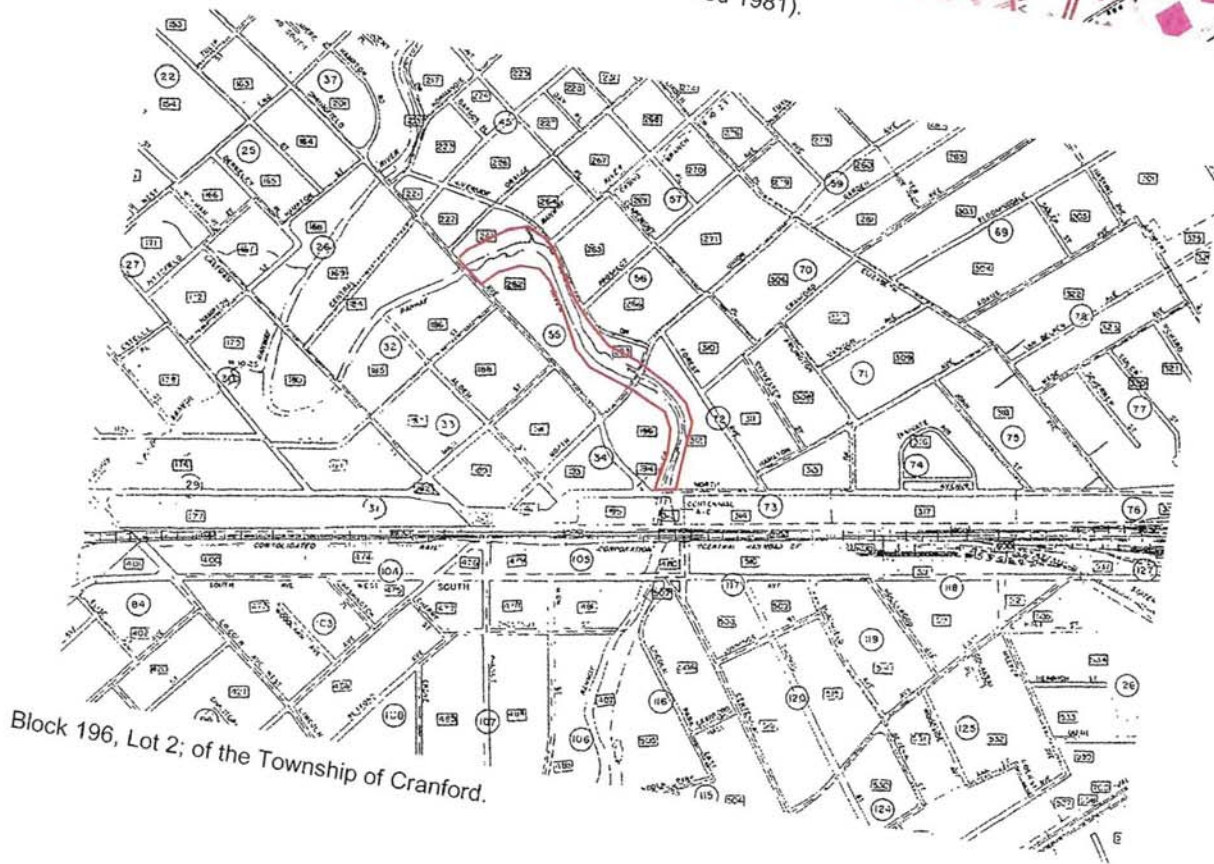
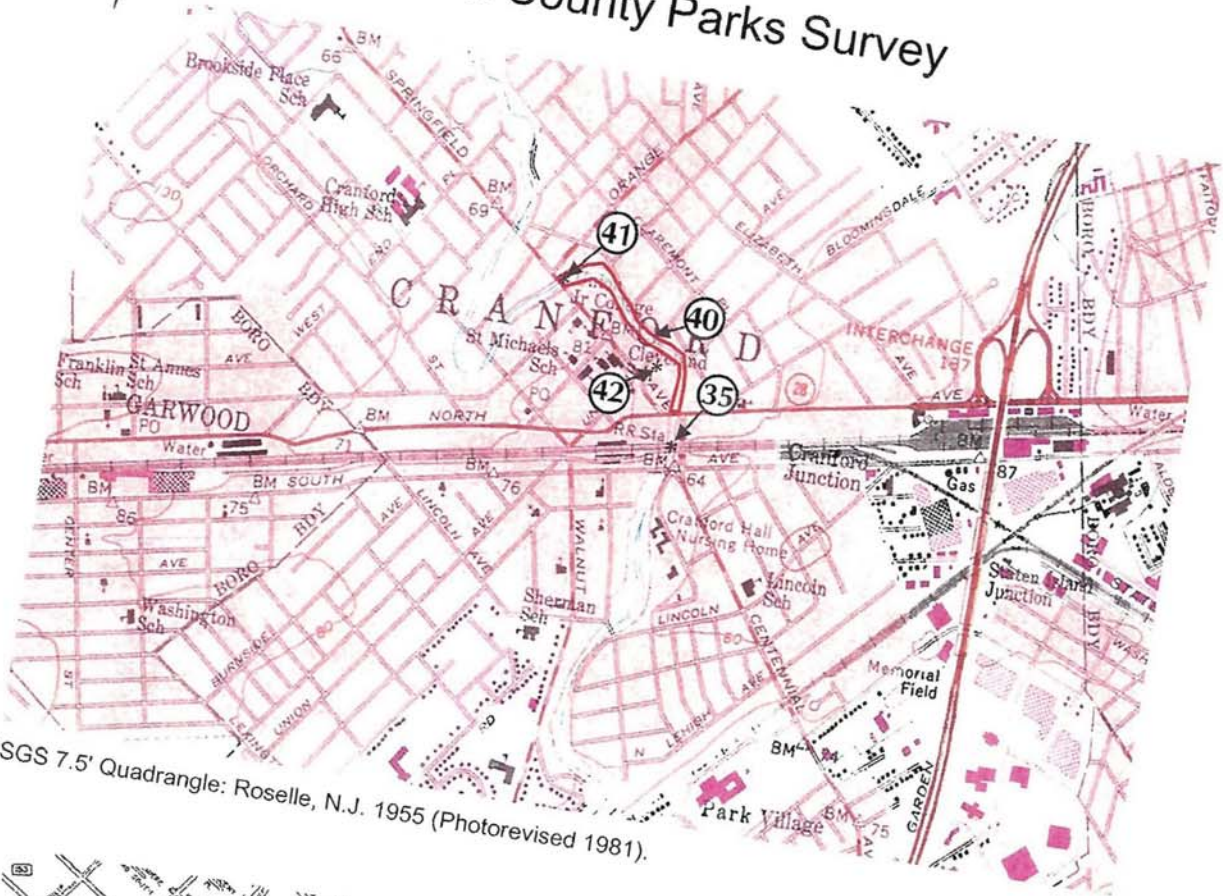
Acquired and developed within the period of significance (1921-1964), Rahway River Parkway: Sperry Park is eligible under Criterion A for having made contributions to the broad patterns of history in the areas of Community Planning, Entertainment/Recreation, and Landscape Architecture on the national level; and in the area of Conservation on the state level. Under Criterion C, it is significant for its association with the nationally renowned Olmsted Brothers Landscape Architecture Firm of Brookline, Massachusetts. Under Criterion D, it is eligible for having the capacity to yield important information related to history. With regard to historic integrity, in spite of numerous bridge replacements along its waterway, Rahway River Parkway has retained most of its original location, design, feeling and association.

Surveyor Gregory Dietrich



Date 10/21/03



A-271



Union County Parks Survey



Gregory Dietrich
10/21/03

Inventory of Identified Architectural Resources			
Property	Property Information	Description	NRHP Eligibility
 <p>Photograph Number: 94 (section on Riverside Drive)</p>	<p>Panamerican Resource Number: 94</p> <p>Sperry Park (ID #4424.026)</p> <p>Riverside Drive/North Union Avenue/Springfield Avenue</p> <p>Municipality: Cranford, Union County</p> <p>UTM coordinates (1983 Zone 18N): 4501156N, 558955E</p> <p>Date: ca. 1926</p> <p>Other: Park extends across Union Avenue and takes in the Crane House</p>	<p>Style: informal</p> <p>Stories: na</p> <p>Plan: walking areas with seating</p> <p>Roof: na</p> <p>Exterior: na</p> <p>Foundation: na</p> <p>Windows: na</p> <p>Details: Park is part of the Rahway River Parkway and the Union County Park System. It includes a section on Springfield Avenue with a large memorial to Cranford victims of 9/11.</p>	<p>Sperry Park is eligible as a contributing element to the NRE Rahway River Parkway HD (ID #4079), NRE Union County Park System HD (ID #4424), and NRE North Cranford HD (ID #3838).</p>
 <p>Photograph Number: 95</p>	<p>Panamerican Resource Number: 95</p> <p>Cranford Towers Condos</p> <p>18 Springfield Avenue</p> <p>Municipality: Cranford, Union County</p> <p>UTM coordinates (1983 Zone 18N): 4501110N, 558897E</p> <p>Date: ca. 1930</p> <p>Other: Overlooks the Rahway River and Sperry Park</p>	<p>Style: none</p> <p>Stories: 7</p> <p>Plan: rectangular</p> <p>Roof: flat</p> <p>Exterior: brick</p> <p>Foundation: concrete</p> <p>Windows: replacement metal</p> <p>Details: Although this is a large building in a significant part of town, it lacks any real stylistic element; it is two big, red brick boxes.</p>	<p>This structure is not eligible since it does not meet any of the eligibility criteria.</p>

Inventory of Identified Architectural Resources			
Property	Property Information	Description	NRHP Eligibility
 <p>Photograph Number: 92 (east side of contemporary house)</p>	<p>Panamerican Resource Number: 92</p> <p>House and Culvert</p> <p>107 Riverside Drive (see PRN 91)</p> <p>Municipality: Cranford, Union County</p> <p>UTM coordinates (1983 Zone 18N): 4501362N, 558768E</p> <p>Date: unknown</p> <p>Other: This contemporary house was photographed only on the side in relationship to the culvert.</p>	<p>Style: concrete culvert</p> <p>Stories: na</p> <p>Plan: na</p> <p>Roof: na</p> <p>Exterior: metal fence blocks immediate access to the culvert</p> <p>Foundation: concrete</p> <p>Windows: na</p> <p>Details: The culvert allows a channelized stream to flow under Riverside Drive and into the Rahway River at 91 and 92 Riverside Drive.</p>	<p>The house is non-contributing to the NRE North Cranford HD since its construction falls outside of the period of significance, and is not eligible for the NRHP since it fails to meet any of the eligibility criteria. The culvert and walls should be considered eligible as contributing elements to the district.</p>
 <p>Photograph Number: 93</p>	<p>Panamerican Resource Number: 93</p> <p>Apartment Building</p> <p>22 Riverside Drive at Casino Avenue</p> <p>Municipality: Cranford, Union County</p> <p>UTM coordinates (1983 Zone 18N): 4501326N, 558864E</p> <p>Date: ca. 1970</p> <p>Other: Overlooks Rahway River and Hanson Park</p>	<p>Style: Contemporary with faux Mansard roof</p> <p>Stories: 4</p> <p>Plan: L shape</p> <p>Roof: flat</p> <p>Exterior: brick, Mansard roof is metal</p> <p>Foundation: concrete</p> <p>Windows: combination of metal types including glass doors that lead onto balconies</p> <p>Details: modern apartment building in an area of apartment buildings</p>	<p>Outside of the North Cranford HD; it is not eligible since it does not yet meet the age criteria.</p>

Inventory of Identified Architectural Resources			
Property	Property Information	Description	NRHP Eligibility
 <p>Photograph Number: 90</p>	<p>Panamerican Resource Number: 90</p> <p>Garage</p> <p>107 Riverside (see PRN 89)</p> <p>Municipality: Cranford, Union County</p> <p>UTM coordinates (1983 Zone 18N): 4501359N, 558731E</p> <p>Date: ca. 1930</p> <p>Other:</p>	<p>Style: none—painted to match house; two-bay garage</p> <p>Stories: 1</p> <p>Plan: rectangular</p> <p>Roof: gable</p> <p>Exterior: wood siding</p> <p>Foundation: concrete</p> <p>Windows: none</p> <p>Details:</p>	<p>This house is a non-contributing element to the NRE North Cranford HD (ID #3838) and not eligible for the NRHP since it does not meet any of the eligibility criteria.</p>
 <p>Photograph Number: 91</p>	<p>Panamerican Resource Number: 91</p> <p>102 Riverside Drive (see PRN 92)</p> <p>Municipality: Cranford, Union County</p> <p>UTM coordinates (1983 Zone 18N): 4501385N, 558778E</p> <p>Date: ca. 1930</p> <p>Other: The fence seen in the photograph is a culvert at Riverside Drive.</p>	<p>Style: Whatever stylistic elements the house once had are now gone</p> <p>Stories: 2.5</p> <p>Plan: rectangular</p> <p>Roof: gable and skirt</p> <p>Exterior: aluminum siding</p> <p>Foundation: concrete with basement</p> <p>Windows: metal replacements with decorative shutters</p> <p>Details: The house has lost its porch. It is constructed over a channelized stream that flows into the Rahway River through a portion of what would be the house's basement.</p>	<p>This house is a non-contributing element to the NRE North Cranford HD since its construction date falls outside of the period of significance. It is also not eligible for the NRHP since it fails to meet any of the eligibility criteria. The culvert, walls and fence should be considered eligible as contributing elements to the district.</p>

New Jersey Department of Environmental Protection
Historic Preservation Office

Page 1

BASE FORM**Historic Sites #:****Property Name:** 102 Riverside Drive, house with flood control culvert, retaining walls, and fence**Street Address:** **Street #:** 102 **Apartment #:** _____
(Low) (High) (Low) (High)**Prefix:** _____ **Street Name:** Riverside Drive **Suffix:** _____ **Type:** RD**County(s):** Union **Zip Code:** 07016**Municipality(s):** Cranford **Block(s):** _____**Local Place Name(s):** _____ **Lot(s):** _____**Ownership::** _____ **USGS Quad(s)** Roselle 1981**Photograph:**

Description: Whatever stylistic elements the house once had are now gone. Circa 1930 2.5 story rectangular plan house with gable roof and skirt, aluminum siding and a concrete foundation with basement windows. Metal replacement windows with decorative shutters. The house has lost its porch. It is constructed over a channelized stream that flows into the Rahway River through a portion of what would be the house's basement. This house is a **non-contributing element** to the NRE North Cranford HD since its construction date falls outside of the period of significance. It is also **not eligible** for the NRHP since it fails to meet any of the eligibility criteria. The **culvert, walls and fence** should be considered **eligible as contributing elements** to the district.

**Registration and
Status Dates:****National Historic
Landmark:** _____**National Register:** _____**New Jersey Register:** _____**Determination of Eligibility:** _____**SHPO Opinion:** _____**Local Designation:** _____**Other Designation:** _____**Other Designation Date:** _____

*Phase IA Cultural Resources Investigation of the Rahway River Flood Risk Management
and Ecosystem Restoration Project, Westfield, Townships of Springfield, Cranford, and
Union, and Borough of Kenilworth, Union County, New Jersey*

Survey Name: _____**Date:** May 13, 2013**Surveyor:** Nolte, Kelly, Donald Smith, Mark Steinback, and Michael Cinquino.

A-276

Organization: Panamerican Consultants, Inc.

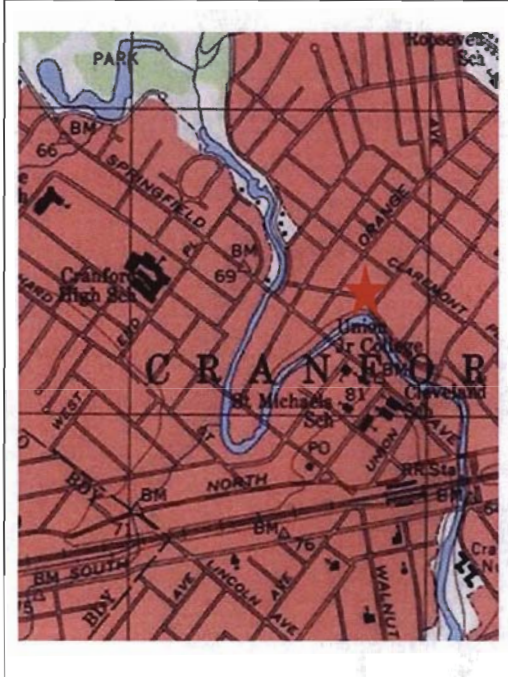
New Jersey Department of Environmental Protection
Historic Preservation Office

Page 2

BASE FORM

Historic Sites #:

Location Map:



Site Map:



Bibliography/Sources:

Additional Information:

More Research Needed? ☒ Yes ☐ No

INTENSIVE LEVEL USE ONLY



Attachments Included: ☐ Building ☐ Structure ☐ Object ☐ Bridge
☐ Landscape ☐ Industry

Within Historic District? ☐ Yes ☐ No

Status: ☐ Key-Contributing ☐ Contributing ☐ Non-Contributing

Associated Archaeological Site/Deposit? ☐ Yes
(Known or potential Sites – if yes, please describe briefly)

Inventory of Identified Architectural Resources

Property	Property Information	Description	NRHP Eligibility
 <p>Photograph Number: 86</p>	<p>Panamerican Resource Number: 86</p> <p>House and garage</p> <p>104 Orange Street at corner of Riverside Drive</p> <p>Municipality: Cranford, Union County</p> <p>UTM coordinates (1983 Zone 18N): 4501404N, 558747E</p> <p>Date: ca. 1930</p> <p>Other: House includes a small garage</p>	<p>Style: Colonial Revival</p> <p>Stories: 2.5</p> <p>Plan: rectangular</p> <p>Roof: gable with cornice return</p> <p>Exterior: aluminum siding</p> <p>Foundation: obscured</p> <p>Windows: metal replacements, decorative shutters</p> <p>Details: The front stoop was enclosed as was the side porch. Decorative elements, if present originally, were eliminated when the aluminum siding was applied. The one-bay garage has a gable roof with a cornice return and like the house is covered in aluminum siding.</p>	<p>This house falls outside the boundaries of the North Cranford HD. It is not eligible for the NRHP since it does not meet any of the eligibility criteria.</p>
 <p>Photograph Number: 87</p>	<p>Panamerican Resource Number: 87</p> <p>House and garage</p> <p>104 Riverside Drive</p> <p>Municipality: Cranford, Union County</p> <p>UTM coordinates (1983 Zone 18N): 4501393N, 558762E</p> <p>Date: ca. 1920</p> <p>Other:</p>	<p>Style: Dutch Colonial</p> <p>Stories: 2.5 with basement</p> <p>Plan: rectangular</p> <p>Roof: gambrel</p> <p>Exterior: aluminum siding</p> <p>Foundation: concrete</p> <p>Windows: replacement 6/1 with storm windows.</p> <p>Details: Side porch enclosed. Any decorative detail was eliminated with addition of siding. Rear one-bay garage has a hipped roof that sports a rather fanciful cupola.</p>	<p>This house is a contributing element to the NRE North Cranford HD (ID #3838).</p>

New Jersey Department of Environmental Protection
Historic Preservation Office

Page 1

BASE FORM

Historic Sites #:

Property Name: 104 Riverside Drive

Street Address: Street #: 104 (Low) (High) Apartment #: (Low) (High)

Prefix: Street Name: Riverside Drive Suffix: Type: RD

County(s): Union Zip Code: 07016

Municipality(s): Cranford

Block(s):

Local Place Name(s):

Lot(s):

Ownership::

USGS Quad(s) Roselle 1981

Photograph:



Description: Date: ca. 1920. 2.5 story, rectangular plan Dutch Colonial/Colonial Revival with basement. Gambrel roof, aluminum siding. Replacement 6/1 windows with storm windows. Concrete foundation, enclosed side porch. Any decorative detail was eliminated with addition of siding. Rear one-bay garage has a hipped roof that supports a rather fanciful cupola. This house is recommended as a **contributing element** to the NRE North Cranford HD (ID #3838).

**Registration and
Status Dates:**National Historic
Landmark:

National Register:

New Jersey Register:

Determination of Eligibility:

SHPO Opinion:

Local Designation:

Other Designation:

Other Designation Date:

Survey Name: Phase IA Cultural Resources Investigation of the Rahway River Flood Risk Management and Ecosystem Restoration Project, Westfield, Townships of Springfield, Cranford, and Union, and Borough of Kenilworth, Union County, New Jersey
Surveyor: Nolte, Kelly, Donald Smith, Mark Steinback, and Michael Cinquino.
Organization: Panamerican Consultants, Inc.

Date: May 13, 2013

A-279

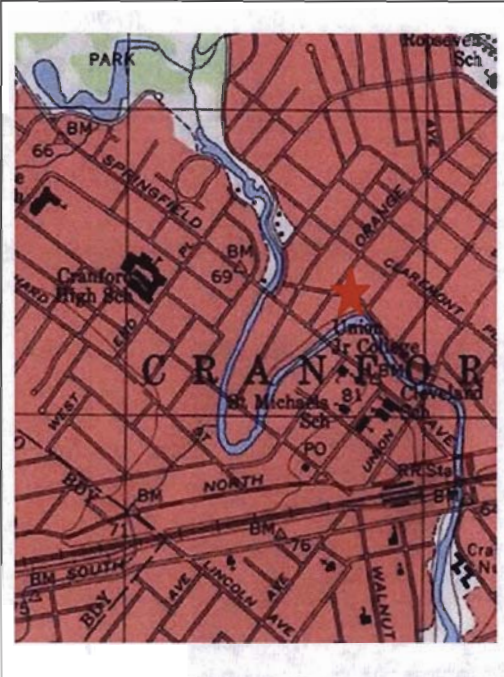
New Jersey Department of Environmental Protection
Historic Preservation Office

Page 2

BASE FORM

Historic Sites #:

Location Map:



Site Map:



Bibliography/Sources:

Additional Information:



More Research Needed? ☒ Yes ☐ No**INTENSIVE LEVEL USE ONLY**

Attachments Included: ☐ Building ☐ Structure ☐ Object ☐ Bridge
☐ Landscape ☐ Industry

Within Historic District? ☐ Yes ☐ NoStatus: ☐ Key-Contributing ☐ Contributing ☐ Non-Contributing

Associated Archaeological Site/Deposit? ☐ Yes
(Known or potential Sites – if yes, please describe briefly)

Inventory of Identified Architectural Resources

Property	Property Information	Description	NRHP Eligibility
 <p>Photograph Number: 90</p>	<p>Panamerican Resource Number: 90</p> <p>Garage</p> <p>107 Riverside (see PRN 89)</p> <p>Municipality: Cranford, Union County</p> <p>UTM coordinates (1983 Zone 18N): 4501359N, 558731E</p> <p>Date: ca. 1930</p> <p>Other:</p>	<p>Style: none—painted to match house; two-bay garage</p> <p>Stories: 1</p> <p>Plan: rectangular</p> <p>Roof: gable</p> <p>Exterior: wood siding</p> <p>Foundation: concrete</p> <p>Windows: none</p> <p>Details:</p>	<p>This house is a non-contributing element to the NRE North Cranford HD (ID #3838) and not eligible for the NRHP since it does not meet any of the eligibility criteria.</p>
 <p>Photograph Number: 91</p>	<p>Panamerican Resource Number: 91</p> <p>102 Riverside Drive (see PRN 92)</p> <p>Municipality: Cranford, Union County</p> <p>UTM coordinates (1983 Zone 18N): 4501385N, 558778E</p> <p>Date: ca. 1930</p> <p>Other: The fence seen in the photograph is a culvert at Riverside Drive.</p>	<p>Style: Whatever stylistic elements the house once had are now gone</p> <p>Stories: 2.5</p> <p>Plan: rectangular</p> <p>Roof: gable and skirt</p> <p>Exterior: aluminum siding</p> <p>Foundation: concrete with basement</p> <p>Windows: metal replacements with decorative shutters</p> <p>Details: The house has lost its porch. It is constructed over a channelized stream that flows into the Rahway River through a portion of what would be the house's basement.</p>	<p>This house is a non-contributing element to the NRE North Cranford HD since its construction date falls outside of the period of significance. It is also not eligible for the NRHP since it fails to meet any of the eligibility criteria. The culvert, walls and fence should be considered eligible as contributing elements to the district.</p>

New Jersey Department of Environmental Protection
Historic Preservation Office

Page 1

BASE FORM

Historic Sites #:

Property Name: 107 Riverside, Garage

Street Address: Street #: 107 (Low) (High) Apartment #: (Low) (High)

Prefix: Street Name: Riverside Suffix: Type: RD

County(s): Union Zip Code: 07016

Municipality(s): Cranford

Block(s):

Local Place Name(s):

Lot(s):

Ownership::

USGS Quad(s) Roselle 1981

Photograph:



Description: Circa 1930. Two bay, one story garage, painted to match house. Rectangular plan, gabled roof. Wood siding exterior with concrete foundation. This house is a non-contributing element to the NRE North Cranford HD (ID #3838) and not eligible for the NRHP since it does not meet any of the eligibility criteria.

**Registration and
Status Dates:**National Historic
Landmark: _____

SHPO Opinion: _____

National Register: _____

Local Designation: _____

New Jersey Register: _____

Other Designation: _____

Determination of Eligibility: _____

Other Designation Date: _____

Survey Name: Phase IA Cultural Resources Investigation of the Rahway River Flood Risk Management

and Ecosystem Restoration Project, Westfield, Townships of Springfield, Cranford, and

Date: May 13, 2013

Surveyor: Nolte, Kelly, Donald Smith, Mark Steinback, and Michael Cinquino.

Organization: Panamerican Consultants, Inc.

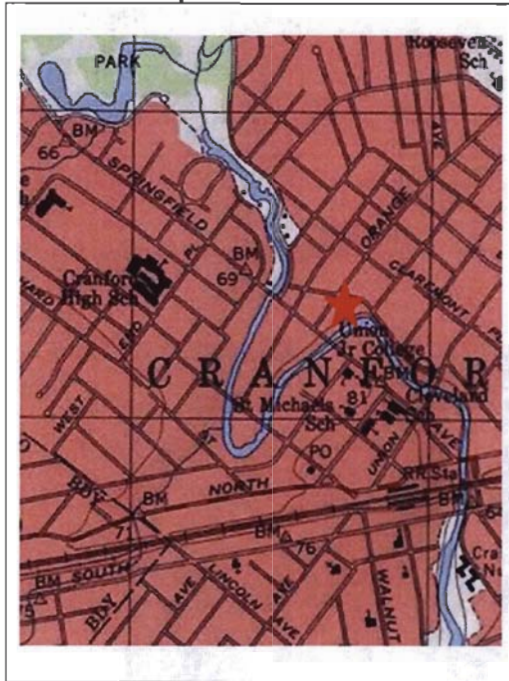
A-282

New Jersey Department of Environmental Protection
Historic Preservation Office

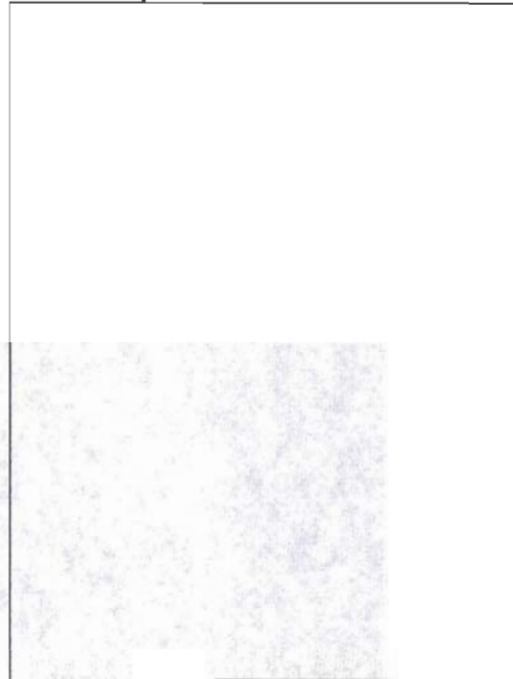
Page 2

BASE FORMHistoric Sites #: **MS-238A**

Location Map:



Site Map:



Bibliography/Sources:

Additional Information:

More Research Needed? ☒ Yes ☐ No

INTENSIVE LEVEL USE ONLY



Attachments Included: ☐ Building ☐ Structure ☐ Object ☐ Bridge
☐ Landscape ☐ Industry

Within Historic District? ☐ Yes ☐ No

Status: ☐ Key-Contributing ☐ Contributing ☐ Non-Contributing

Associated Archaeological Site/Deposit? ☐ Yes
(Known or potential Sites – if yes, please describe briefly)

Inventory of Identified Architectural Resources

Property	Property Information	Description	NRHP Eligibility
 <p>Photograph Number: 82</p>	<p>Panamerican Resource Number: 82</p> <p>121 Riverside Drive</p> <p>Municipality: Cranford, Union County</p> <p>UTM coordinates (1983 Zone 18N): 4501371N, 558666E</p> <p>Date: ca. 1910</p> <p>Other: This house backs up to the Rahway River.</p>	<p>Style: Dutch Colonial/Colonial Revival</p> <p>Stories: 2</p> <p>Plan: rectangular</p> <p>Roof: gambrel</p> <p>Exterior: First floor—wood siding; second floor—wood shingles</p> <p>Foundation: concrete</p> <p>Windows: 6/1 wood originals with storm windows</p> <p>Details: enclosed side porch, highly decorative, original front entry</p>	<p>This house is a contributing element to the NRE North Cranford HD (ID #3838).</p>
 <p>Photograph Number: 83 (rear view, first floor dock level on the Rahway River)</p>	<p>Panamerican Resource Number: 83</p> <p>Cranford Canoe Club</p> <p>8 Springfield Avenue</p> <p>Municipality: Cranford, Union County</p> <p>UTM coordinates (1983 Zone 18N): 4501271N, 558656E</p> <p>Dates: various</p> <p>Other: This building sits right on the Rahway River in order to launch canoes.</p>	<p>Style: none</p> <p>Stories: 2</p> <p>Plan: rectangular</p> <p>Roof: gable with cat slide</p> <p>Exterior: vinyl siding</p> <p>Foundation: concrete</p> <p>Windows: mixture of types</p> <p>Details: A canoe club building has been at this site since 1907. The first floor of the building is at dock level as shown in the photograph, and the second floor of the building is at street level.</p>	<p>The Canoe Club is a contributing element to the NRE North Cranford HD (ID #3838).</p>

Panamerican Consultants, Inc.

6-43

Rahway River Phase 1A

New Jersey Department of Environmental Protection
Historic Preservation Office

Page 1

BASE FORM

Historic Sites #:

Property Name: 121 Riverside Drive

Street Address: Street #: 121 (Low) (High) Apartment #: _____ (Low) (High)

Prefix: _____ Street Name: Riverside Drive Suffix: _____ Type: RD

County(s): Union Zip Code: 07016

Municipality(s): Cranford

Block(s): _____

Local Place Name(s): _____

Lot(s): _____

Ownership:: _____

USGS Quad(s) Roselle 1981

Photograph:



Description: Date: ca. 1910. 2 story, rectangular plan Dutch Colonial/Colonial Revival. Gambrel roof, wood siding/wood shingles. 6/1 wood originals with storm windows. Concrete foundation, enclosed side porch, original highly decorative front entry. This house is recommended as a contributing element to the NRE North Cranford HD (ID #3838). This house backs up to the Rahway River.

Registration and
Status Dates:

National Historic
Landmark: _____

SHPO Opinion: _____

National Register: _____

Local Designation: _____

New Jersey Register: _____

Other Designation: _____

Determination of Eligibility: _____

Other Designation Date: _____

Survey Name: *Phase IA Cultural Resources Investigation of the Rahway River Flood Risk Management and Ecosystem Restoration Project, Westfield, Townships of Springfield, Cranford, and Union, and Borough of Kenilworth, Union County, New Jersey*

Date: May 13, 2013

Surveyor: Nolte, Kelly, Donald Smith, Mark Steinback, and Michael Cinquino.

Organization: Panamerican Consultants, Inc.

A-285

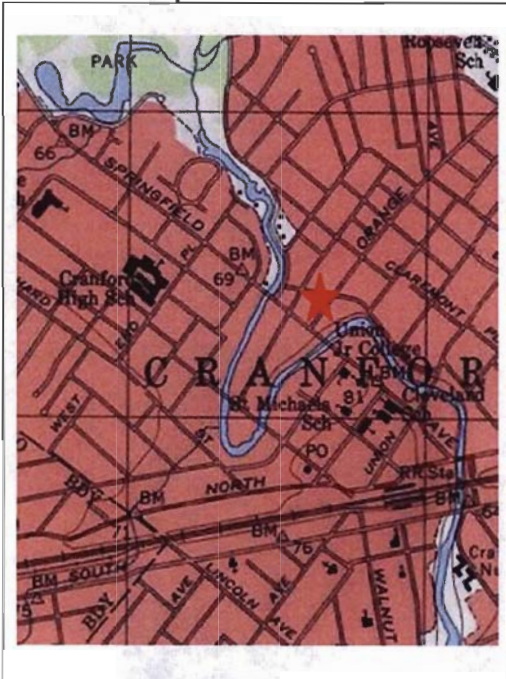
New Jersey Department of Environmental Protection
Historic Preservation Office

Page 2

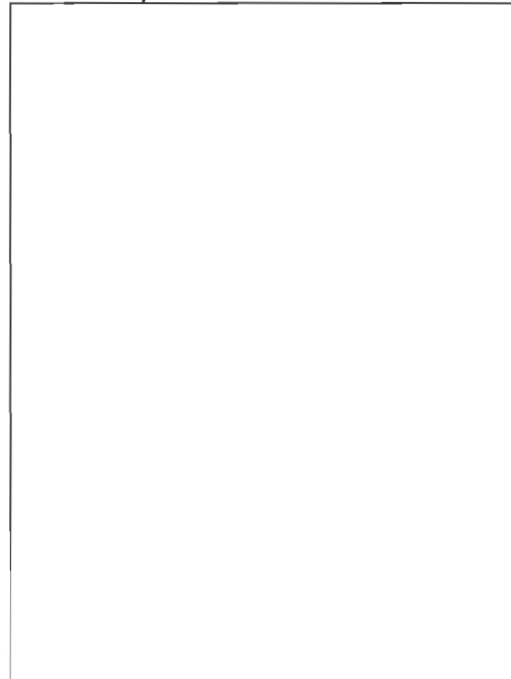
BASE FORM

Historic Sites #:

Location Map:



Site Map:



Bibliography/Sources:

Additional Information:

More Research Needed? ☒ Yes ☐ No



INTENSIVE LEVEL USE ONLY

Attachments Included: ☐ Building ☐ Structure ☐ Object ☐ Bridge
☐ Landscape ☐ Industry

Within Historic District? ☐ Yes ☐ No

Status: ☐ Key-Contributing ☐ Contributing ☐ Non-Contributing

Associated Archaeological Site/Deposit? ☐ Yes
(Known or potential Sites – if yes, please describe briefly)

Inventory of Identified Architectural Resources			
Property	Property Information	Description	NRHP Eligibility
 <p>Photograph Number: 80</p>	<p>Panamerican Resource Number: 80</p> <p>House and garage</p> <p>123 Riverside Drive</p> <p>Municipality: Cranford, Union County</p> <p>UTM coordinates (1983 Zone 18N): 4501372N, 558651E</p> <p>Date: ca. 1920</p> <p>Other: Backs up to the Rahway River</p>	<p>Style: Four square</p> <p>Stories: 2.5 with basement</p> <p>Plan: square</p> <p>Roof: gable</p> <p>Exterior: wood drop siding</p> <p>Foundation: concrete</p> <p>Windows: replacement metal with storm windows and decorative shutters</p> <p>Details: enclosed one side of the full width porch; small front gable, single-bay garage built to match house is also on the lot.</p>	<p>This house and garage are contributing elements to the NRE North Cranford HD (ID #3838).</p>
 <p>Photograph Number: 81</p>	<p>Panamerican Resource Number: 81</p> <p>101 Orange Street</p> <p>Municipality: Cranford, Union County</p> <p>UTM coordinates (1983 Zone 18N): 4501412N, 558686E</p> <p>Date: unknown</p> <p>Other:</p>	<p>Style: This building has been changed so much that its original style is indiscernible.</p> <p>Stories: 2</p> <p>Plan: rectangular</p> <p>Roof: hipped</p> <p>Exterior: Drivt with faux rock</p> <p>Foundation: obscured</p> <p>Windows: metal replacements</p> <p>Details:</p>	<p>Non-contributing and not eligible, lacks integrity</p>

New Jersey Department of Environmental Protection
Historic Preservation Office

Page 1

BASE FORM

Historic Sites #:

Property Name: 123 Riverside Drive

Street Address: Street #: 123 Apartment #: _____
(Low) (High) (Low) (High)

Prefix: _____ Street Name: Riverside Drive Suffix: _____ Type: RD

County(s): Union Zip Code: 07016

Municipality(s): Cranford Block(s): _____

Local Place Name(s): _____ Lot(s): _____

Ownership: _____ USGS Quad(s) Roselle 1981

Photograph:



Description: Date: ca. 1920. 2.5 story, square plan Four Square house. Gabled roof, wood drop siding. Replacement metal windows with storm windows and decorative shutters. Concrete foundation, enclosed one side of the full width porch; small front gable, single-bay garage built to match house is also on the lot. This house is **non-contributing** to the NRE North Cranford HD (ID #3838) and is **not eligible** due to lack of integrity.

Registration and Status Dates:

National Historic Landmark: _____

SHPO Opinion: _____

National Register: _____

Local Designation: _____

New Jersey Register: _____

Other Designation: _____

Determination of Eligibility: _____

Other Designation Date: _____

Phase IA Cultural Resources Investigation of the Rahway River Flood Risk Management and Ecosystem Restoration Project, Westfield, Townships of Springfield, Cranford, and Union, and Borough of Kenilworth, Union County, New Jersey

Survey Name: _____ Date: May 13, 2013

Surveyor: Nolte, Kelly, Donald Smith, Mark Steinback, and Michael Cinquino.

Organization: Panamerican Consultants, Inc.

A-288

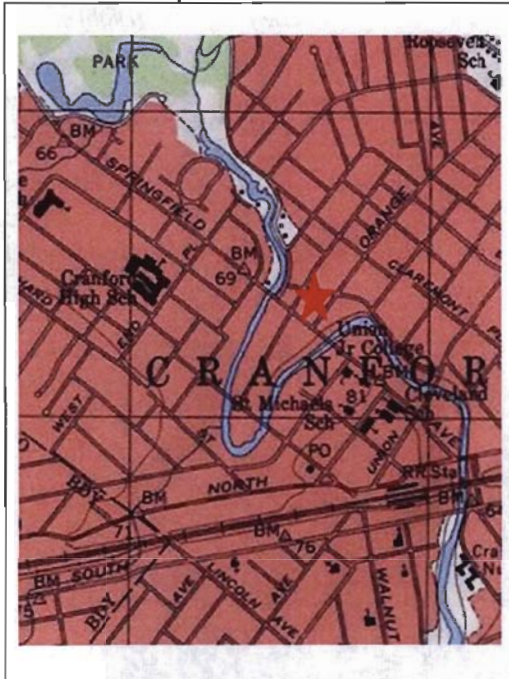
New Jersey Department of Environmental Protection
Historic Preservation Office

Page 2

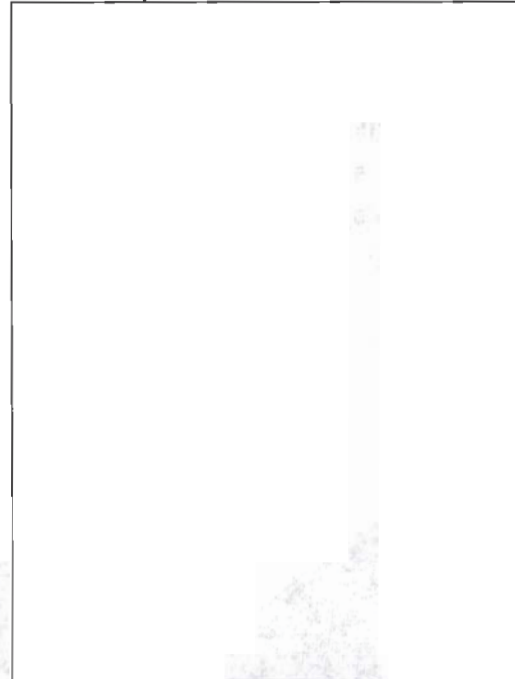
BASE FORM

Historic Sites #:

Location Map:



Site Map:



Bibliography/Sources:

Additional Information:



More Research Needed? ☒ Yes ☐ No**INTENSIVE LEVEL USE ONLY**

Attachments Included: ☐ Building ☐ Structure ☐ Object ☐ Bridge
☐ Landscape ☐ Industry

Within Historic District? ☐ Yes ☐ NoStatus: ☐ Key-Contributing ☐ Contributing ☐ Non-Contributing

Associated Archaeological Site/Deposit? ☐ Yes
(Known or potential Sites – if yes, please describe briefly)

Inventory of Identified Architectural Resources

Property	Property Information	Description	NRHP Eligibility
 <p>Photograph Number: 78</p>	<p>Panamerican Resource Number: 78</p> <p>201 Central Avenue</p> <p>Municipality: Cranford, Union County</p> <p>UTM coordinates (1983 Zone 18N): 4501380N, 558599E</p> <p>Date: 1941</p> <p>Other: House overlooks Memorial Park</p>	<p>Style: Colonial Revival</p> <p>Stories: 2</p> <p>Plan: rectangular</p> <p>Roof: gable</p> <p>Exterior: brick and wood</p> <p>Foundation: obscured</p> <p>Windows: 6/6 with storm windows, front door side lights, operable shutters</p> <p>Details: The house has a number of odd architectural details—Spanish-style wrought-iron balconies on the front and side, scalloped bed molding at the eaves, dentil molding on the projecting front entry—that seem to indicate numerous changes to the house or a truly fanciful interpretation of Colonial Revival.</p>	<p>This house is a non-contributing element to the NRE North Cranford HD (ID #3838; Bary 2012), and is not eligible since it lacks integrity.</p>
 <p>Photograph Number: 79</p>	<p>Panamerican Resource Number: 79</p> <p>127 Riverside Drive</p> <p>Municipality: Cranford, Union County</p> <p>UTM coordinates (1983 Zone 18N): 4501377N, 558629E</p> <p>Date: ca. 1965</p> <p>Other: Backs up to the Rahway River</p>	<p>Style: Split level</p> <p>Stories: 2</p> <p>Plan: rectangular</p> <p>Roof: gable</p> <p>Exterior: brick veneer first floor and wood shingles</p> <p>Foundation: obscured</p> <p>Windows: replacement decorative with decorative shutters</p> <p>Details: 1960s split level wedged between houses primarily from the early twentieth century</p>	<p>Not eligible and non-contributing; it is not yet old enough to be assessed. Further, given its style and the abundance of structures of this type, the house will probably never be eligible for the NRHP.</p>

New Jersey Department of Environmental Protection
Historic Preservation Office

Page 1

BASE FORMHistoric Sites #: 15 12 19

Property Name: 127 Riverside Drive

Street Address: Street #: 127 (Low) (High) Apartment #: _____ (Low) (High)Prefix: _____ Street Name: Riverside Drive Suffix: _____ Type: RDCounty(s): Union Zip Code: 07016Municipality(s): Cranford

Block(s): _____

Local Place Name(s): _____

Lot(s): _____

Ownership: _____ USGS Quad(s) Roselle 1981

Photograph:



Description: Date: ca. 1965. 2 story split-level, rectangular plan house. Gabled roof, and replacement decorative windows with decorative shutters. This house is **non-contributing** to the NRE North Cranford HD (ID #3838) and it is not yet old enough to be assessed individually. Further, given its style and the abundance of structures of this type, the house will probably never be eligible for the NRHP. Backs up to the Rahway River.

**Registration and
Status Dates:**National Historic
Landmark: _____

SHPO Opinion: _____

National Register: _____

Local Designation: _____

New Jersey Register: _____

Other Designation: _____

Determination of Eligibility: _____

Other Designation Date: _____

Survey Name: Phase IA Cultural Resources Investigation of the Rahway River Flood Risk Management and Ecosystem Restoration Project, Westfield, Townships of Springfield, Cranford, and Union, and Borough of Kenilworth, Union County, New Jersey

Date: May 13, 2013Surveyor: Nolte, Kelly, Donald Smith, Mark Steinback, and Michael Cinquino.Organization: Panamerican Consultants, Inc.

A-291

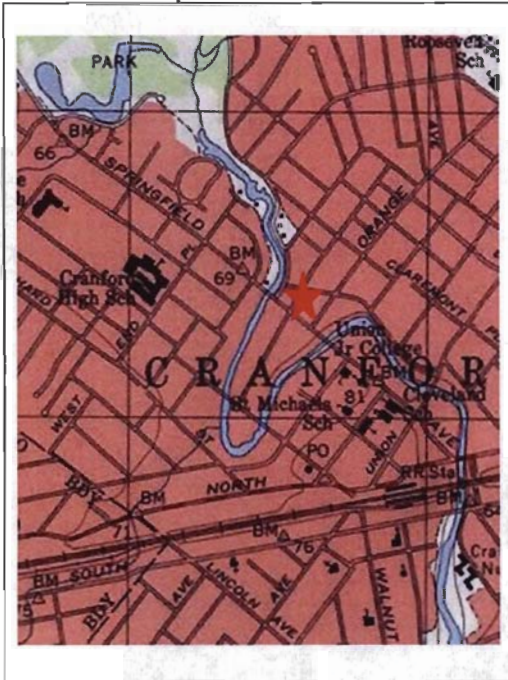
New Jersey Department of Environmental Protection
Historic Preservation Office

Page 2

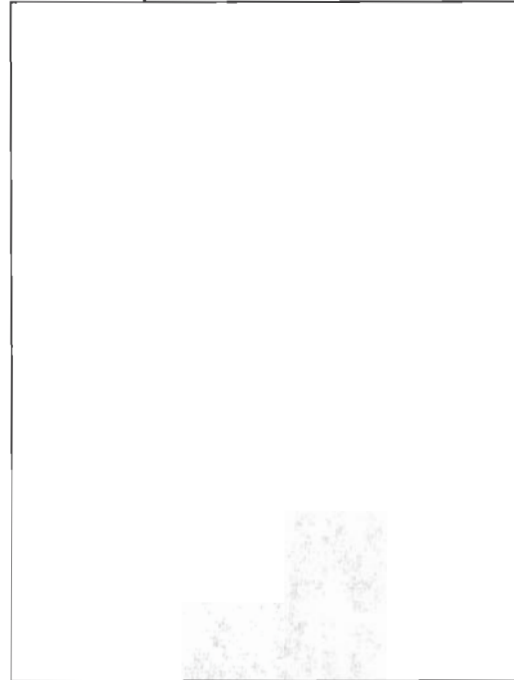
BASE FORM

Historic Sites #:

Location Map:



Site Map:



Bibliography/Sources:

Additional Information:

More Research Needed? ☒ Yes ☐ No**INTENSIVE LEVEL USE ONLY**

Attachments Included: ☐ Building ☐ Structure ☐ Object ☐ Bridge
☐ Landscape ☐ Industry

Within Historic District? ☐ Yes ☐ NoStatus: ☐ Key-Contributing ☐ Contributing ☐ Non-Contributing

Associated Archaeological Site/Deposit? ☐ Yes
(Known or potential Sites – if yes, please describe briefly)

NEW JERSEY OFFICE OF HISTORIC PRESERVATION HISTORIC SITES INVENTORY #200309
INDIVIDUAL STRUCTURE SURVEY FORM

HISTORIC NAME: Pierson House
LOCATION: 420 Riverside Drive

COMMON NAME: Robert M. Crane House
BLOCK/LOT 223/7

MUNICIPALITY: Cranford
USGS QUAD: Roselle
OWNER/ADDRESS: Crane, Robert M.
420 Riverside Dr.
Cranford, NJ

COUNTY: Union
UTM REFERENCES: Zone/Northing/Easting

DESCRIPTION

Construction Date: 1740-50/19th C/1929

Source of Date: Visual

Architect:

Builder:

Style: Colonial, Neo-Georgian

Form/Plan Type: Central Hall
five-bay Georgian Plan

Number of Stories: 2½

Foundation: not visible

Exterior Wall Fabric: Clapboard siding

Fenestration: Five bay facade. 6/6 sash windows

Roof/Chimneys: Gable roof. End brick chimneys.

Additional Architectural Description:

Three bay, 2½ story addition added to the southeast side of the house.
Pediment over central door.

PHOTO Negative File # Roll 32 frame 15

R 8, 33

Map (Indicate North)



A-293

DEPARTMENT OF ENVIRONMENTAL PROTECTION, BUREAU OF PARKS AND FORESTRY
OFFICE OF HISTORIC PRESERVATION, 109 WEST STATE STREET, TRENTON, NEW JERSEY 08625

(609) 292-2023

SITING, BOUNDARY DESCRIPTION, AND RELATED STRUCTURES:

The house is located across from Memorial Park on a lot sitting on the N.W. corner of Riverside and Normandie. The house sits towards the back of the lot. A slate walk runs along the Riverside Dr. line of the property.

SURROUNDING ENVIRONMENT: Urban ☐ Suburban ☒ Scattered Buildings ☐
Open Space ☐ Woodland ☐ Residential ☒ Agricultural ☐
Industrial ☐ Downtown Commercial ☐ Highway Commercial ☐ Other ☐

In an area of large, single family suburban homes on large lots.

SIGNIFICANCE:

The original house was a 1½ story wooden structure built between 1740 and 1750 by a Mr. Pierson. Parts of the original house remain within the later additions and alterations. The house is adjacent to the site of Crane's Ford, the 18th Century Rahway River crossing which gave Cranford its name. The house was much altered over time and has been "restored" to a Georgian style house.

Today, the house is frequently referred to as the "Robert Crane House." Robert Crane is no relation to the Crane's who settled Cranford.

ORIGINAL USE: Residence	PRESENT USE: Residence
PHYSICAL CONDITION: Excellent <input checked="" type="checkbox"/> Good <input type="checkbox"/>	Fair <input type="checkbox"/> Poor <input type="checkbox"/>
REGISTER ELIGIBILITY: Yes <input type="checkbox"/> Possible <input type="checkbox"/>	No <input checked="" type="checkbox"/> Part of District <input type="checkbox"/>
THREATS TO SITE: Roads <input type="checkbox"/> Development <input type="checkbox"/>	Zoning <input type="checkbox"/> Deterioration <input type="checkbox"/>
No Threat <input checked="" type="checkbox"/> Other <input type="checkbox"/>	
COMMENTS:	

Excellent condition but much "restored" and remodelled.

REFERENCES:

Cranford Historical Society

RECORDED BY: JWF
ORGANIZATION: UCCHPAB

DATE: 1981





CRANFORD
Union Co.

200309
r. 32/15

Pierson House
420 Riverside Drive
looking N.E.

1981

Inventory of Identified Architectural Resources

Property	Property Information	Description	NRHP Eligibility
 <p>Photograph Number: 31</p>	<p>PanamERICAN Resource Number: 31</p> <p>Crane's Ford Monument</p> <p>Riverside Drive at Springfield Avenue, east bank of Rahway River</p> <p>Municipality: Cranford, Union County</p> <p>UTM coordinates (1983 Zone 18N): 4501420N, 558499E</p> <p>Date: 1929</p> <p>Other: The river bank at this point has been built up since Crane's and Washington's occupation.</p>	<p>Style: bronze plaque on boulder</p> <p>Stories: na</p> <p>Plan: na</p> <p>Roof: na</p> <p>Exterior: na</p> <p>Foundation: cement and rock</p> <p>Windows: na</p> <p>Details: The plaque indicates that the monument was erected on July 4, 1929 by the Cranford Historical Society at the site of Crane's Ford, which was guarded during the Revolutionary War by Light Horseman from Washington's Army at Morristown.</p>	<p>This monument should be considered eligible as a contributing element to the NRE Rahway River Parkway HD (ID #4079) and the North Cranford HD (ID #3838).</p>
 <p>Photograph Number: 32</p>	<p>PanamERICAN Resource Number: 32</p> <p>420 Riverside Drive</p> <p>Municipality: Cranford, Union County</p> <p>UTM coordinates (1983 Zone 18N): 4501446N, 558539E</p> <p>Date: ca. 1920</p> <p>Other: This property overlooks Memorial Park on the front (south) and the Rahway River Parkway on the west side.</p>	<p>Style: Colonial Revival</p> <p>Stories: 2.5</p> <p>Plan: rectangular</p> <p>Roof: gable</p> <p>Exterior: wood</p> <p>Foundation: obscured</p> <p>Windows: 6/6 wood with storm windows, some fixed decorative, some Gibb windows; operational shutters</p> <p>Details: This house is in exceptionally good condition and, although it has a large rear addition, it has retained its original look and feeling.</p>	<p>This house is eligible as a contributing element to the North Cranford HD (ID #3838).</p>

New Jersey Department of Environmental Protection
Historic Preservation Office

Page 1

BASE FORM

Historic Sites #:

Property Name: 420 Riverside Drive

Street Address: Street #: 420 (Low) (High) Apartment #: _____ (Low) (High)

Prefix: _____ Street Name: Riverside Drive Suffix: _____ Type: RD

County(s): Union Zip Code: 07016

Municipality(s): Cranford

Block(s): _____

Local Place Name(s): _____ Lot(s): _____

Ownership: _____ USGS Quad(s) Roselle 1981

Photograph:



Description: Date: ca. 1920. 2.5 story Colonial Revival house. Rectangular plan, gabled roof, wood exterior and replacement decorative windows with decorative shutters. This house is **non-contributing** to the NRE North Cranford HD (ID #3838) and it is not yet old enough to be assessed individually. Further, given its style and the abundance of structures of this type, the house will probably never be eligible for the NRHP. Backs up to the Rahway River.

Registration and Status Dates:

National Historic Landmark: _____

SHPO Opinion: _____

National Register: _____

Local Designation: _____

New Jersey Register: _____

Other Designation: _____

Determination of Eligibility: _____

Other Designation Date: _____

Survey Name: *Phase IA Cultural Resources Investigation of the Rahway River Flood Risk Management and Ecosystem Restoration Project, Westfield, Townships of Springfield, Cranford, and Union, and Borough of Kenilworth, Union County, New Jersey*

Date: May 13, 2013

Surveyor: Nolte, Kelly, Donald Smith, Mark Steinback, and Michael Cinquino.

Organization: Panamerican Consultants, Inc.

A-297

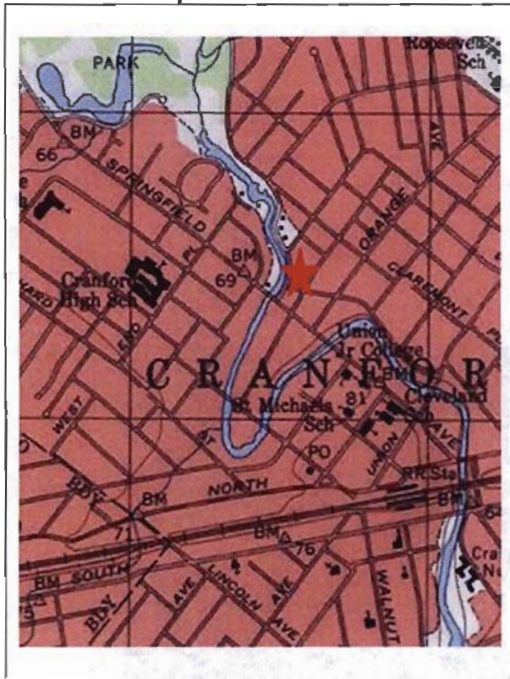
New Jersey Department of Environmental Protection
Historic Preservation Office

Page 2

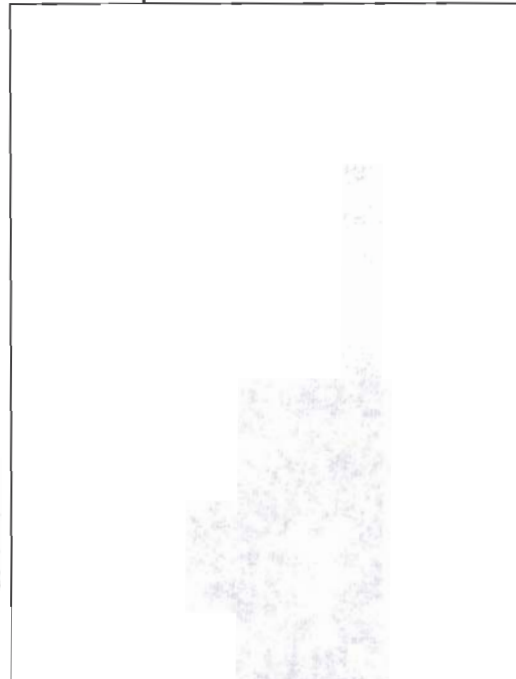
BASE FORM

Historic Sites #:

Location Map:



Site Map:



Bibliography/Sources:



Additional Information:



More Research Needed? ☒ Yes ☐ No**INTENSIVE LEVEL USE ONLY**



Attachments Included: ☐ Building ☐ Structure ☐ Object ☐ Bridge
☐ Landscape ☐ Industry

Within Historic District? ☐ Yes ☐ NoStatus: ☐ Key-Contributing ☐ Contributing ☐ Non-Contributing

Associated Archaeological Site/Deposit? ☐ Yes
(Known or potential Sites – if yes, please describe briefly)

Inventory of Identified Architectural Resources			
Property	Property Information	Description	NRHP Eligibility
 <p>Photograph Number: 27</p>	<p>Panamerican Resource Number: 27</p> <p>502 Riverside Drive</p> <p>Municipality: Cranford, Union County</p> <p>UTM coordinates (1983 Zone 18N): 4501797N, 558446E</p> <p>Date: ca. 1930s</p> <p>Other: Directly across from the walking path on the levee between Nomahegen Park and Memorial Park</p>	<p>Style: Bungalow</p> <p>Stories: 2</p> <p>Plan: rectangular</p> <p>Roof: gambrel</p> <p>Exterior: Asphalt shingles and interior paneling</p> <p>Foundation: covered</p> <p>Windows: 6/1 sash and 1/1 vinyl</p> <p>Details: This is a much altered Bungalow-type house with a poorly enclosed full-width front porch.</p>	<p>Not Eligible, does not meet the NRHP criteria for eligibility</p>
 <p>Photograph Number: 28</p>	<p>Panamerican Resource Number: 28</p> <p>500 Riverside Drive</p> <p>Municipality: Cranford, Union County</p> <p>UTM coordinates (1983 Zone 18N): 4501787N, 558460E</p> <p>Date: ca. 1960s</p> <p>Other: Directly across from the walking path on the levee between Nomahegen Park and Memorial Park</p>	<p>Style: Minimal traditional with Colonial Revival elements</p> <p>Stories: 2</p> <p>Plan: rectangular</p> <p>Roof: gambrel and gable</p> <p>Exterior: hardy board</p> <p>Foundation: concrete</p> <p>Windows: 1/1 sash replacement and front bay window</p> <p>Details: "Colonial American" style has decorative shutters and attached garage</p>	<p>Not Eligible, does not meet the NRHP criteria for eligibility</p>

Inventory of Identified Architectural Resources			
Property	Property Information	Description	NRHP Eligibility
 <p>Photograph Number: 33</p>	<p>Panamerican Resource Number: 33</p> <p>Rock Wall</p> <p>Riverside Drive immediately west of 420 Riverside Drive</p> <p>Municipality: Cranford, Union County</p> <p>UTM coordinates (1983 Zone 18N): 4501461N, 558512E</p> <p>Date: ca. 1930s</p> <p>Other: Possibly a WPA or CCC construction project</p>	<p>Style: Rustic</p> <p>Stories: na</p> <p>Plan: na</p> <p>Roof: na</p> <p>Exterior: rusticated cut stone, random pattern, topped with stone edge</p> <p>Foundation: none</p> <p>Windows: NA</p> <p>Details: This stone wall stands approximately 2.5' and acts both as a levee and a road guard.</p>	<p>This stone wall should be considered eligible as a contributing element to the NRE Rahway River Parkway HD (ID #4079).</p>
 <p>Photograph Number: 34</p>	<p>Panamerican Resource Number: 34</p> <p>652 Riverside Drive</p> <p>Municipality: Kenilworth, Union County</p> <p>UTM coordinates (1983 Zone 18N): 4502573N, 558259E</p> <p>Date: ca. 2005</p> <p>Other: This house was surveyed because it lies directly in the flood plain. The house overlooks Nomahegan Park.</p>	<p>Style: Post Modern</p> <p>Stories: 2.5</p> <p>Plan: irregular</p> <p>Roof: multiple</p> <p>Exterior: drivet</p> <p>Foundation: obscured</p> <p>Windows: multiple metal types</p> <p>Details: modern house with a corner tower and walled yard</p>	<p>Not eligible, does not yet meet criteria</p>

Inventory of Identified Architectural Resources			
Property	Property Information	Description	NRHP Eligibility
 <p>Photograph Number: 110 (East bound balustrade, note the rock wall and urn from Lincoln Park incorporated into the balustrade)</p>	<p>Panamerican Resource Number: 110</p> <p>Bridge (NJDOT #2003028) at South Avenue and Centennial Avenue at Lincoln Park crossing the Rahway River</p> <p>Municipality: Cranford, Union County</p> <p>UTM coordinates (1983 Zone 18N): 4500699N, 559115E</p> <p>Date: 1983</p> <p>Other:</p>	<p>Style: concrete stringer</p> <p>Stories: na</p> <p>Plan: na</p> <p>Roof: na</p> <p>Exterior: balustrades—metal safety guards</p> <p>Foundation: concrete abutments</p> <p>Windows: na</p> <p>Details: The west side of the bridge is poorly incorporated into the entry area of Lincoln Park by use of a rock wall and urn.</p> <p>There was an earlier (1909) iron bridge at this site; the original plaque was reused on this bridge.</p>	<p>Although this is a state-owned bridge and is not yet 50 years old, it is clear that it is regarded as part of Lincoln Park. Lincoln Park is eligible for the NRHP as a contributing element to both the NRE Rahway River Parkway HD (ID #4079) and NRE Union County Park System HD (ID #4424).</p>
 <p>Photograph Number: 111</p>	<p>Panamerican Resource Number: 111</p> <p>Entry gates, walls, urns, seating area for Lincoln Park</p> <p>Corner of South Avenue and Centennial Avenue</p> <p>Municipality: Cranford, Union County</p> <p>UTM coordinates (1983 Zone 18N): 4500675N, 559147E</p> <p>Date: ca. 1917</p> <p>Other: Sits on the Rahway River.</p>	<p>Style: Rustic</p> <p>Stories: na</p> <p>Plan: na</p> <p>Roof: na</p> <p>Exterior: stone rubble knee walls, benches incorporated into walls and monumental urns and entry plinths mark the entrance to the park</p> <p>Foundation: na</p> <p>Windows: na</p> <p>Details: Unfortunately, this area is encroached upon by an apartment complex, the odd juxtaposition of the road system, and a public transportation waiting area.</p>	<p>Lincoln Park is a contributing element to the both NRE Rahway River Parkway HD (ID #4079) and NRE Union County Park System HD (ID #4424). The entry gates, walls, urns, seating and plinths are contributing elements to NRE Lincoln Park.</p>

N.J. Office of Cultural and Environmental Services, 109 W. State Street,
Trenton, N.J. 08625 609-292-2023
Prepared by Heritage Studies, Inc. Princeton, N.J. 08540 609-452-1754

RR 2003-

Survey # 8-1

NEW JERSEY TRANSIT RAILROAD STATION SURVEY

1. IDENTIFICATION

- A. Name: Common Cranford Line: Raritan Valley
Historic (Jersey Central)
- B. Address or location: East of intersection, Union County: Union
and Walnut Avenue Municipality: Cranford Township
Cranford, N.J. Block & lot: part of 800 / 3
- C. Owner's name: N.J. Transit
Address: Newark, N.J.
- D. Location of legal description:
- E. Representation in existing surveys: (give number, category, etc.,
as appropriate)

HABS _____ HAER _____ ELRR Improvement _____ NY&LB Improvement _____

Plainfield Corridor J3 pix NR(name, if HD) _____

NJSR (name, if HD) _____

NJHSI (#) _____

Northeast Corridor _____

Local _____ (date _____)

Modernization Study: site plan X floor plan X aerial photo _____other views X photos of NR quality? X

2. EVALUATION

- A. Determination of eligibility: SHPO comment? _____ (date _____)
NR det.? _____ (date _____)
- B. Potentially eligible for NR: yes _____ possible _____ no X
individual _____ thematic _____
- C. Survey Evaluation: 95/125 points

-2

FACILITY NAME: Cranford

RR 2003-
Survey # 8-1

3. DESCRIPTION--COMPLEX IN GENERAL

Describe the entire railroad complex at this site; mention all buildings and structures, with notation of which are not historic. Check items which apply and discuss in narrative:

- ☐ Moved buildings (original location, date of and reason for move)
☐ Any non-railroad uses in complex (military recruiting, etc.)
☐ Any unusual railroad building types, such as crew quarters, etc. (specify)
☐ Known threats to complex or individual structures

Surroundings: ☒ urban ☐ suburban ☐ scattered buildings ☐ open space
☐ residential ☐ woodland ☐ agricultural ☐ industrial
☒ downtown commercial ☐ highway commercial ☐ other (specify)

Relationship of station grade to track grade:

- ☐ Station and track grade at same level ☐ Station at street grade, track depressed
☐ Station spans track ☒ Track elevated above street grade, multi-level station

of tracks: 5

Pedestrian access across tracks:

- ☐ Pedestrian bridge: ☐ at street grade ☐ elevated
☐ Pedestrian/vehicular bridge: ☐ at street grade ☐ elevated
☒ Tunnel
☐ None provided

Discuss character of vehicular and pedestrian approaches to complex; landscaping; relationship to parking. Refer to, and key with, site plan.

The Cranford complex consists of a one story red brick station with Shingle derivative massing, an adjoining wood frame and steel canopy at street level, and at track level, platforms and wood frame and steel canopies with brick shelters and stair pavilions built beneath them. The tracks are elevated on an E-W embankment, while the station is at street grade, adjoining the embankment on the S. The platforms and canopies are linked to the station and street via stairs (A, B, D, E) and a tunnel through the embankment. Pedestrians may approach the platforms also from Union Avenue at the west end of the complex (C, F). There are large parking lots north and south of the tracks (1, 3, 4) with additional space available along adjoining streets. The periphery of the lots is well landscaped.

-3

RR 2008-
Survey #8-1

FACILITY NAME: Cranford

3. DESCRIPTION--PLATFORMS AND CANOPIES

☒ Inbound☐ Outbound☐ Between tracks

792' concrete platform, lined with incan.
lighting fixtures on iron pipe poles.
Poles are original; fixtures are recent.

Nature and extent of existing original material and alterations: roof type, material, supports; freestanding or attached to building; seating; lighting; signage; other.

The inbound island is 7 bays long and consists of a wood frame hipped roof, sheathed in slate and supported on a double row of steel columns and brackets (pink). Until recently the canopy extended several bays further east. Originally, framing was open, but has since been enclosed with a plaster board soffit (white). Lighting consists of soffit attached fluorescent fixtures which probably replaced incandescent fixtures attached to the framing. Gutters and downspouts are aluminum. Originals were probably copper. The canopy shelters 2 stairways from the tunnel. Stairs have iron pipe rail balustrades and pipe rails (paint). Built beneath the canopy are an elevator, its concrete tower with copper flashing projecting above the roof and (2) shelters (see description).

The tunnel and its 4 branches paralleling the tracks are finished in poured concrete (white). Lighting consists of ceiling mounted fluorescent fixtures, which replace incandescent originals. Perpendicular to the north tunnel entry is a cantilevered steel frame canopy with pent roof, wood deck, and slate sheathing. There are several original board signs, hanging from the framing and mounted to the wall (red, white type). The gutter is copper and original. An oversized advertisement clock, a recent addition, is suspended over the entry.

-3

RR 2003-
Survey #8-1

FACILITY NAME: Cranford

3. DESCRIPTION--PLATFORMS AND CANOPIES

☐ Inbound☒ Outbound☐ Between tracks

800' concrete platform, lined with incandescent lighting fixtures on iron pipe poles. Poles are original; fixtures are recent.

Nature and extent of existing original material and alterations: roof type, material, supports; freestanding or attached to building; seating; lighting; signage; other.

The outbound island canopy is nearly identical to the eastbound canopy (see previous description). The major exceptions are in the location of the built-in elements, which are in reverse order from those across the tracks, i.e., - (E to W) stairs-elevator-stairs-shelter. Additionally, there is only one shelter, while the inbound canopy has shelters at either end of the series, i.e., (E to W) shelter-stairs-elevator-stairs-shelter. (Of the 3 shelters, only the inbound, east end, is accessible.)

FACILITY NAME Cranford

RR 2003-
Survey # 8-1

4. DESCRIPTION - BUILDINGS (EXTERIOR)

Fill out separate sheet for each building at facility. Refer to, and key with, site plan.

Account for original materials and finishes where different from existing.

Station X Shelter Freight House Other (specify)

General architectural description, including style, shape, roof type, # stories, # bays, orientation to track, location of entrances, etc.

The Cranford station, located south of the elevated tracks, consists of a 1 story red brick block with a hipped slate roof its main ridge paralleling the tracks. On the south facade is a central porte-cochere with hipped roof flanked symmetrically by 2 dormers with louvered facades. The end bays on the east and west sides of the block assume the form of loggias with round arch openings on the south, east, and west. The 3 open sides of the porte-cochere have similar surround treatment. The south facade contains a central 6 pane double door with 5 p. transom, flanked on either side by (3) 3/1 windows (2 boarded), while the north facade contains similar double doors, flanked on the east by (east to west) double freight doors and (2) 3/1 windows, and on the west by (4) 3/1 windows (2 boarded). The east facade contains a double freight door and a window. The west facade contains (2) 3 pane windows with another 3 pane window in the north half of the loggia's exterior wall. A square brick chimney rises south of the main ridge from the east half of the roof.

Adjoining the west facade and paralleling the elevated tracks is a frame 3 bay hipped canopy supported on a single row of steel columns. A soffit of gypsum board has recently been added and (3) fluorescent fixtures attached. Originally the framing was open and lighting probably consisted of hanging incandescent fixtures. Like that of the station, roofing is slate. Joining the north facade of the station and the concrete retaining wall of the track elevation is a steel frame canopy with linear skylight. The canopy shelters a walkway to the underpass entry and is a later addition since the station predates the track elevation.

-4H

FACILITY NAME Cranford

RR 2003-
Survey # 8-1

4. DESCRIPTION - BUILDINGS (EXTERIOR) CONTD.

Fill out separate sheet for each building at facility.

Station X Shelter _____ Freight House _____ Other _____ (specify)

EXTERIOR MATERIALS AND SYSTEMS:

	<u>Original</u>	<u>Existing, if different</u>
Structural system:	<u>Bearing wall/frame roof</u>	
Foundation:	<u>Poured concrete</u>	
Base course:	<u>Poured concrete</u>	
Walls:	<u>Brick (red, mod. Eng. bond)</u>	
Trim:	<u>Brick sills, voussoirs, lintels, exposed rafter ends</u>	
Doors:	<u>(2) 6 p. double with 5 p. transom</u>	
Roofing:	<u>Slate</u>	
Soffit:	<u>Match boards in port cochere sides (prob. stain)</u>	<u>gypsum board</u> paint (white)
Windows:	<u>3/1</u>	
Lighting:	<u>Attach. incan. fix. in porte cochere</u>	
Signage:	<u>None</u>	
Drainage:	<u>Copper g&d?</u>	<u>Galvanized g&d</u>
Other:	<u>Square chimney, copper flashing, hipped dormers, louvered fascia (E end). 3 bay hipped canopy (W), steel beam columns, single row.</u>	<u>Gypsum board soffit, 3 attached fluorescent fixs.</u>

-5

FACILITY NAME Cranford

RR 2003-
Survey #8-1

4. DESCRIPTION - BUILDINGS (INTERIOR)

Fill out separate sheets for each building at facility. Refer to, and key with, floor plan.

Station X Shelter Freight House Other (specify)

General architectural description of all spaces, including original materials and finishes, if known, any remodeling, etc.

Except for the basement, the spaces of the Cranford station are all on one level. The central half of the block comprises the Waiting Room (1). To the west are the Lounges and rest rooms (3, 4, 5M, 5W) and to the east, the Ticket Office (2), a storage room (7) and Baggage Room (6). Public spaces have similar finish treatment: terrazzo or tile floors, tile wainscots, and plaster walls and ceilings. All materials appear original.

FACILITY NAME: Cranford

-5A
RR 2003-
Survey #8-1

4. DESCRIPTION - BUILDINGS (INTERIOR) CONTD.

STATION - ROOM AND FINISH SCHEDULE

NUMBER OF ROOMS 9NAME Waiting RoomNUMBER ON FLOOR PLAN 1

	<u>Original</u>	<u>Existing, if different</u>
Floors:	<u>terrazzo (tan), charcoal border</u>	<u></u>
Base:	<u>terrazzo (charcoal)</u>	<u></u>
Wainscot:	<u>tile (blue)</u>	<u></u>
Walls:	<u>plaster</u>	<u>paint (white)</u>
Ceiling:	<u>plaster, exposed beams, cushions (stain)</u>	<u>paint (white)</u>
Trim:	<u>frames (stain)</u>	<u></u>
Doors:	<u>(4) 1 panel; double 3 pane with single 5 pane transom</u>	<u></u>
Seating:	<u>(2) fixed double slat board benches, carved volutes on arms, acanthus leaves</u>	<u></u>
Lighting:	<u>(4) hang. metal chandeliers, 12 globe lights on circular frame</u>	<u>paint (copper color)</u>
Other:	<u>(2) hang. incan. fixs. over ticket windows, splayed leaf pattern base</u>	<u></u>
	<u>(2) iron radiators between benches, relief type "Tickets" signs over windows (metal)</u>	<u>wall mounted clock W. wall</u>

FACILITY NAME: Cranford

-5A
RR 2003-
Survey # 8-1

4. DESCRIPTION - BUILDINGS (INTERIOR) CONTD.

STATION - ROOM AND FINISH SCHEDULE

NUMBER OF ROOMS 9NAME Ticket OfficeNUMBER ON FLOOR PLAN 2

	<u>Original</u>	<u>Existing, if different</u>
Floors:	<u>board</u>	<u>linoleum (red)</u>
Base:	<u>board (stain)</u>	<u></u>
Wainscot:	<u>none</u>	<u></u>
Walls:	<u>plaster</u>	<u>paint (tan)</u>
Ceiling:	<u>plaster</u>	<u>paint (white)</u>
Trim:	<u>wood plain surrounds (stain)</u>	<u></u>
Doors:	<u>(2) 1 panel (stain)</u>	<u></u>
Seating:	<u>none</u>	<u></u>
Lighting:	<u>2 hang. incan. fixs. with conical metal shades</u>	<u>(1) hang. fluorescent fix.</u>
Other:	<u>built-in wood cabinet under ticket windows</u> <u>wood molded frame pass through, shelf to gang room (stain) (E)</u>	<u>wall mounted clock (E)</u>

FACILITY NAME: Cranford

-5A
RR 2003-
Survey # 8-1

4. DESCRIPTION - BUILDINGS (INTERIOR) CONTD.

STATION - ROOM AND FINISH SCHEDULE

NUMBER OF ROOMS 9NAME Men's RoomNUMBER ON FLOOR PLAN 3

	<u>Original</u>	<u>Existing, if different</u>
Floors:	<u>tile (gray)</u>	<u></u>
Base:	<u>tile (gray)</u>	<u></u>
Wainscot:	<u>cer. tile (biege)</u>	<u></u>
Walls:	<u>plaster</u>	<u>paint (white)</u>
Ceiling:	<u>plaster</u>	<u>paint (white)</u>
Trim:	<u>wood molded frames (stain)</u>	<u></u>
Doors:	<u>(4) 1 panel (metal)</u>	<u>paint (brown)</u>
Seating:	<u>none</u>	<u></u>
Lighting:	<u>hang. incan. fix.</u>	<u>(1) hang. fluorescent fix.</u>
Other:	<u>metal partitions iron radiator (N) plumbing fixtures</u>	<u>paint (brown)</u>

-5A

RR 2003-
Survey # 8-1

FACILITY NAME: Cranford

4. DESCRIPTION - BUILDINGS (INTERIOR). CONTD.

STATION - ROOM AND FINISH SCHEDULE

NUMBER OF ROOMS 9NAME Ladies' RoomNUMBER ON FLOOR PLAN 4

	<u>Original</u>	<u>Existing, if different</u>
Floors:	<u>tile (gray)</u>	<u></u>
Base:	<u>tile (gray)</u>	<u></u>
Wainscot:	<u>cer. tile (beige)</u>	<u></u>
Walls:	<u>plaster</u>	<u>paint (white)</u>
Ceiling:	<u>plaster</u>	<u>paint (white)</u>
Trim:	<u>wood molded frames (stain)</u>	<u></u>
Doors:	<u>(4) 1 panel (metal)</u>	<u>paint (brown) 1 missing</u>
Seating:	<u>none</u>	<u></u>
Lighting:	<u>hang. incan. fixs.</u>	<u>(1) hang. fluorescent fix.</u>
Other:	<u>iron radiator (W) plumbing fixs. metal partitions</u>	<u>paint (brown)</u>

FACILITY NAME: Cranford

-5A
RR 2003-
Survey # 8-1

4. DESCRIPTION - BUILDINGS (INTERIOR) CONTD.

STATION - ROOM AND FINISH SCHEDULE

NUMBER OF ROOMS 9NAME Men's LoungeNUMBER ON FLOOR PLAN 5M

	<u>Original</u>	<u>Existing, if different</u>
Floors:	<u>terrazzo (tan) charcoal border</u>	<u></u>
Base:	<u>terrazzo (charcoal)</u>	<u></u>
Wainscot:	<u>tile (blue)</u>	<u></u>
Walls:	<u>plaster</u>	<u>paint (white)</u>
Ceiling:	<u>plaster</u>	<u>paint (white)</u>
Trim:	<u>plaster cornice molding wood molded frames (stain)</u>	<u>paint (white)</u>
Doors:	<u>(2) 1 panel</u>	<u>1 missing</u>
Seating:	<u>fixed bench, slat board (S)</u>	<u></u>
Lighting:	<u>hang. incan. fixs.</u>	<u>(1) hang. fluorescent fix.</u>
Other:	<u>iron radiator (N) drinking fountain (W wall, attach.)</u>	

-5A

RR 2003-

Survey #8-1

FACILITY NAME: Cranford

4. DESCRIPTION - BUILDINGS (INTERIOR) CONTD.

STATION - ROOM AND FINISH SCHEDULE

NUMBER OF ROOMS 9NAME Ladies' LoungeNUMBER ON FLOOR PLAN 5W

	<u>Original</u>	<u>Existing, if different</u>
Floors:	<u>terrazzo (tan) charcoal border</u>	<u></u>
Base:	<u>terrazzo (charcoal)</u>	<u></u>
Wainscot:	<u>tile (blue)</u>	<u></u>
Walls:	<u>plaster</u>	<u>paint (white)</u>
Ceiling:	<u>plaster</u>	<u>paint (white)</u>
Trim:	<u>plaster cornice molding wood molded frames (stain)</u>	<u>paint (white)</u>
Doors:	<u>(2) 1 panel</u>	<u>one missing</u>
Seating:	<u></u>	<u>(1) wood chair</u>
Lighting:	<u>hang. incan. fix.</u>	<u>(1) hang. fluorescent fix.</u>
Other:	<u>iron radiator (S) drinking fount. (W), wall mounted</u>	<u>wall mounted mirror (E)</u>

FACILITY NAME: Cranford

-5A
RR 2003-
Survey # 8-1

4. DESCRIPTION - BUILDINGS (INTERIOR) CONTD.

STATION - ROOM AND FINISH SCHEDULE

NUMBER OF ROOMS 9NAME Track Gang Room (Baggage Room) NUMBER ON FLOOR PLAN 6

	<u>Original</u>	<u>Existing, if different</u>
Floors:	<u>poured concrete</u>	<u></u>
Base:	<u>none</u>	<u></u>
Wainscot:	<u>none</u>	<u></u>
Walls:	<u>brick, mod. Eng. bond (E, N, S)</u>	<u>paint (black)</u>
Ceiling:	<u>matched boards, prob. stain</u>	<u>paint (black)</u>
Trim:	<u>none</u>	<u></u>
Doors:	<u>6/panel, doubled; 6/panel prob. stain</u>	<u>paint (black)</u>
Seating:	<u>none</u>	<u></u>
Lighting:	<u>2 hang. incan. fixs., circular metal shade</u>	<u>1 hang. fluorescent fix.</u>
Other:	<u>(2) iron radiator (E) basin (W) iron pipe rail, balustrade, steps to basement</u>	<u></u>

FACILITY NAME: Cranford

-5A
RR 2003-
Survey # 8-1

4. DESCRIPTION - BUILDINGS (INTERIOR) CONTD.

STATION - ROOM AND FINISH SCHEDULE

NUMBER OF ROOMS 9NAME StorageNUMBER ON FLOOR PLAN 7

	<u>Original</u>	<u>Existing, if different</u>
Floors:	<u>poured concrete</u>	<u></u>
Base:	<u>none</u>	<u></u>
Wainscot:	<u>none</u>	<u></u>
Walls:	<u>brick (N), Eng. bond mod. concrete block (S, E, W)</u>	<u>paint (lt. green)</u>
Ceiling:	<u>match board, prob. stain</u>	<u>paint (lt. green)</u>
Trim:	<u>plain wood frame (stain)</u>	<u></u>
Doors:	<u>(2) 1 panel (stain)</u>	<u></u>
Seating:	<u>none</u>	<u></u>
Lighting:	<u>(1) hang. incan. fix., circular metal shade</u>	<u></u>
Other:	<u></u>	<u></u>

-5A

FACILITY NAME: Cranford

RR 2003-
Survey # 8-1

4. DESCRIPTION - BUILDINGS (INTERIOR) CONTD.

STATION - ROOM AND FINISH SCHEDULE

NUMBER OF ROOMS 9NAME BasementNUMBER ON FLOOR PLAN

	<u>Original</u>	<u>Existing, if different</u>
Floors:	<u>poured concrete</u>	<u></u>
Base:	<u>none</u>	<u></u>
Wainscot:	<u>none</u>	<u></u>
Walls:	<u>poured concrete</u>	<u></u>
Ceiling:	<u>poured concrete</u>	<u></u>
Trim:	<u>none</u>	<u></u>
Doors:	<u>none</u>	<u></u>
Seating:	<u>none</u>	<u></u>
Lighting:	<u>incan. fixs.</u>	<u></u>
Other:	<u>boiler</u>	<u></u>

FACILITY NAME Cranford

RR 2008-
Survey # 8-1

4. DESCRIPTION - BUILDINGS (EXTERIOR)

Fill out separate sheet for each building at facility. Refer to, and key with, site plan.

Account for original materials and finishes where different from existing.

Station _____ Shelter X Freight House _____ Other _____ (specify)

General architectural description, including style, shape, roof type, # stories, # bays, orientation to track, location of entrances, etc.

The Cranford shelters (3) are built beneath the canopies, 2 on the inbound side, 1 on the outbound side. Walls are poured concrete; the canopies serve as roofs. The north and south facades each contain ~~(7)~~ 3/1 windows, while the east and west facades each contain a single door. Most openings have been boarded; otherwise, materials appear original.

-5B

RR 2003-
Survey # 8-1

FACILITY NAME: Cranford

4. DESCRIPTION - BUILDINGS (INTERIOR) CONTD.

Fill out separate sheet for each additional structure in complex.
Refer to, and key with, site plan.

Shelter X Freight House _____ Other _____ (specify)
Inbound, East end (other 3 shelters boarded, not accessible)
INTERIOR FINISH SCHEDULE Materials are original

Number of rooms: 1

	<u>Original</u>	<u>Existing, if different</u>
Floors:	<u>board</u>	_____
Base:	<u>board</u>	_____
Wainscot:	<u>none</u>	_____
Walls:	<u>match boards, poss. stain</u>	<u>paint (green)</u>
Ceiling:	<u>match boards, poss. stain</u>	<u>paint (white)</u>
Trim:	<u>wood molded surrounds, cornice molding (stain)</u>	_____
Doors:	<u>(2) ?</u>	<u>(1) 1/plain (metal) (boarded)</u>
Seating:	<u>(2) match board benches (N, S) (stain)</u>	_____
Lighting:	<u>incan. fixs.</u>	_____
Other:	<u>iron radiators behind benches</u>	_____

FACILITY NAME: Cranford

RR 2003-
Survey # 8-1

5. SIGNIFICANCE OF COMPLEX (ARCHITECTURAL/HISTORICAL):

Buildings and Bridges

Architect A.E. Owen, Chief Eng., source (plans)Date 1929 Source (plans) Alteration dates _____ Source _____Style Vernacular Railroad, Shingle derivative detail and massing# passenger trains/day (present) 44 in 1980 Peak (#, Yr.) 73 in 1941No Original station on site 56 in 1912

- 1st (1844) Elizabethtowne and Somerville, small frame station-store
- 2nd (1865) Central of New Jersey, "standard plan" similar to stations at Westfield, Clinton St., Plainfield, Dunellen, and Somerville.
- 3rd (1906) Central of New Jersey, twin brick buildings with tile roofs and elaborate interiors, designed by Bradford L. Gilbert (plans, CHS).

The Cranford station, the fourth to be built on the site and the first track elevation, was planned following a major collision at the Union Avenue crossing between a truck and a Reading Philadelphia Express. The entire project, station, platforms, and track elevations, were completed at a cost of \$2,257,758 and dedicated on New Year's Day, 1930 (Cranford).

Although the station presently serves a large number of commuters, it was an important junction point until 1967; many CNJ local trains from Jersey City began and ended their runs at Cranford. Approximately one mile east of the station is a large storage yard and servicing facility for local trains. Adjacent to the yards were the CNJ greenhouses. Because the greenhouses were close to the Cranford station, ornamental plants were often placed in the waiting room on an experimental basis. If they flourished there, they were distributed to other stations on the line. (Tino)

20

FACILITY NAME: Cranford

-7
RR 2003-
Survey # 8-1

6. GEOGRAPHIC DATA

Verbal boundary description (if Block/Lot not available)

Boundaries are outlined on the site plan.

Acreage: c. 2 acres

UTM coordinates: Zone: --/Easting: --/Northing: --
USGS Quad Roselle Scale 1:24,000

7. REFERENCES

BIBLIOGRAPHIC:

Tino, Nicholas A. Jr., conversation with the author, May 1981.
plans, Cranford station (1929), Conrail, Philadelphia, Pa.
plans, Cranford station (1906), Cranford Historical Society.
Cranford Citizen and Chronicle, 50th Anniversary issue,
24 June 1943.

ICONOGRAPHIC:

plans, Ibid. (1929)
plans, Ibid. (1906)

photos: railroad memorabilia collection, Cranford Historical Society

8. PHOTO

Negative index # unknown or NJT photo # - slide # 8-1
Date 1978 Photographer Richard Browne Assocs.
Loc. of negative NJ Transit Direction of view: Station from South

~ 1

RR 2003-8
Survey # 8-1

FACILITY NAME: Cranford

9. CRITERIA FOR EVALUATION

A. HISTORICAL SIGNIFICANCE

- i. Associated with important events or broad movements
in history
 nationally _____ (30)
 state-wide _____ (25)
 locally _____ (20)
- ii. Representative of significant changes in railroad history
and/or technology
 rare _____ (30)
 unusual _____ (25)
 common x (10) grade separation
- iii. Original station on site _____ (15)
- iv. Representative of a line's standard design _____ (10)
- v. Constructed prior to 1900 _____ (15)
- vi. Junction station _____ (10)
- vii. Former long-distance service x (10)
- viii. Other _____ (10)
- ix. Less than 50 years old _____ (-30)

20

B. ARCHITECTURAL SIGNIFICANCE

- i. STYLE Vernacular Railroad with Shingle derivative detail and massing
 a. Example of a particular architectural style (check one)
 Outstanding _____ (50)
 Excellent _____ (40)
 Very good _____ (30)
 Good _____ (20)
 Fair _____ (10)
- b. Rare survivor of style
 nationally _____ (20)
 state-wide _____ (15)
 locally _____ (10)
- c. As example of railroad architecture
 rare _____ (30)
 unusual or early _____ (15)

FACILITY NAME: Cranford

RR 2003- -9
Survey # 8-1

CRITERIA CONT.

ii. ARCHITECT (check one)

a. building by architect important

nationally	____	(25)
state-wide	____	(20)
locally	____	(15)

b. building designed by railroad and is known or appears to be the work of the supervising architect or engineer or chief designer ____ (20)

c. building designed by railroad and is known or appears to be the work of the staff ____ X (5)

d. architect identified but not considered to be of special importance ____ (5)

iii. OVERALL ARCHITECTURAL QUALITY (check one)

a. Outstanding composition, siting, or craftsmanship ____ (40)

b. Notable composition, siting, or craftsmanship, or possessing especially picturesque or unusual exterior detailing ____ (25)

c. Possessing some detail(s) of particular interest and/or quality ____ (15)

d. Average quality or interest ____ (5)

iv. SPECIAL QUALITIES

a. Noteworthy overall interior design or detailing ____ (15)

b. Some noteworthy interior detailing ____ X (5) globe light
(____ interior not accessible) chandeliersc. Part of cohesive complex
1) station and shelter ____ X (5)
2) more than two buildings ____ (10)

v. CONSTRUCTION

a. Noteworthy example of particular construction method ____ (30)

b. Rare or early survivor of particular method ____ (20)

c. Interesting example of method ____ (5)

RR 2003- -10
Survey # 8-1

FACILITY NAME: Cranford

CRITERIA CONT.

C. CONDITIONS

1. INTEGRITY

- | | |
|---|---------------------|
| a. Original condition | <u> X </u> (40) |
| b. Alterations and/or additions, beneficial | <u> </u> (30) |
| c. Alterations and/or additions, not detrimental | <u> </u> (20) |
| d. Minor detrimental alterations and/or additions, not affecting overall integrity | <u> </u> (10) |
| e. Detrimental alterations and/or additions, reversible at considerable expense | <u> </u> (-25) |
| f. Detrimental alterations and/or additions, essentially irreversible | <u> </u> (-75) |

II. PHYSICAL CONDITION

- | | |
|--------------------------|---------------------|
| a. Excellent | <u> </u> (10) |
| b. Good | <u> X </u> (5) |
| c. Fair | <u> </u> (0) |
| d. Poor | <u> </u> (-10) |
| e. Severely deteriorated | <u> </u> (-25) |

III. RELATIONSHIP TO COMMUNITY

- | | |
|-------------------------------|---------------------|
| a. Pivotal building | <u> </u> (40) |
| b. Integral part of townscape | <u> </u> (30) |
| c. Compatible with townscape | <u> X </u> (20) |
| d. Unrelated to townscape | <u> </u> (0) |
| e. Incompatible | <u> </u> (-30) |

IV. SUITABILITY FOR ADAPTIVE USE

- | | |
|------------------------------|--------------------|
| a. Excellent | <u> </u> (30) |
| b. Very Good | <u> X </u> (25) |
| c. Good | <u> </u> (20) |
| d. Average | <u> </u> (15) |
| e. Possible, with difficulty | <u> </u> (10) |

90

TOTAL

125

RR 200

Survey #8-1 -11

FACILITY NAME: Cranford

Attach copy of site plan

____continuation sheets attached

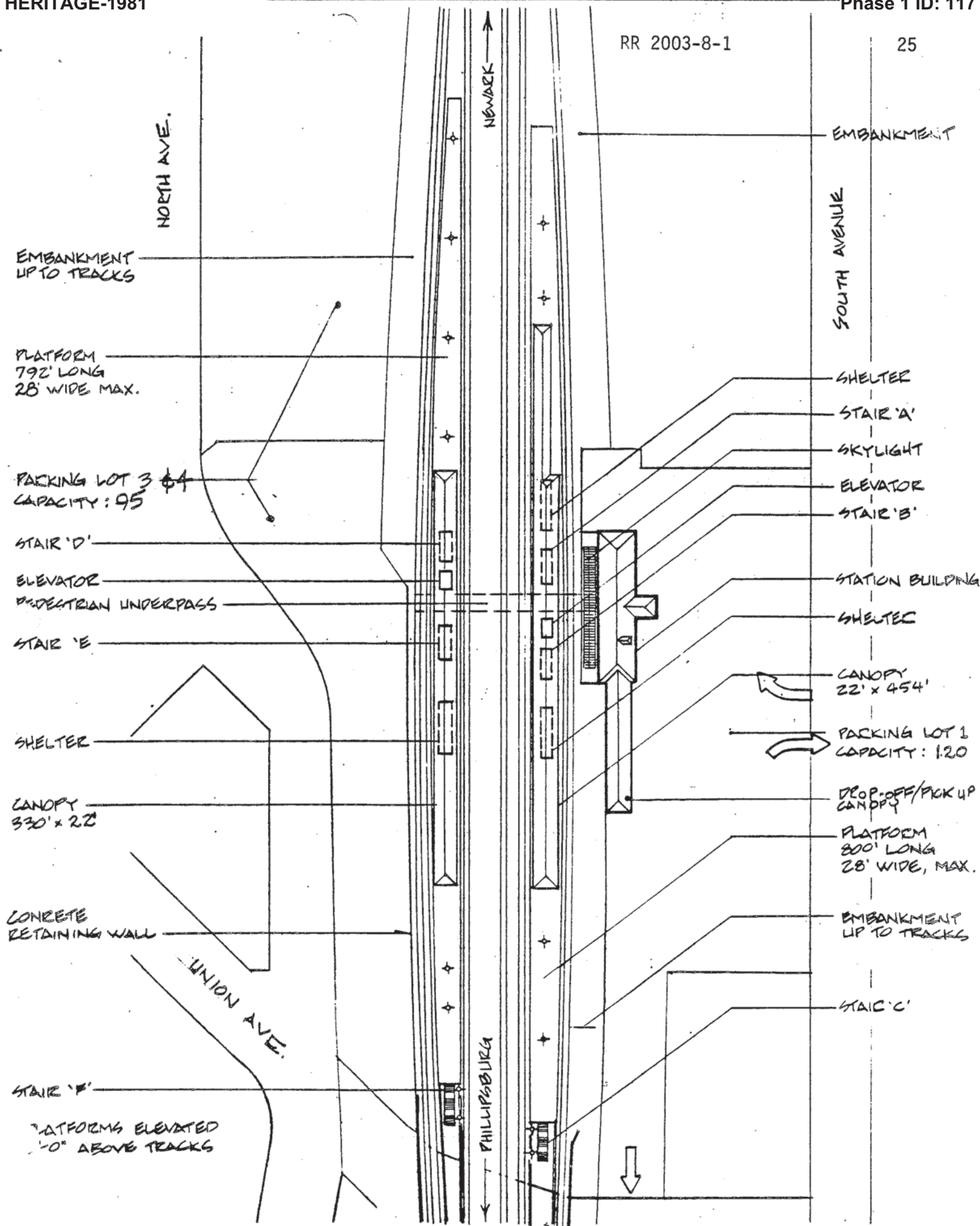
FORM PREPARED BY: Richard Meyer

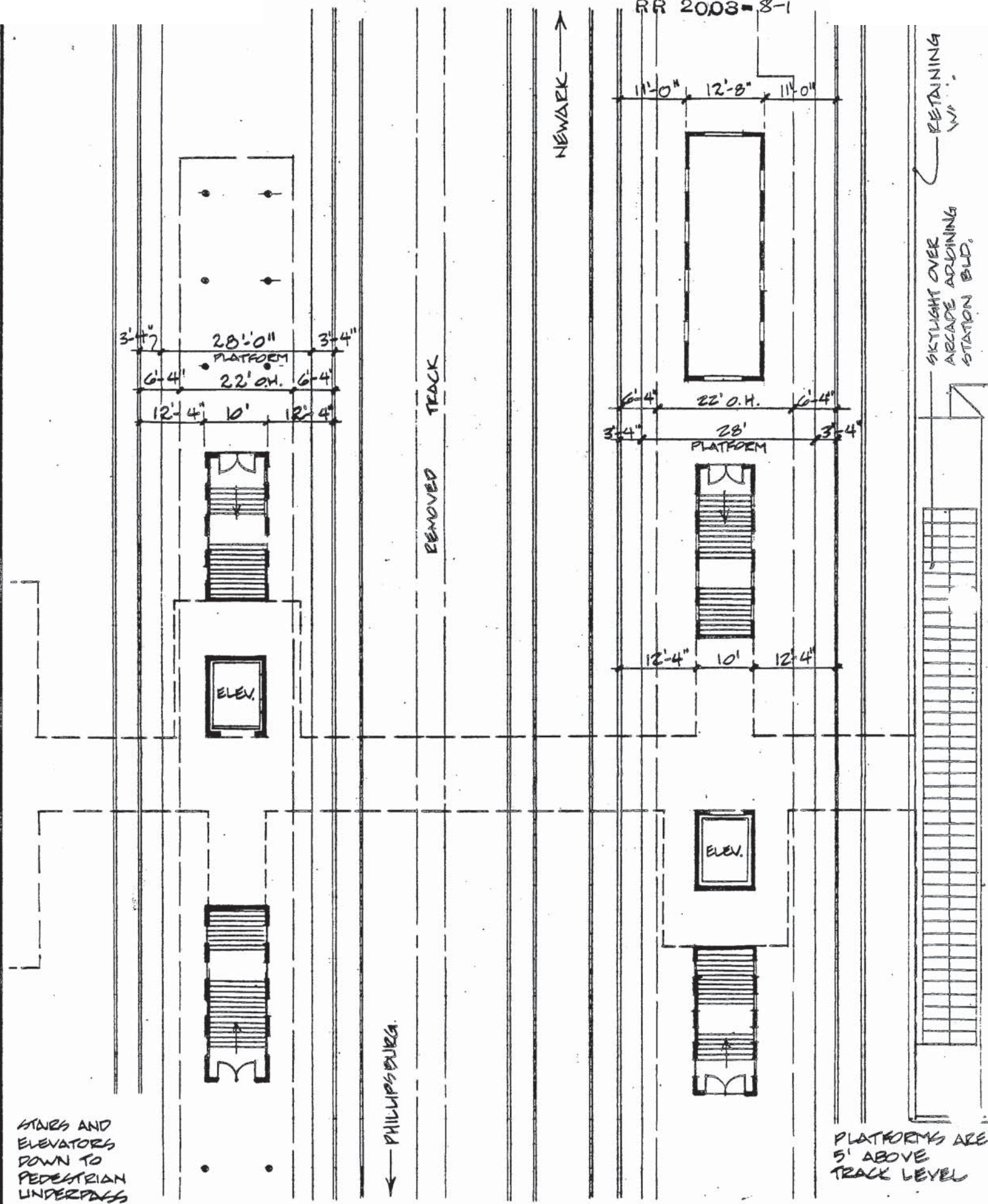
Date: August 1981

HERITAGE STUDIES, INC.
RD 4 Box 864, Mapleton Road
Princeton, N.J. 08540
609-452-1754

RR 2003-8-1

25

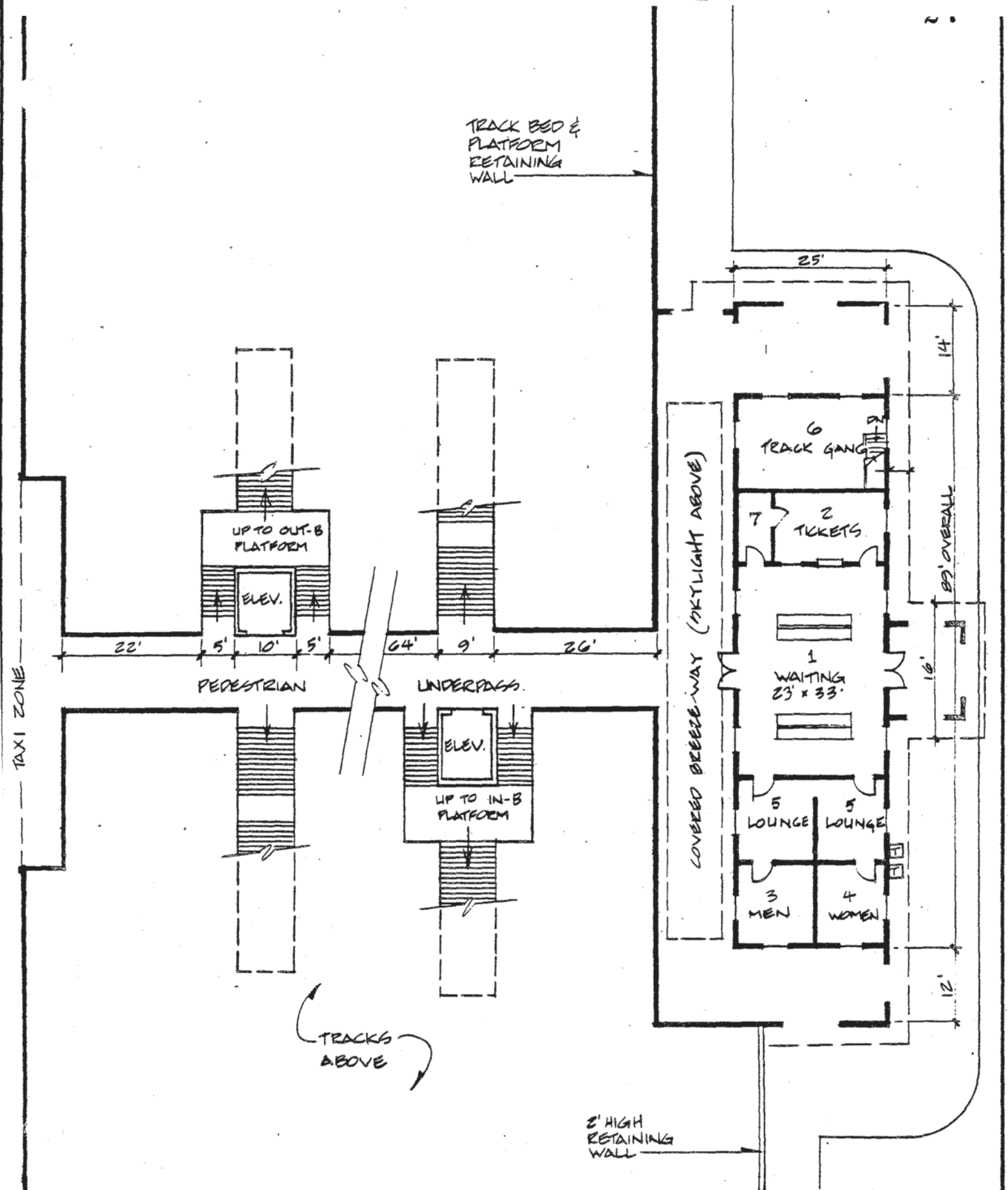




STAIRS AND
ELEVATORS
DOWN TO
PEDESTRIAN
UNDERPASS

PHILIPSBURG.

PLATFORMS ARE
5' ABOVE
TRACK LEVEL



CRANFORD
RARITAN VALLEY LINE

SCALE: 1" = 20'

DATE: NOV. 1978

N



SUMMARY

Station: CranfordLine: Raritan Valley

Index:

Field Survey Conducted October, 1978

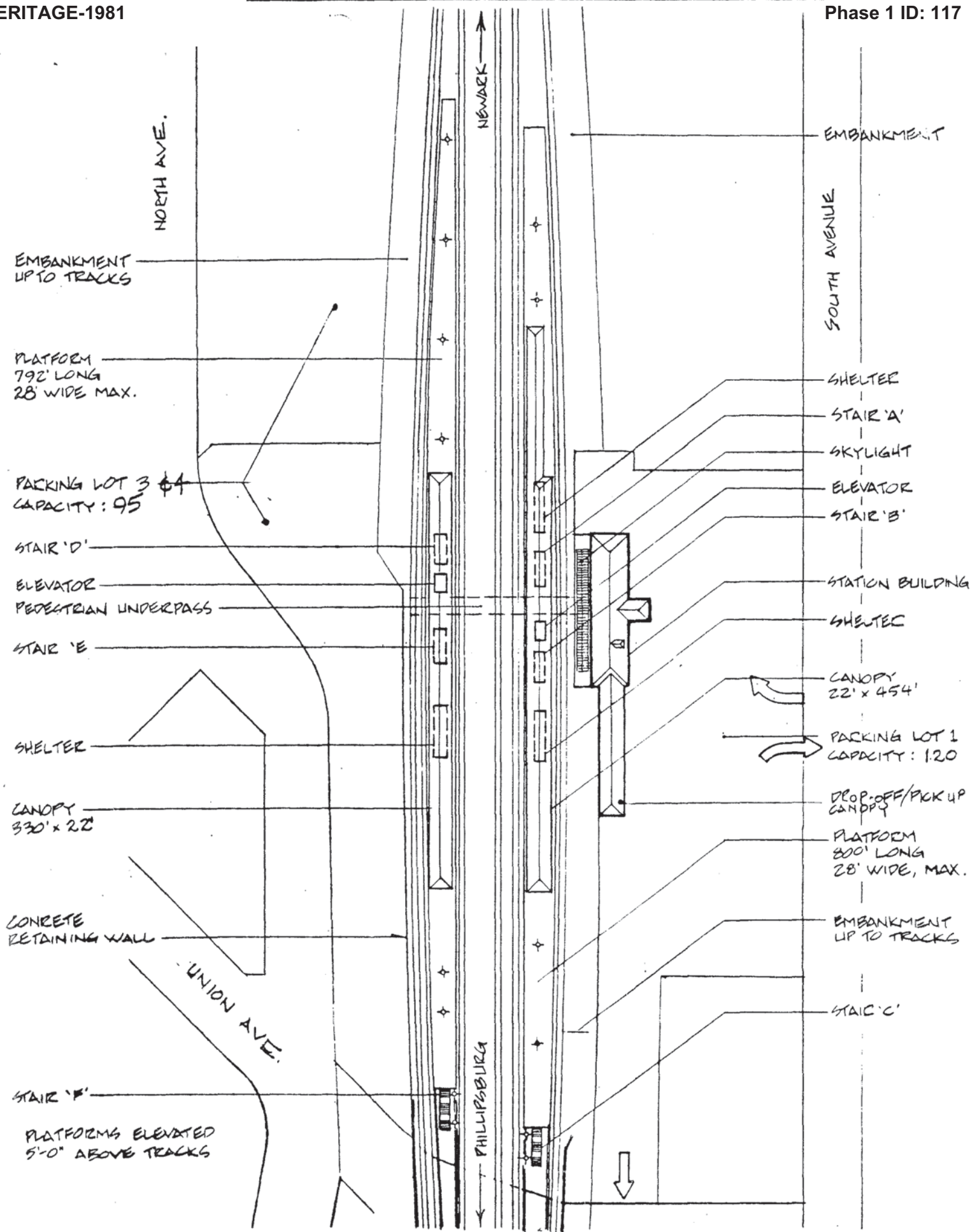
- | | |
|----------|--|
| <u>X</u> | 1. Site Base at 1" = 100' |
| <u>X</u> | 2. Floor Plan at 1" = 20' |
| <u>X</u> | 3. Platform and Canopies |
| <u>X</u> | 4. Station Building <u> </u> Structural <u> </u> Mech. & Elec. |
| <u>X</u> | 5. Track Crossings and Barriers |
| <u>X</u> | 6. Parking Access and Circulation |
| <u>X</u> | 7. Information System |
| <u>X</u> | 8. Notes on Community & Security Aspects |
| <u>X</u> | 9. Record Photograph of Station <u> X </u> Detailed Field Photographs |

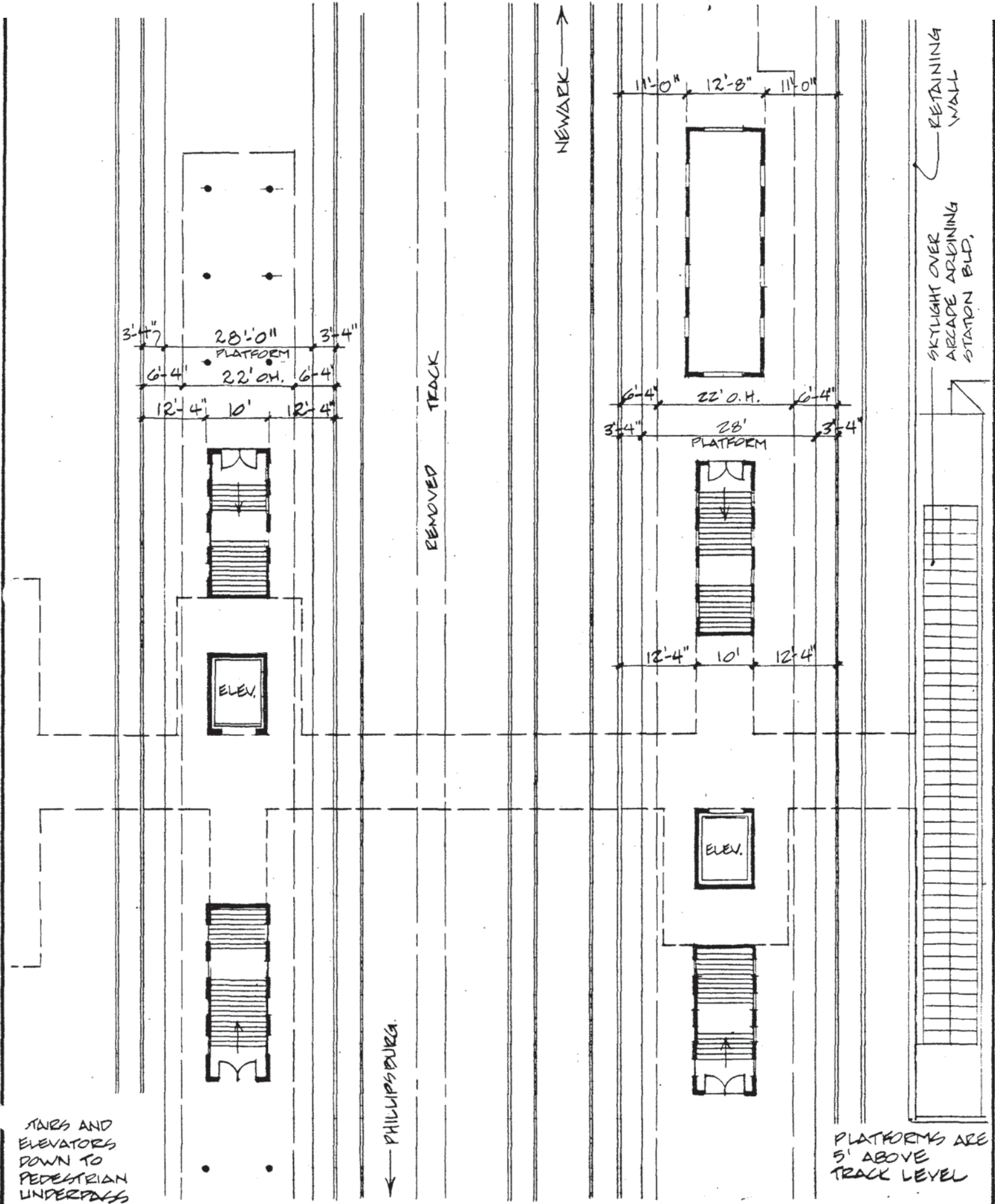
Information File:

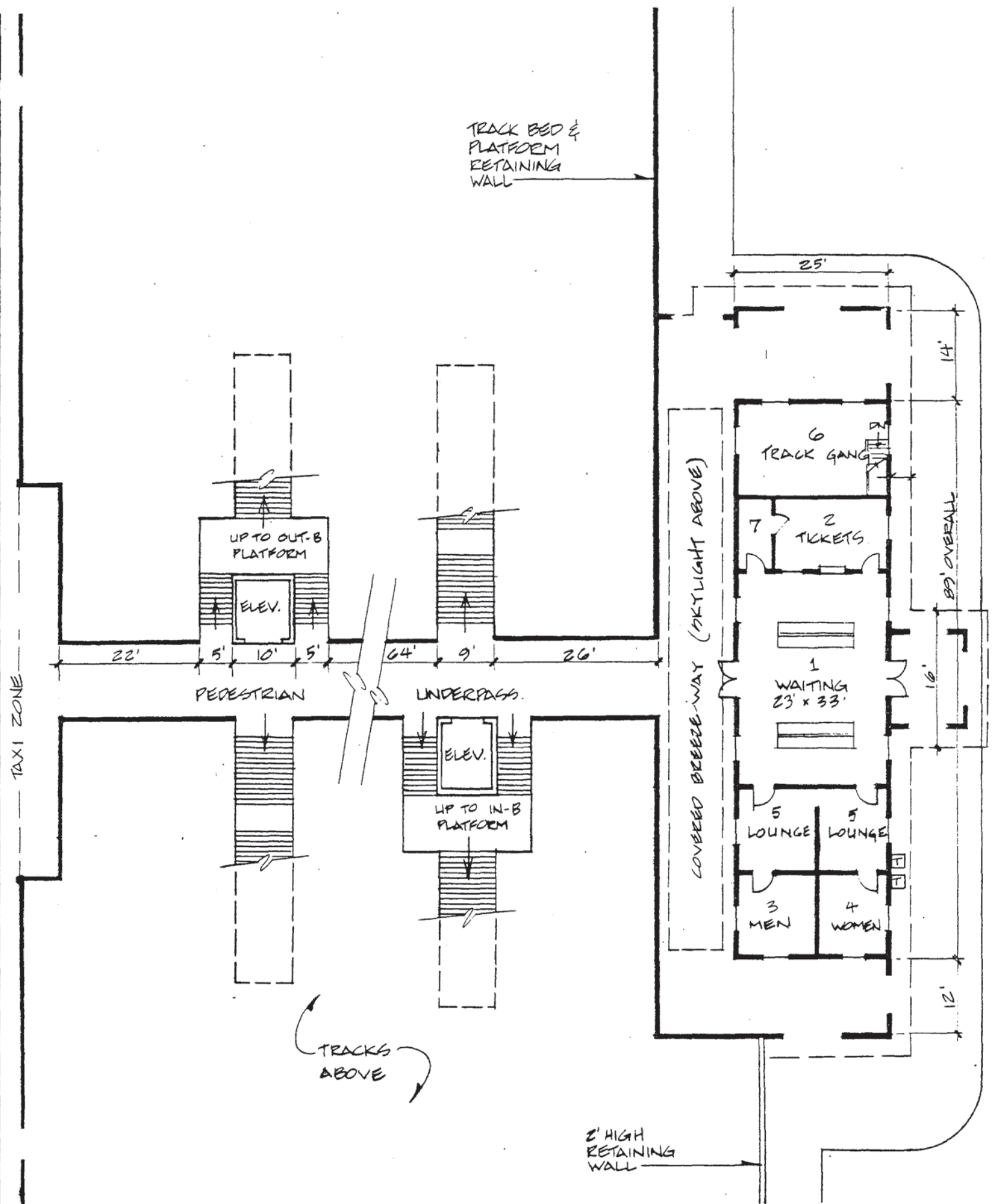
- | | |
|---------------|---|
| <u>X</u> | Aerial Photograph at 1" = 200' |
| <u>X</u> | Station Location Plan from USGS maps or Hagstrom Maps |
| <u>X</u> | Proposed Taking Lines of 900 Day Option Station Parcels |
| <u>X</u> | Summer 1970 Ground Survey of Rail Parking - NJ DOT |
| <u>X</u> | September 1974 Survey - NJ DOT (Dept. of Commuter Services) |
| <u> </u> | Tri-State Aerial Photo Survey of Rail Parking 1970 |
| <u>X</u> | Conrail Data Survey for Station |
| <u> </u> | TOPICS or Traffic Improvements Planned in Station Area |
| <u> </u> | Community Renewal Plans for Station |
| <u> </u> | Historical File for Station |
| <u>X</u> | Schedule of Trains and Buses |
| <u>X</u> | Other Union County Station Parking and Access Study |
| | 1978 Raritan Valley Upgrade Study |

Conrail Count May 1977 - All Day (Week Day) Boarding Passengers 886Station Ridership Category: 3 Ownership: N.J.D.O.T.Agent: Yes Hrs/Days: 5:40 a.m. - 2:15 p.m. Monday through Friday

Rehabilitated (10 years or less) & Description:







NEW JERSEY OFFICE OF HISTORIC PRESERVATION HISTORIC SITES INVENTORY # 200324
INDIVIDUAL STRUCTURE SURVEY FORMHISTORIC NAME: Cranford Railroad Station
LOCATION: South AvenueCOMMON NAME: Railroad Station
BLOCK/LOT 479/3

MUNICIPALITY: Cranford

COUNTY: Union

USGS QUAD: Roselle

UTM REFERENCES:

OWNER/ADDRESS: Consolidated Rail Corp.

Zone/Northing/Easting

DESCRIPTION Railroad Station

Construction Date: 1928-33

Source of Date: Documentation

Architect:

Builder:

Style: "Prairie" style adaptation

Form/Plan Type:

Number of Stories: 1½

Foundation: Concrete

Exterior Wall Fabric: Brick American Bond

Fenestration:

Roof/Chimneys: Hipped roof with overhanging eaves. Slate shingles.

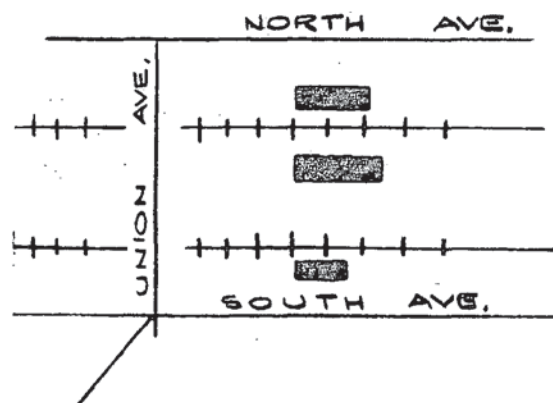
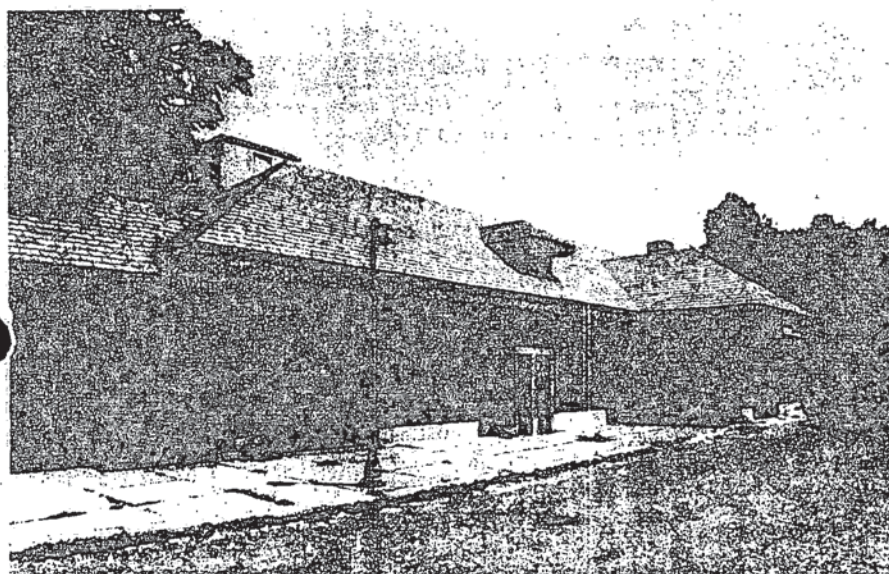
Additional Architectural Description:

Strong horizontal lines in roofed platforms, punctuated by a two story concrete square tower with "Cranford" sign.

Station itself does not break long, narrow lines of platform roof - it is only an enclosed continuation of the platform.

PHOTO Negative File # Roll 33 frame 34, 36, 37

Map (Indicate North)



RY DESCRIPTION, AND RELATED STRUCTURES:

Parts of the station located on each side of the railroad tracks,
connected by a tunnel,

SURROUNDING ENVIRONMENT: Urban ☐ Suburban ☒ Scattered Buildings ☐
Open Space ☐ Woodland ☐ Residential ☐ Agricultural ☐
Industrial ☐ Downtown Commercial ☒ Highway Commercial ☐ Other ☐

SIGNIFICANCE:

The present railroad station replaced a 1902-04 station, which in turn,
had replaced a station of the 1870's,

The Central Railroad of New Jersey began the present station in conjunction with raising the tracks above street level and eliminating at-grade crossings in town. The work of completing overpass bridges, retaining walls, parking areas, and other features was taken up as a WPA project in Cranford,

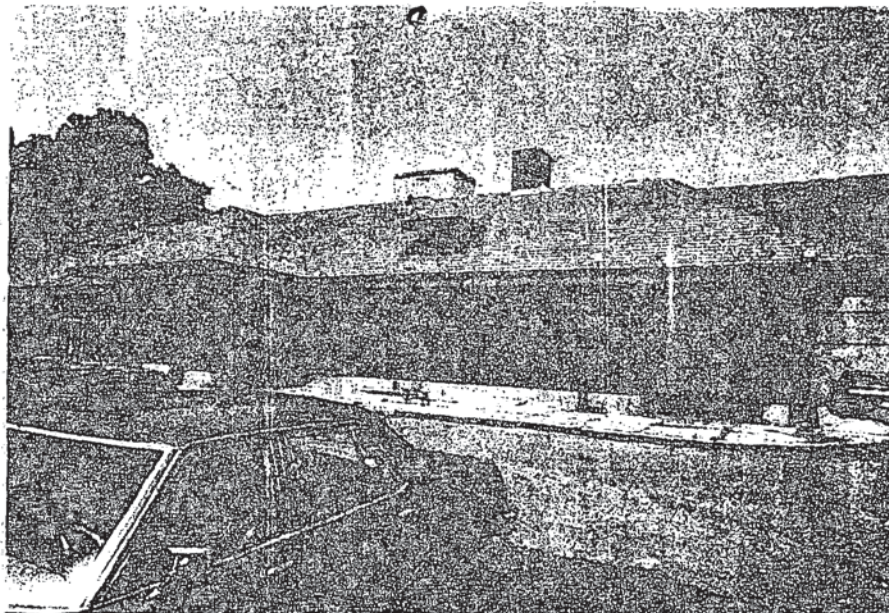
ORIGINAL USE: Railroad station	PRESENT USE: Railroad Station
PHYSICAL CONDITION: Excellent <input type="checkbox"/> Good <input type="checkbox"/>	Fair <input checked="" type="checkbox"/> Poor <input type="checkbox"/>
REGISTER ELIGIBILITY: Yes <input type="checkbox"/> Possible <input type="checkbox"/>	No <input checked="" type="checkbox"/> Part of District <input type="checkbox"/>
THREATS TO SITE: Roads <input type="checkbox"/> Development <input type="checkbox"/>	Zoning <input type="checkbox"/> Deterioration <input checked="" type="checkbox"/>
No Threat <input type="checkbox"/> Other <input type="checkbox"/>	
COMMENTS:	

REFERENCES:

Nick Tino. Railroad Study for Heritage Associates.
Cranford Historical Society

RECORDED BY: JWF
ORGANIZATION: UCCHPAB

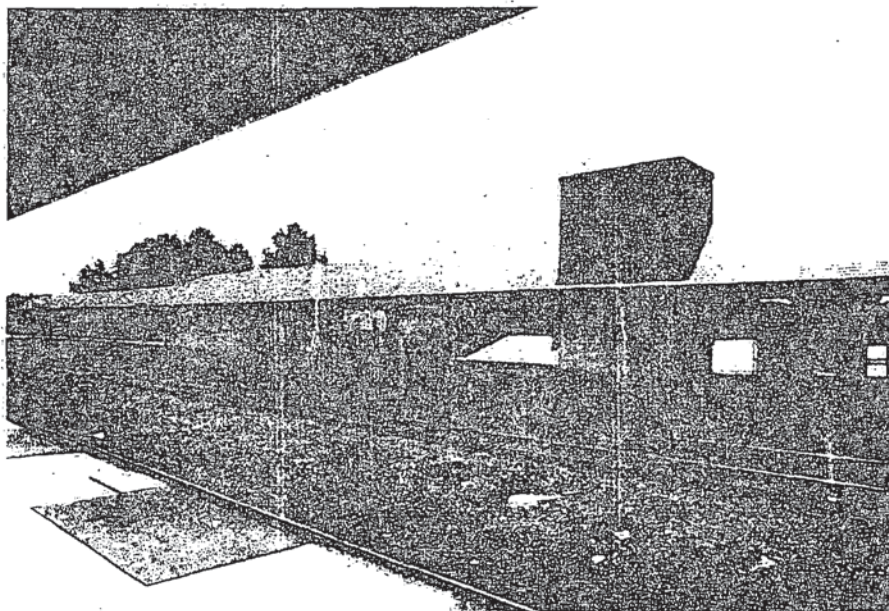
DATE: 1981



Neg. # R33/37

South Avenue

Neg. # R33/34





CRANFORD
Union Co.

* 200324
R33/36

Cranford Railroad Station
South Ave.
looking N.E

1981



CRANFORD
Union Co.

* 200324
R33/3A

Cranford Railroad Station
South Ave.
looking S.E.

1981



CRANFORD
Union Co.

005AA

* 200324

R33/37

Cranford Railroad Station
South Ave.
looking N.W.

1981

New Jersey Department of Environmental Protection
Historic Preservation Office

Page 1 of 1

CONTINUATION SHEET

Historic Sites #:

Resource Name: Cranford Railroad Station
Address: East of Intersection, Union and Walnut Avenues
Block and Lot #: Part of 800/3
Municipality: Cranford Township
County: Union

Representation in Existing Surveys:

The Cranford Railroad Station complex was surveyed in August, 1981 (form enclosed) as part of NJ Transit's *Railroad Station Survey*.

National Register Eligibility:

The 1981 survey only evaluated the Cranford station for its individual eligibility, not as a component of a historic district. In that context, the station was evaluated as not eligible for inclusion in the National Register of Historic Places.

In 1995, the entire Central Railroad of New Jersey Main Line was determined to be eligible for inclusion in the National Register of Historic Places as a linear historic district. Therefore, the purpose of the current study is to determine if individual resources such as the Cranford station contribute to the historic district's overall significance.

Site Development:

Ca. 1839, Cranford was established as a CNJ station stop. The first station was built in 1844; the second in 1865; the third in 1905-06; and the present station in 1929-30. The 1882 Union County Atlas and the 1893 Sanborn map show the passenger station opposite the freight depot. Noteworthy structures in the vicinity include a coal shed marked "track on roof", a coal and lumber company, and the Cranford Hotel.

The 1929 Sanborn map shows the new eastbound passenger station connected by a tunnel to a temporary passenger station. Noteworthy structures in the vicinity include a small newsstand (adjacent to the eastbound station), a cement storage building (with a siding), the coal and lumber company, and the Cranford Hotel.

Changes in Physical Description Since the 1981 Survey:

- The two side entrances to the station's south façade central porte-cochere have now been enclosed with modern metal doors.
- The central double door on the station's south façade has been replaced with modern 15 pane double doors.
- The canopy soffits at track level have been covered with synthetic siding; the brackets are no longer visible.
- Track level shelters have been replaced with modern shelters.

Historic Significance:

The 1981 survey included the following evaluation of the complex's architectural and/or historic significance:

“Although the station presently serves a large number of commuters, it was an important junction point until 1967; many CNJ local trains from Jersey City began and ended their runs at Cranford. Approximately one mile east of the station is a large storage yard and servicing facility for local trains. Adjacent to the yards were the CNJ greenhouses. Because the greenhouses were close to the Cranford station, ornamental plants were often placed in the waiting room on an experimental basis. If they flourished there, they were distributed to other stations on the line.”

As part of the current study, the Cranford Station Complex has been evaluated as a contributing element within the CRRNJ Main Line Historic District. The complex includes the multi-level station with an adjoining street level canopy, two track level platforms and canopies, and a connecting tunnel. The station's complex character defining features include:

Station

- Features representative of “typical” railroad structure architecture for passenger stations.
 - Masonry brick construction. By the time of this structure's construction, the CNJ and other railroad companies had switched from frame to more durable masonry construction.
 - Rectangular form
 - Typical size, 89' x 25' (Cranford's size is consistent with an early twentieth century recommendation for small passenger stations. Of the three sizes recommended in 1911 by John A. Droege, Cranford and Plainfield (eastbound) are the two stations along the CRRNJ Main Line which are approximately in the middle range of the three proposed sizes, 80'-90' by 30'-35')
 - Representative floor plan with prominent waiting area

- Wide overhanging eaves
- Interior trim, including terrazzo floor, tile wainscoting, and built-in wood benches
- Platforms
 - Single pole
 - Steel columns and trusses with a lattice beam
 - Gable roof
- Features representative of a main architectural style utilized for railroad stations: Colonial Revival.
 - Multi-pane windows
 - Dutch Bond
 - Symmetric
 - Elliptical arches
 - Prominent roof
 - Hipped roof
 - Slate
 - Dormers
 - Large chimneys
- Relationship to track/representative of a grade crossing elimination campaign
 - Early bi-level station
- Setting typical for the CRRNJ Main Line's eastern sites
 - Parallel to and set back from the street
 - Landscaping

Attachments:

- Current photographs
- 1882 E. Robinson and Company Atlas of Union County
- 1893 Sanborn map
- 1918 photograph of the station grounds. Interstate Commerce Commission, Division of Valuation. *Engineering Field Notes of Interstate Commerce Commission Parties Surveying the Physical Property of Railroads*
- July 10, 1928 Plans for "Proposed New Passenger Station" (on file at NJ Transit)
- 1929 Sanborn map
- 1981 NJ Transit Railroad Station Survey form for Cranford passenger station

Survey Name: NJ Transit Raritan Valley Line Study
Surveyor: Nancy L. Zerbe
Organization: ARCH², Inc.

Date: July, 1999



Cranford Station. 1882 E. Robinson and Company Atlas of Union County.

Survey Name: NJ Transit Raritan Valley Line Study
Surveyor: Nancy L. Zerbe
Organization: ARCH², Inc.

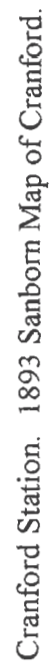
Date: July, 1999



Cranford Station (South Façade)

Survey Name: NJ Transit Raritan Valley Line Study
Surveyor: Nancy L. Zerbe
Organization: ARCH², Inc.

Date: July, 1999

Date: July, 1999

Survey Name: NJ Transit Raritan Valley Line Study

Surveyor: Nancy L. Zerbe

Organization: ARCH², Inc.

1918

INTERSTATE COMMERCE COMMISSION
DIVISION OF VALUATION

Phase 1 ID: 117

Carrier C & D MAIN LINE

Valuation Section

H. B. Cunningham
V. Heintz
for I. C. C.Fountain at
Cranford N.J. Sta. Grounds.

Cranford Station Grounds, Fountain, 1918. ICC, Division of Valuation. *Engineering Field Notes of Interstate Commerce Commission Parties Surveying the Physical Property of Railroads.*

Survey Name: NJ Transit Raritan Valley Line Study
Surveyor: Nancy L. Zerbe
Organization: ARCH², Inc.

Date: July, 1999

New Jersey Department of Environmental Protection
Historic Preservation Office

Page 1 of 1

CONTINUATION SHEET

Historic Sites #:

Resource Name: Cranford Railroad Station (demolished)

Address:

Block and Lot #:

Municipality: Cranford Township

County: Union

Representation in Existing Surveys:

N/A

Description and History:

The Cranford stations shown in the 1910-1911 Revaluation were described by the Interstate Commerce Commission as:

- Passenger station (westbound); built in 1906; 1 story; 15' x 22'; brick and stone.
- Passenger station (eastbound); built in 1906; 1 story; 21' x 71'; brick.

These stations were replaced in 1929-1930 by the current stations.

Attachments:

- Site Plan. State Board of Assessors. *Records of the 1910-1911 Revaluation of Railroads and Canals in New Jersey*. Field Books, Central Railroad of New Jersey.

Sources:

- Interstate Commerce Commission, Division of Valuation. *Engineering Field Notes of Interstate Commerce Commission Parties Surveying the Physical Property of Railroads*, 1918 (revised 1923).
- State Board of Assessors. *Records of the 1910-1911 Revaluation of Railroads and Canals in New Jersey*. Field Books, Central Railroad of New Jersey.

Survey Name: NJ Transit Raritan Valley Line Study
Surveyor: Nancy L. Zerbe
Organization: ARCH², Inc.

Date: July, 1999



Date: July, 1999

INDIVIDUAL STRUCTURE SURVEY FORM

HISTORIC NAME: Terrance Brennan House
 LOCATION: 206 South Ave. E.

COMMON NAME:
 BLOCK/LOT 481/7

MUNICIPALITY: Cranford

COUNTY: Union

USGS QUAD: Roselle

UTM REFERENCES:

OWNER/ADDRESS: Griffin, Joseph & June
 38 Henley Ave., Cranford, N.J.

Zone/Northing/Easting

DESCRIPTION

Construction Date: ca. 1870/1910

Source of Date: Documentation

Architect:

Builder:

Style: Planbook Type

Form/Plan Type: "L" shape plan

Number of Stories: 2½

Foundation: Brick

Exterior Wall Fabric: Wooden shingles - unpainted

Fenestration: Two bay facade. 6/1 sash windows.

Roof/Chimneys: Gable roof. Interior brick chimneys.

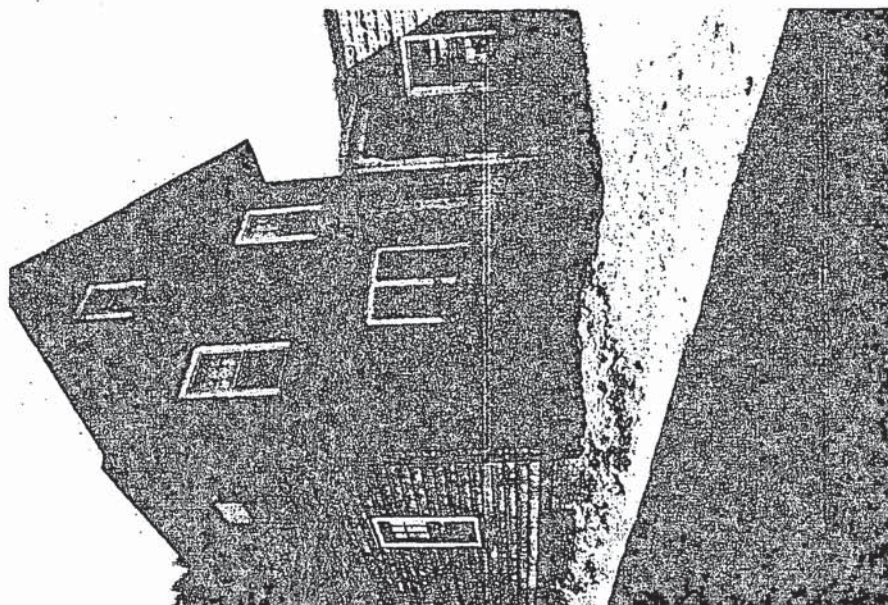
Additional Architectural Description:

Gable end facade-Bargeboard trim along eaves. One and one-half story wing on west side of house. Brick foundation. Natural shingle siding.

Rusticated stone, 1 story commercial addition made to front of house.

PHOTO Negative File # Roll 33 frame 26A

Map (Indicate North)



SOUTH

AVE.

H
I
G
H
S
T.



The house sits 20' from street; one story addition sits on sidewalk line. There is a gas station to the west; a commercial building 20' to the east.

SURROUNDING ENVIRONMENT: Urban ☐ Suburban ☒ Scattered Buildings ☐
Open Space ☐ Woodland ☐ Residential ☐ Agricultural ☐
Industrial ☐ Downtown Commercial ☐ Highway Commercial ☒ Other ☐

South Avenue between Union Avenue and the Rahway River is a commercial area in Cranford.

SIGNIFICANCE:

The Brennan house is a good example of a simplified Victorian style. The "Planbook" house was popular in the mid-19th Century, and published plans for it could be followed by the builders in every town. Except for the 20th Century addition in front of the house, it is well-preserved, and retains many original features.

ORIGINAL USE: Residence	PRESENT USE: Commercial/Residence
PHYSICAL CONDITION: Excellent <input type="checkbox"/> Good <input checked="" type="checkbox"/>	Fair <input type="checkbox"/> Poor <input type="checkbox"/>
REGISTER ELIGIBILITY: Yes <input type="checkbox"/> Possible <input checked="" type="checkbox"/>	No <input checked="" type="checkbox"/> Part of District <input type="checkbox"/>
THREATS TO SITE: Roads <input type="checkbox"/> Development <input type="checkbox"/>	Zoning <input checked="" type="checkbox"/> Deterioration <input checked="" type="checkbox"/>
No Threat <input type="checkbox"/> Other <input checked="" type="checkbox"/>	
COMMENTS:	

As long as the Brennan House is in a commercial zone, it is subject to demolition, or total disfigurement of the house through insensitive additions and remodeling.

REFERENCES:

Union County Landmarks Inventory, 1976.

RECORDED BY: JWF
ORGANIZATION: UCCHPAB

DATE: 1981



CRAUFORD
Union Co.

* 200326
R 33/26

Terrance Brennan Hse
206 South Ave. E.
looking S.W.

1981

BASE SURVEY FORM

Historic Sites #:

Property Name: Robbins & Allison, Inc. Storage

Street Address: Street #: 211 (Low) 215 (High) Apartment #: (Low) (High)

Street Name: South

Prefix: South

County(s): Union

Municipality(s): Cranford

Local Place Name(s):

Ownership: USGS Quad: Koselle

Block(s): Lot(s):

Zip Code: Suffix: Type: AVE

Description:
The Robbins & Allison, Inc. Storage company building is located on the north side of South Avenue, adjacent to the NJ Transit parking lot. This 3- and 4-story buttressed brick building has a flat, parapeted roof and a center entrance on the south elevation which is flanked by a door and window. All of the openings have segmental-arched lintels of header bricks. The south elevation windows are 4/4 double-hung wood sash; windows on the side elevations are infilled.

Registration and Status

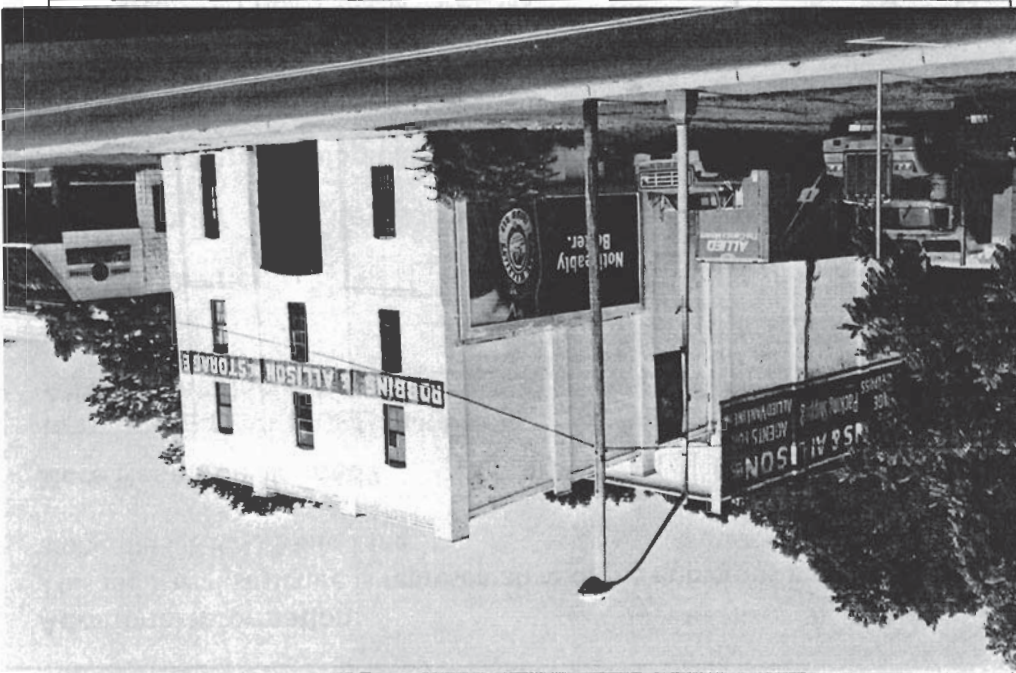
National Historic Landmark: SHPO Opinion: / /

National Register: / / Local Designation: / /

Dates: New Jersey Register: / / Other Designation: / /

Determination of Eligibility: / / Other Designation Date: / /

Photograph:



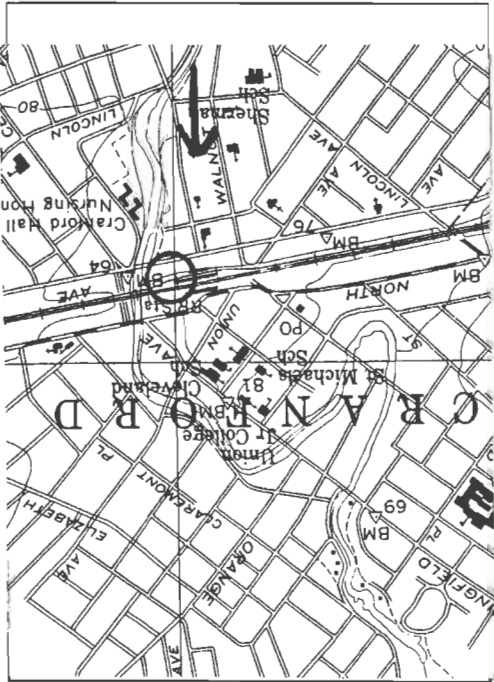
Survey Name: NJ Transit Raritan Valley Line
Surveyor: Nancy L. Zerbe, Stacy E. Spies
Organization: ARCH², Inc.

Date: September, 1999

BASE SURVEY FORM

Historic Sites #:

Location Map:



Bibliography/Sources:

Additional Information:

This industrial structure is representative of the numerous industries in Garwood located along the CRNJ Main Line.

More Research Needed? ☒ Yes ☐ No

INTENSIVE-LEVEL USE ONLY:

Attachments included: _____ Building/Element _____ Landscape _____ Farm _____ Bridge _____ Industry _____

Historic District: ☐ Yes ☐ No Historic District Name: _____

Status: _____ Key Contributing _____ Contributing _____ Non-contributing

Associated Archaeological Site/Deposits? ☐ Yes ☐ No (Known or potential sites – if yes, please describe briefly)

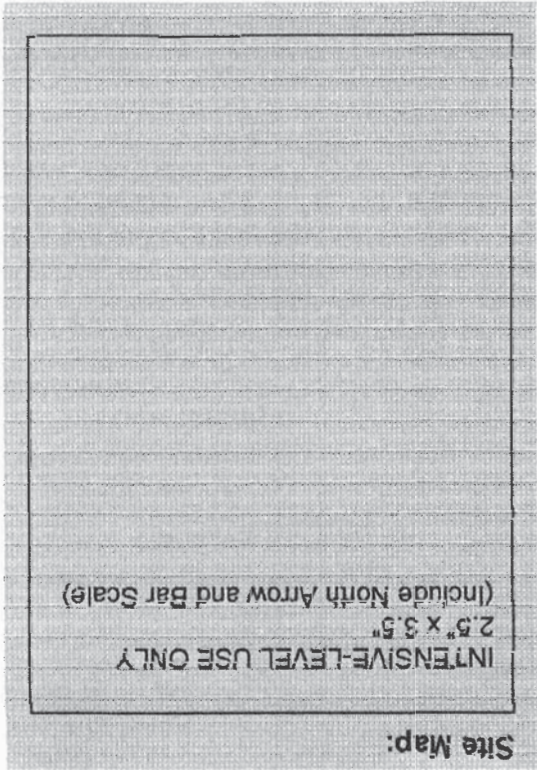
Survey Name: NJ Transit Raritan Valley Line

Surveyor: Nancy L. Zerbe, Stacy E. Spies

Organization: ARCH², Inc.

Date: September, 1999

Site Map:



INTENSIVE-LEVEL USE ONLY
2.5" x 3.5"
(Include North Arrow and Bar Scale)

ICE OF HISTORIC PRESERVATION HISTORIC SITES INVENTORY # 200325

INDIVIDUAL STRUCTURE SURVEY FORM

HISTORIC NAME: Trolley Power House
LOCATION: South Ave. E

COMMON NAME: Public Service Building
BLOCK/LOT 479/5

MUNICIPALITY: Cranford

COUNTY: Union

USGS QUAD: Roselle

UTM REFERENCES:

OWNER/ADDRESS: P.S. Elect. & Gas Co. c/o Tax Dept.

Zone/Northing/Easting

80 Park Place, Newark, N.J.

DESCRIPTION

Construction Date: ca. 1900

Source of Date: Documentation

Architect:

Builder:

Style: Neo-classical

Form/Plan Type: Rectangular

Number of Stories: 2

Foundation: Cut stone

Exterior Wall Fabric: Large ashlar blocks

Fenestration: Nine bay facade. Heavy moldings around windows capped by extra-large keystones.

Roof/Chimneys:

Additional Architectural Description:

Fortress-like building. Dark, strong lines of the building.

Bays set in slightly recessed arches.

Central door.

Originally had a very tall smokestack. Now removed.

Wide frieze band at top.

Only "ornament" are two iron "torch" shaped wall lamps flanking the main door with acanthus leaf design on them.

PHOTO Negative File # Roll 33 Frame 24A 25A/17/36 Map (Indicate North)



SOUTH AVE.

RIVER
RAHWAY

Located on a property bounded on the north by the Central Railroad of New Jersey, on the east by the Rahway River, and on the south by South Avenue, a main commercial street in Cranford.

SURROUNDING ENVIRONMENT: Urban ☐ Suburban ☐ Scattered Buildings ☐
Open Space ☒ Woodland ☐ Residential ☐ Agricultural ☐
Industrial ☐ Downtown Commercial ☒ Highway Commercial ☐ Other ☐

The Rahway River Parkway provides adjacent "open space", but the general appearance of the neighborhood is of scattered commercial development.

SIGNIFICANCE:

The Main Line of the New York and Philadelphia Trolley passed through Cranford, generally following the route of today "South Avenue", through Union County, from Elizabeth to Plainfield. This was one of several power stations along the way which helped keep the trolleys running.

The trolley was operating from about 1906 to 1928, when it was replaced with bus routes. At that time, Cranford took over the building and used it to generate electricity for the town.

The fortress-like building is an impressive structure, a well known local landmark. The simple embellishment of the building gives its utilitarian nature a romantic twist, conjuring up images of castles.

ORIGINAL USE: Power Station	PRESENT USE: Power Station
PHYSICAL CONDITION: Excellent <input type="checkbox"/> Good <input checked="" type="checkbox"/>	Fair <input type="checkbox"/> Poor <input type="checkbox"/>
REGISTER ELIGIBILITY: Yes <input checked="" type="checkbox"/> Possible <input type="checkbox"/>	No <input type="checkbox"/> Part of District <input type="checkbox"/>
THREATS TO SITE: Roads <input type="checkbox"/> Development <input type="checkbox"/>	Zoning <input type="checkbox"/> Deterioration <input type="checkbox"/>
No Threat <input checked="" type="checkbox"/> Other <input type="checkbox"/>	
COMMENTS:	

Very well maintained, and an indestructable-looking building.

REFERENCES:

Cranford Historical Society

RECORDED BY: JWF
ORGANIZATION: UCCHPAB

DATE: 1981



CRANFORD
Union Co.

* 200325
R33/24

Trolley Power Station
south Ave. E.
looking N.E

1981



CRANFORD
Union Co.
TROLLEY POWER STATION

200325
R33/25

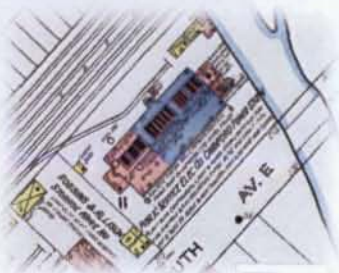
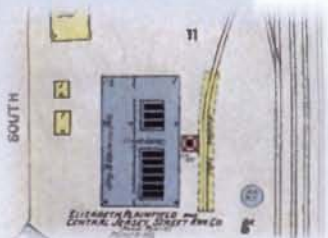
South Ave. E.
looking N.E.

1981

Historic Overview of the Cranford Substation

225 South Avenue E.
(Block 479 Lot 5)

*Cranford Township
Union County, New Jersey*



Prepared for:
Public Service Electric & Gas Company
4000 Hadley Road
South Plainfield, New Jersey 07080



Prepared by:
E2 Project Management, LLC
87 Hibernia Avenue
Rockaway, NJ 07866



April 2015

RECEIVED

MAY 13 2015

HISTORIC PRESERVATION A-358

Historic Overview of the Cranford Substation

**225 South Avenue E.
(Block 479 Lot 5)**
*Cranford Township
Union County, New Jersey*

Prepared for:

Public Service Electric & Gas Company
4000 Hadley Road
South Plainfield, New Jersey 07080

Prepared by

Marianne Walsh
E2 Project Management, LLC (E2PM)
87 Hibernia Avenue
Rockaway, NJ 07866

E2PM Project # P-15-07-01

APRIL 2015

TABLE OF CONTENTS

CHAPTER	PAGE
TABLE OF CONTENTS.....	i
LIST OF FIGURES.....	i
LIST OF PLATES.....	i
1.0 INTRODUCTION and ACKNOWLEDGMENTS	1
2.0 HISTORIC OVERVIEW.....	1
3.0 BIBLIOGRAPHY	31

APPENDIX A: Street Railway Journal, Volume 23. 1904

APPENDIX B: The Engineering Record, Building Record and Sanitary Engineer, Volume 49. 1904

APPENDIX C: Historic Plans, Cranford Substation 1942

LIST OF FIGURES

FIGURE	PAGE
1 Location of Cranford Substation	2
2 Location of Cranford Substation	3
3 Elizabeth, Plainfield and Central Jersey Street Railway Co. Power House (Cranford Substation) in 1903	15
4 1903 Bird's-Eye View of Cranford.....	16
5 1903 Bird's-Eye View of Cranford Showing Detail of The Cranford Trolley Power House (Cranford Substation)	16
6 Cranford Trolley Power House (Cranford Substation) in 1904.....	17
7 Map of Public Service Divisions in 1906.....	18
8 Trolley Lines in Northern New Jersey in 1908	20
9 1908 Public Service Railway trolley timetable for the Main Line (No. 165).	20
10 Cranford Power House in 1908	21
11 Cranford Power House in 1915. A second chimney has been added since 1908.....	22
12 Cranford Power House in 1922	23
13 1929 Aerial Photograph of the Cranford Power House (Substation)	25
14 1929 Bird's - Eye View of the Cranford Power House (Substation)	26
15 Design Plans Representing the 1942 Alterations to the Cranford Substation.....	27
16 1957 Aerial Photograph of the Cranford Power House (Substation).	29
17 1966 Aerial Photograph of the Cranford Power House (Substation)	30

LIST OF PLATES

PLATE	PAGE
1 Cranford Substation. View South.....	4
2 Cranford Substation. View West.....	4
3 Cranford Substation – Public Service Medallion. View South.	5
4 Cranford Substation. View East.	6
5 Interior of Cranford Substation. View East.....	7
6 Interior of Cranford Substation. View West	8
7 Interior of Cranford Substation. View East	9
8 Interior of Cranford Substation – Condenser and Engine Room. View West.....	10
9 Interior of Cranford Substation – Stair Between Substation Room and Basement (1903 Construction). View North.....	11
10 Interior of Cranford Substation – Basement Beneath Main Substation Room (1903 Construction). View East.....	12
11 Interior of Cranford Substation – Basement Condenser and Former Engine Rooms (1910 Construction). View West.....	13

1.0 INTRODUCTION and ACKNOWLEDGMENTS

The Cranford Substation is located at 225 South Avenue E. (Block 479 Lot 5) in the Township of Cranford on a plot of land where South Avenue and the rails of New Jersey Transit's Raritan Valley Line cross the Rahway River (Figures 1 and 2; Plates 1 and 2). South Avenue is a fairly quiet two-lane thoroughfare traveling through the heart of Cranford, lined primarily with small, local businesses and single-family homes. For over one hundred years, the Cranford Substation has housed the operations that supply the municipality with electric power and light.

The report author would like to thank the following persons for their assistance in the research of this history: Claudia Rocca, Licensing Project Manager at Public Service Electric & Gas (PSE&G); Sean Redmond, Engineer with PSE&G; Anne Dontzin, Information Resources Manager of Library Services also with PSE&G; and Maureen Wakeman, of the Cranford Historical Society.

2.0 HISTORIC OVERVIEW

The Cranford Substation, or Cranford Trolley Power House as it was also called, is a 1-story, flat-roofed concrete block, brick, and steel structure, Neo-Classical in style, nearly 5 times as long as it is wide. Both stylistically and structurally, the building is divided into 10 bays with an arched, multi-paned, double-height window gracing each bay. While the building is constructed of concrete block (circa 1903) and brick (circa 1910), the surface of the front (South Avenue), and side facades are stuccoed and treated as dressed, cut stone. A wide and simple molded concrete cornice wraps around three facades at the rooftop; a cast circular Public Service medallion is situated within the cornice frieze over the (former) front entrance on the South Avenue façade (Plates 3 and 4).

the former main entrance situated within the fifth (from left or west) bay, road. Rear and side entrances, with plain steel doors, now allow access.

Phase 1 ID: 120

The interior of the substation features a 2-story substation space, separated from a 2-story condenser room and 2-story former engine (now storage) room. The steel, brick, and concrete construction of the building are fully evident as there is no interior wall cladding; neither is there ceiling material to obscure the steel truss supported, concrete plank roof (Plates 5 through 9). A full basement is located under the entire building (Plates 10 and 11).

The Cranford Trolley Power House was constructed in 1903. Thirteen years previously, the first electric trolleys or streetcars in New Jersey were rolling through the streets of Newark. Numerous railway, or 'traction' companies, sprang up to build and operate the lines that could service the Newark metropolitan area and its surrounding suburbs. In 1893, the Township of Cranford granted permission and the authority to the Elizabeth and Plainfield Railway Company to install tracks and operate an electric trolley through the township; however this permission was revoked one year later (Cranford Chronicle 12/19/1894).

After township committee debate in early 1898 regarding the widening of North Avenue for the purpose of laying trolley tracks (which was not popular with the citizens either), it was decided later that year to grant the Westfield-Elizabeth Street Railway Company a franchise to instead use South Avenue for the same purpose for 90 years (Cranford Chronicle 4/23/1898, 9/24/1898). The electric trolleys finally started running through Cranford on December 1, 1899 (Cranford Chronicle 12/5/1899). The Westfield-Elizabeth Street Railway Company did not operate the line for long; in November of 1900 the trolley lines in Elizabeth, Rahway, and Plainfield, with the exception of the North Jersey Street Railway Company's system in Elizabeth, were consolidated. The merged company took the name of the Elizabeth, Plainfield and Central Jersey Railway (NY Times 11/28/1900).

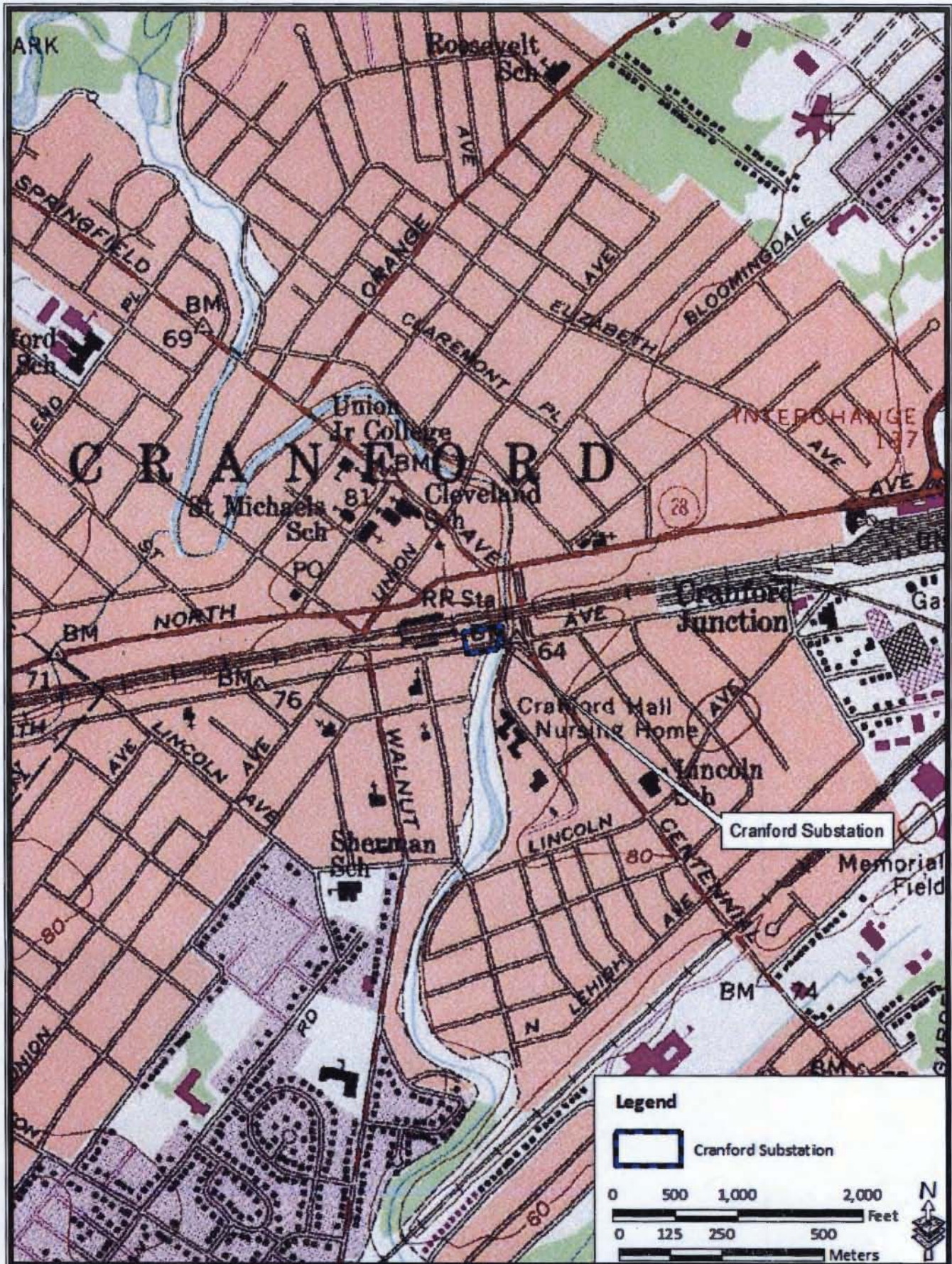


Figure 1: Location of Cranford Substation.

Source: USGS 1995 Roselle, NJ 7.5 Minute Series Topographic Quadrangle



Figure 2: Location of Cranford Substation.

Source: <http://njwebmap.state.nj.us/njimager> 2012

At that point, the Plainfield Electric Light Company was furnishing power for only a portion of the line, on which the trolley cars depended (Street Railway Journal 1902). However, after consolidating and acquiring the lines throughout the region, including Cranford, and not content to rely on an outside supplier, it was reported in August 1901, that the Elizabeth, Plainfield and Central Jersey Railway was planning to build a power house on South Avenue to supply power for the entire line (Cranford Chronicle 8/30/1901) (Street Railway Journal 1902). Nine months later, this proposed power house was described as "one of the largest in the state . . . for the operation of cars between this place [Cranford] and, Perth Amboy, Sewaren, Carteret, Rahway, Elizabethport, Elizabeth and Plainfield" (Cranford Chronicle 5/9/1902).

The citizens' excitement over the construction of the power house was palpable as reported by the *Cranford Chronicle* on September 12, 1902 on the occasion of the building's dedication and cornerstone laying:

"[the building will supply] 6000 horse power to be accomplished by four engines and generators and 12 boilers whose max capacity is 55 hp each. It will be largest railway power plant in NJ outside of Newark. The building will be erected by Berlin Construction Co. of Berlin Conn., machinery installed by the Elizabeth, Plainfield and Central Jersey Railway Co. under the direction of Mr. McKee, and it will run all the lines belonging to the company in Elizabeth, Plainfield, and Rahway to Boynton Beach. Engines are being built in Quincy Ill, after a new design by the Quincy Engine Works and will follow the most successful marine practice, with slight changes to adapt them for electrical work. Generators are built after the latest designs by Westinghouse Electric Company of Pittsburgh. All machinery is designed for heavy work. The building is entirely composed of concrete (no brick or wood), completely fireproof, presenting an attractive grey exterior closely resembling stone. Completed it will be 127 by 75 by 45 feet high, floor space of 125 by 32 by 35 high for the engine room, and 125 by 41 by 25 high for the boiler room. The engine room has a traveling crane of 15 ton capacity. No cost was spared. [The company] wanted plant perfect in every way. Estimated cost of \$200,000 and estimated date of completion is January 1, 1903."

The 1903 Sanborn Insurance map of Cranford shows the Cranford Substation, or the Elizabeth, Plainfield and Central Jersey Railway Power House, property as it was proposed and/or partially completed (Figure 3). It depicts on the property several buildings including the main, concrete-block power house itself, two stories tall facing South Avenue housing the 'engine and dynamo room', the one-story rear room housing the boilers. A 150-foot tall brick chimney is situated behind the power house, a water tower for the Central Railroad of New Jersey (CNJ), a one-story frame electric pump house (for drawing water from the river to cool the power house engines) and a double hydrant are structures located close to the Rahway River, while a frame coal trestle veers off from the main CNJ railroad line to deposit coal behind the power house. Two small, one-story frame structures (likely temporary and construction-related) are situated between the power house and South Avenue. Four other, unidentified structures - three frame structures and a two-room, brick structure are also depicted near the power house but are not associated with the property.

A 1903 bird's-eye-view rendering of Cranford depicts the Cranford trolley power house and most of the buildings described above in charming detail: the arched windows and rusticated 'stone' facade of the power house, as well as the brick smokestack and railroad water tower (Figures 4 and 5).

Whether the power house was finished by January 1, 1903 as was estimated at the time of its dedication is not known, however, the *Cranford Chronicle* reported that trolleys were powered by electricity supplied by the power house for the first time on April 29, 1903 (Cranford Chronicle 5/1/1903).

Not much longer after this inaugural event, the Public Service Corporation of New Jersey (Public Service) was incorporated on May 6, 1903, and in the words of Arthur McCarter, the Corporation's first president: ". . . to acquire and take over these various properties [State gas, electric and railway entities] by lease or otherwise, to resuscitate the dilapidated railways, to build up the electric properties, and to provide the necessary additional capital for the gas companies." Public Service eventually became the holding company for Public Service Railway Company, (a consolidation of the larger street railways, and formed in 1907), Public Gas Company, (formed in 1909 to operate the gas properties), and the Public Service Electric Company, (formed in 1910 to operate the electric companies). In 1924, Public Service Gas Company and Public Service Electric Company, together with United Electric Company of New Jersey, merged to form Public Service Electric and Gas Company or PSE&G (Public Service Corporation of New Jersey 1943).

In 1903 there were 12 operating railway companies (comprised of 96 original companies) in what eventually became Public Service operating territory. Public Service bought, leased or controlled these subsidiary companies between 1903 and 1907, one of these being the Elizabeth, Plainfield and Central Jersey Railway Company, the builder of the Cranford trolley power house (Eid 2007). As reported in the Cranford Chronicle in July 1903, "Public Service has for some time owned the trolley line".

The construction of the trolley power house not only attracted the excitement of the citizens of Cranford and the editors at the Chronicle. In March of 1904, The Engineering Record and The Street Railway Journal ran quite thorough, nearly identical, articles that described in excellent detail the construction of the building, the equipment employed, the contractors hired, and the operations of the facility (Appendices A, B). Of particular note, the articles feature interior and exterior photographs, plans and cross-sections of the building (see Appendix B).

The building apparently became a point of local pride as evidenced by its inclusion in a 1904 promotional publication highlighting Cranford in 1904. In its photograph, the building appears a bright white or light grey, simply yet elegantly styled upon a plain grassy plot with the Rahway River in the foreground (Figure 6).

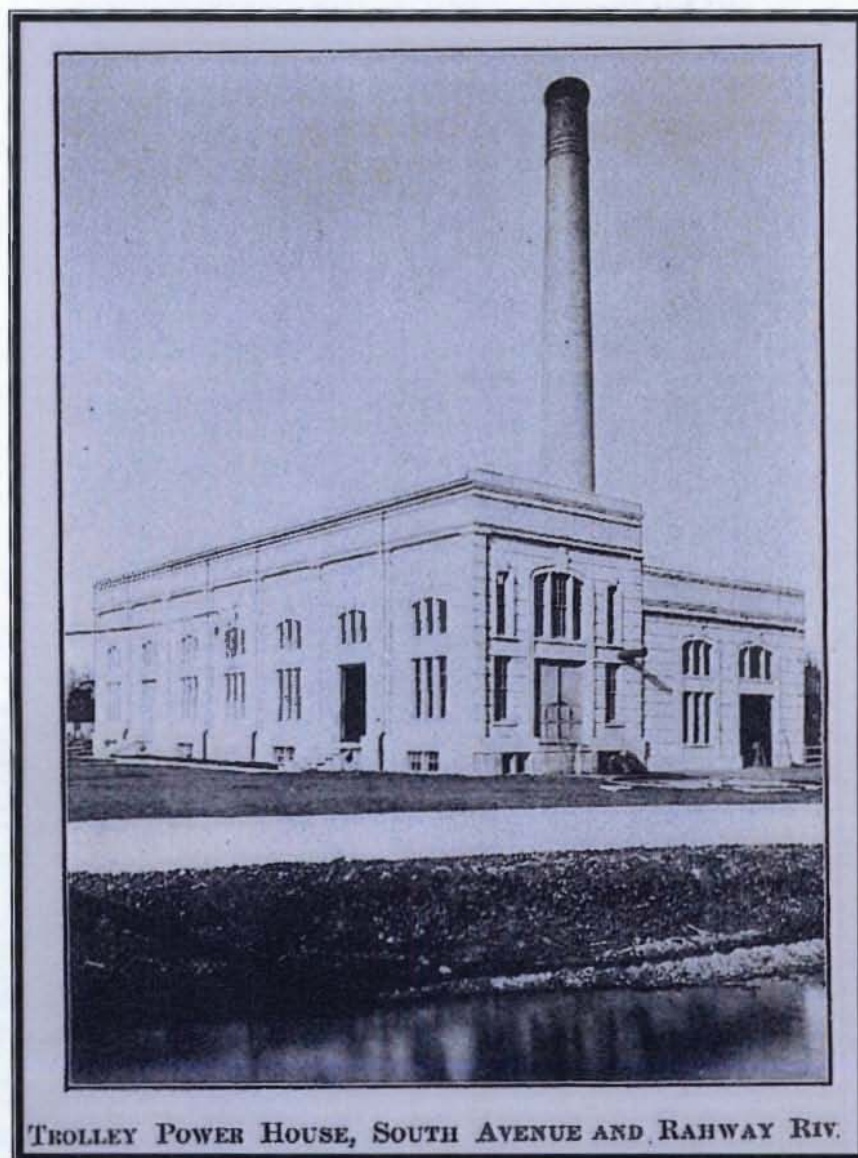


Figure 6: Cranford Trolley Power House (Cranford Substation) in 1904.

Source: Hendrickson 1904

As previously mentioned, between the years of 1903 and 1907, Public Service acquired control of numerous New Jersey railway companies. In August 1907, Public Service Railway Company (which was in turn controlled by Public Service Corporation through ownership of a 99% majority of stock) was incorporated, and through the efficient control of one operating company, allowed connections between Paterson, Passaic, Newark, Jersey City, Bayonne, Hoboken, Hackensack, Elizabeth the Oranges, Westfield, Plainfield, New Brunswick, Rahway and Camden (Nicholas 1912). That same year, in order to more efficeintly manage this large territory, three divisions were created: the Northern, Southern, and Central Divisions (Figure 7). The Central Division, of which the Cranford trolley power house was part, boasted a total of 163 miles of track (Eid 2007).

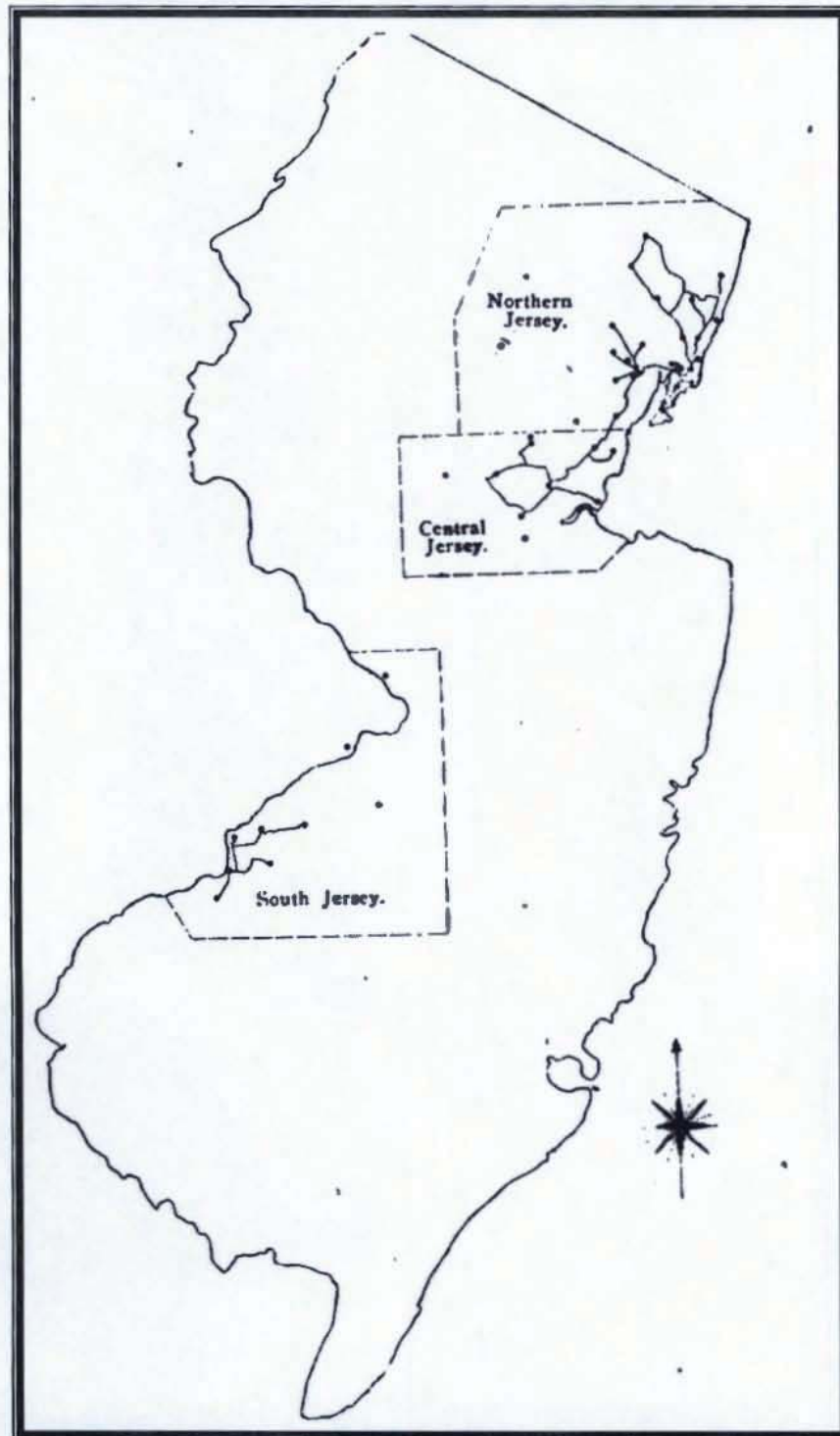


Figure 7: Map of Public Service Divisions in 1906.

Source: Holman 1906

Distinct from its sister company, the Public Service Railroad Company, Public Service Railway Company operated local and suburban streetcar lines, while Public Service Railroad Company was the state's first inter-urban trolley operation (Hamm 1991). A 1908 map of trolley lines in Northern New Jersey depicts the myriad of interconnecting routes through the region, allowing relatively quick travel between the more urban areas in Essex and Union Counties and the outlying suburban, even semi-rural areas of Somerset and Morris Counties (Figure 8). The Cranford trolley power house was located on the Main Line and supplied the power for the entire line. The Main Line No. 165, later renamed and re-numbered in 1916 to the Union Car Line No. 49, was one of the most important and heavily patronized routes in the Central Division, and the last Public Service line to remain in operation in the County (Hamm, 1991). According to a 1908 timetable, one could travel for 30 cents from one end of the line to the other in 2 hours and 40 minutes over a route that was almost 28 miles long (Bullinger 1908) (Figure 9).

Very soon after its construction, the trolley power house took on added importance to Public Service when in 1906 'new machinery' was installed for producing electric light current to supplement what was then brought from Elizabeth (Cranford Chronicle 8/2/1906). It's not known exactly what that new machinery was but improvements continued on the property through the next decade. A 1908 map indicates that the building was indeed a Public Service 'power house' (Sanborn 1908) (Figure 10). A major alteration took place in 1909-1910 when the original 7-bay wide, concrete block enlarged to the west with another three bays (stylistically identical to the original, but constructed in brick) to accommodate two additional transformers (Cranford Chronicle 11/4/1909). A year later Public Service built a dam (demolished sometime before 1929) on the adjacent Rahway River north of the South Avenue bridge to ensure a more reliable source of water for the power house's electric pump house (Cranford Chronicle 2/2/1911). By 1915, the 1903 electric pump house near the river was replaced by a brick, two-room pump room with water purifier connected to the eastern end of the main power house, a brick machine shop was built near the River along with two additional frame store houses, and a second 150-foot tall chimney was located behind the power house (Sanborn 1915) (Figure 11). Conditions remained relatively the same through 1922 (Sanborn 1922) (Figure 12).

In an ever continuing quest for efficiency, Public Service company policy stated (speaking, in part, about the company's previous decade) in 1916 that "the first step toward economy was the rebuilding of some of the generating stations, usually replacing belt-driven generators and old boilers with direct-connected generators and modern boilers. With the advent of the steam turbine and large generating units, it was possible to centralize large amounts of generating capacity under one roof, and it became necessary to raise the transmission potential generally throughout the system" (Conniff 1978). To that end, an additional capacity of 4,000 kilowatts were added to the trolley power house at Cranford making it a combined light, power, and railway generating station. From its first day of operation, the power house was manually operated, 24 hours a day (Conniff 1978).

Almost from the first days of its operation, the Main (Union) Line (as well as all the other electric trolley lines in New Jersey) operated in competition with gasoline-powered jitney buses. As early as 1916, Public Service themselves reported that jitney buses were taking passengers from street railway companies due to lower fares and convenience of location (1916 annual report). This trend continued through the 1920s and 30s, compounded by pressure from individually, consumer- owned automobiles (Mitchell, n.d.). Public Service attempted to counter this competition with a hybrid vehicle of their own – the 'all-service' vehicle. Public Service's 1934 Annual Report announced that their own engineers designed this new type of transportation that could operate from existing overhead lines or under its own gasoline power. They touted that the vehicle's "quick pick-up, curb loading, silent operation and cheap electric power" would soon give the competition a run for its money.

Electric trolley service was discontinued on September 14, 1935 when Public Service substituted the all-service vehicles for all of their rail cars on the Union (old Main) Line (Hamm 1991). Passenger revenue increased on the Union Line and all-service vehicle (or strictly gas-driven vehicle) substitutions continued on other Northern New Jersey lines through the 1930s (Public Service, 1935, 1936, 1937). However, the consumer-owned automobile continued to gain popularity over mass transit through the 1940s and 50s (Mitchell, n.d.).

The gradual and eventual abandonment of the electric lines in favor of fully gas-powered vehicles shifted the operating emphasis at the Cranford power house from transportation to its other employment as distributing electrical current for power and lighting. During the late 1920s through the late 1940s, more improvements and alterations took place at the building. A 1942 Public Service blueprint, one of several drawn to illustrate previous alterations as well as improvements planned for that year, depicts a steady campaign of mostly interior modifications (see Appendix C):

- 1923: alterations were performed but not depicted or described on this particular sheet
- 1928: an interior partition constructed between the fourth and fifth bay
- 1929: a 72-foot long portion of the rear boiler wing was demolished for the installation of outside electrical transformers
- 1930: first floor was altered and reinforced, an interior gallery removed, and a set of stairs to the basement relocated, all primarily in the first five bays
- 1942: first floor slab reinforcing was performed in the third and fourth bays, apparently in preparation for the installation of new condenser units

Additionally, an aerial photograph and a hand-drawn bird's-eye-view image, both from 1929, show that the 150-foot tall chimney is no longer extant suggesting that the power house was no longer burning coal to generate electricity for the electric lines (Anonymous 1929; Hughes and Cinquin 1929) (Figures 13 and 14). The installation of outside transformers that same year (as seen in the 1942 plans) appears to support this (Figure 15; see Appendix C).

Public Service announced in 1947 that "operation of equipment used to supply the demands of Public Service Coordinated Transport [successor to Public Service Railway] was shut down at several substations because of the abandonment of the all-service vehicles. This permitted the retirement from service of equipment used for converting alternating current to direct. The Company is continuing its policy of changing substations over from manual to automatic operation for the purpose of saving manpower." (Public Service Electric Dept. 1947). One of the affected substations was likely the power house in Cranford, allowing its operation to focus fully on distributing power to customers, if it hadn't been doing so already (Sean Redmond conversation February 2015). The following year, major capacity additions in the form of additional transformer units were made at five substations including Cranford (Public Service Electric Dept. 1948).



The last significant physical change to take place was the demolition of the remainder of the old boiler wing, to the rear of the main substation, sometime between 1957 and 1966, according to historic aerial photos (NETR, 2015) (Figures 16 and 17). When first constructed in 1903, the Cranford Trolley Power House measured 127 feet long by 73 feet wide. The 1910 construction enlarged the power house/substation to approximately 181 feet long. The combined 1929 and circa late 1950s demolitions reduced the building's width to 42 feet.

The Cranford Substation continues to supply electrical power to Cranford and the surrounding area to both commercial and residential customers. Electricity produced in generation stations is transmitted first to the substation as transmission voltage¹. In the case of Cranford Substation, voltage transmitted from the generation stations is 26 kV. At the substation it is stepped-down to distribution voltage, which since 1916 at Cranford Substation has been 4 kV, and sent out over the distribution wires on poles or through conduits to transformers on poles when it is once more stepped down for use by the customer or group of customers. In addition to transforming current from high to lower voltages, substations may also convert alternating current to direct current for use by industrial customers whose apparatus calls for this form of current (Public Service Corporation of New Jersey, 1924). Distribution lines in the substations are equipped with regulators to maintain a constant voltage, the current measured, its voltage and amperage recorded, and relays to prevent any interruptions to one circuit being communicated to another, and storage batteries to furnish an independent source of power to operate switches (Public Service Corporation of New Jersey, 1924).

Currently, the Cranford Substation has four 26kV feeds from other substations/switching stations, which is then converted into a 4kV voltage using three transformers and is then fed to customers via 4kV feeder rows. This statement is inclusive of the station as a whole (Sean Redmond, 2015).

The building currently houses 4kV bus work (essentially a solid form of cable), station light and power transformers (transformers that are used to power lower voltage items within the substation), other miscellaneous 4kV equipment, and the 26kV relay equipment. It is not a manually operated substation any longer, but is monitored remotely and inspected periodically (Sean Redmond, 2015).

¹ The voltage of electricity is the measure of its pressure and is expressed in volts. Amperage is the measure of its rate of flow and is measured in amperes. The product of the two gives a measure of apparent power expressed in volt-amperes, or kilovolt-amperes (kV) which are 1000 volt-amperes. A watt is the unit of real power and is the product of volts, amperes, and a factor called power factor (Public Service Corporation of New Jersey, 1924).

Inventory of Identified Architectural Resources			
Property	Property Information	Description	NRHP Eligibility
 <p>Photograph Number: 108</p>	<p>Panamerican Resource Number: 108</p> <p>Cranford Chamber of Commerce</p> <p>4 Centennial Avenue</p> <p>Municipality: Cranford, Union County</p> <p>UTM coordinates (1983 Zone 18N): 4500762N, 559155E</p> <p>Date: ca. 1890</p> <p>Other: Building sits right on the Rahway River.</p>	<p>Style: Second Empire</p> <p>Stories: 3</p> <p>Plan: rectangular</p> <p>Roof: Mansard</p> <p>Exterior: vinyl siding</p> <p>Foundation: obscured</p> <p>Windows: replacement vinyl with decorative shutters</p> <p>Details: first and second story contain full width porches; this house has been so altered that it has lost all former decorative elements.</p>	<p>The house has been so altered that it has lost its integrity. Without integrity, it cannot be eligible for the NRHP. It is recommended as not eligible for the NRHP.</p>
 <p>Photograph Number: 109</p>	<p>Panamerican Resource Number: 109</p> <p>Krauszers</p> <p>Centennial Avenue at South Avenue</p> <p>Municipality: Cranford, Union County</p> <p>UTM coordinates (1983 Zone 18N): 4500746N, 559152E</p> <p>Date: ca. 1960</p> <p>Other: Building sits right on the Rahway River.</p>	<p>Style: store front</p> <p>Stories: 1</p> <p>Plan: rectangular</p> <p>Roof: gable with shed roof entry</p> <p>Exterior: concrete block, Drivitt and brick veneer</p> <p>Foundation: concrete</p> <p>Windows: large store front windows removed and covered with Drivitt</p> <p>Details: Convenience store has a large front parking lot which overlooks South Avenue and the entry to Lincoln Park.</p>	<p>This is a typical store front building from the 1960s and does not meet the NRHP eligibility criteria. Panamerican recommends this building as not eligible for the NRHP.</p>

New Jersey Department of Environmental Protection
Historic Preservation Office

SURVEY RESOURCE NO. 1

Page 1 of 2

BASE SURVEY FORM**Historic Sites #:** _____**Property Name:** E.N. Perrin House**Street Address:** Street #: 113 117 Apartment #: _____
(Low) (High) (Low) (High)**Prefix:** W **Street Name:** South **Suffix:** _____ **Type:** AVE**County(s):** UNION **Zip Code:** 07016**Municipality(s):** Cranford Township**Local Place Name(s):** _____ **Block(s):** 402 **Lot(s):** 19-20**Ownership:** Private **USGS Quad:** Roselle**Description:**

The site is located between Elise Road to the west; a residential property to the south; W. South Avenue to the north; and a commercial property to the east. The bulk of the project site consists of a fairly level lot that is partially overgrown, and until recently was the site of a former residential building; the primary resource, however, is an early 20th-century former residence, now an office, that stands prominently at the northwestern corner of the lot. There is a non-historic garage to the south of the residence.

**Registration
and Status****National Historic Landmark:** _____ **SHPO Opinion:** _____**Dates:** **National Register:** _____ **Legal Designation:** _____**New Jersey Register:** _____ **Other Designation:** _____**Determination of Eligibility:** _____ **Other Designation Date:** _____**Photograph:****Survey Name:** Cranford Valley National Bank**Date:** 7/25/2003**Surveyor:** Douglas Scott**Organization:** Hunter Research, Inc.

New Jersey Department of Environmental Protection
Historic Preservation Office

Page 2 of 2

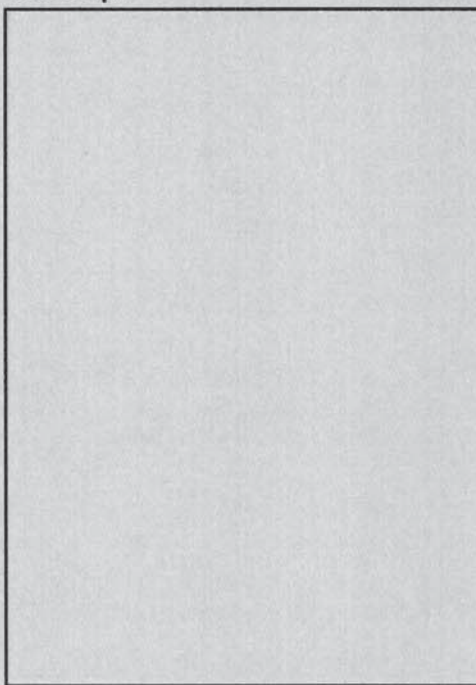
BASE SURVEY FORM

Historic Sites #:

Location Map:



Site Map:



Bibliography/Sources:

Bauer. Map of Cranford, Union County, New Jersey. 1906.

Additional Information:

More Research Needed? ☐ Yes ☒ No

INTENSIVE-LEVEL USE ONLY:

Attachments Included: 1 Building/Element 0 Landscape Farm
0 Bridge 0 Industry

Historic District: ☐ Yes ☒ No Historic District Name: _____

Status: Key Contributing Contributing Non Contributing

Associated Archaeological Site/Deposits? ☒ Yes ☐ No

(Known or potential sites - If yes please describe briefly)

Survey Name: Cranford Valley National Bank
Surveyor: Douglas Scott
Organization: Hunter Research, Inc.

Date: 7/25/2003

New Jersey Department of Environmental Protection
Historic Preservation Office

SURVEY RESOURCE NO. 1

Page 1 of 1

BUILDING/ELEMENT ATTACHMENT☒ BUILDING ☐ STRUCTURE ☐ OBJECT

Historic Sites #:

Common Name: E.N. Perrin House

Historic Name: _____

Present Use: Residential, permanentHistoric Use: Residential, permanentConstruction Date: circa 1915Source: Sanborn Fire Insurance Company. Map of Cranford, New Jersey. 1915

Alteration Date(s): _____

Source: _____

Designer: _____

Physical Condition: Good

Builder: _____

Remaining Historic Fabric: MediumStyle: None VernacularForm (B): Four SquareStories (B): 2.5

Type (B,S): _____

Bays(B): 4Roof Finish Materials (B,S): Asphalt ShingleExterior Finish Materials: Asbestos Siding**Exterior Description:**

The resource consists of a two-and a half story, four-bay frame block with an asbestos tile exterior, topped by a pyramidal hipped roof with three front-facing closed gable dormers. There is a pent roof above the first floor, which formerly sheltered the main entryway door, which has since been moved to the western façade. There is a projecting square bay, two stories high, on the east wall, and a single story enclosed addition on the rear. The primary façade of the building, one containing the main entryway, is now faced in brick, with two large picture windows. Windows throughout the remainder of the building consist of single and paired 1:1 double-hung replacement units. The majority of the building rests atop a concrete foundation

Interior Description:

Access to the interior was not available.

Setting:

Located at the eastern corner of W. South Avenue and Elise Street is a roughly rectangular lot containing a single historic architectural resource with a large parking area to the north, east and south of the building, and a grass area to the west. There is a modern garage to the rear of the residence at the head of the driveway; beyond this to the south is a overgrown, wooded lot. To the east of this property is an overgrown lot where a building stood until recently; beyond this lot to the east is a series of modern offices. Across W. South Avenue is a garage; to the west across Elise Street is an empty lot.

Survey Name: Cranford Valley National BankDate: 6.16/2004Surveyor: Douglas ScottOrganization: Hunter Research, Inc.

New Jersey Department of Environmental Protection
Historic Preservation Office

SURVEY RESOURCE NO. 1

Page 1 of 1

ELIGIBILITY WORKSHEET

Historic Sites #:

History:

No buildings appear on this property on the Hughes map of 1868. According to the map, the property lay outside of the center of town at this point. The lines of the Central Railroad of New Jersey are shown to the north of the property. By 1906 the Bauer Map depicts a modern streetscape bounding the property had been established and the project area divided into four lots. Located at the corner of Elise St. and South Avenue is a building attributed to E.N. Perrin and numbered 117. To the east of this lot is an empty triangular lot. Further east still is a house labeled 113. To the east of 113 is another empty lot.

The same two buildings appear on the Sanborn map of Cranford made in 1949. The four lots were reduced to two lots giving each house a larger lot. Outbuildings had been built to the rear of each of the houses. The houses appear to be similar to those on the neighboring streets as depicted on the map.

Statement of Significance:

This building is an unremarkable four square residence, typical of late 19th/early 20th-century residences found throughout this part of Union County. It has been altered through the application of modern sheathing materials and is not known to have been associated with any individuals or events significant to the history of Union County or Cranford Township.

Eligibility for New Jersey
and National Registers:

☐ Yes ☒ No

National

Register Criteria: ☐ A ☐ B ☐ C ☐ D

Level of Significance: ☐ Local ☐ State ☐ National

Justification of Eligibility/Ineligibility:

This historic architectural property lacks sufficient integrity, historical importance and architectural merit to be considered individually eligible for listing on the New Jersey or National Register of Historic Places.

For Historic Districts Only:

Property Count: Key Contributing: _____ Contributing: _____ Non Contributing: _____

For Individual Properties Only:

List the completed Attachments related to the property's significance:

E.N. Perrin House Building/Element Attachment

E.N. Perrin House Eligibility Attachment

Narrative Boundary Description:

This residential building occupies lot 19-20 of tax block 402. The parcel is bordered by W. South Avenue to the north; Elise Road to the west; lot 21 of block 402 to the east; and lot 18 of block 402 to the south.

Survey Name: Cranford Valley National BankDate: 6/16/2004Surveyor: Douglas ScottOrganization: Hunter Research, Inc.

PHASE IA CULTURAL RESOURCES SURVEY: VALLEY NATIONAL BANK - TOWNSHIP OF CRANFORD





Plate 4. View of 117 West South Avenue, facing southeast. This former residence, now and office, consists of a heavily altered, early 20th-century four-square type building. There is a modern garage to the rear of the resource (Photographer: Douglas Scott, 6/04) [HRI Neg. # 04033/D2:06].

PHASE 1A CULTURAL RESOURCES SURVEY: VALLEY NATIONAL BANK - TOWNSHIP OF CRANFORD



Plate 5. This view depicts 117 West South Avenue as seen from the southeast from the adjoining property (Photographer: Douglas Scott, 6/04) [HRI Neg. # 04033/D2:19].

Inventory of Identified Architectural Resources

Property	Property Information	Description	NRHP Eligibility
 <p>Photograph Number: 122</p>	<p>Panamerican Resource Number: 122</p> <p>Girl Scout Park</p> <p>Springfield Avenue</p> <p>Municipality: Cranford, Union County</p> <p>UTM coordinates (1983 Zone 18N): 4501252N, 558604E</p> <p>Date: unknown</p> <p>Other: to the west of Girl Scout Park are Cranford Clay Tennis Courts, a Cranford Municipal Park</p>	<p>Style: informal, vest pocket park</p> <p>Stories: na</p> <p>Plan: open, small area with scattered picnic tables and benches</p> <p>Roof: na</p> <p>Exterior: na</p> <p>Foundation: na</p> <p>Windows: na</p> <p>Details:</p>	<p>Girl Scout Park is eligible for the NRHP as a contributing element to both the NRE North Cranford HD (ID #3838) and NRE Rahway River Parkway HD (ID #4079).</p>
 <p>Photograph Number: 123</p>	<p>Panamerican Resource Number: 123</p> <p>McConnell Park</p> <p>Eastman Avenue at Central Avenue</p> <p>Municipality: Cranford, Union County</p> <p>UTM coordinates (1983 Zone 18N): 4500961N, 558376E</p> <p>Date: ca. 1919</p> <p>Other: McConnell Park (ID #4424.025) is part of NRE Rahway River Parkway HD.</p>	<p>Style: informal, small park</p> <p>Stories: na</p> <p>Plan: open area, no paths</p> <p>Roof: na</p> <p>Exterior: na</p> <p>Foundation: na</p> <p>Windows: na</p> <p>Details: This park is located in a U-shaped bend in the Rahway River at the end of Central Avenue.</p>	<p>McConnell Park is eligible for the NRHP as a contributing element to the NRE North Cranford HD (ID #3838), NRE Rahway River Parkway HD (ID #4079), and NRE Union County Park System HD (ID #4424).</p>

NEW JERSEY HISTORIC BRIDGE SURVEY

STRUCTURE #: 2003072 COUNTY: UNION OWNER: COUNTY ROUTE: 9020

MILEPOINT: 000000

TOWNSHIP: CRANFORD TOWNSHIP

FACILITY CARRIED: SPRINGFIELD AVENUE

NAME/FEATURE INTERSECTED: SPRINGFIELD AVENUE OVER RAHWAY RIVER

TYPE: THRU GIRDER

DESIGN: PARTIALLY ENCASED

MATERIAL: STEEL

SPANS: 001

LENGTH: 000076

WIDTH: 0314

DATE OF CONSTRUCTION: 1914 ALTERATION:

SOURCE: PLAQUE

DESIGNER/PATENT: JACOB L. BAUER, COUNTY ENGINEER

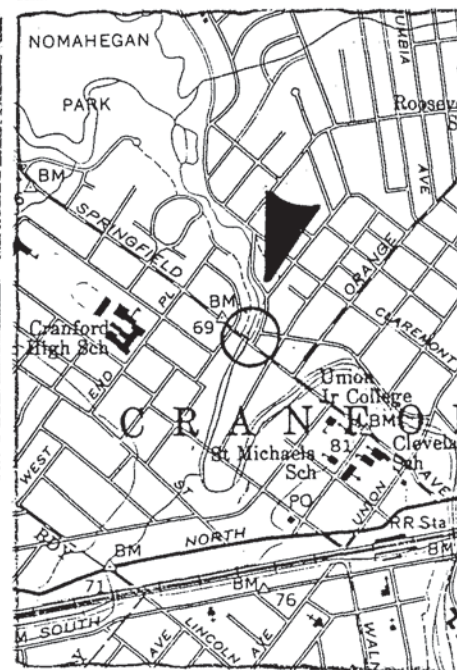
BUILDER: JAS. E. GANO

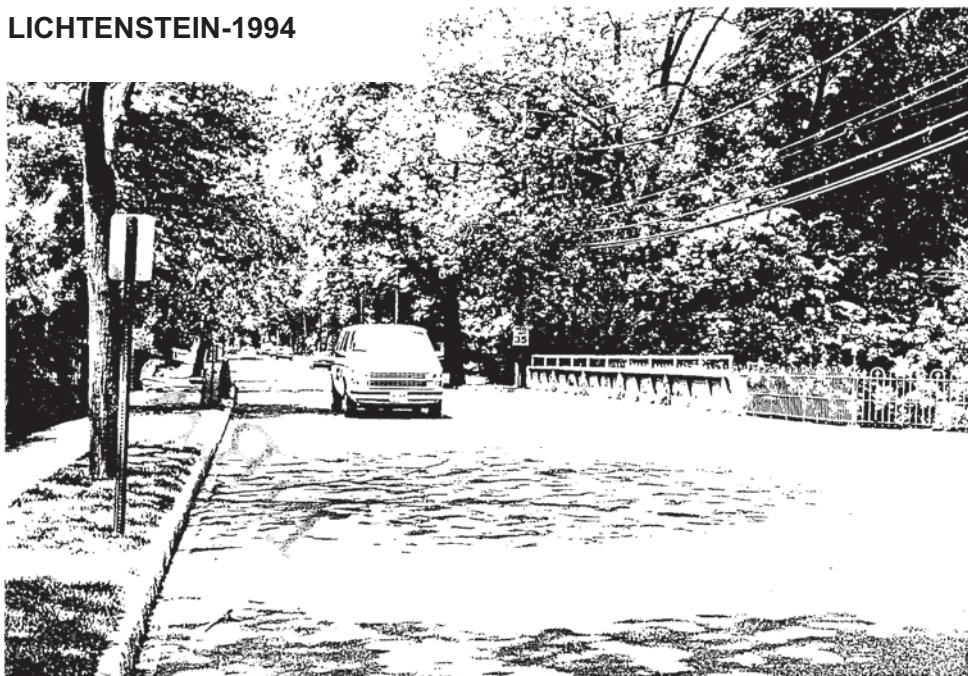
SETTING/CONTEXT: The bridge is located in a potential historic district of predominantly early-20th century picturesque and academic Colonial Revival dwellings. The homes are well-maintained and well-preserved. The bridge carries a two-lane road with two sidewalks over the Rahway River, a winding waterway that has a wooded greenway along both banks through most of Union County.



CURRENT NATIONAL REGISTER STATUS: Not Previously Evaluated**NATIONAL REGISTER RECOMMENDATION:** Eligible

SUMMARY: The thru-girder bridge sits on concrete abutments. The floorbeams are built-up, and encased stringers support the concrete deck. The original iron railings remain along the cantilevered sidewalks, with pipe railings bolted to the top flanges of the girders to protect pedestrians from traffic. The bridge is not technologically or historically distinguished, but is eligible because it contributes to a potential historic district.

PHOTO:159:1-3 REVIEWED BY: EPV/AGL DATE: 05/92 QUAD: Roselle





Inventory of Identified Architectural Resources			
Property	Property Information	Description	NRHP Eligibility
 <p>Photograph Number: 29</p>	<p>Panamerican Resource Number: 29</p> <p>Memorial Park</p> <p>Springfield Avenue at Riverside Drive</p> <p>Municipality: Cranford, Union County</p> <p>UTM coordinates (1983 Zone 18N): 4501392N, 558522E</p> <p>Date: Unknown</p> <p>Other: Borough of Cranford park</p>	<p>Style: vest pocket park</p> <p>Stories: na</p> <p>Plan: formal layout with monuments and landscape furniture</p> <p>Roof: na</p> <p>Exterior: na</p> <p>Foundation: na</p> <p>Windows: na</p> <p>Details: This small park is part of the larger interconnecting Rahway River Parkway HD and the North Cranford HD</p>	<p>Memorial Park is eligible for the NRHP as a contributing element to the NRE North Cranford HD (ID #3838) and NRE Rahway River Parkway HD (ID #4079).</p>
 <p>Photograph Number: 30</p>	<p>Panamerican Resource Number: 30</p> <p>Bridge (NJDOT #2003072)</p> <p>Springfield Avenue at Riverside Drive over Rahway River</p> <p>Municipality: Cranford, Union County</p> <p>UTM coordinates (1983 Zone 18N): 4501401N, 558464E</p> <p>Date: 2003</p> <p>Other: Union County bridge; surveyed since it lies directly in the flood plain</p>	<p>Style: rustic with classical details</p> <p>Stories: na</p> <p>Plan: na</p> <p>Roof: na</p> <p>Exterior: concrete with stone enclosed balustrade topped by decorative metal railing</p> <p>Foundation: concrete abutments on rubble</p> <p>Windows: na</p> <p>Details: This bridge is built on the site of an earlier bridge and at the site of Crane's Ford, for which the municipality is named. Designed by Richard A. Alamo Engineering Co. and erected by Rencor Inc.</p>	<p>Although this bridge is not yet 50 years old, it should be considered a contributing element to both the NRE Rahway River Parkway HD (ID #4079), NRE Union County Park System HD (ID #4424), and NRE North Cranford HD (ID #3838).</p>

NEW JERSEY HISTORIC BRIDGE SURVEY

STRUCTURE #: 2003014 COUNTY: UNION OWNER: COUNTY ROUTE: 9020

MILEPOINT: 000000

TOWNSHIP: CRANFORD TOWNSHIP

FACILITY CARRIED: SPRINGFIELD AVENUE

NAME/FEATURE INTERSECTED: SPRINGFIELD AVENUE OVER RAHWAY RIVER

TYPE: THRU GIRDER

DESIGN: PARTIALLY ENCASED

MATERIAL: STEEL

SPANS: 001 LENGTH: 000086 WIDTH: 0300

DATE OF CONSTRUCTION: 1916 ALTERATION: 1979

SOURCE: PLAQUE

DESIGNER/PATENT: JACOB L. BAUER, COUNTY ENGINEER

BUILDER: LOGAN CONSTRUCTION CO.

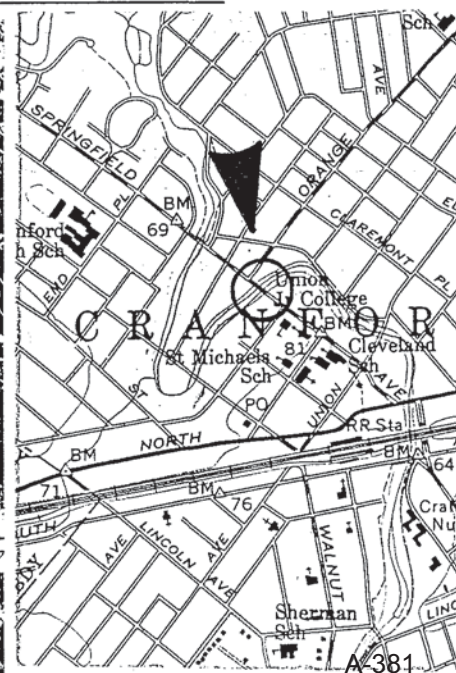
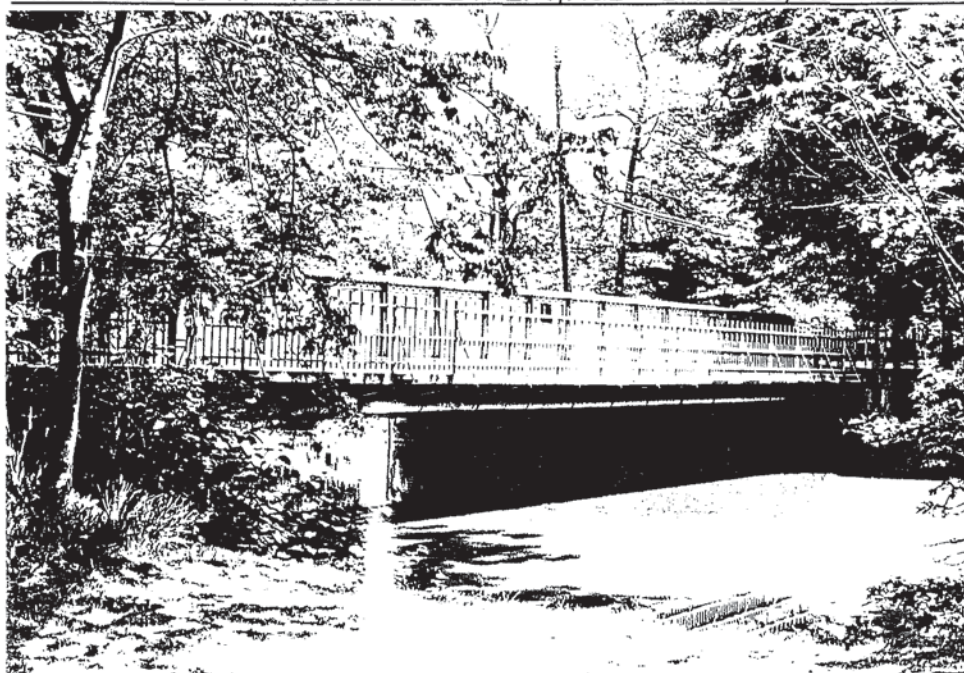
SETTING/CONTEXT: The bridge is located on a main street in Cranford, and it carries two lanes and two sidewalks over the Rahway River. The neighborhood is dominated by well-preserved, architecturally significant early-20th century picturesque and academic Colonial Revival dwellings. The neighborhood has historic district potential. Two other thru-girder bridges contribute to the potential district, and one reinforced concrete arch bridge.

CURRENT NATIONAL REGISTER STATUS: Not Previously Evaluated

NATIONAL REGISTER RECOMMENDATION: Eligible

SUMMARY: The one-span thru-girder bridge is supported by concrete abutments with wingwalls. The rolled floorbeams are encased except for the bottom flanges. The cantilevered sidewalks have reinforced concrete decks and the original iron railings. The bridge deck was replaced in 1979, with a slight crest curve and curbs on the inside face of the girders. Though a representative example of a common bridge type, the bridge is significant as a contributing element to the potential historic district.

PHOTO:159:43-44 REVIEWED BY: EPV/AGL DATE: 05/92 QUAD: Roselle





DRAFT

2003014

Bibliography: Cranford Board of Trade. Cranford, New Jersey. 1913. Central Railroad of New Jersey. "Why Not Own Your Own Home on the Line of the Central Railroad of New Jersey." 1891.

Physical Description: The 1916 one-span built-up thru girder with floor beams bridge is supported on a concrete substructure. The floor beams are rolled, and both sides of the girders have knee braces. The concrete deck dates to 1979. The cantilevered sidewalks are enclosed with the original decorative iron fence-like railings. The girders themselves appear to be unaltered.

Historical and Technological Significance: The 1916 thru girder bridge was designed by County Engineer Jacob L. Bauer. It is a representative example of a popular early-20th century bridge type. The span is historically significant because of its location in a locally identified National Register-eligible historic district made up of well-preserved late-19th and early-20th century neighborhoods developed as part of the suburbanization of the greater New York area. Cranford, a township established in 1871, is located on the Central Railroad of New Jersey (CNJ) main line to its terminal in Jersey City and New York City connections. Because of its location on the rail line, the CNJ and local interests promoted Cranford as an ideal situation for successful New York City businessmen who were desirous of a comfortable home in a country setting within commuting distance to the city. With the absence of industry and the advantage of the scenic Rahway River meandering through the town, Cranford was consciously developed as a residential community geared to wealthy New York professionals. Promoted as "The Venice of New Jersey," the town is characterized by large well-preserved picturesque and academic Colonial Revival homes on generous, casually landscaped lots. The river played a large factor in both the physical development of the town community, and its image, as recreational use of the Rahway has been highlighted throughout the town's postbellum history. The river was treated as an amenity to enhance the residential development of the community. In many places the riverbank is lined with stone walls and other ornamentation, and both streets and houses are oriented toward the most dominant physical feature in the potential historic district.



The North Cranford Historic District is a large residential enclave that retains its pre-1930 character. The well-preserved neighborhood ranks as one of the premier railroad-stimulated suburbs in New Jersey. Architecturally it reflects the architectural preferences of the pre-1929 Depression era, and historically it illustrates the strong influence of the railroad on postbellum and early-20th century development in northern New Jersey. Cranford is located about 17 miles west of New York City, and in 1913, the community was serviced by 93 daily trains. The trip, including the ferry connection, was 45 minutes. The initial stimulus for Cranford becoming a "railroad suburb" came in 1864 when the CNJ opened its first bridge over Newark Bay and a new passenger station in the center of what was a crossroads community known as Cranville. Shortly thereafter farms were converted into subdivisions.

The area was identified as a potential National Register historic district after a community wide review by the Cranford Historical Society in 1989. The formal application for National Register listing has not been completed, but the area clearly meets the criteria for inclusion in the Register. This bridge is one of four contributing spans identified in the potential historic district that has a period of significance from 1838, when train service was introduced to the town, through 1929. This bridge is one of four identified in the potential historic district that has a period of significance from 1838, when train service was introduced to the town, through 1929.

(2003025, 2003008, 2003014, 2003072).

DRAFT

Boundary Description and Justification: The bridge is not individually significant, but it is located within and is completely surrounded by an potential architecturally significant late-19th and early-20th century residential historic district with a period of significance through until 1929. Since the 1916 bridge was built within that period of significance and it is not significantly altered, it is a contributing resource. Therefore, the bridge and its surroundings are significant.

Inventory of Identified Architectural Resources			
Property	Property Information	Description	NRHP Eligibility
 <p>Photograph Number: 84 (north balustrade as seen from the Cranford Canoe Club dock)</p>	<p>Panamerican Resource Number: 84</p> <p>Bridge (NJDOT #2003014) on Springfield Avenue at Orange Street crossing Rahway River</p> <p>Municipality: Cranford, Union County</p> <p>UTM coordinates (1983 Zone 18N): 4501245N, 558662E</p> <p>Date: 2010</p> <p>Other: This bridge was recorded because it lies directly in within the 100' APE. It was designed by CMX; constructed by Marbro, Inc.</p>	<p>Style: Rustic w/classical elements</p> <p>Stories: na</p> <p>Plan: steel stringer</p> <p>Roof: na</p> <p>Exterior: concrete enclosed balustrades with rubble veneer face topped with decorative open work metal railing with a classical feeling</p> <p>Foundation: concrete abutments with rubble veneer on concrete and rock</p> <p>Windows: na</p> <p>Details: This bridge replaced an earlier bridge on this site. It was designed to blend with the adjacent Girl Scout Park and neighborhood.</p>	<p>Although this bridge is not yet 50 years old, it should be considered a contributing element to both the NRE Rahway River Parkway HD (ID #4079) and NRE Union County Park System HD (ID #4424).</p>
 <p>Photograph Number: 85</p>	<p>Panamerican Resource Number: 85</p> <p>16 Orange Street</p> <p>Municipality: Cranford, Union County</p> <p>UTM coordinates (1983 Zone 18N): 4501371N, 558701E</p> <p>Date: ca. 1880</p> <p>Other: This is one of the oldest buildings recorded during this survey.</p>	<p>Style: Stick style with Gothic elements</p> <p>Stories: 2.5</p> <p>Plan: L shape</p> <p>Roof: gable</p> <p>Exterior: wood siding</p> <p>Foundation: obscured</p> <p>Windows: many original wood 2/2 and 4/4 with storm windows</p> <p>Details: House has a deep wrap-around porch. Unfortunately, the house appears to be divided into apartments and it is difficult to find the front entry.</p>	<p>This house is a contributing element to the NRE North Cranford HD (ID #3838; Bary 2012).</p>

NEW JERSEY OFFICE OF HISTORIC PRESERVATION HISTORIC SITES INVENTORY # 200307
INDIVIDUAL STRUCTURE SURVEY FORM

HISTORIC NAME: First Presbyterian Church
LOCATION: 11 Springfield Ave.

COMMON NAME: First Presbyterian Church
BLOCK/LOT 191/3

MUNICIPALITY: Cranford

COUNTY: Union

USGS QUAD: Roselle

UTM REFERENCES:

OWNER/ADDRESS: First Presbyterian Church
11 Springfield Ave., Cranford

Zone/Northing/Easting

DESCRIPTION

Construction Date: 1893

Source of Date: Church history

Architect:

Builder:

Style: Romanesque Revival

Form/Plan Type: Square Plan

Number of Stories: Two

Foundation: Rusticated stone

Exterior Wall Fabric: Natural wooden shingles

Fenestration: Rounded arched windows

Roof/Chimneys: Steep gable roofs.

Additional Architectural Description:

Octagonal dome in center of church.

Four story square tower on corner of building.

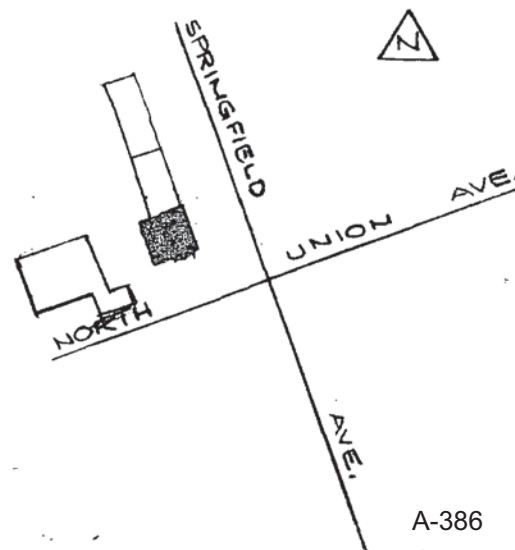
Steep copper roof. Contains clock and bell from first Presbyterian church building (1851).

Large stained glass windows by Tiffany.

Interior is entirely oak.

PHOTO Negative File #Roll 11 frame 4

Map (Indicate North)



A-386

SITING, BOUNDARY DESCRIPTION, AND RELATED STRUCTURES:

The church is located on the N.W. corner of Springfield and Union Aves. The structure sits on a 10' rise approximately 40' from Springfield Ave. A 1968 addition connects the church to a 1950 building to form a long ell. Both portions of the ell are two-story, flat-roofed brick structures.

SURROUNDING ENVIRONMENT: Urban ☐ Suburban ☒ Scattered Buildings ☐
Open Space ☐ Woodland ☐ Residential ☐ Agricultural ☐
Industrial ☐ Downtown Commercial ☒ Highway Commercial ☐ Other ☐

To the east of the church is a park and the Rahway River, to the west is the downtown center of Cranford.

SIGNIFICANCE:

The First Presbyterian Church is the oldest house of worship in Cranford, and it is the oldest congregation in Cranford. The present building is the third Presbyterian church building in Cranford, the second on this site. The building is a good translation of the Romanesque Revival style, so frequently worked in stone, to the shingle siding which was a traditional exterior covering in New Jersey, and was newly popular at the turn-of-the-century with the introduction of the Shingle Style.

The church is noted for its styling in Gateways to Architecture, a book about Union County Architecture by Sandy & Beverly Brown.

ORIGINAL USE: Church
PHYSICAL CONDITION: Excellent ☒ Good ☐ Fair ☐ Poor ☐
REGISTER ELIGIBILITY: Yes ☒ Possible ☐ No ☐ Part of District ☐
THREATS TO SITE: Roads ☐ Development ☐ Zoning ☐ Deterioration ☐
No Threat ☒ Other ☐
COMMENTS:

A beautiful building, very well maintained. The church is quite proud of its structure, and welcomes the public to view it.

REFERENCES:

First Presbyterian Church
Cranford Historical Society

RECORDED BY: JWF
ORGANIZATION: UCCHPAB

DATE: 1981



SPRINGFIELD &
UNION

CRANFORD

N.J.

1st Presbyterian Church

1892

* 200307

11 Springfield & Union
Cranford N.J.
Union Co.

Roll 11/4

Table 1: Historic Properties within the APE and Determination of Eligibility

Address	Description	Distance*/ Direction	Eligibility	Determination of Effect	Photo Plate
Crane-Phillips House, 124 N. Union Avenue, Cranford, New Jersey	c. mid-19th century, 2-story, vernacular Carpenter Gothic dwelling/house museum with gable roof, clapboard siding and brick foundation, full-width flat roofed front porch with simple Carpenter Gothic style trim work and vergeboards at eaves. Shed roofed additions to rear, and flat roofed wing to side.	1000 feet/SE outside of APE	SR (6/3/97) NR (8/14/97)	n/a	B.5.4
First Presbyterian Church, 11 Springfield Avenue, Cranford, New Jersey	1893, 2 story Romanesque Revival church with copper roofing, wooden shingle siding, rusticated stone foundation. Tiffany designed large stained glass windows on primary facade. Four story square tower on corner of building.	300 feet/SE	Eligible NJSHPO Site Inventory # 200307	No Adverse Effect	B.5.6
123 N. Union Avenue, Cranford, New Jersey	1913, 4 story, institutional building, flat roofed, decorative bands of alternating red brick and stone work, limestone sills, replacement windows, c. 1980 modern style portico constructed on all facades.	Proposed site	Not Eligible	n/a	B.5.1
Manger Building, 108 N. Union Avenue, Cranford, New Jersey	c. 1940, 2-story, Colonial Revival, asphalt shingle roofing, red brick, windows have overscaled drip molding and decorative shutters, portico is 2 full stories and has decorative railing above.	1000 feet/SW	Not Eligible	n/a	B.5.7
117-119 N. Union Avenue, Cranford, New Jersey	c. 1945 one-story commercial building, flat roofed with shingled pent roof, permastone wall cladding, large plate glass storefront windows.	1000 feet/SW	Not Eligible	n/a	B.5.9



PHOTO LOCATOR MAP

**Plate B.5.6:**

The First Presbyterian Church at the Corner of North Union Avenue and Springfield Avenue. The Cranford 2 site rises behind the church.

Photo view: Northwest

Photographer: Elaine Kiernan Gold

Date: August 1, 2001

PROPERTY REPORT

Property ID: 1012572599

Property Name: First Presbyterian Church of Cranford
 Address: 11 Springfield AVE

Ownership: Non-profit
 Apartment #: ZIP: 07016

PROPERTY LOCATION(S):

County:	Municipality:	Local Place Name:	USGS Quad:	Block:	Lot:
UNION	Cranford township		Roselle	191	3

Property Photo:



Old HSI Number:

NRIS Number:

HABS/HAER Number:

Description:

The First Presbyterian Church of Cranford is an exuberant example of Shingle Style architecture executed for an ecclesiastical building. All of its elevations are clad in naturally dark shingles and trimmed with white moldings. Strong elements that characterize the style include its Romanesque arched windows, rows of small windows and shingle-clad columns.

Setting:

The First Presbyterian Church is a visible architectural landmark in the heart of Cranford's commercial district with its imposing Shingle style and Richardsonian massing. It is located at the intersection of Springfield and North Union Avenues on the southwest corner facing southeast. It is landscaped with trees and shrubbery and set back from the main thoroughfare with footpath crisscrossing the expansive front lawn.

Registration
and Status
Dates:

National Historic Landmark?: ☐

National Register:

New Jersey Register:

Determination of Eligibility:

Certification of Eligibility:

SHPO Opinion:

Local Designation:

Other Designation:

Other Designation Date:

☒ Eligibility Worksheet included in present survey?☐ Is this Property an identifiable farm or former farm?

Survey Name: Women's Historic Sites Survey

Principal Investigator: Ellen Schultz

Organization: Preservation Partners

Property ID:

☒ (Primary Contact)

1012572599

Page 1

Location map:



Site Map:

(Site Map Not Available)

BIBLIOGRAPHY:

Author:	Title:	Year:	HPO Accession #: (if applicable)
(None Listed)	Clippings file, Cranford Historical Society. Handwritten notes in her hand on her life and accomplishments in or around her 90th year.		
(None Listed)	"She helped women Win Right to Vote" The Daily Journal, January 18, 1968	1968	
(None Listed)	"School Election will Bring Changes in Board Control" Clippings file. Cranford Historical Society	1927	
(None Listed)	Clippings file, Cranford Historical Society. "WV Hails Cranford Women Who Aided Suffrage Fight" by Mrs. Alfred L. Miller	1960	
(None Listed)	Clippings file. Cranford Historical Society. Miscellaneous articles -- "Cranford's Woman of the Month"		
(None Listed)	Historical Notes In Celebration of the 100th Anniversary of the Sanctuary of The First Presbyterian Church of Cranford, June 19, 1894- June 19, 1994	1994	
(None Listed)	Obituary "Elizabeth Bates, longtime civic leader, dies at age 101" 9/18/80. Clippings file Cranford Historical Society	1980	

Additional Information:

More Research Needed? ☐ (checked=Yes)

INTENSIVE-LEVEL USE ONLY:

Attachments Included: 1 Building 0 Bridge
 0 Structure 0 Landscape
 0 Object 0 Industry

Historic District ? ☐

District Name: not applicable

Status:

Survey Name: Women's Historic Sites Survey

Principal Investigator : Ellen Schultz

Organization: Preservation Partners

☒ (Primary Contact)

Property ID:

1012572599

Page 2

al Site/Deposits? ☐
sites. If Yes, please describe briefly)

Conversion Problem? ☐ ConversionNote: Null

Date form completed: 2/1/2002

Survey Name: Women's Historic Sites Survey
ncipal Investigator : Ellen Schultz
Organization: Preservation Partners

☒ (Primary Contact)

1012572599

Property ID: Page 3

BUILDING ATTACHMENT**Common Name:** First Presbyterian Church of Cranford**Historic Name:** First Presbyterian Church of Cranford**Present Use:** Institutional, religious assembly**Historic Use:** Institutional, religious assembly**ConstructionDate:** 1893**Source:** Cornerstone**Construction****Start Date:****Construction****End Date:****Style:** Shingle☐ Vernacular Style?**Form:** Irregular**Physical Condition:** Excellent**Type:** Other**Remaining Historic Fabric:** High**Roof Finish Materials:** Asphalt Shingle**Stories:** 2.5**Exterior Finish Materials:** Wood, Shingles**Bays:** 5**Exterior Description:**

The First Presbyterian Church of Cranford is an exuberant example of Shingle Style architecture executed for an ecclesiastical building. All of its elevations are clad in naturally dark shingles and trimmed with white moldings. Strong elements that characterize the style include its Romanesque arched windows, rows of small windows and shingle-clad columns.

The church's cross-gable form has a broad, center octagonal tower rising above the point at which the gables intersect. Strips of four small arched windows form a clerestory at its base and its steep roof terminates with an octagonal spire. The corners of the octagon are embellished with projecting shingled column-like corners that pierce the roof and are capped with cone-shaped tops.

The main elevation occurs on North Union Avenue. Two towers flank its broad gable-end. The south tower rises three stories, while the north tower soars at twice the height. Placed at the corner of the two public views of the building, this tower is the predominant feature of the church. It contains a clock, the belfry and a steep, hipped, copper-clad roof.

Entrance occurs under a porch supported on shingled columns, also projecting above the roof to terminate with cone-shaped caps. Its peaked roof has a gable end shaped and trimmed to mimic the circular window found behind it on the main façade. Below the circular window, the entrance doors, executed in hard wood, consist of four glazed and paneled doors. The lower sections have four panels each, which are topped with four-light upper sections.

Flanking the entrance, a set of modified Palladian windows, all with Romanesque arches, add a strong element to the ground floor. Above, a projecting cornice divides the floors and marks the beginning of the gable. Within the gable end there is a rose window that has sets of three, arched topped windows placed on either side. Near the peak, a row of five arched topped windows finishes its fenestration.

The main tower has three exposed elevations. On the base its single semi-circular arched window has an upper transom with a pair of casement windows set below. Above this, two rows of four, square, fixed sash are stacked on top of each other. A pent roof marks the ascent to the belfry. This section of the tower has the column-shaped corners that reach up through the roof to be capped with copper clad cone-shaped tops. The stair tower is lit with sets of three elongated, arched topped windows. Above them, the clock faces are centered on each elevation. The belfry has sets of three recessed louvered openings trimmed by painted columns.

The lower tower has an open third floor trimmed with a painted wood balustrade. Its ground floor has the same window found on the ground floor of the north tower, and the second floor has a pair of tall rectangular windows. This tower is also embellished with rounded column-like corners projecting to cone shaped tops.

As the other public elevation, the Springfield Avenue side shares the main tower. Its gable end is flanked by two bays, each with unique fenestration. The nearest bays contain a flat-arched opening with paired casements topped with fixed transoms. The outer bays have the arched opening. Inside these are windows similar to those found in the ground floors of the towers. The bays are separated by the shingle-clad columns, again rising above the roof and topped with cone-shaped caps. The gable end has a set of three huge modified Palladian windows, as they rise the full, two-story height. Each has a Romanesque arch with a circular framed window inserted in the arch. The center window's arch is filled with a rose window.

Interior Description:**Alteration Dates:**

Alteration(s):	Circa Date:	Date Range:	Source:
Physical alteration		to	1951- Education Building; 1956 - Fellowship Hall,

Survey Name: Women's Historic Sites Survey**Property ID:**

Page 4

Principal Investigator : Ellen Schultz☒ (Primary Contact)

1012572599

Organization: Preservation Partners

now honoring George and Elizabeth Bates; 1
Memorial Hall, creating new functional space.
Source: Historical Notes/The First Presbyterian
Church of Cranford

Architect/Designer::

Type:	Name:	Person/Firm Description:
Architect	Charles G. Jones	230 Broadway, New York City
Builder	Diedrich Kreie	Operated a South Avenue, Cranford Carpenter Shop and a Hardware Store

Date form completed: 2/1/2002

Survey Name: Women's Historic Sites Survey

Principal Investigator : Ellen Schultz

Organization: Preservation Partners

☒ (Primary Contact)

Property ID:

1012572599

Page 5

ELIGIBILITY WORKSHEET - Properties

Property ID 1012572599

History:

The First Presbyterian Church is significant because of its association with Cranford civic leader Elizabeth M. Bates. Her mother-in-law "Fannie", was known as the Mother of Cranford, the founder of the Village Improvement Society. Elizabeth married her son, George. She moved to Cranford when she was 9 years old. In 1913, Mrs. George C. Hughes became Cranford's first woman school board trustee and a year later Mrs. Bates was elected to fill her unexpired term. She was one of the first women to be ordained an elder of the First Presbyterian Church, sometime around 1957. She joined the church in 1893, present at the groundbreaking and dedication of the sanctuary and was a member for over 100 years. On her 100th birthday, in 1979, the Fellowship Hall addition to the church was renamed after her and her husband -- Bates Hall. During the 1910s, she became an ardent suffragist and a member of the New Jersey Equal Franchise League of Cranford. In an article from "The Daily Journal," January 18, 1968, Mrs. Bates was said to have recalled a time when she was sitting at the polls when a drunk man showed up to vote "...and he was so drunk he could hardly write his name. This made me angry because it seems so unfair. I couldn't vote and yet I thought I could make a better decision that(sic) he." She was active in promoting the passage of the 19th Amendment giving women the right to vote. Bates recalls walking in two suffrage parades in Newark and New York. Her other civic accomplishments included serving as a member of the Board of Managers of the Cranford Public Library for 20 years, and head of the Village Improvement Association in 1913. She volunteered during World War I at a Navy Hospital, aided veterans after the war, and worked for the Red Cross for which she received a 45 year pin. She was active in the Daughters of the American Revolution, the Women's Christian Temperance Union and carried on her love of birds to programs with the Boy Scouts and Girl Scouts. She credits her husband for encouraging her to become involved in community activities outside their home.

Statement of Significance:

Elizabeth M. Bates was a significant local activist in the Cranford community in areas of health, working for the Red Cross, in education, serving on the School Board, and fighting for woman's suffrage. She was an active member of the First Presbyterian Church since 1893 and was one of the first women ordained as an elder. On the occasion of her 100th birthday, having the new addition renamed in her honor. At the beginning of the 20th century, women's charitable impulse often found expression in the settlement house movement and in social reform through the large number of women's clubs that proliferated from the 1870s to the 1920s. The earlier pattern of working with needy individuals through local charitable organizations continued, but increasingly women sought legislative solutions for urban problems. Women were just beginning to serve on school boards, often before they could vote themselves.

Eligibility for New Jersey and National Registers: ☒ Yes ☐ NoNational Register Criteria: ☐ A ☒ B ☒ C ☐ DLevel of Significance: ☒ Local ☐ State ☐ National

Justification of Eligibility/Ineligibility:

This is the only extant site associated with this locally significant woman, whose activist role typifies the women's growing activism in the early 20th century. The Church is an exuberant example of Shingle Style architecture executed in the ecclesiastical style with Romanesque features. The building has a high degree of integrity.

Total Number of Attachments: 1

List of Element Names: Building

Narrative Boundary Description:

The property is located in Block 191, Lot 3 of the Roselle USGS quadrangle

Date Form Completed: 2/1/2002

Survey Name: Women's Historic Sites Survey

Principal Investigator: Ellen Schultz

Organization: Preservation Partners

Property ID:

☒ (Primary Contact)

1012572599

Page 6

HPO 09-0228
HPO-G2012-143-PROD*State of New Jersey*

MAIL CODE 501-04B

DEPARTMENT OF ENVIRONMENTAL PROTECTION

NATURAL & HISTORIC RESOURCES

HISTORIC PRESERVATION OFFICE

P.O. Box 420

Trenton, NJ 08625-0420

TEL. (609) 984-0176 FAX (609) 984-0578

CHRIS CHRISTIE

Governor

BOB MARTIN

Commissioner

KIM GUADAGNO

Lt. Governor

July 17, 2012

CERTIFICATION of ELIGIBILITY

Stephen W. Price
211 Hampton Street
Cranford, NJ 07016

Dear Mr. Price:

This letter is in response to your request for a formal certification of eligibility for the First Presbyterian Church located at 11 Springfield Avenue in Cranford Township, Union County, New Jersey.

Based on a review of available documentation, it is my opinion that the First Presbyterian Church is individually eligible for listing in the New Jersey and National Registers of Historic Places under Criterion C as an exceptionally fine example of the late 19th Romanesque Revival architectural style. Built in 1893/4 according to plans by architect Charles Granville Jones (b. 1865 – d. 1938), the building exhibits several significant architectural elements including: an octagonal dome; stained glass windows; and a substantially intact 19th century interior. Although substantial additions were made in 1951, 1956 and 1968; they are discretely designed, and largely obscured from the main public view by topography.

If you wish to pursue registration, please contact Charles Scott of my staff at either (609-633-2396) or Charles.Scott@dep.state.nj.us. To help expedite our review and response, if additional consultation with the HPO is needed regarding the nomination of this property, please reference the HPO project number (09-0228) in any future call, emails, or written correspondence.

Thank you for your interest in New Jersey's irreplaceable historic resources. If you have any further questions, please feel free to contact Andrea Tingey of my staff at either 609-984-0539 or Andrea.Tingey@dep.state.nj.us.

Sincerely,

Daniel D. Saunders
Acting Administrator &
Deputy State Historic
Preservation Officer

115 Miln Street, Cranford, New Jersey	c. 1910, two-story, Dutch Colonial Revival-style house with asphalt shingle roofing, aluminum siding, paired windows, enclosed porch, pedimented entry with simple posts,	900 feet/NW	Not Eligible	n/a	B.5.18
211 Miln Street, Cranford, New Jersey	c. 1890, two-and-a-half story, vernacular Victorian style house with cross gable, asphalt shingle roof, aluminum siding, 1/1 windows, brick foundation, vergeboard at gable end of attic window.	300 feet/N	Not Eligible	n/a	B.5.20
215 Miln Street, Cranford, New Jersey	c.1890, two-and-a-half-story, vernacular Queen Anne-style residence, green asphalt shingle roofing, clapboard siding, cloche domed tower, wrap around porch with decorative woodwork.	300 feet/N	Not Eligible	n/a	B.5.22
12 Springfield Avenue, Cranford, New Jersey	c.1940, two-story, Colonial Revival-style residence, asphalt shingle roofing, shed roof dormers with casement windows, shingle siding, 6/6 windows at 2nd story, 6/9 windows at 1st story.	600 feet E	Not Eligible	n/a	B.5.23
First Church of Christ Scientist, 21 Springfield Street, Cranford, New Jersey	1908, Neo-Classical style, two-story, green tile hipped roof, stucco walls and foundation. Five bays with three central bays projecting, pedimented entry with Ionic columns, windows grouped in threes.	700 feet/NE	Eligible NJSHPO Site Inventory #200308	No Adverse Effect	B.5.25
24 Springfield Avenue, Cranford, New Jersey	c. 1940, three-story, Tudor Revival-style apartment building, slate roofing, brick exterior on lower two stories, upper 1/1/2 stories half-timbering. Replacement 1/1 paired windows.	800 feet/NE	not eligible	n/a	B.5.27



* Distances are rounded to the nearest 100 feet.



PHOTO LOCATOR MAP

**Plate B.5.23:**

12 Springfield Avenue.
 Photo view: Northeast
 Photographer: Elaine Kiernan Gold
 Date: August 1, 2001

Inventory of Identified Architectural Resources			
Property	Property Information	Description	NRHP Eligibility
 <p>Photograph Number: 94 (section on Riverside Drive)</p>	<p>Panamerican Resource Number: 94</p> <p>Sperry Park (ID #4424.026)</p> <p>Riverside Drive/North Union Avenue/Springfield Avenue</p> <p>Municipality: Cranford, Union County</p> <p>UTM coordinates (1983 Zone 18N): 4501156N, 558955E</p> <p>Date: ca. 1926</p> <p>Other: Park extends across Union Avenue and takes in the Crane House</p>	<p>Style: informal</p> <p>Stories: na</p> <p>Plan: walking areas with seating</p> <p>Roof: na</p> <p>Exterior: na</p> <p>Foundation: na</p> <p>Windows: na</p> <p>Details: Park is part of the Rahway River Parkway and the Union County Park System. It includes a section on Springfield Avenue with a large memorial to Cranford victims of 9/11.</p>	<p>Sperry Park is eligible as a contributing element to the NRE Rahway River Parkway HD (ID #4079), NRE Union County Park System HD (ID #4424), and NRE North Cranford HD (ID #3838).</p>
 <p>Photograph Number: 95</p>	<p>Panamerican Resource Number: 95</p> <p>Cranford Towers Condos</p> <p>18 Springfield Avenue</p> <p>Municipality: Cranford, Union County</p> <p>UTM coordinates (1983 Zone 18N): 4501110N, 558897E</p> <p>Date: ca. 1930</p> <p>Other: Overlooks the Rahway River and Sperry Park</p>	<p>Style: none</p> <p>Stories: 7</p> <p>Plan: rectangular</p> <p>Roof: flat</p> <p>Exterior: brick</p> <p>Foundation: concrete</p> <p>Windows: replacement metal</p> <p>Details: Although this is a large building in a significant part of town, it lacks any real stylistic element; it is two big, red brick boxes.</p>	<p>This structure is not eligible since it does not meet any of the eligibility criteria.</p>

NEW JERSEY OFFICE OF HISTORIC PRESERVATION HISTORIC SITES INVENTORY # 200308
INDIVIDUAL STRUCTURE SURVEY FORM

HISTORIC NAME: First Church of Christ,
LOCATION: Scientist
21 Springfield Ave.

COMMON NAME:
BLOCK/LOT 188/11

MUNICIPALITY: Cranford
USGS QUAD: Roselle

COUNTY: Union
UTM REFERENCES:

OWNER/ADDRESS: First Church of Christ, Scientist
21 Springfield Ave., Cranford

Zone/Northing/Easting

DESCRIPTION

Construction Date: 1908

Source of Data: Cornerstone

Architect:

Builder:

Style: Neo-Classical style

Form/Plan Type: Rectangular

Number of Stories: One story

Foundation: Stucco covered

Exterior Wall Fabric: Stucco covered

Fenestration: Triple windows of colored glass, Five bay facade.

Roof/Chimneys: Hip roof. Green tile.

Additional Architectural Description:

Pediment over entry with four Ionic columns.

Box cornice runs around building, below level of attic "windows."

Central three bays project.

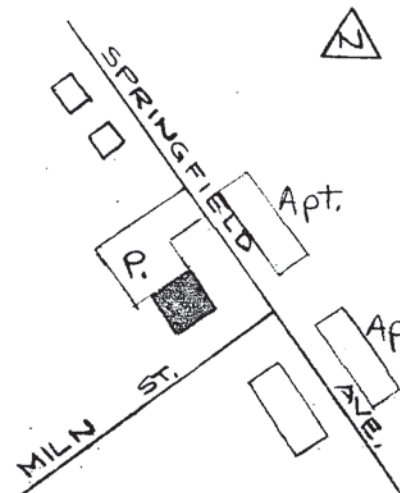
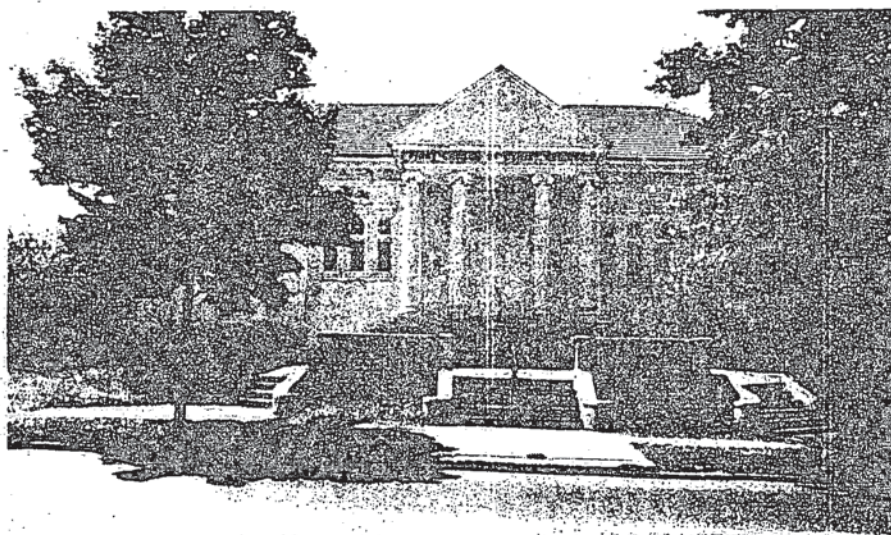
Restrained classical styling.

High basement

'Union Jack' cross in transom of each window

PHOTO Negative File # Roll 32 frames 3,4

Map (Indicate North)



A-401

SITING, BOUNDARY DESCRIPTION, AND RELATED STRUCTURES:

Church sits on a 6' rise approximately 40' from Springfield Ave. Large parking lot to the N.E. of church.
(Original Use) Church, Church (Additional Arch. Details)
High basement.
'Union Jack' cross in transom of each window.

SURROUNDING ENVIRONMENT: Urban ☐ Suburban ☒ Scattered Buildings ☐
Open Space ☐ Woodland ☐ Residential ☒ Agricultural ☐
Industrial ☐ Downtown Commercial ☐ Highway Commercial ☐ Other ☐

Apartment complexes are across the street. North of the church are late 19th Century residential areas.

SIGNIFICANCE:

The Christian Science Church was established in Cranford around 1900. The first meeting place of the congregation was a building (no longer extant) on the corner of Eastman Street and North Avenue in Cranford.

The congregation gathered together to build this church in 1908. It is an outstanding example of classical styling applied to a small structure to give it importance and dignity. The building was featured in turn-of-the-Century post-cards and photographs of Cranford.

ORIGINAL USE: ChurchPHYSICAL CONDITION: Excellent ☐ Good ☐REGISTER ELIGIBILITY: Yes ☒ Possible ☐THREATS TO SITE: Roads ☐ Development ☐No Threat ☒ Other ☐

COMMENTS:

PRESENT USE: Church

Fair ☒ Poor ☐No ☐ Part of District ☐Zoning ☐ Deterioration ☐

REFERENCES:

Cranford Historical Society

RECORDED BY: JWF

ORGANIZATION: UCCHPAB

DATE: 1981



CRANFORD

Union Co.

* 200308

r. 32/4

First Church of Christ,
Scientist
21 Springfield Ave.
looking S.W.

1981



CRANFORD
Union Co.

*200308
r. 32/3

First Church of Christ,
Scientist
21 Springfield Ave.
looking N.W.

1981

115 Miln Street, Cranford, New Jersey	c. 1910, two-story, Dutch Colonial Revival-style house with asphalt shingle roofing, aluminum siding, paired windows, enclosed porch, pedimented entry with simple posts,	900 feet/NW	Not Eligible	n/a	B.5.18
211 Miln Street, Cranford, New Jersey	c. 1890, two-and-a-half story, vernacular Victorian style house with cross gable, asphalt shingle roof, aluminum siding, 1/1 windows, brick foundation, vergeboard at gable end of attic window.	300 feet/N	Not Eligible	n/a	B.5.20
215 Miln Street, Cranford, New Jersey	c.1890, two-and-a-half-story, vernacular Queen Anne-style residence, green asphalt shingle roofing, clapboard siding, cloche domed tower, wrap around porch with decorative woodwork.	300 feet/N	Not Eligible	n/a	B.5.22
12 Springfield Avenue, Cranford, New Jersey	c.1940, two-story, Colonial Revival-style residence, asphalt shingle roofing, shed roof dormers with casement windows, shingle siding, 6/6 windows at 2nd story, 6/9 windows at 1st story.	600 feet E	Not Eligible	n/a	B.5.23
First Church of Christ Scientist, 21 Springfield Street, Cranford, New Jersey	1908, Neo-Classical style, two-story, green tile hipped roof, stucco walls and foundation. Five bays with three central bays projecting, pedimented entry with Ionic columns, windows grouped in threes.	700 feet/NE	Eligible NJSHPO Site Inventory #200308	No Adverse Effect	B.5.25
24 Springfield Avenue, Cranford, New Jersey	c. 1940, three-story, Tudor Revival-style apartment building, slate roofing, brick exterior on lower two stories, upper 1/1/2 stories half-timbering. Replacement 1/1 paired windows.	800 feet/NE	not eligible	n/a	B.5.27

* Distances are rounded to the nearest 100 feet.



PHOTO LOCATOR MAP

**Plate B.5.25:**

First Church of Christ Scientist. 21 Springfield Avenue.
 Photo view: Southwest
 Photographer: Elaine Kiernan Gold
 Date: August 1, 2001

115 Miln Street, Cranford, New Jersey	c. 1910, two-story, Dutch Colonial Revival-style house with asphalt shingle roofing, aluminum siding, paired windows, enclosed porch, pedimented entry with simple posts,	900 feet/NW	Not Eligible	n/a	B.5.18
211 Miln Street, Cranford, New Jersey	c. 1890, two-and-a-half story, vernacular Victorian style house with cross gable, asphalt shingle roof, aluminum siding, 1/1 windows, brick foundation, vergeboard at gable end of attic window.	300 feet/N	Not Eligible	n/a	B.5.20
215 Miln Street, Cranford, New Jersey	c.1890, two-and-a-half-story, vernacular Queen Anne-style residence, green asphalt shingle roofing, clapboard siding, cloche domed tower, wrap around porch with decorative woodwork.	300 feet/N	Not Eligible	n/a	B.5.22
12 Springfield Avenue, Cranford, New Jersey	c.1940, two-story, Colonial Revival-style residence, asphalt shingle roofing, shed roof dormers with casement windows, shingle siding, 6/6 windows at 2nd story, 6/9 windows at 1st story.	600 feet E	Not Eligible	n/a	B.5.23
First Church of Christ Scientist, 21 Springfield Street, Cranford, New Jersey	1908, Neo-Classical style, two-story, green tile hipped roof, stucco walls and foundation. Five bays with three central bays projecting, pedimented entry with Ionic columns, windows grouped in threes.	700 feet/NE	Eligible NJSHPO Site Inventory #200308	No Adverse Effect	B.5.25
24 Springfield Avenue, Cranford, New Jersey	c. 1940, three-story, Tudor Revival-style apartment building, slate roofing, brick exterior on lower two stories, upper 1/1/2 stories half-timbering. Replacement 1/1 paired windows.	800 feet/NE	not eligible	n/a	B.5.27

* Distances are rounded to the nearest 100 feet.



PHOTO LOCATOR MAP

**Plate B.5.27:**

24 Springfield Avenue.
Photo view: North
Photographer: Elaine Kiernan Gold
Date: August 1, 2001

**State of New Jersey**

DEPARTMENT OF ENVIRONMENTAL PROTECTION

Natural and Historic Resources, Historic Preservation Office

PO Box 404, Trenton, NJ 08625

TEL: (609) 292-2023 FAX: (609) 984-0578

www.state.nj.us/dep/hpo

HPO-i2007-99 PROD

LOG# 07-1939

JON S. CORZINE
GovernorLISA P. JACKSON
Commissioner

September 13, 2007

CERTIFICATION OF ELIGIBILITY

Ms. Christine M. Glazer
Chair, Board of Trustees, Cranford Historical Society, Inc.
Hanson House Annex
38 Springfield Avenue
Cranford, NJ 07016

Dear Ms. Glazer:

This letter is in response to your request for a formal certification of eligibility for the Hanson House, at 38 Springfield Avenue, in Cranford Township, Union County, for inclusion in the New Jersey and National Registers of Historic Places.

Based on a review of the submitted documentation and other information already on file, I find that a State Historic Preservation Officer opinion was issued on August 23, 2001, which found that a North Cranford Historic District was eligible for listing in the Registers. This opinion was confirmed in a Certification of Eligibility letter for the same district issued on May 19, 2005. A review of the current preliminary application for the Hanson house reveals that although the building stands just outside of where the suggested boundary was drawn, the Hanson house dates from within the period of significance of the district and that it stands at a point that closes the northern vista of Holly Street, establishing a visual relationship with an important streetscape of the district. As Deputy State Historic Preservation Officer, I therefore find that the Hanson house should be considered a contributing building in the North Cranford Historic District, and that the suggested boundaries of the district should be amended to include it.

If you have further questions, please contact Mr. Robert Craig of my staff, at (609) 984-0541, or by email at bob.craig@dep.state.nj.us

Sincerely,

A handwritten signature in dark ink, appearing to read "Dorothy P. Guzzo".

Dorothy P. Guzzo
Deputy State Historic
Preservation Officer

DPG/BC: 1199

New Jersey Department of Environmental Protection
Historic Preservation Office

BASE FORM**Historic Sites #:****Property Name:** The Hanson House**Street Address:** Street # 38 Apartment #**Prefix:** Street Name: Springfield **Suffix:** Type: AVE**County(s):** Union **Zip Code:** 07016**Municipality(s):** Cranford Twp. 2003 **Block(s):** 262**Local Place Name(s):** Lot(s): 3.01**Ownership:** Non-profit **USGS QUAD:** Roselle**Description:**

The Hanson House is a 2½ -story, 5-bay, Greek Revival-style dwelling that was constructed circa 1870. (See Building Attachment)

Registration and Status Dates:	National Historic Landmark: _____	SHPO Opinion: _____
	National Register: _____	Local Designation: _____
	New Jersey Register: _____	Other Designation: _____
	Determination of Eligibility: _____	Other Designation Date: _____

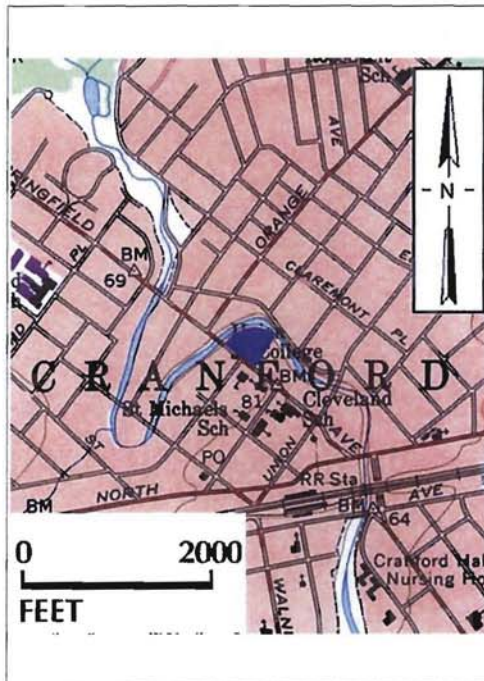
Photograph:**Survey Name:** Vanessa Zeoli **Date:** September 2008**Surveyor:** Phase II Springfield Ave. Bridge, Cranford, NJ**Organization:** Cultural Resource Consulting Group

New Jersey Department of Environmental Protection
Historic Preservation Office

BASE SURVEY FORM

Historic Sites #

Location Map:



Site Map:



Bibliography/Sources:

USGS 7.5' Quadrangle: Roselle, N. J. 1955 (Photorevised 1981).

Tax Maps Township of Cranford, Union County New Jersey, revised 1993.

Additional Information:

More Research Needed? ☐ Yes ☒ No

INTENSIVE-LEVEL USE ONLY:

Attachments Included: ☒ Building/Element ☐ Landscape ☐ Farm
☐ Bridge ☐ Industry

Historic District: Yes Historic District Name: North Cranford Historic District

Status: ☐ Key Contributing ☒ Contributing ☐ Non Contributing

Associated Archaeological Site/Deposite? ☐ Yes ☒ No

(Known or potential sites - if yes, please describe briefly)

Survey Name: Vanessa Zeoli

Date: September 2008

Surveyor: Phase II Springfield Ave. Bridge, Cranford, NJ

Organization: Cultural Resource Consulting Group

New Jersey Department of Environmental Protection
Historic Preservation Office

BUILDING ATTACHMENT**Historic Sites #**

Common Name: <u>The Hanson House</u>	
Historic Name: _____	
Present Use: <u>Residential Activity, Permanent</u>	
Historic Use: <u>Residential Activity, Permanent</u>	
Construction Date: <u>1870</u>	Source: <u>1882 E. Robinson map; visual evidence</u>
Alteration Date(s): <u>late-20th century</u>	Source: <u>Visual Evidence</u>
Designer: <u>N/A</u>	Physical Condition: <u>Good</u>
Builder: <u>Unknown</u>	Remaining Historic Fabric: <u>Medium</u>
Style: <u>Greek Revival</u>	
Form: <u>Gable Front</u>	Stories(B): <u>1</u>
Type: _____	Bays(B): <u>3</u>
Roof Finish Materials (B,S): <u>Asphalt Shingle</u>	
Exterior Finish Materials: <u>Wood, Shingles</u>	

Exterior Description:

The Hanson House is a 2½ -story, 5-bay, Greek Revival-style dwelling that was constructed circa 1870. The building is ell-shaped and frame constructed. The cross-gable roof is covered with asphalt shingles and has a slight overhang supported by paired brackets. The eave exhibits a partial return at the gable ends. Two brick chimney are located on interior roof slopes. The exterior walls are clad in wood shingles. Most window openings contain 6/1 wood sash units. The first floor, front (southeast) façade, has two paired multi-pane wood doors that do not appear original. The attic window is an arched 4/4 wood sash unit. A 1-story, flat-roof addition is located on the side-gable portion of the front façade and contains a string of six 10-pane casement windows. The offset main entrance contains a wood panel door and it situated under an eyebrow-shaped hood supported by Doric columns. The contemporary poured concrete porch pad is accessed by several steps and a handicap ramp. On the southeast is a 1-story, side-gable addition with brick veneer and asbestos shingle exterior. Windows on the addition are 6/6 wood sash units and the door is wood panel. (See Continuation Sheet)

Interior Description:

N/A

Setting:

The Hanson House is located approximately 250 feet southeast of the bridge on the northeast side of the road. The house functions as the Cranford Historical Society and Union County Historical Society. The Hanson House and adjacent Hanson Park (Union County Park Commission-operated) properties are characterized by winding trails and manicured landscaping consisting of young and mature trees.

Survey Name: <u>Vanessa Zeoli</u>	Date: <u>September 2008</u>
Surveyor: <u>Phase II Springfield Ave. Bridge, Cranford, NJ</u>	
Organization: <u>Cultural Resource Consulting Group</u>	

New Jersey Department of Environmental Protection
Historic Preservation Office

ELIGIBILITY WORKSHEET

Historic Sites #

History:

Comparison between the 1850 Sidney map and 1882 E. Robinson map combined with visual evidence suggest that The Hanson House was constructed circa 18750. Visual evidence also suggests the building had an addition constructed on the southeast elevation in the late 20th-century. The Hanson House is named after Cranford pediatrician and noted strep throat researcher Dr. Carl Hanson (Cranford Environmental Commission IV-3). Hanson moved to Cranford in 1933 to practice medicine until his death in 1980 Cranford Chronicle 1980). In 1991 the Cranford Historical Society took up residence in the house and continues to occupy it today. (See Continuation Sheet)

Statement of Significance:

The Hanson House is not individually eligible for the National Register, but is eligible as a contributing resource to the North Cranford Historic District. The Hanson House was determined a contributing resource to the North Cranford Historic District by the NJDEP-HPO on 5/19/05. The historic district meets Criteria A for its contribution to Cranford's residential growth and development during the late 19th and early 20th century as the township transformed from a rural farming community into a suburban neighborhood for wealthy businessmen. (See Continuation Sheet)

Eligibility for New Jersey
and National Registers:

☐ Yes ☒ No

National

Register Criteria: ☒ A ☐ B ☒ C ☐ D

Level of Significance: ☒ Local ☐ State ☐ National

Justification of Eligibility/Ineligibility:

The Hanson House retains its integrity of location, feeling, workmanship, setting, and association, but has undergone alterations that have compromised its integrity of materials and design. These changes include a 1-story, 2-bay contemporary addition on the southeast elevation and a poured concrete entry ramp. As a result, the Hanson House is no longer able to convey any potential historic and/or design significance as an individually eligible resource; however, it is still capable of conveying its significance as a contributing resource to the North Cranford Historic District.

For Historic Districts Only:

Property Count: _____ Key Contributing: _____ Contributing: _____ Non Contributing: _____

For Individual Properties Only:

List the Completed Attachments related to the property's significance:

Narrative Boundary Description

All of the property contained in Block 262, Lot 3.01 in the Township of Cranford, Union County, New Jersey.

Survey Name: Vanessa Zeoli

Date: September 2008

Surveyor: Phase II Springfield Ave. Bridge, Cranford, NJ

Organization: Cultural Resource Consulting Group

CONTINUATION SHEET

Historic Sites #

Bibliography/Sources (cont.)

Bauer, Jacob L.

1906 Atlas of Union County, New Jersey. E. Robinson & Co., New York, N.Y.

Hendrickson, Louie E.

1904 Cranford, New Jersey, Illustrated. Cranford, N.J.

Robinson, E. & R. H. Pidgeon

1882 Atlas of Union County, New Jersey. E. Robinson, New York, N.Y.

Sanborn Map Company

1903 Cranford, Union County, New Jersey. Sheet 4. On file at Rutgers University Libraries, New Brunswick, N.J.

1922 Cranford, Union County, New Jersey. Sheet 3. On file at Rutgers University Libraries, New Brunswick, N.J.

1959 Cranford, Union County, New Jersey. Sheet 3. On file at Rutgers University Libraries, New Brunswick, N.J.

1929 Cranford, Union County, New Jersey. Sheet 3. On file at Rutgers University Libraries, New Brunswick, N.J.

Sidney, J. C.

1850 Map of Essex County, New Jersey. Hiram A. Belding, Newark, N.J.

Building Description (cont.)

The foundation is covered with stucco. The lot also contains a 1-story, front-gable, 2-car automobile garage constructed circa 1925. The exterior is clad in wood shingles and the doors are overhead wood doors. The roofline is extended on either side to accommodate shed space.

History (cont.)

In September 2007 the NJDEP-HPO determined that the Hanson House was a contributing building to the North Cranford Historic District and suggested the boundaries of the district be amended to include it (Guzzo 2007). Hanson Park, situated immediately to the northwest, was established in the 1990s following the acquisition of the Hanson House by the township and the death of Mr. and Mrs. Hanson. Hanson Park, however, is owned by the Union County Park Commission. A dwelling that once stood on the Hanson Park lot was demolished in 1980.

Statement of Significance (cont.)

The Rahway River and the Central Railroad of New Jersey were key factors in the success of Cranford's development. The river was a strong influence in the community, forming a part of picturesque neighborhoods and providing recreational activities for residents. The railway enabled Cranford to attract New York professional who could live in the community and commute to work in the city. The historic district is also eligible for listing in the National Register under Criterion C as a historic district with representative examples of numerous styles and types of architecture that include Queen Anne and Colonial Revival, with lesser numbers of Tudor Revival, Shingle style and Craftsman. The historic district's period of significance is 1860 to 1930 (CRCG 2001: 24). No evidence was found to suggest that the district is eligible under Criterion B for association with significant individuals, or under Criterion D for having the ability to yield information important to history and/or pre-history. Since the Hanson House was constructed within the historic district's period of significance (1860-1930) and retains the majority of its character-defining features, it remains a contributing element to the North Cranford Historic District.

Survey Name: Vanessa ZeoliDate: September 2008Surveyor: Phase II Springfield Ave. Bridge, Cranford, NJOrganization: Cultural Resource Consulting Group



CONTINUATION SHEET

Historic Sites #



Garage

Survey Name: Vanessa ZeoliDate: September 2008Surveyor: Phase II Springfield Ave. Bridge, Cranford, NJOrganization: Cultural Resource Consulting Group

Inventory of Identified Architectural Resources			
Property	Property Information	Description	NRHP Eligibility
 <p>Photograph Number: 120</p>	<p>Panamerican Resource Number: 120</p> <p>Hanson Park/Hanson House</p> <p>Springfield Avenue</p> <p>Municipality: Cranford, Union County</p> <p>UTM coordinates (1983 Zone 18N): 4501261N, 558790E</p> <p>Date: ca. 1990</p> <p>Other: The Hanson House is home of the Cranford and Union County Historical Societies, and Hanson Park Conservancy.</p>	<p>Style: Informal</p> <p>Stories: na</p> <p>Plan: walking paths, sections dedicated to specific concepts</p> <p>Roof: na</p> <p>Exterior: na</p> <p>Foundation: na</p> <p>Windows: na</p> <p>Details: This park occupies the yard associated with the Dr. Carl Hanson House and sits on the Rahway River.</p>	<p>The SHPO (2007) recommended that the NRE North Cranford HD boundary be moved to include the Hanson House and property. Given this recommendation, it should be assumed that Hanson Park is eligible as a contributing element to the to the NRE North Cranford Historic District (ID #3838).</p>
 <p>Photograph Number: 121 (at the lake)</p>	<p>Panamerican Resource Number: 121</p> <p>Nomahegan Park (ID #4424.021)</p> <p>Springfield Avenue</p> <p>Municipality: Cranford, Union County</p> <p>UTM coordinates (1983 Zone 18N): 4502386N, 557946E</p> <p>Date: 1926</p> <p>Other: A Union County Park, it has been enlarged twice.</p>	<p>Style: informal</p> <p>Stories: na</p> <p>Plan: series of paths, segregated activity areas, a manmade lake, and wild areas</p> <p>Roof: na</p> <p>Exterior: na</p> <p>Foundation: na</p> <p>Windows: na</p> <p>Details: The original design—area shown in the photograph at left—was by Olmsted Brothers. Lenape and Nomahegan parks were joined across Kenilworth Boulevard in 1963.</p>	<p>Nomahegan Park is eligible for the NRHP as a contributing element to both the NRE Union County Park System HD (ID #4424) and NRE Rahway River Parkway HD (ID #4079).</p>



State of New Jersey

Richard J. Codey
Acting Governor

Department of Environmental Protection
Natural and Historic Resources, Historic Preservation Office
PO Box 404, Trenton, NJ 08625-0404
TEL: (609) 292-2023 FAX: (609) 984-0578
www.state.nj.us/dep/hpo

HPO-E05-219 **PROD**
LOG# 05-0872

Bradley M. Campbell
Commissioner

May 19, 2005

CERTIFICATION OF ELIGIBILITY

Mr. Daniel Aschenbach
Deputy Mayor, Township of Cranford
8 Springfield Avenue
Cranford, NJ 07016

Dear Mr. Aschenbach:

This letter is in response to your request for a formal certification of eligibility for the North Cranford Historic District, in Cranford Township, Union County, for inclusion in the New Jersey and National Registers of Historic Places.

Based on a review of the submitted documentation and other information already on file, I find that a SHPO opinion was issued on August 23, 2001 that the North Cranford Historic District was eligible for listing in the Registers under National Register Criteria A and C. Based on a recent site visit by a member of my staff, I find that the district still possesses its character-defining features, and therefore it is my opinion that the North Cranford Historic District remains eligible for inclusion into the New Jersey and National Registers of Historic Places. I further find that the Cranford Canoe Club is a contributing building within this district.

If you have further questions, please contact Mr. Robert Craig of my staff, at (609) 984-0541, or by email at bob.craig@dep.state.nj.us

Sincerely,

Dorothy P. Guzzo
Deputy State Historic
Preservation Officer

DPG/BC: ..\e219

c: Jennifer Stark, Stark & Assoc.

Department of Environmental Protection
Historic Preservation Office

BASE FORM**Historic Sites #:**Property Name: Cranford Canoe ClubStreet Address: Street # 250Apartment # Prefix: Street Name: SpringfieldSuffix: Type: AVECounty(s): UnionZip Code: 07016Municipality(s): Cranford Twp. 2003Block(s): 261Local Place Name(s): Lot(s): 1Ownership: PublicUSGS QUAD: **Description:**

250 Springfield Avenue is a 1½-story, 4-bay, vernacular boat house constructed circa 1910. (See Building Attachment)

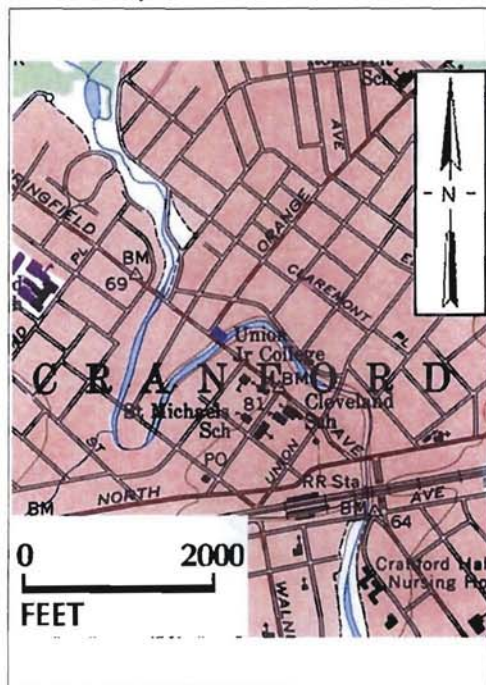
Registration and
Status Dates: National Historic
Landmark: SHPO Opinion: 3/18/05National Register: Local Designation: New Jersey Register: Other Designation: Determination of Eligibility: Other Designation Date: **Photograph:**Survey Name: Vanessa ZeoliDate: September 2008Surveyor: Phase II Springfield Ave. Bridge, Cranford, NJOrganization: Cultural Resource Consulting Group

Department of Environmental Protection
Historic Preservation Office

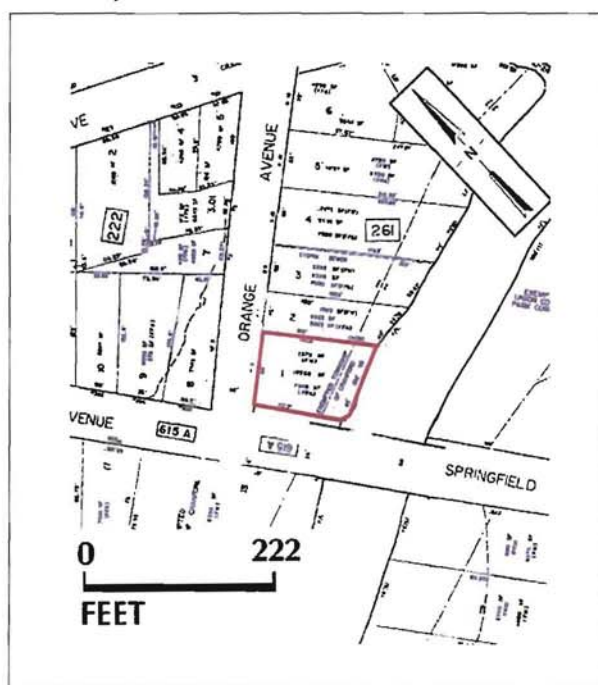
BASE SURVEY FORM

Historic Sites #

Location Map:



Site Map:



Bibliography/Sources:

USGS 7.5' Quadrangle: Roselle, N. J. 1955 (Photorevised 1981).
Tax Maps Township of Cranford, Union County New Jersey, revised 1993.

Additional Information:

More Research Needed? ☐ Yes ☒ No

INTENSIVE-LEVEL USE ONLY:

Attachments Included: ☒ Building/Element ☐ Landscape ☐ Farm
☐ Bridge ☐ Industry

Historic District: ☒ Yes Historic District Name: Part of 3 districts (See Contin. Sheets)

Status: ☐ Key Contributing ☒ Contributing ☐ Non Contributing

Associated Archaeological Site/Deposite? ☐ Yes ☒ No

(Known or potential sites - if yes, please describe briefly)

Survey Name: Vanessa Zeoli

Date: September 2008

Surveyor: Phase II Springfield Ave. Bridge, Cranford, NJ

Organization: Cultural Resource Consulting Group

New Jersey Department of Environmental Protection
Historic Preservation Office

BUILDING ATTACHMENT**Historic Sites #**

Common Name: <u>Cranford Canoe Club</u>	
Historic Name: _____	
Present Use: <u>Recreational and Entertainment Activity, Active recreation - outdoor</u>	
Historic Use: <u>Recreational and Entertainment Activity, Active recreation - outdoor</u>	
Construction Date: <u>1910</u>	Source: <u>visual evidence; historic photographs</u>
Alteration Date(s): <u>late-20th-century</u>	Source: <u>Visual Evidence</u>
Designer: <u>N/A</u>	Physical Condition: <u>Good</u>
Builder: <u>Unknown</u>	Remaining Historic Fabric: <u>Low</u>
Style: <u>None</u>	
Form: <u>Other</u>	Stories(B): <u>1</u>
Type: _____	Bays(B): <u>4</u>
Roof Finish Materials (B,S): <u>Wood, Shingle</u>	
Exterior Finish Materials: <u>Wood, Clapboard</u>	

Exterior Description:

250 Springfield Avenue is a 1½-story, 4-bay, vernacular boat house constructed circa 1910. The side-gable roof is covered with wood shingles. The exterior is clad in wood siding. Windows contain 8/8 and 6/6 replaced wood sash units. The building features new copper gutters. The front (southeast) façade contains two overhead garage door openings; one much larger than the other. The pedestrian entrance contains a modern steel door a string of three window panes. The roofline extends over the entrances creating an open porch and covering an upper-level loft space. The porch is supported by square wood posts and the porch deck is poured concrete. A 1-story shed section protrudes from the rear façade. The foundation is covered with stucco.

Interior Description:

N/A

Setting:

The boat house currently known as the Cranford Canoe Club is located immediately north of the bridge on the northeast side of the road. The building is situated on Block 261, Lot 1 and situated on a property that also contains a gravel parking lot at the corner of Springfield Avenue and Orange Avenue. The building fronts the Rahway River and maintains a wood slip dock that extends approximately 20 feet from the porch to the river.

Survey Name: Vanessa Zeoli
Surveyor: Phase II Springfield Ave. Bridge, Cranford, NJ
Organization: Cultural Resource Consulting Group

Date: September 2008

New Jersey Department of Environmental Protection
Historic Preservation Office

ELIGIBILITY WORKSHEET

Historic Sites #

History:

The boathouse currently known as the Cranford Canoe Club was constructed circa 1910 as the Ulhigh Canoe Club. This building underwent several alterations in its history. An early photograph that dates from the 1920s or 1930s shows a clapboard-clad building with five doors and an open roof over the porch. The slip dock was slightly smaller. A sign on the roof says "Canoes for Hire." A later photograph, dating to about 1980 shows the building clad in wood shingles with the enclosed loft space. By the time this photo was taken, two of the entrances were combined into one big opening and another entrance was converted to a window.

Statement of Significance:

The Cranford Canoe Club are not individually eligible for the National Register, but eligible as a contributing resource to the North Cranford Historic District, the Rahway River Parkway Historic District, and the Union County Park System Historic District. (See Continuation Sheet)

Eligibility for New Jersey
and National Registers:

☐ Yes ☒ No

National

Register Criteria: ☒ A ☐ B ☒ C ☐ D

Level of Significance: ☒ Local ☐ State ☐ National

Justification of Eligibility/Ineligibility:

The Cranford Canoe Club retains its integrity of location, feeling, workmanship, setting, and association, but has undergone alterations that have compromised its integrity of materials and design. These changes include the pre-1980 rearrangement of the original five openings on the front facade and the partial enclosure of its porch roof to form a loft space. More recently, new window sashes and copper gutters were added. As a result, the Cranford Canoe Club is no longer able to convey any potential historic and/or design significance as an individually eligible resource; however, it is still capable of conveying its significance as a contributing resource to the North Cranford Historic District, the Rahway River Parkway Historic District, and the Union County Park System Historic District.

For Historic Districts Only:

Property Count: _____ Key Contributing: _____ Contributing: _____ Non Contributing: _____

For Individual Properties Only:

List the Completed Attachments related to the property's significance:

Narrative Boundary Description

All of the property contained in Block 261, Lot 1 in the Township of Cranford, Union County, New Jersey.

Survey Name: Vanessa Zeoli

Date: September 2008

Surveyor: Phase II Springfield Ave. Bridge, Cranford, NJ

Organization: Cultural Resource Consulting Group

CONTINUATION SHEET**Historic Sites #****Bibliography/Sources (cont.)**

Bauer, Jacob L.

1906 Atlas of Union County, New Jersey. E. Robinson & Co., New York, N.Y.

Hendrickson, Louie E.

1904 Cranford, New Jersey, Illustrated. Cranford, N.J.

Robinson, E. & R. H. Pidgeon

1882 Atlas of Union County, New Jersey. E. Robinson, New York, N.Y.

Sanborn Map Company

1903 Cranford, Union County, New Jersey. Sheet 4. On file at Rutgers University Libraries, New Brunswick, N.J.

1922 Cranford, Union County, New Jersey. Sheet 3. On file at Rutgers University Libraries, New Brunswick, N.J.

1959 Cranford, Union County, New Jersey. Sheet 3. On file at Rutgers University Libraries, New Brunswick, N.J.

1929 Cranford, Union County, New Jersey. Sheet 3. On file at Rutgers University Libraries, New Brunswick, N.J.

Sidney, J. C.

1850 Map of Essex County, New Jersey. Hiram A. Belding, Newark, N.J.

History (cont.)

The Canoe Club was purchased by the township in 1993 and underwent alterations last year that included replacement of the wood shingles with wood siding and replacement of the windows.

Statement of Significance (cont.)

The North Cranford Historic District was determined eligible for listing on the National Register of Historic Places by the NJDEP-HPO on 5/19/05. The historic district meets Criteria A for its contribution to Cranford's residential growth and development during the late 19th and early 20th century as the township transformed from a rural farming community into a suburban neighborhood for wealthy businessmen. The Rahway River and the Central Railroad of New Jersey were key factors in the success of Cranford's development. The river was a strong influence in the community, forming a part of picturesque neighborhoods and providing recreational activities for residents. The railway enabled Cranford to attract New York professional who could live in the community and commute to work in the city. The historic district is also eligible for listing in the National Register under Criterion C as a historic district with representative examples of numerous styles and types of architecture that include Queen Anne and Colonial Revival, with lesser numbers of Tudor Revival, Shingle style and Craftsman. The historic district's period of significance is 1860 to 1930 (CRCG 2001: 24). No evidence was found to suggest that the district is eligible under Criterion B for association with significant individuals, or under Criterion D for having the ability to yield information important to history and/or pre-history. Since the Cranford Canoe Club building was constructed within the historic district's period of significance (1860-1930), retains the majority of its character-defining features, and falls within its boundaries, the canoe club is a contributing element to the North Cranford Historic District. Furthermore, it is one of the only surviving canoe clubs in Cranford from a period when the Rahway River was the center for leisure and sporting activities and canoe clubs were a common amenity to river life. Furthermore, the Cranford Canoe Club falls within the boundaries of the North Cranford Historic District.

Survey Name: Vanessa ZeoliDate: September 2008Surveyor: Phase II Springfield Ave. Bridge, Cranford, NJOrganization: Cultural Resource Consulting Group

CONTINUATION SHEET

Historic Sites #

Statement of Significance (cont.)



On 9/18/02 the Rahway River Parkway Historic District was determined eligible for listing on the National Register of Historic Places by NJDEP-HPO. The district meets Criterion A "for its significant historical association with the conservation movement, a movement that originated with the nineteenth century national parks system" and for its role "in an evolving social movement to develop public parkland in the face of growing urbanization" (RGA 2002: 9-16). The district also meets Criterion C for "its association with a master landscape architectural firm – Olmsted Brothers Landscape Architects of Brookline, Massachusetts" (RGA 2002: 9-17). The district's period of significance is 1921-1968. Though the Cranford Canoe Club was not specifically discussed in RGA's report, its significant contribution to recreational activities on the Rahway River in Cranford and the larger Rahway River Parkway make it a contributing resource to the district. Furthermore, it still retains the majority of its character-defining features and has continued to function as a canoe club up to the present day. Furthermore the Cranford Canoe Club falls within the boundaries of the Rahway River Parkway Historic District.

In October 2004, CRCG determined the Rahway River Parkway was a contributing resource to the Union County Park System Historic District and on 3/18/05 the district was determined eligible for listing on the National Register of Historic Places by NJDEP-HPO. The district meets statewide significance under Criterion A "in the category of Conservation for its substantial accomplishments in preserving New Jersey's natural resources, both within the region of Union County and in cooperation with adjacent counties, such as Essex, Somerset, Morris, and Passaic Counties." It also possesses national significance under Criterion A "for its contributions to the broad patterns of development in the areas of Community Planning, Entertainment/Recreation, and Landscape Architecture" (CRCG 2004: 134). In addition, some of the parks within the system were considered eligible under Criterion D for their capacity to yield future information pertaining to prehistory and history. No evidence was found to suggest that the district is eligible under Criterion B for association with significant individuals. The period of significance is 1921-1964. The Cranford Canoe Club is a contributing resource to the Union County Park System Historic District for the same reasons that it contributes to the Rahway River Parkway Historic District: for its significant contribution to recreational activities on the Rahway River in Cranford and the larger Rahway River Parkway, because it still retains the majority of its character-defining features, and because it continues to function as a canoe club up to the present day. Furthermore, the Cranford Canoe Club falls within the boundaries of the Union County Park System Historic District.

Survey Name: Vanessa ZeoliDate: September 2008Surveyor: Phase II Springfield Ave. Bridge, Cranford, NJOrganization: Cultural Resource Consulting Group

A-423

Inventory of Identified Architectural Resources

Property	Property Information	Description	NRHP Eligibility
 <p>Photograph Number: 82</p>	<p>Panamerican Resource Number: 82</p> <p>121 Riverside Drive</p> <p>Municipality: Cranford, Union County</p> <p>UTM coordinates (1983 Zone 18N): 4501371N, 558666E</p> <p>Date: ca. 1910</p> <p>Other: This house backs up to the Rahway River.</p>	<p>Style: Dutch Colonial/Colonial Revival</p> <p>Stories: 2</p> <p>Plan: rectangular</p> <p>Roof: gambrel</p> <p>Exterior: First floor—wood siding; second floor—wood shingles</p> <p>Foundation: concrete</p> <p>Windows: 6/1 wood originals with storm windows</p> <p>Details: enclosed side porch, highly decorative, original front entry</p>	<p>This house is a contributing element to the NRE North Cranford HD (ID #3838).</p>
 <p>Photograph Number: 83 (rear view, first floor dock level on the Rahway River)</p>	<p>Panamerican Resource Number: 83</p> <p>Cranford Canoe Club</p> <p>8 Springfield Avenue</p> <p>Municipality: Cranford, Union County</p> <p>UTM coordinates (1983 Zone 18N): 4501271N, 558656E</p> <p>Dates: various</p> <p>Other: This building sits right on the Rahway River in order to launch canoes.</p>	<p>Style: none</p> <p>Stories: 2</p> <p>Plan: rectangular</p> <p>Roof: gable with cat slide</p> <p>Exterior: vinyl siding</p> <p>Foundation: concrete</p> <p>Windows: mixture of types</p> <p>Details: A canoe club building has been at this site since 1907. The first floor of the building is at dock level as shown in the photograph, and the second floor of the building is at street level.</p>	<p>The Canoe Club is a contributing element to the NRE North Cranford HD (ID #3838).</p>

Department of Environmental Protection
Historic Preservation Office

BASE FORM**Historic Sites #:****Property Name:** 300 Springfield Avenue**Street Address:** Street # 300Apartment #**Prefix:** Street Name: Springfield**Suffix:** Type: AVE**County(s):** Union**Zip Code:** 07016**Municipality(s):** Cranford Twp. 2003**Block(s):** 222**Local Place Name(s):** **Lot(s):** 8**Ownership:** Private**USGS QUAD:** Roselle**Description:**

300 Springfield Avenue is a 1-story, 3-bay, Craftsman-style dwelling constructed circa 1925. (See Building Attachment)

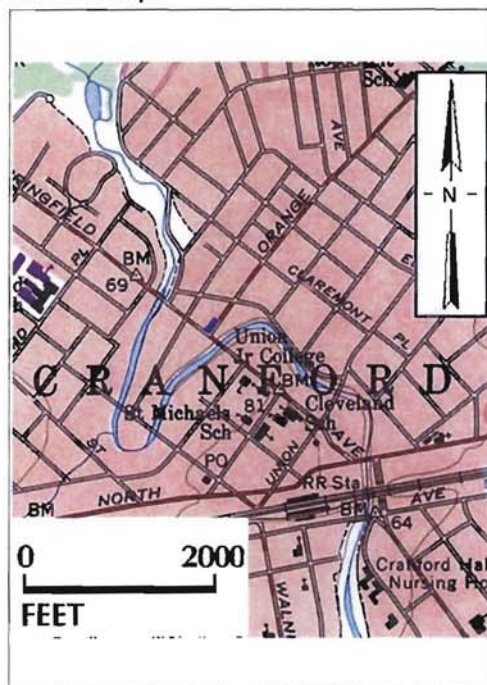
**Registration and
Status Dates:****National Historic
Landmark:** **SHPO Opinion:** **National Register:** **Local Designation:** **New Jersey Register:** **Other Designation:** **Determination of Eligibility:** **Other Designation Date:** **Photograph:****Survey Name:** Vanessa Zeoli**Date:** September 2008**Surveyor:** Phase II Springfield Ave. Bridge, Cranford, NJ**Organization:** Cultural Resource Consulting Group

Department of Environmental Protection
Historic Preservation Office

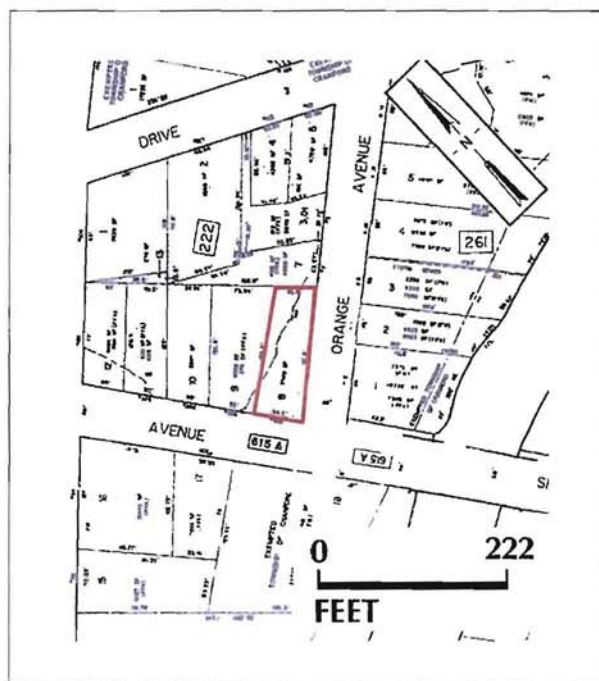
BASE SURVEY FORM

Historic Sites #

Location Map:



Site Map:



Bibliography/Sources:

USGS 7.5' Quadrangle: Roselle, N. J. 1955 (Photorevised 1981).
Tax Maps Township of Cranford, Union County New Jersey, revised 1993.

Additional Information:

More Research Needed? ☐ Yes ☒ No

INTENSIVE-LEVEL USE ONLY:

Attachments Included: ☒ Building/Element ☐ Landscape ☐ Farm
☐ Bridge ☐ Industry

Historic District: ☒ Yes Historic District Name: North Cranford Historic District

Status: ☐ Key Contributing ☒ Contributing ☐ Non Contributing

Associated Archaeological Site/Deposite? ☐ Yes ☒ No

(Known or potential sites - if yes, please describe briefly)

Survey Name: Vanessa Zeoli

Date: September 2008

Surveyor: Phase II Springfield Ave. Bridge, Cranford, NJ

Organization: Cultural Resource Consulting Group

New Jersey Department of Environmental Protection
Historic Preservation Office

BUILDING ATTACHMENT**Historic Sites #**

Common Name: 300 Springfield Avenue	
Historic Name: _____	
Present Use: Residential Activity, Permanent	
Historic Use: Residential Activity, Permanent	
Construction Date: 1925	Source: 1922 and 1929 Sanborn maps; visual evidence
Alteration Date(s): late-20th century	Source: Visual Evidence
Designer: N/A	Physical Condition: Excellent
Builder: Unknown	Remaining Historic Fabric: Medium
Style: Craftsman	
Form: Bungalow	Stories(B): 1.5
Type: _____	Bays(B): 3
Roof Finish Materials (B,S): Asphalt Shingle	
Exterior Finish Materials: Stucco	

Exterior Description:

300 Springfield Avenue is a 1-story, 3-bay, Craftsman-style dwelling constructed circa 1925. The roof is side gable with clipped ends and covered with asphalt shingles. The wide eave is supported by wood brackets. A brick chimney pierces the front roof slope. The exterior walls are clad in stucco. A three-sided bay window on the front façade contains a single-pane picture window flanked by wood sash units and a standing seam metal roof. Under the porch is a wood sash window flanked by two casements, all with pointed wood panes. The side façade contains synthetic sash units. The front porch is incised under the roof line and supported by square stucco covered posts and arched openings. The centralized entrance contains a Craftsman-style wood door. The porch is accessed by brick steps and a metal handrail. The water table is trimmed with a band of bricks. The foundation is stucco covered. The parcel also has a 1-story, single-car automobile garage with a front-gable roof covered in asphalt shingles and exterior walls covered with stucco. Exposed rafter ends peak out from the eave. Window openings contain 6/6 wood sash units and the entrance has an overhead wood door.

Interior Description:

N/A

Setting:

300 Springfield Avenue is located approximately 150 feet northeast of the bridge at the north corner of Springfield and Orange Avenues. The dwelling is situated on Block 222, Lot 8. The property has poured concrete sidewalks, is entered by a macadam driveway accessible from Orange Avenue, and is adorned with several mature and young trees and shrubbery.

Survey Name: Vanessa Zeoli	Date: September 2008
Surveyor: Phase II Springfield Ave. Bridge, Cranford, NJ	
Organization: Cultural Resource Consulting Group	

New Jersey Department of Environmental Protection
Historic Preservation Office

ELIGIBILITY WORKSHEET

Historic Sites #

History:

Comparison between the 1922 and 1929 Sanborn maps combined with visual evidence suggest that 300 Springfield Avenue was constructed circa 1925. Visual evidence also suggests the building had several windows replaced in the late 20th-century.

Statement of Significance:

300 Springfield Avenue is not individually eligible for the National Register, but is eligible as a contributing resource to the North Cranford Historic District. The North Cranford Historic District was determined eligible for listing on the National Register of Historic Places by the NJDEP-HPO on 5/19/05. The historic district meets Criteria A for its contribution to Cranford's residential growth and development during the late 19th and early 20th century as the township transformed from a rural farming community into a suburban neighborhood for wealthy businessmen. (See Continuation Sheet)

Eligibility for New Jersey
and National Registers:

☐ Yes ☒ No

National

Register Criteria: ☒ A ☐ B ☒ C ☐ D

Level of Significance: ☒ Local ☐ State ☐ National

Justification of Eligibility/Ineligibility:

300 Springfield Avenue retains its integrity of design, location, feeling, workmanship, setting, and association, but has undergone minimal alterations that have compromised its integrity of materials. These changes include replacement of some of the window sashes and possible replacement of the roofing material. As a result, 300 Springfield Avenue is no longer able to convey any potential historic and/or design significance as an individually eligible resource; however, it is still capable of conveying its significance as a contributing resource to the North Cranford Historic District.

For Historic Districts Only:

Property Count: _____ Key Contributing: _____ Contributing: _____ Non Contributing: _____

For Individual Properties Only:

List the Completed Attachments related to the property's significance:

Narrative Boundary Description

All of the property contained in Block 222, Lot 8 in the Township of Cranford, Union County, New Jersey.

Survey Name: Vanessa Zeoli

Date: September 2008

Surveyor: Phase II Springfield Ave. Bridge, Cranford, NJ

Organization: Cultural Resource Consulting Group

CONTINUATION SHEET

Historic Sites #

Bibliography/Sources (cont.)

Bauer, Jacob L.

1906 Atlas of Union County, New Jersey. E. Robinson & Co., New York, N.Y.

Hendrickson, Louie E.

1904 Cranford, New Jersey, Illustrated. Cranford, N.J.

Robinson, E. & R. H. Pidgeon

1882 Atlas of Union County, New Jersey. E. Robinson, New York, N.Y.

Sanborn Map Company

1903 Cranford, Union County, New Jersey. Sheet 4. On file at Rutgers University Libraries, New Brunswick, N.J.

1922 Cranford, Union County, New Jersey. Sheet 3. On file at Rutgers University Libraries, New Brunswick, N.J.

1959 Cranford, Union County, New Jersey. Sheet 3. On file at Rutgers University Libraries, New Brunswick, N.J.

1929 Cranford, Union County, New Jersey. Sheet 3. On file at Rutgers University Libraries, New Brunswick, N.J.

Sidney, J. C.

1850 Map of Essex County, New Jersey. Hiram A. Belding, Newark, N.J.

Statement of Significance (cont.)

The Rahway River and the Central Railroad of New Jersey were key factors in the success of Cranford's development. The river was a strong influence in the community, forming a part of picturesque neighborhoods and providing recreational activities for residents. The railway enabled Cranford to attract New York professional who could live in the community and commute to work in the city. The historic district is also eligible for listing in the National Register under Criterion C as a historic district with representative examples of numerous styles and types of architecture that include Queen Anne and Colonial Revival, with lesser numbers of Tudor Revival, Shingle style and Craftsman. The historic district's period of significance is 1860 to 1930 (CRCG 2001: 24). No evidence was found to suggest that the district is eligible under Criterion B for association with significant individuals, or under Criterion D for having the ability to yield information important to history and/or pre-history. Since 300 Springfield Avenue was constructed within the historic district's period of significance (1860-1930), retains the majority of its character-defining features, and falls within the district's boundaries, it is a contributing element to the North Cranford Historic District.

Survey Name: Vanessa ZeoliDate: September 2008Surveyor: Phase II Springfield Ave. Bridge, Cranford, NJOrganization: Cultural Resource Consulting Group

CONTINUATION SHEET

Historic Sites #



Garage

Survey Name: Vanessa ZeoliDate: September 2008Surveyor: Phase II Springfield Ave. Bridge, Cranford, NJOrganization: Cultural Resource Consulting Group

Department of Environmental Protection
Historic Preservation Office

BASE FORM**Historic Sites #:**Property Name: Cranford Clay CourtsStreet Address: Street # 301 Apartment #Prefix: Street Name: Springfield Suffix: Type: Other;County(s): Union Zip Code: 07016Municipality(s): Cranford Twp. 2003 Block(s): 184Local Place Name(s): Lot(s): 18Ownership: Private USGS QUAD: Roselle**Description:**

The Cranford Clay Courts consists of three green clay tennis courts constructed in the early 1920s. (See Building Attachment)

Registration and Status Dates:	National Historic Landmark: _____	SHPO Opinion: _____
	National Register: _____	Local Designation: _____
	New Jersey Register: _____	Other Designation: _____
	Determination of Eligibility: _____	Other Designation Date: _____

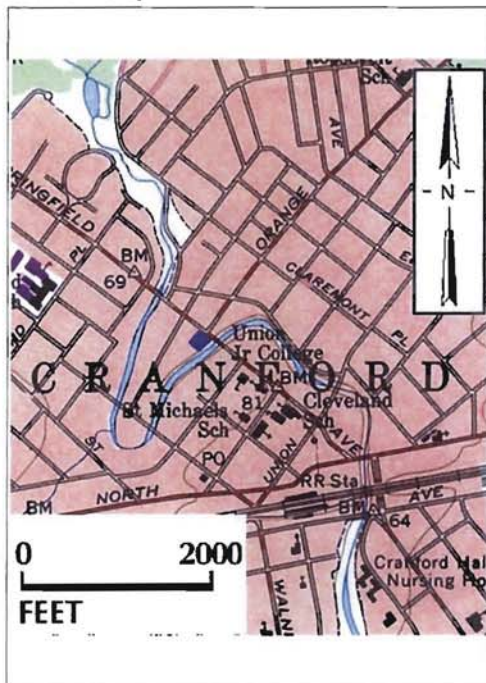
Photograph:Survey Name: Vanessa Zeoli Date: September 2008Surveyor: Phase II Springfield Ave. Bridge, Cranford, NJOrganization: Cultural Resource Consulting Group

Department of Environmental Protection
Historic Preservation Office

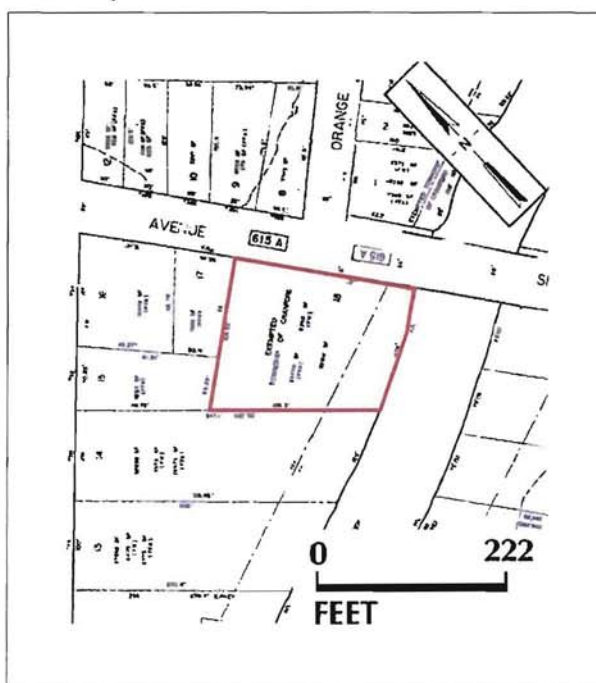
BASE SURVEY FORM

Historic Sites #

Location Map:



Site Map:



Bibliography/Sources:

USGS 7.5' Quadrangle: Roselle, N. J. 1955 (Photorevised 1981).
Tax Maps Township of Cranford, Union County New Jersey, revised 1993.

Additional Information:

More Research Needed? ☐ Yes ☒ No

INTENSIVE-LEVEL USE ONLY:

Attachments Included: ☒ Building/Element ☐ Landscape ☐ Farm
☐ Bridge ☐ Industry

Historic District: Yes Historic District Name: Part of 3 districts (See Contin. Sheet)

Status: ☐ Key Contributing ☒ Contributing ☐ Non Contributing

Associated Archaeological Site/Deposite? ☐ Yes ☒ No

(Known or potential sites - if yes, please describe briefly)

Survey Name: Vanessa Zeoli

Date: September 2008

Surveyor: Phase II Springfield Ave. Bridge, Cranford, NJ

Organization: Cultural Resource Consulting Group

New Jersey Department of Environmental Protection
Historic Preservation Office

STRUCTURE ATTACHMENT

Historic Sites #

Common Name: Cranford Clay Courts
Historic Name: Cranford Canoe Club
Present Use: Recreational and Entertainment Activity, Active recreation - outdoor
Historic Use: Recreational and Entertainment Activity, Active recreation - outdoor
Construction Date: 1920 **Source:** 1927 Photograph, Goodman 2008
Alteration Date(s): Late 20th-century **Source:** Visual Evidence
Designer: N/A **Physical Condition:** Good
Builder: Unknown **Remaining Historic Fabric:** Medium
Type: Other; Tennis Courts
Roof Finish Materials: _____
Exterior Finish Materials: Other; Clay

Exterior Description:

The Cranford Clay Courts consists of three green clay tennis courts constructed in the early 1920s. The tennis courts are situated several feet below the grade of the road in a low-lying area on the northwest bank of the Rahway River. The courts are enclosed by a 10-foot high green chain-link fence. The southwest and northeast sides of the courts are protected from view by tall hedge and fence-climbing vines. The northwestern fence is covered with tarp to allow privacy for the residences beyond. The court nets are modern and is strung on circular steel poles. At the southwest and northeast ends of the interior of the court are modern, steel flood lights that are approximately 20 feet high. Girl Scout Park is located on the same parcel and situated southeast of the courts on the riverfront. The park is characterized by young and mature trees, natural-looking plantings, a bird bath, and a small footbridge used for Girl Scout ceremonies.

Interior Description:

N/A

Setting:

The Cranford Clay Courts are located approximately 70 feet northwest of the subject property on the southwest side of the road. The courts are bordered on the southeast by Girl Scout Park and on the north by a single-family residence. The courts and the park are situated on the same parcel identified as Block 184, Lot 18.

Survey Name: Vanessa Zeoli

Date: September 2008

Surveyor: Phase II Springfield Ave. Bridge, Cranford, NJ

Organization: Cultural Resource Consulting Group

New Jersey Department of Environmental Protection
Historic Preservation Office

ELIGIBILITY WORKSHEET

Historic Sites #

History:

The Cranford Clay Courts were constructed circa 1920 as an amenity to the Cranford Canoe Club. The courts are shown in the background of a historic photograph (held by the Cranford Historical Society) that dates from July 4, 1927. The courts are not shown on a map until 1959. The clay courts hosted the legendary Wimbledon champion Bill Tilden, who in 1920 became the first American to win the contest (Mary Goodman 2008). Clay courts are more traditional and cheaper to construct, but more costly to maintain. Within the last ten years a sprinkler system was installed in order to maintain the proper water content and the court surface must be raked regularly (Mary Goodman 2008). (See Continuation Sheet)

Statement of Significance:

The Cranford Clay Courts are not individually eligible for the National Register, but eligible as a contributing resource to the North Cranford Historic District, the Rahway River Parkway Historic District, and the Union County Park System Historic District. (See Continuation Sheet)

Eligibility for New Jersey
and National Registers:

☐ Yes ☒ No

National

Register Criteria: ☒ A ☐ B ☒ C ☐ D

Level of Significance: ☒ Local ☐ State ☐ National

Justification of Eligibility/Ineligibility:

The Cranford Clay Courts retains its integrity of location, feeling, workmanship, setting, and association, but have undergone alterations that have compromised their integrity of materials. These changes include the addition of modern floodlights and a 10-foot chain-link fence. In addition, the age of nets and poles is uncertain. As a result, the Cranford Clay Courts are not able to convey any potential historic and/or design significance as an individually eligible resource; however, they are still capable of conveying their significance as a contributing resource to the North Cranford Historic District, the Rahway River Parkway Historic District, and the Union County Park System Historic District.

For Historic Districts Only:

Property Count: _____ Key Contributing: _____ Contributing: _____ Non Contributing: _____

For Individual Properties Only:

List the Completed Attachments related to the property's significance:

Narrative Boundary Description

All of the property contained in Block 184, Lot 18 in the Township of Cranford, Union County, New Jersey.

Survey Name: Vanessa Zeoli

Date: September 2008

Surveyor: Phase II Springfield Ave. Bridge, Cranford, NJ

Organization: Cultural Resource Consulting Group

CONTINUATION SHEET

Historic Sites #

Bibliography/Sources (cont.)

Bauer, Jacob L.

1906 Atlas of Union County, New Jersey. E. Robinson & Co., New York, N.Y.

Hendrickson, Louie E.

1904 Cranford, New Jersey, Illustrated. Cranford, N.J.

Robinson, E. & R. H. Pidgeon

1882 Atlas of Union County, New Jersey. E. Robinson, New York, N.Y.

Sanborn Map Company

1903 Cranford, Union County, New Jersey. Sheet 4. On file at Rutgers University Libraries, New Brunswick, N.J.

1922 Cranford, Union County, New Jersey. Sheet 3. On file at Rutgers University Libraries, New Brunswick, N.J.

1959 Cranford, Union County, New Jersey. Sheet 3. On file at Rutgers University Libraries, New Brunswick, N.J.

1929 Cranford, Union County, New Jersey. Sheet 3. On file at Rutgers University Libraries, New Brunswick, N.J.

Sidney, J. C.

1850 Map of Essex County, New Jersey. Hiram A. Belding, Newark, N.J.

History (cont.)

As boating fell out of favor, the club house was converted into the Neva Sykes Girl Scout House. The canoe club building was demolished around 1959 and subsequently the property was established as Girl Scout Park (Fridlington 1995: 18). The clay courts, now known as the Cranford Clay Courts, continued to host tournaments into the 1970s, but the high cost of maintaining the clay pushed the township to offer the local players the opportunity to lease the courts and establish a private club. The Cranford Clay Courts Club, Inc. was established in 1992 and continues to function every year during the months of April and November.

Statement of Significance (cont.)

The North Cranford Historic District was determined eligible for listing on the National Register of Historic Places by the NJDEP-HPO on 5/19/05. The historic district meets Criteria A for its contribution to Cranford's residential growth and development during the late 19th and early 20th century as the township transformed from a rural farming community into a suburban neighborhood for wealthy businessmen. The Rahway River and the Central Railroad of New Jersey were key factors in the success of Cranford's development. The river was a strong influence in the community, forming a part of picturesque neighborhoods and providing recreational activities for residents. The railway enabled Cranford to attract New York professional who could live in the community and commute to work in the city. The historic district is also eligible for listing in the National Register under Criterion C as a historic district with representative examples of numerous styles and types of architecture that include Queen Anne and Colonial Revival, with lesser numbers of Tudor Revival, Shingle style and Craftsman. The historic district's period of significance is 1860 to 1930 (CRCG 2001: 24). No evidence was found to suggest that the district is eligible under Criterion B for association with significant individuals, or under Criterion D for having the ability to yield information important to history and/or pre-history. Since the Cranford Clay Courts were constructed within the historic district's period of significance (1860-1930), retain their major character-defining feature (the clay courts), and falls within the district boundaries, the Cranford Clay Courts is a contributing element to the North Cranford Historic District. Originally part of the first Cranford Canoe Club, the Cranford Clay Courts are a surviving recreational amenity from one of the premier canoe clubs in Cranford that operated during a period when the Rahway River was the center for leisure and sporting activities.

Survey Name: Vanessa ZeoliDate: September 2008Surveyor: Phase II Springfield Ave. Bridge, Cranford, NJOrganization: Cultural Resource Consulting Group

CONTINUATION SHEET

Historic Sites #

Statement of Significance (cont.)

On 9/18/02 the Rahway River Parkway Historic District was determined eligible for listing on the National Register of Historic Places by NJDEP-HPO. The district meets Criterion A "for its significant historical association with the conservation movement, a movement that originated with the nineteenth century national parks system" and for its role "in an evolving social movement to develop public parkland in the face of growing urbanization" (RGA 2002: 9-16). The district also meets Criterion C for "its association with a master landscape architectural firm – Olmsted Brothers Landscape Architects of Brookline, Massachusetts" (RGA 2002: 9-17). The district's period of significance is 1921-1968. Though the Cranford Clay Courts were not specifically discussed in RGA's report, their significant contribution to recreational activities on the Rahway River in Cranford and the larger Rahway River Parkway make the courts a contributing resource to the district. Furthermore, the resource still retains its major character-defining feature and has continued to function as a tennis club up to the present day. Furthermore the Cranford Clay Courts fall within the boundaries of the Rahway River Parkway Historic District.

In October 2004, CRCG determined the Rahway River Parkway was a contributing resource to the Union County Park System Historic District and on 3/18/05 the district was determined eligible for listing on the National Register of Historic Places by NJDEP-HPO. The district meets statewide significance under Criterion A "in the category of Conservation for its substantial accomplishments in preserving New Jersey's natural resources, both within the region of Union County and in cooperation with adjacent counties, such as Essex, Somerset, Morris, and Passaic Counties." It also possesses national significance under Criterion A "for its contributions to the broad patterns of development in the areas of Community Planning, Entertainment/Recreation, and Landscape Architecture" (CRCG 2004: 134). In addition, some of the parks within the system were considered eligible under Criterion D for their capacity to yield future information pertaining to prehistory and history. No evidence was found to suggest that the district is eligible under Criterion B for association with significant individuals. The period of significance is 1921-1964. The Cranford Clay Courts are a contributing resource to the Union County Park System Historic District for the same reasons that they contribute to the Rahway River Parkway Historic District: for their significant contribution to recreational activities on the Rahway River in Cranford and the larger Rahway River Parkway, and because this resource still retains its major character-defining feature, and continues to function as a tennis club up to the present day. Furthermore, the Cranford Clay Courts fall within the boundaries of the Union County Park System Historic District.

Survey Name: Vanessa ZeoliDate: September 2008Surveyor: Phase II Springfield Ave. Bridge, Cranford, NJOrganization: Cultural Resource Consulting Group

Department of Environmental Protection
Historic Preservation Office

BASE FORM**Historic Sites #:**Property Name: 304 Springfield AvenueStreet Address: Street # 304 Apartment #Prefix: Street Name: Springfield Suffix: Type: AVECounty(s): Union Zip Code: 07016Municipality(s): Cranford Twp. 2003 Block(s): 222Local Place Name(s): Lot(s): 9Ownership: Private USGS QUAD: Roselle**Description:**

304 Springfield Avenue is a 2½-story, 2-bay, Queen Anne-style dwelling constructed circa 1890. (See Building Attachment)

Registration and Status Dates:	National Historic Landmark: <u></u>	SHPO Opinion: <u></u>
	National Register: <u></u>	Local Designation: <u></u>
	New Jersey Register: <u></u>	Other Designation: <u></u>
	Determination of Eligibility: <u></u>	Other Designation Date: <u></u>

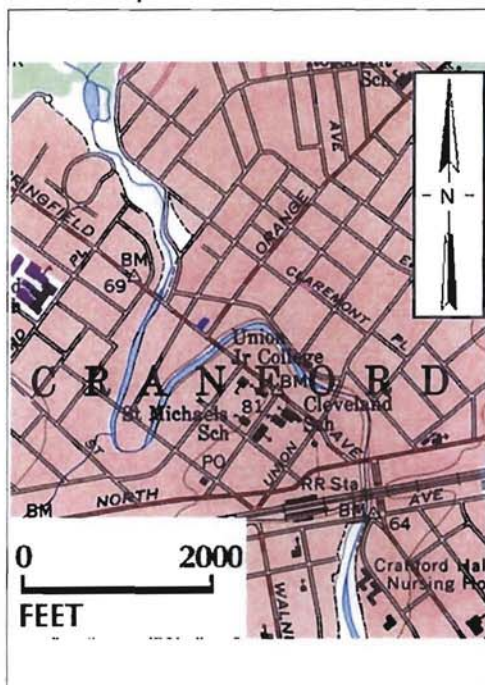
Photograph:Survey Name: Vanessa Zeoli Date: September 2008Surveyor: Phase II Springfield Ave. Bridge, Cranford, NJOrganization: Cultural Resource Consulting Group

Department of Environmental Protection
Historic Preservation Office

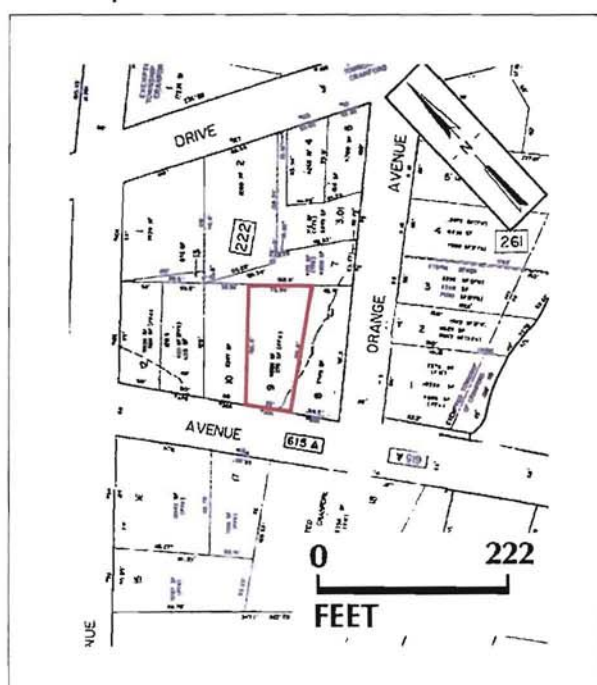
BASE SURVEY FORM

Historic Sites #

Location Map:



Site Map:



Bibliography/Sources:

USGS 7.5' Quadrangle: Roselle, N. J. 1955 (Photorevised 1981).

Tax Maps Township of Cranford, Union County New Jersey, revised 1993.

Additional Information:

More Research Needed? ☐ Yes ☒ No

INTENSIVE-LEVEL USE ONLY:

Attachments Included: ☒ Building/Element ☐ Landscape ☐ Farm
☐ Bridge ☐ Industry

Historic District: ☒ Yes

Historic District Name: _____

Status: ☐ Key Contributing ☒ Contributing ☐ Non Contributing

Associated Archaeological Site/Deposite? ☐ Yes ☒ No

(Known or potential sites - if yes, please describe briefly)

Survey Name: Vanessa Zeoli

Date: September 2008

Surveyor: Phase II Springfield Ave. Bridge, Cranford, NJ

Organization: Cultural Resource Consulting Group

New Jersey Department of Environmental Protection
Historic Preservation Office

BUILDING ATTACHMENT**Historic Sites #**

Common Name: <u>304 Springfield Avenue</u>	
Historic Name: _____	
Present Use: <u>Residential Activity, Permanent</u>	
Historic Use: <u>Residential Activity, Permanent</u>	
Construction Date: <u>1890</u>	Source: <u>1882 map, 1903 Sanborn; visual evidence</u>
Alteration Date(s): <u>late-20th century</u>	Source: <u>Visual Evidence</u>
Designer: <u>N/A</u>	Physical Condition: <u>Excellent</u>
Builder: <u>Unknown</u>	Remaining Historic Fabric: <u>Medium</u>
Style: <u>Queen Anne</u>	
Form: <u>Irregular</u>	Stories(B): <u>2.5</u>
Type: _____	Bays(B): <u>2</u>
Roof Finish Materials (B,S): <u>Asphalt Shingle</u>	
Exterior Finish Materials: <u>Asbestos Siding</u>	

Exterior Description:

304 Springfield Avenue is a 2½-story, 2-bay, Queen Anne-style dwelling constructed circa 1890. The cross-gable roof is covered with asphalt shingles and the overhanging eave is encased in vinyl. A brick chimney is located near the rear façade. The pedimented gable end and some window frames exhibit decorative woodwork including pilasters and swags. The exterior wall surface is covered in asbestos shingles and flares between the 1st and 2nd stories. Window openings contain 1/1 and diamond-pattern, synthetic sash units with wood surrounds. Windows on the front façade have inoperable synthetic shutters. The front façade contains a full-width, 1-story porch and a partial-width 2nd-story porch. Both open porches are supported by grouped Doric columns that sit on piers and contains spindled balustrade. The 1st-floor porch deck is wood and accessed by wide wood steps. The foundation is covered with stucco and the porch sits on masonry piers with lattice-work filling in the openings. The lot also contains a 1-story, 2-car automobile garage with a pyramidal roof that was constructed circa 1925. The roof is covered with asphalt shingles and the walls are clad in siding. The doors are overhead garage doors.

Interior Description:

N/A

Setting:

304 Springfield Avenue is located approximately 215 feet northwest of the bridge on the northeast side of the road. The lot has a macadam driveway, a poured concrete walkway, and is adorned with young trees, grass, and some plantings. The lot is identified as Block 222, Lot 9.

Survey Name: <u>Vanessa Zeoli</u>	Date: <u>September 2008</u>
Surveyor: <u>Phase II Springfield Ave. Bridge, Cranford, NJ</u>	
Organization: <u>Cultural Resource Consulting Group</u>	

New Jersey Department of Environmental Protection
Historic Preservation Office

ELIGIBILITY WORKSHEET

Historic Sites #

History:

Comparison between the 1882 E. Robinson map and the 1903 Sanborn map combined with visual evidence suggest that 304 Springfield Avenue was constructed circa 1890. Visual evidence also suggests the building had its windows and siding replaced.

Statement of Significance:

304 Springfield Avenue is not individually eligible for the National Register, but is eligible as a contributing resource to the North Cranford Historic District. The North Cranford Historic District was determined eligible for listing on the National Register of Historic Places by the NJDEP-HPO on 5/19/05. The historic district meets Criteria A for its contribution to Cranford's residential growth and development during the late 19th and early 20th century as the township transformed from a rural farming community into a suburban neighborhood for wealthy businessmen. (See Continuation Sheet)

Eligibility for New Jersey
and National Registers:

☐ Yes ☒ No

National

Register Criteria: ☒ A ☐ B ☒ C ☐ D

Level of Significance: ☒ Local ☐ State ☐ National

Justification of Eligibility/Ineligibility:

304 Springfield Avenue retains its integrity of design, location, feeling, workmanship, setting, and association, but has undergone alterations that have compromised its integrity of materials. These changes include replacement of the exterior wall material with asbestos shingles, installation of synthetic window sash units and synthetic inoperable shutters, and possible replacement of the roofing material. As a result, 304 Springfield Avenue is no longer able to convey any potential historic and/or design significance as an individually eligible resource; however, it is still capable of conveying its significance as a contributing resource to the North Cranford Historic District.

For Historic Districts Only:

Property Count: _____ Key Contributing: _____ Contributing: _____ Non Contributing: _____

For Individual Properties Only:

List the Completed Attachments related to the property's significance:

Narrative Boundary Description

All of the property contained in Block 222, Lot 9 in the Township of Cranford, Union County, New Jersey.

Survey Name: Vanessa Zeoli

Date: September 2008

Surveyor: Phase II Springfield Ave. Bridge, Cranford, NJ

Organization: Cultural Resource Consulting Group

CONTINUATION SHEET**Historic Sites #****Bibliography/Sources (cont.)**

Bauer, Jacob L.

1906 Atlas of Union County, New Jersey. E. Robinson & Co., New York, N.Y.

Hendrickson, Louie E.

1904 Cranford, New Jersey, Illustrated. Cranford, N.J.

Robinson, E. & R. H. Pidgeon

1882 Atlas of Union County, New Jersey. E. Robinson, New York, N.Y.

Sanborn Map Company

1903 Cranford, Union County, New Jersey. Sheet 4. On file at Rutgers University Libraries, New Brunswick, N.J.

1922 Cranford, Union County, New Jersey. Sheet 3. On file at Rutgers University Libraries, New Brunswick, N.J.

1959 Cranford, Union County, New Jersey. Sheet 3. On file at Rutgers University Libraries, New Brunswick, N.J.

1929 Cranford, Union County, New Jersey. Sheet 3. On file at Rutgers University Libraries, New Brunswick, N.J.

Sidney, J. C.

1850 Map of Essex County, New Jersey. Hiram A. Belding, Newark, N.J.

Statement of Significance (cont.)

The Rahway River and the Central Railroad of New Jersey were key factors in the success of Cranford's development. The river was a strong influence in the community, forming a part of picturesque neighborhoods and providing recreational activities for residents. The railway enabled Cranford to attract New York professional who could live in the community and commute to work in the city. The historic district is also eligible for listing in the National Register under Criterion C as a historic district with representative examples of numerous styles and types of architecture that include Queen Anne and Colonial Revival, with lesser numbers of Tudor Revival, Shingle style and Craftsman. The historic district's period of significance is 1860 to 1930 (CRCG 2001: 24). No evidence was found to suggest that the district is eligible under Criterion B for association with significant individuals, or under Criterion D for having the ability to yield information important to history and/or pre-history. Since 304 Springfield Avenue was constructed within the historic district's period of significance (1860-1930), retains the majority of its character-defining features, and falls within the district's boundaries, it is a contributing element to the North Cranford Historic District.

Survey Name: Vanessa ZeoliDate: September 2008Surveyor: Phase II Springfield Ave. Bridge, Cranford, NJOrganization: Cultural Resource Consulting Group

CONTINUATION SHEET

Historic Sites #



Garage

Survey Name: Vanessa ZeoliDate: September 2008Surveyor: Phase II Springfield Ave. Bridge, Cranford, NJOrganization: Cultural Resource Consulting Group

Department of Environmental Protection
Historic Preservation Office

BASE FORM**Historic Sites #:**Property Name: 306 Springfield AvenueStreet Address: Street # 306 Apartment #Prefix: Street Name: Springfield Suffix: Type: AVECounty(s): Union Zip Code: 07016Municipality(s): Cranford Twp. 2003 Block(s): 222Local Place Name(s): Lot(s): 20Ownership: Private USGS QUAD: Roselle**Description:**

306 Springfield Avenue is a 1½-story, 3-bay, Dutch Colonial Revival dwelling constructed circa 1925. (See Building Attachment)

Registration and Status Dates:	National Historic Landmark: <u></u>	SHPO Opinion: <u></u>
	National Register: <u></u>	Local Designation: <u></u>
	New Jersey Register: <u></u>	Other Designation: <u></u>
	Determination of Eligibility: <u></u>	Other Designation Date: <u></u>

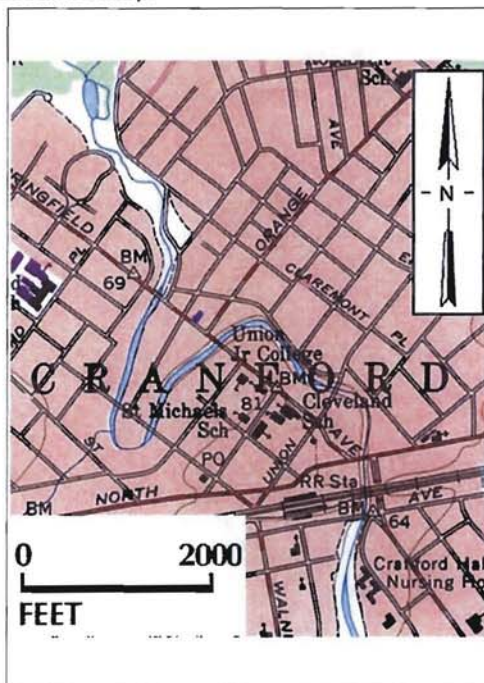
Photograph:Survey Name: Vanessa Zeoli Date: September 2008Surveyor: Phase II Springfield Ave. Bridge, Cranford, NJOrganization: Cultural Resource Consulting Group

Department of Environmental Protection
Historic Preservation Office

BASE SURVEY FORM

Historic Sites #

Location Map:



Site Map:



Bibliography/Sources:

USGS 7.5' Quadrangle: Roselle, N. J. 1955 (Photorevised 1981).
Tax Maps Township of Cranford, Union County New Jersey, revised 1993.

Additional Information:

More Research Needed? ☐ Yes ☒ No

INTENSIVE-LEVEL USE ONLY:

Attachments Included: ☒ Building/Element ☐ Landscape ☐ Farm
☐ Bridge ☐ Industry

Historic District: ☒ Yes Historic District Name: North Cranford Historic District

Status: ☐ Key Contributing ☒ Contributing ☐ Non Contributing

Associated Archaeological Site/Deposite? ☐ Yes ☒ No

(Known or potential sites - if yes, please describe briefly)

Survey Name: Vanessa Zeoli

Date: September 2008

Surveyor: Phase II Springfield Ave. Bridge, Cranford, NJ

Organization: Cultural Resource Consulting Group

New Jersey Department of Environmental Protection
Historic Preservation Office

BUILDING ATTACHMENT**Historic Sites #****Common Name:** 306 Springfield Avenue**Historic Name:** _____**Present Use:** Residential Activity, Permanent**Historic Use:** Residential Activity, Permanent**Construction Date:** 1925 **Source:** 1922 and 1929 Sanborn maps; visual evidence**Alteration Date(s):** late 20th-century **Source:** Visual Evidence**Designer:** N/A**Physical Condition:** Good**Builder:** Unknown**Remaining Historic Fabric:** Medium**Style:** Dutch Colonial Revival**Form:** Other**Stories(B):** 1.5**Type:** _____**Bays(B):** 3**Roof Finish Materials (B,S):** Asphalt Shingle**Exterior Finish Materials:** Vinyl Siding**Exterior Description:**

306 Springfield Avenue is a 1½-story, 3-bay, Dutch Colonial Revival dwelling constructed circa 1925. The gambrel roof is covered with asphalt shingles and exhibits a slight return on the gable ends. An interior gable end chimney is located on the southeast end. The exterior wall surface is clad in vinyl siding. Window openings on the 1st floor contain two Palladian-style windows with multi-pane wood casements and a wood fanlight. The shed dormer contains multi-pane-over-one wood sash units with inoperable shutters. The main entrance accesses an enclosed vestibule with a front-gable roof exhibiting a partial eave return. The wood panel door is flanked by multi-pane sidelights and topped with a fanlight. The entrance is accessed by masonry steps and a metal railing. On the southeast elevation is a 1-story, flat-roof wing with a balustrade around the perimeter of the roofline. The roof has a wide overhang. Windows openings contain 6/6 synthetic sash units. The foundation is covered with stucco. The lot also contains a 1-story, single-car garage that was constructed circa 1925. The front gable roof is clad in asphalt shingles, the gable end is covered with wood shingles, and the exterior walls are clad in siding. The auto entrance has an overhead garage door.

Interior Description:N/A**Setting:**

306 Springfield Avenue is located approximately 260 feet northwest of the bridge on the northeast side of the road. The lot has a macadam driveway, walkway of decorative pavers, several mature and young trees, and shrubs along the front façade. It is situated close to the road and situated on Block 222, Lot, 10.

Survey Name: Vanessa Zeoli**Date:** September 2008**Surveyor:** Phase II Springfield Ave. Bridge, Cranford, NJ**Organization:** Cultural Resource Consulting Group

New Jersey Department of Environmental Protection
Historic Preservation Office

ELIGIBILITY WORKSHEET

Historic Sites #

History:

Comparison between the 1922 and 1929 Sanborn maps combined with visual evidence suggest that 306 Springfield Avenue was constructed circa 1925. Visual evidence also suggests the building had its siding replaced.

Statement of Significance:

306 Springfield Avenue is not individually eligible for the National Register, but is eligible as a contributing resource to the North Cranford Historic District. The North Cranford Historic District was determined eligible for listing on the National Register of Historic Places by the NJDEP-HPO on 5/19/05. The historic district meets Criteria A for its contribution to Cranford's residential growth and development during the late 19th and early 20th century as the township transformed from a rural farming community into a suburban neighborhood for wealthy businessmen. (See Continuation Sheet)

Eligibility for New Jersey
and National Registers:

☐ Yes ☒ No

National

Register Criteria: ☒ A ☐ B ☒ C ☐ D

Level of Significance: ☒ Local ☐ State ☐ National

Justification of Eligibility/Ineligibility:

306 Springfield Avenue retains its integrity of design, location, feeling, workmanship, setting, and association, but has undergone alterations that have compromised its integrity of materials. These changes include replacement of the exterior wall material with vinyl siding, installation of some synthetic window sash units, and possible replacement of the roofing material. As a result, 306 Springfield Avenue is no longer able to convey any potential historic and/or design significance as an individually eligible resource; however, it is still capable of conveying its significance as a contributing resource to the North Cranford Historic District.

For Historic Districts Only:

Property Count: _____ Key Contributing: _____ Contributing: _____ Non Contributing: _____

For Individual Properties Only:

List the Completed Attachments related to the property's significance:

Narrative Boundary Description

All of the property contained in Block 222, Lot 20 in the Township of Cranford, Union County, New Jersey.

Survey Name: Vanessa Zeoli

Date: September 2008

Surveyor: Phase II Springfield Ave. Bridge, Cranford, NJ

Organization: Cultural Resource Consulting Group

CONTINUATION SHEET

Historic Sites #

Bibliography/Sources (cont.)

Bauer, Jacob L.

1906 Atlas of Union County, New Jersey. E. Robinson & Co., New York, N.Y.

Hendrickson, Louie E.

1904 Cranford, New Jersey, Illustrated. Cranford, N.J.

Robinson, E. & R. H. Pidgeon

1882 Atlas of Union County, New Jersey. E. Robinson, New York, N.Y.

Sanborn Map Company

1903 Cranford, Union County, New Jersey. Sheet 4. On file at Rutgers University Libraries, New Brunswick, N.J.

1922 Cranford, Union County, New Jersey. Sheet 3. On file at Rutgers University Libraries, New Brunswick, N.J.

1959 Cranford, Union County, New Jersey. Sheet 3. On file at Rutgers University Libraries, New Brunswick, N.J.

1929 Cranford, Union County, New Jersey. Sheet 3. On file at Rutgers University Libraries, New Brunswick, N.J.

Sidney, J. C.

1850 Map of Essex County, New Jersey. Hiram A. Belding, Newark, N.J.

Statement of Significance (cont.)

The Rahway River and the Central Railroad of New Jersey were key factors in the success of Cranford's development. The river was a strong influence in the community, forming a part of picturesque neighborhoods and providing recreational activities for residents. The railway enabled Cranford to attract New York professional who could live in the community and commute to work in the city. The historic district is also eligible for listing in the National Register under Criterion C as a historic district with representative examples of numerous styles and types of architecture that include Queen Anne and Colonial Revival, with lesser numbers of Tudor Revival, Shingle style and Craftsman. The historic district's period of significance is 1860 to 1930 (CRCG 2001: 24). No evidence was found to suggest that the district is eligible under Criterion B for association with significant individuals, or under Criterion D for having the ability to yield information important to history and/or pre-history. Since 306 Springfield Avenue was constructed within the historic district's period of significance (1860-1930), retains the majority of its character-defining features, and falls within the district's boundaries, it is a contributing element to the North Cranford Historic District.

Survey Name: Vanessa ZeoliDate: September 2008Surveyor: Phase II Springfield Ave. Bridge, Cranford, NJOrganization: Cultural Resource Consulting Group

A-447

CONTINUATION SHEET

Historic Sites #



Garage

Survey Name: Vanessa ZeoliDate: September 2008Surveyor: Phase II Springfield Ave. Bridge, Cranford, NJOrganization: Cultural Resource Consulting Group

Department of Environmental Protection
Historic Preservation Office

BASE FORM**Historic Sites #:****Property Name:** 320 Springfield Avenue**Street Address:** Street # 320Apartment #**Prefix:** Street Name: Springfield**Suffix:** Type: AVE**County(s):** Union**Zip Code:** 07016**Municipality(s):** Cranford Twp. 2003**Block(s):** 222**Local Place Name(s):****Lot(s):** 11**Ownership:** Private**USGS QUAD:** Roselle**Description:**

320 Springfield Avenue is a 2½-story, 3-bay, dwelling constructed circa 1880. (See Building Attachment)

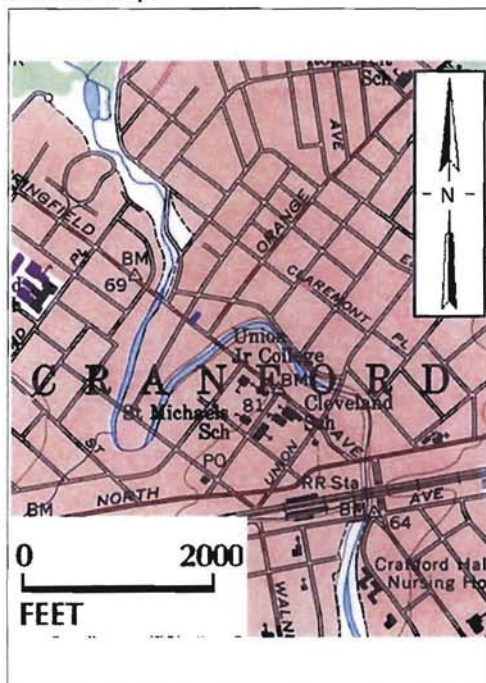
**Registration and
Status Dates:****National Historic
Landmark:****SHPO Opinion:****National Register:****Local Designation:****New Jersey Register:****Other Designation:****Determination of Eligibility:****Other Designation Date:****Photograph:****Survey Name:** Vanessa Zeoli**Date:** September 2008**Surveyor:** Phase II Springfield Ave. Bridge, Cranford, NJ**Organization:** Cultural Resource Consulting Group

Department of Environmental Protection
Historic Preservation Office

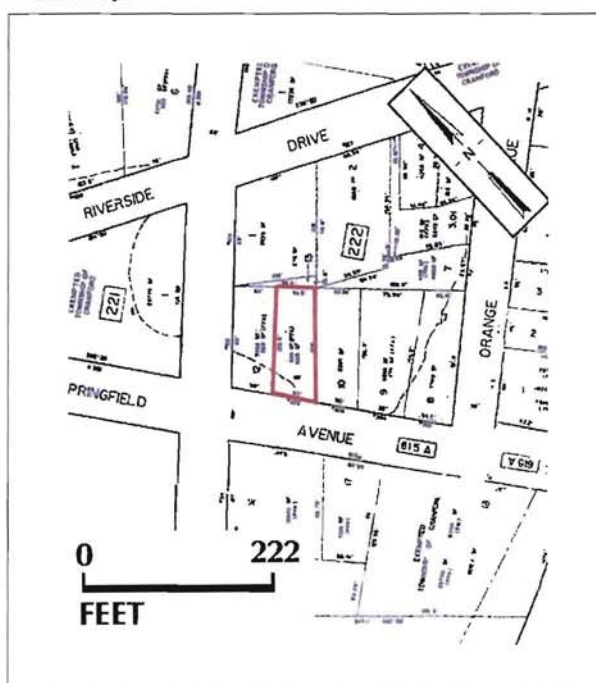
BASE SURVEY FORM

Historic Sites #

Location Map:



Site Map:



Bibliography/Sources:

USGS 7.5' Quadrangle: Roselle, N. J. 1955 (Photorevised 1981).
Tax Maps Township of Cranford, Union County New Jersey, revised 1993.

Additional Information:

More Research Needed? ☐ Yes ☒ No

INTENSIVE-LEVEL USE ONLY:

Attachments Included: ☒ Building/Element ☐ Landscape ☐ Farm
☐ Bridge ☐ Industry

Historic District: ☒ Yes Historic District Name: North Cranford Historic District

Status: ☐ Key Contributing ☒ Contributing ☐ Non Contributing

Associated Archaeological Site/Deposite? ☐ Yes ☒ No

(Known or potential sites - if yes, please describe briefly)

Survey Name: Vanessa Zeoli

Date: September 2008

Surveyor: Phase II Springfield Ave. Bridge, Cranford, NJ

Organization: Cultural Resource Consulting Group

New Jersey Department of Environmental Protection
Historic Preservation Office

BUILDING ATTACHMENT**Historic Sites #**

Common Name: 320 Springfield Avenue

Historic Name: _____

Present Use: Residential Activity, Permanent

Historic Use: Residential Activity, Permanent

Construction Date: 1880 **Source:** 1882 E. Robinson map; Visual evidence

Alteration Date(s): late 20th-century **Source:** Visual Evidence

Designer: N/A **Physical Condition:** Good

Builder: Unknown **Remaining Historic Fabric:** Low

Style: Queen Anne

Form: Irregular **Stories(B):** 2.5

Type: _____ **Bays(B):** 2

Roof Finish Materials (B,S): Asphalt Shingle

Exterior Finish Materials: Vinyl Siding

Exterior Description:

320 Springfield Avenue is a 2½-story, 3-bay, dwelling constructed circa 1880. The cross-gable roof is clad in asphalt shingles and the overhanging eave is encased in vinyl. A brick corbelled chimney rises from the southeast roof slope. The exterior walls are clad in vinyl siding. Most window openings contain 6/1 vinyl sash units, but those on the 1st floor the front façade contain large 2/2 synthetic units. The front (southwest) façade has a 1-story, wrap-around porch with a hipped roof and supported by square posts encased in vinyl and vinyl-covered half-walls. The main entry door is obscured by paired, aluminum storm doors. A 1-story, 3-sided bay window is located on the southeast elevation. In addition, a long 1-story 5-bay addition extends from the rear (northeast) elevation. The foundation is covered with stucco.

Interior Description:

N/A

Setting:

320 Springfield Avenue is located approximately 330 feet northwest of the bridge on the northeast side of the road. The lot contains a macadam driveway, a walkway of concrete pavers, young and mature trees, and shrubbery along the front façade. It is situated close to the road and situated on Block 222, Lot, 11.

Survey Name: Vanessa Zeoli

Surveyor: Phase II Springfield Ave. Bridge, Cranford, NJ

Organization: Cultural Resource Consulting Group

Date: September 2008

New Jersey Department of Environmental Protection
Historic Preservation Office

ELIGIBILITY WORKSHEET

Historic Sites #

History:

Comparison between the 1850 Sidney map and 1882 E. Robinson map combined with visual evidence suggest that 320 Springfield Avenue was constructed circa 1880. Visual evidence also suggests the building had its windows and siding replaced.

Statement of Significance:

320 Springfield Avenue is not individually eligible for the National Register, but is eligible as a contributing resource to the North Cranford Historic District. The North Cranford Historic District was determined eligible for listing on the National Register of Historic Places by the NJDEP-HPO on 5/19/05. The historic district meets Criteria A for its contribution to Cranford's residential growth and development during the late 19th and early 20th century as the township transformed from a rural farming community into a suburban neighborhood for wealthy businessmen. (See Continuation Sheet)

Eligibility for New Jersey
and National Registers:

☐ Yes ☒ No

National

Register Criteria: ☒ A ☐ B ☒ C ☐ D

Level of Significance: ☒ Local ☐ State ☐ National

Justification of Eligibility/Ineligibility:

320 Springfield Avenue retains its integrity of location, feeling, workmanship, setting, and association, but has undergone alterations that have compromised its integrity of design and materials. These changes include the massive 1-story addition on the rear elevation, replacement of the exterior wall material with vinyl siding, installation of synthetic window sash units, and possible replacement of the roofing material. As a result, 320 Springfield Avenue is no longer able to convey any potential historic and/or design significance as an individually eligible resource; however, it is still capable of conveying its significance as a contributing resource to the North Cranford Historic District.

For Historic Districts Only:

Property Count: _____ Key Contributing: _____ Contributing: _____ Non Contributing: _____

For Individual Properties Only:

List the Completed Attachments related to the property's significance:

Narrative Boundary Description

All of the property contained in Block 222, Lot 11 in the Township of Cranford, Union County, New Jersey.

Survey Name: Vanessa Zeoli

Date: September 2008

Surveyor: Phase II Springfield Ave. Bridge, Cranford, NJ

Organization: Cultural Resource Consulting Group

CONTINUATION SHEET

Historic Sites #

Bibliography/Sources (cont.)

Bauer, Jacob L.

1906 Atlas of Union County, New Jersey. E. Robinson & Co., New York, N.Y.

Hendrickson, Louie E.

1904 Cranford, New Jersey, Illustrated. Cranford, N.J.

Robinson, E. & R. H. Pidgeon

1882 Atlas of Union County, New Jersey. E. Robinson, New York, N.Y.

Sanborn Map Company

1903 Cranford, Union County, New Jersey. Sheet 4. On file at Rutgers University Libraries, New Brunswick, N.J.

1922 Cranford, Union County, New Jersey. Sheet 3. On file at Rutgers University Libraries, New Brunswick, N.J.

1959 Cranford, Union County, New Jersey. Sheet 3. On file at Rutgers University Libraries, New Brunswick, N.J.

1929 Cranford, Union County, New Jersey. Sheet 3. On file at Rutgers University Libraries, New Brunswick, N.J.

Sidney, J. C.



1850 Map of Essex County, New Jersey. Hiram A. Belding, Newark, N.J.

Statement of Significance (cont.)

The Rahway River and the Central Railroad of New Jersey were key factors in the success of Cranford's development. The river was a strong influence in the community, forming a part of picturesque neighborhoods and providing recreational activities for residents. The railway enabled Cranford to attract New York professional who could live in the community and commute to work in the city. The historic district is also eligible for listing in the National Register under Criterion C as a historic district with representative examples of numerous styles and types of architecture that include Queen Anne and Colonial Revival, with lesser numbers of Tudor Revival, Shingle style and Craftsman. The historic district's period of significance is 1860 to 1930 (CRCG 2001: 24). No evidence was found to suggest that the district is eligible under Criterion B for association with significant individuals, or under Criterion D for having the ability to yield information important to history and/or pre-history. Since 320 Springfield Avenue was constructed within the historic district's period of significance (1860-1930), retains the majority of its character-defining features, and falls within the district's boundaries, it is a contributing element to the North Cranford Historic District.

Survey Name: Vanessa ZeoliDate: September 2008Surveyor: Phase II Springfield Ave. Bridge, Cranford, NJOrganization: Cultural Resource Consulting Group

A-453

Inventory of Identified Architectural Resources			
Property	Property Information	Description	NRHP Eligibility
 <p>Photograph Number: 29</p>	<p>Panamerican Resource Number: 29</p> <p>Memorial Park</p> <p>Springfield Avenue at Riverside Drive</p> <p>Municipality: Cranford, Union County</p> <p>UTM coordinates (1983 Zone 18N): 4501392N, 558522E</p> <p>Date: Unknown</p> <p>Other: Borough of Cranford park</p>	<p>Style: vest pocket park</p> <p>Stories: na</p> <p>Plan: formal layout with monuments and landscape furniture</p> <p>Roof: na</p> <p>Exterior: na</p> <p>Foundation: na</p> <p>Windows: na</p> <p>Details: This small park is part of the larger interconnecting Rahway River Parkway HD and the North Cranford HD</p>	<p>Memorial Park is eligible for the NRHP as a contributing element to the NRE North Cranford HD (ID #3838) and NRE Rahway River Parkway HD (ID #4079).</p>
 <p>Photograph Number: 30</p>	<p>Panamerican Resource Number: 30</p> <p>Bridge (NJDOT #2003072)</p> <p>Springfield Avenue at Riverside Drive over Rahway River</p> <p>Municipality: Cranford, Union County</p> <p>UTM coordinates (1983 Zone 18N): 4501401N, 558464E</p> <p>Date: 2003</p> <p>Other: Union County bridge; surveyed since it lies directly in the flood plain</p>	<p>Style: rustic with classical details</p> <p>Stories: na</p> <p>Plan: na</p> <p>Roof: na</p> <p>Exterior: concrete with stone enclosed balustrade topped by decorative metal railing</p> <p>Foundation: concrete abutments on rubble</p> <p>Windows: na</p> <p>Details: This bridge is built on the site of an earlier bridge and at the site of Crane's Ford, for which the municipality is named. Designed by Richard A. Alamo Engineering Co. and erected by Rencor Inc.</p>	<p>Although this bridge is not yet 50 years old, it should be considered a contributing element to both the NRE Rahway River Parkway HD (ID #4079), NRE Union County Park System HD (ID #4424), and NRE North Cranford HD (ID #3838).</p>

NEW JERSEY OFFICE OF HISTORIC PRESERVATION HISTORIC SITES INVENTORY # 200310
INDIVIDUAL STRUCTURE SURVEY FORMHISTORIC NAME: Dykeman Farm House
LOCATION: 400 Springfield Ave.COMMON NAME:
BLOCK/LOT 200/48MUNICIPALITY: Cranford
USGS QUAD: Roselle
OWNER/ADDRESS: Brooks, Oliver & Helen
400 Springfield Ave.
CranfordCOUNTY: Union
UTM REFERENCES:
Zone/Northing/Easting

DESCRIPTION

Construction Date: 1929

Source of Date: Documentation

Architect:

Builder:

Style: Dutch Colonial

Form/Plan Type: Central hall

Number of Stories: 2

Foundation: Stone

Exterior Wall Fabric: Clapboard and stone

Fenestration: 3 x 2 bays; 6/6

Roof/Chimneys: Gambrel roof with bell cast eaves. Three brick chimneys.

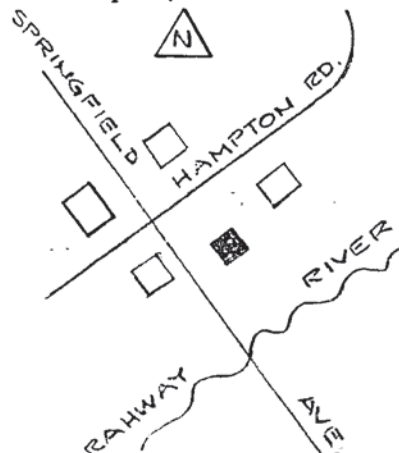
Additional Architectural Description:

Gabled wing, one-and-one-half-story to N.W. elevation.
Lean to off wing (Roof/Chim) - Three gabled dormers with
6/6 windows

PHOTO Negative File # Roll 8 frame 31,32



Map (Indicate North)



A-455

The house sits on a lot bounded by Springfield Ave., Hampton Rd., and the Rahway River. The house lies approximately 50' from Springfield Ave. and 60' from the river.

SURROUNDING ENVIRONMENT: Urban ☐ Suburban ☒ Scattered Buildings ☐
Open Space ☐ Woodland ☐ Residential ☒ Agricultural ☐
Industrial ☐ Downtown Commercial ☐ Highway Commercial ☐ Other ☐

SIGNIFICANCE:

The house is a copy of the historic Dykeman Farm House, built in New York City in 1783. The original Dykeman House was recorded by HABS.

The main entry is sheltered by a porch created by the overhang roof.

In the original Dykeman House, a ca. 1725 bakehouse was joined to the main building. This wing addition is retained in the reproduction. It has an exposed stone end wall, and is 1½ stories tall, with two bays on the facade.

ORIGINAL USE: Residence

PHYSICAL CONDITION: Excellent ☒ Good ☐

REGISTER ELIGIBILITY: Yes ☐ Possible ☒

THREATS TO SITE: Roads ☐ Development ☐

No Threat ☒ Other ☐

COMMENTS:

The height of period-house or revival mania - an exact copy of an historic building. This one was well done, and has been well preserved.

PRESENT USE: Residence

Fair ☐ Poor ☐

No ☒ Part of District ☐

Zoning ☐ Deterioration ☐

REFERENCES:

Cranford Historical Survey.

Carole Rifkind. A Field Guide to American Architecture. 1980.

RECORDED BY: WJF
ORGANIZATION: UCCHPAB

DATE: 1981



400 SPRINGFIELD

CRANFORD

copy of Dykman House

1929

#200310

r. 8/32

looking N.E.
400 Springfield
Cranford / Union Co.

Photo: 1981

Roll 8



400 SPRINGFIELD

CRANFORD

copy of Dykmann House



1929
* 200310

5.8/31

looking S.E.

400 Springfield
Cranford / Union Co

Roll 8

Inventory of Identified Architectural Resources			
Property	Property Information	Description	NRHP Eligibility
 <p>Photograph Number: 43</p>	<p>Panamerican Resource Number: 43</p> <p>400 Springfield Avenue</p> <p>Municipality: Cranford, Union County</p> <p>UTM coordinates (1983 Zone 18N): 4501447N, 558457E</p> <p>Date: ca. 1935</p> <p>Other: At the edge of the Rahway River Parkway</p>	<p>Style: Colonial Revival</p> <p>Stories: 1.5</p> <p>Plan: rectangular</p> <p>Roof: gambrel</p> <p>Exterior: wood shingles</p> <p>Foundation: concrete</p> <p>Windows: mixture of replacement types</p> <p>Details: This house has been much altered with a new front porch, new window placement on the first story front façade, the addition of a garage and small storage building, and the addition of decorative shutters.</p>	Not eligible, lacks integrity
 <p>Photograph Number: 44</p>	<p>Panamerican Resource Number: 44</p> <p>411 Springfield Avenue</p> <p>Municipality: Cranford, Union County</p> <p>UTM coordinates (1983 Zone 18N): 4501400N, 558425E</p> <p>Date: ca. 1910</p> <p>Other: Right on the edge of the Rahway River, and thus the Rahway River Parkway HD</p>	<p>Style: Vernacular Victorian</p> <p>Stories: 2.5</p> <p>Plan: rectangular</p> <p>Roof: multiple types</p> <p>Exterior: wood shingles</p> <p>Foundation: obscured; garage—concrete</p> <p>Windows: 1/1 covered with storm windows, fixed modern arched window; some window surrounds have pieces of dentil molding</p> <p>Details: House has second story incised porch. It has been extensively altered and is missing most of its decorative detailing. The decorative detailing now on the house in most cases has been added and is wrong for the style of the house.</p>	Not eligible, lacks integrity

NEW JERSEY HISTORIC BRIDGE SURVEY

STRUCTURE #: 2003016 COUNTY: UNION OWNER: COUNTY ROUTE: 9020

MILEPOINT: 000000

TOWNSHIP: CRANFORD TOWNSHIP

FACILITY CARRIED: NORTH UNION AVENUE

NAME/FEATURE INTERSECTED: NORTH UNION AVENUE OVER RAHWAY RIVER

TYPE: DECK ARCH

DESIGN: ELLIPTICAL

MATERIAL: REINFORCED CONCRETE # SPANS: 001 LENGTH: 000120 WIDTH: 0400

DATE OF CONSTRUCTION: 1916 ALTERATION: SOURCE: PLAQUE

DESIGNER/PATENT: JACOB L. BAUER, COUNTY ENGINEER BUILDER: WELDON CONTRACTING
COMPANY

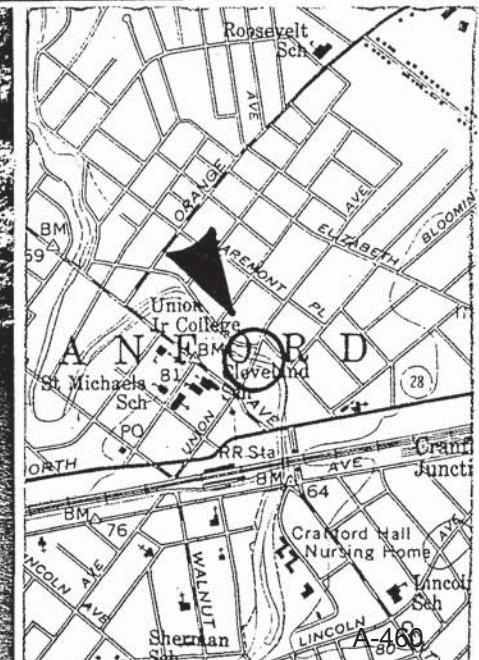
SETTING/CONTEXT: The bridge is located in Sperry Park, a casually landscaped Union County park that is part of a greenway along the Rahway River. The bridge carries a two-lane, two sidewalk street over the river. The surrounding neighborhood is dominated by post-WW II residences and commercial structures. The park does not have landscape architecture significance.

CURRENT NATIONAL REGISTER STATUS: Not Previously Evaluated



NATIONAL REGISTER RECOMMENDATION: Not Eligible

SUMMARY: The single reinforced concrete arch bridge has been rehabilitated with gunite on the fascias as well as the intrados. The repairs do not detract from the structural integrity. The original metal railings that cross the bridge are flanked at the approaches by concrete parapets. The 1916 bridge is a representative example of a common bridge type from the early-20th century, and is one of 9 pre-World War II deck arches in the county. The bridge is not technologically or historically distinguished.

PHOTO: 157:34 REVIEWED BY: EPV/AGL DATE: 05/92 QUAD: Roselle





Inventory of Identified Architectural Resources			
Property	Property Information	Description	NRHP Eligibility
 <p>Photograph Number: 96</p>	<p>Panamerican Resource Number: 96</p> <p>Flood Control Dam</p> <p>Rahway River, Sperry Park Riverside Drive</p> <p>Municipality: Cranford, Union County</p> <p>UTM coordinates (1983 Zone 18N): 4501118N, 558961E</p> <p>Date: unknown</p> <p>Other:</p>	<p>Style: none</p> <p>Stories: na</p> <p>Plan: na</p> <p>Roof: na</p> <p>Exterior: concrete</p> <p>Foundation: concrete</p> <p>Windows: na</p> <p>Details: Concrete dam that can be opened and closed to regulate the flow of water in this section of the river.</p>	<p>This structure is not eligible. It is a part of county and/or municipal infrastructure used to control water flow of the Rahway River. Control of flood waters is an important part of governmental responsibility in this part of New Jersey, but the dam does not represent any new technology or special comment (see PRN 118).</p>
 <p>Photograph Number: 97 (north balustrade)</p>	<p>Panamerican Resource Number: 97</p> <p>Bridge (NJDOT #2003016) on Union Avenue crossing the Rahway River at Sperry Park</p> <p>Municipality: Cranford, Union County</p> <p>UTM coordinates (1983 Zone 18N): 4501096N, 559030E</p> <p>Date: 1916</p> <p>Other: Portions of this bridge have been replaced or added to.</p>	<p>Style: Concrete single arch</p> <p>Stories: na</p> <p>Plan: na</p> <p>Roof: na</p> <p>Exterior: combination of closed concrete balustrades with sunk panels and open metal work balustrades</p> <p>Foundation: concrete abutments</p> <p>Windows: na</p> <p>Details: Bridge was rehabilitated with new railings added to some portions. The bridge was designed to blend with the park and the neighborhood. A plaque indicates that the 1916 bridge was erected by Weldon Contracting Co. and the County Engineer was Jacob L. Bauer.</p>	<p>This bridge is eligible as a contributing element to the NRE Rahway River Parkway HD (ID #4079), NRE Union County Park System HD (ID #4424), and NRE North Cranford HD (ID #3838). The A.G. Lichtenstein & Associates survey (1994) recommended it as not eligible.</p>

NEW JERSEY OFFICE OF HISTORIC PRESERVATION HISTORIC SITES INVENTORY # 200320
INDIVIDUAL STRUCTURE SURVEY FORM

HISTORIC NAME: Smith Building
LOCATION: 13-15 N. Union Ave.

COMMON NAME: Robinson's
BLOCK/LOT 189/19

MUNICIPALITY: Cranford
USGS QUAD: Roselle
OWNER/ADDRESS: Robinson, Ellis & Florence
13-15 N. Union Ave., Cranford

COUNTY: Union
UTM REFERENCES:
Zone/Northing/Easting

DESCRIPTION

Construction Date: ca. 1915

Source of Date: Visual/maps.

Architect:

Builder:

Style: Late Victorian - Commercial

Form/Plan Type: Rectangular

Number of Stories: 3

Foundation: Not visible

Exterior Wall Fabric: Yellow brick with pink brick accents.

Fenestration: Two bay - Two story bay windows containing 6/6 sash windows.

Roof/Chimneys: Flat roof behind wooden cornice.

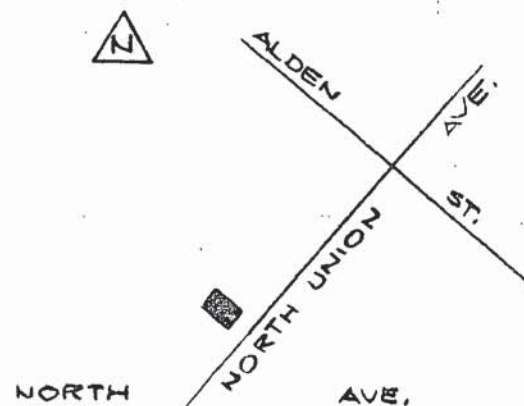
Additional Architectural Description:

First floor storefront altered.

PHOTO Negative File # Roll 43 Frame 1



Map (Indicate North)



SETTING, BOUNDARY DESCRIPTION, AND RELATED STRUCTURES:

The building faces Union Avenue, once the "main street" of Cranford.

SURROUNDING ENVIRONMENT: Urban ☐ Suburban ☐ Scattered Buildings ☐
Open Space ☐ Woodland ☐ Residential ☐ Agricultural ☐
Industrial ☐ Downtown Commercial ☒ Highway Commercial ☐ Other ☐

Located in the center of the 19th Century commercial district of Cranford.
This area is now declining somewhat.

SIGNIFICANCE:

The Smith building, and adjacent Reusch building (200321) are survivors of Cranford's heyday as a booming suburban town. The Smith Building is simply styled, and the few decorative details on it would more likely place it in the 19th Century, rather than the 20th. However, maps of the area and photographs indicate it was built about 1915, thus showing the conservatism of architecture, particularly in non-urban areas.

Phase 1 ID: 150

ORIGINAL USE: Store/Apartment	PRESENT USE: Store/Apartment
PHYSICAL CONDITION: Excellent <input type="checkbox"/> Good <input checked="" type="checkbox"/>	Fair <input type="checkbox"/> Poor <input type="checkbox"/>
REGISTER ELIGIBILITY: Yes <input type="checkbox"/> Possible <input checked="" type="checkbox"/>	No <input checked="" type="checkbox"/> Part of District <input type="checkbox"/>
THREATS TO SITE: Roads <input type="checkbox"/> Development <input type="checkbox"/>	Zoning <input type="checkbox"/> Deterioration <input type="checkbox"/>
No Threat <input type="checkbox"/> Other <input checked="" type="checkbox"/>	
COMMENTS:	

Incompatible remodelling of the facade and storefronts has robbed the building of some integrity already. Further misguided efforts could destroy so simple a structure.

REFERENCES:

Cranford Historical Society

RECORDED BY: JWF
ORGANIZATION: UCCHPAB

DATE: 1981



CRANFORD

UNION CO.

ROBINSON

~~ROBINSON~~ Bldg

13-15 ~~St~~ N. UNION

looking west

200320

NEW JERSEY OFFICE OF HISTORIC PRESERVATION HISTORIC SITES INVENTORY # 200321
INDIVIDUAL STRUCTURE SURVEY FORMHISTORIC NAME: Reusch Building
LOCATION: 17 No. Union Ave.COMMON NAME: Bell's Pharmacy
BLOCK/LOT 189/18MUNICIPALITY: Cranford
USGS QUAD: Roselle
OWNER/ADDRESS: Krasner, Sanford & Harriet
89 Lowell Ave., West Orange, NJCOUNTY: Union
UTM REFERENCES: Zone/Northing/Easting

DESCRIPTION

Construction Date: ca. 1912-1914

Source of Date: Visual, maps

Architect:

Builder:

Style: Revival - Classical
Commerical Building

Form/Plan Type: Rectangular

Number of Stories: Three

Foundation:

Exterior Wall Fabric: Yellow brick with sand color accents.

Fenestration: Three bay.

Roof/Chimneys: Flat roof marked by bracketed cornice. Applied swag-motif decoration in architrave.

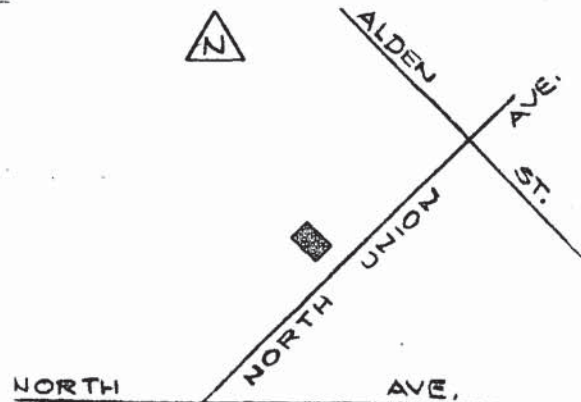
Additional Architectural Description:

Central bays are single 1/1 sash windows. Third floor window has an arched stone hood mold with classical swag motif carved in it. Side bays on upper floors are two story bay windows, each with an ornately decorated pediment on top.

First floor storefront altered for commercial purposes.

PHOTO Negative File # Roll 33 frame 31A

Map (Indicate North)



SITING, BOUNDARY DESCRIPTION, AND RELATED STRUCTURES:

The building faces Union Avenue, once the "main street" of Cranford.

SURROUNDING ENVIRONMENT: Urban ☐ Suburban ☐ Scattered Buildings ☐
Open Space ☐ Woodland ☐ Residential ☐ Agricultural ☐
Industrial ☐ Downtown Commercial ☒ Highway Commercial ☐ Other ☐

Located in the center of the 19th Century commercial district of Cranford.
This area is now declining somewhat.

SIGNIFICANCE:

The Reusch Building, and adjacent Smith Building (200320) are survivors of Cranford's heyday as a booming suburban town.

The Reusch Building is a good example of the Classical Revival Style, popular at the turn of the century, and used for many homes in Cranford, grafted onto the standard commercial building.

ORIGINAL USE: Store/Apartments

PHYSICAL CONDITION: Excellent ☐ Good ☒

REGISTER ELIGIBILITY: Yes ☐ Possible ☒

THREATS TO SITE: Roads ☐ Development ☐

No Threat ☐ Other ☒

COMMENTS:

PRESENT USE: Store/Apartments

Fair ☐ Poor ☐

No ☒ Part of District ☐

Zoning ☐ Deterioration ☐

Incompatible remodelling of the facade could hide or destroy much of the detailing which gives this building charm.

REFERENCES:

Cranford Historical Society

RECORDED BY: JWF
ORGANIZATION: UCCHPAB

DATE: 1981



CRANFORD
Union Co.

* 200321
R 33/31

Reusch Bldg.
17 N. Union Ave
looking W.

1981

NEW JERSEY OFFICE OF HISTORIC PRESERVATION HISTORIC SITES INVENTORY # 200322
INDIVIDUAL STRUCTURE SURVEY FORMHISTORIC NAME: Masonic Temple
LOCATION: 17-25 Alden St.COMMON NAME:
BLOCK/LOT 189/16MUNICIPALITY: Cranford
USGS QUAD: Roselle
OWNER/ADDRESS: Goodman, Joel & Eleanor
547 Salem Rd., Union, N.J.COUNTY: Union
UTM REFERENCES:
Zone/Northing/Easting

DESCRIPTION

Construction Date: 1902

Source of Date: Documentation

Architect:

Builder:

Style: Commercial/Italianate

Form/Plan Type: Rectangular

Number of Stories: 3

Foundation: yellow brick

Exterior Wall Fabric: yellow brick

Fenestration: 4 bay, each bay a recessed round arch extending for 2 stories.

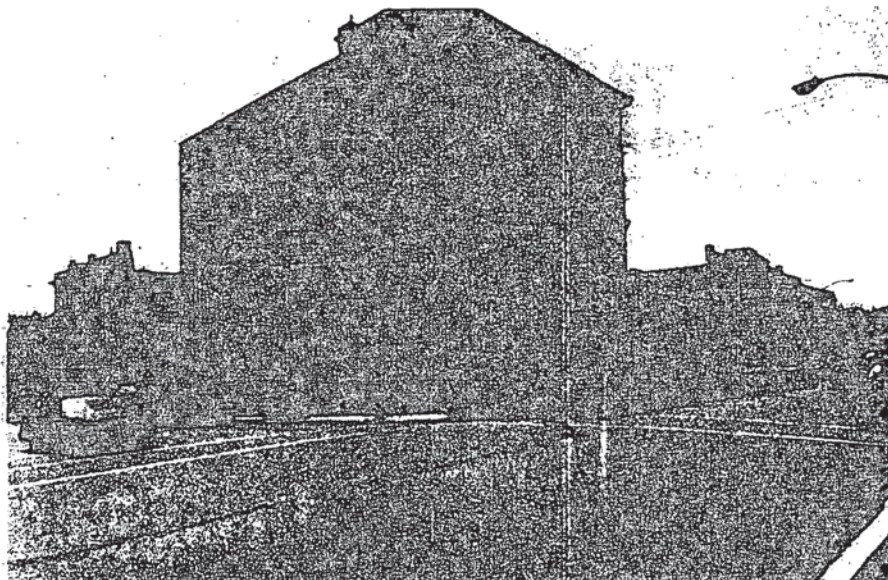
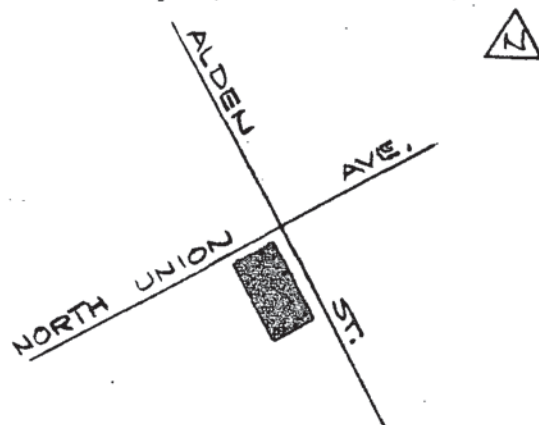
Roof/Chimneys: Flat roof. Metal cornice along roof line.

Additional Architectural Description:

First floor facade greatly altered by storefront changes.

PHOTO Negative File # Roll 33 frame 30A

Map (Indicate North)



SITING, BOUNDARY DESCRIPTION, AND RELATED STRUCTURES:

The building faces Union Avenue, once the "main street" of Cranford.

SURROUNDING ENVIRONMENT: Urban ☐ Suburban ☐ Scattered Buildings ☐
Open Space ☐ Woodland ☐ Residential ☐ Agricultural ☐
Industrial ☐ Downtown Commercial ☒ Highway Commercial ☐ Other ☐

Located in the center of the 19th Century commercial district of Cranford.
This area is now declining somewhat.

SIGNIFICANCE:

First floor had a grocery store and the township offices in it when the building was constructed. It cost \$25,000 to build.

Although built for the Masonic Hall Association of Cranford, the Masonic building also provided meeting space and a social hall for many other organizations in town. It was a focal point of many indoor activities in the early 20th Century.

The building is the largest commercial structure in the downtown area. Its architecture is conservative for 1902, but it looks like the type of 19th Century commercial building which defines "downtown" all across America.

ORIGINAL USE: Social hall and stores	PRESENT USE: Stores
PHYSICAL CONDITION: Excellent <input type="checkbox"/> Good <input type="checkbox"/>	Fair <input checked="" type="checkbox"/> Poor <input type="checkbox"/>
REGISTER ELIGIBILITY: Yes <input checked="" type="checkbox"/> Possible <input type="checkbox"/>	No <input type="checkbox"/> Part of District <input type="checkbox"/>
THREATS TO SITE: Roads <input type="checkbox"/> Development <input type="checkbox"/>	Zoning <input type="checkbox"/> Deterioration <input checked="" type="checkbox"/>
No Threat <input type="checkbox"/> Other <input type="checkbox"/>	
COMMENTS:	

Detail work at the cornice and around windows is beginning to deteriorate.

REFERENCES:

Cranford Historical Society

RECORDED BY: JWF
ORGANIZATION: UCCHPAB

DATE: 1981



CRANFORD
Union Co.

* 200322
R 33/30

Masonic Temple
17-25 Alden St.
looking S,

1981

Table 1: Historic Properties within the APE and Determination of Eligibility

Address	Description	Distance*/ Direction	Eligibility	Determination of Effect	Photo Plate
Crane-Phillips House, 124 N. Union Avenue, Cranford, New Jersey	c. mid-19th century, 2-story, vernacular Carpenter Gothic dwelling/house museum with gable roof, clapboard siding and brick foundation, full-width flat roofed front porch with simple Carpenter Gothic style trim work and vergeboards at eaves. Shed roofed additions to rear, and flat roofed wing to side.	1000 feet/SE outside of APE	SR (6/3/97) NR (8/14/97)	n/a	B.5.4
First Presbyterian Church, 11 Springfield Avenue, Cranford, New Jersey	1893, 2 story Romanesque Revival church with copper roofing, wooden shingle siding, rusticated stone foundation. Tiffany designed large stained glass windows on primary facade. Four story square tower on corner of building.	300 feet/SE	Eligible NJSHPO Site Inventory # 200307	No Adverse Effect	B.5.6
123 N. Union Avenue, Cranford, New Jersey	1913, 4 story, institutional building, flat roofed, decorative bands of alternating red brick and stone work, limestone sills, replacement windows, c. 1980 modern style portico constructed on all facades.	Proposed site	Not Eligible	n/a	B.5.1
Manger Building, 108 N. Union Avenue, Cranford, New Jersey	c. 1940, 2-story, Colonial Revival, asphalt shingle roofing, red brick, windows have overscaled drip molding and decorative shutters, portico is 2 full stories and has decorative railing above.	1000 feet/SW	Not Eligible	n/a	B.5.7
117-119 N. Union Avenue, Cranford, New Jersey	c. 1945 one-story commercial building, flat roofed with shingled pent roof, permastone wall cladding, large plate glass storefront windows.	1000 feet/SW	Not Eligible	n/a	B.5.9



PHOTO LOCATOR MAP

**Plate B.5.7:**

Manger Building - 108 North Union Avenue.
Photo view: Northwest
Photographer: Elaine Kiernan Gold
Date: August 1, 2001

Table 1: Historic Properties within the APE and Determination of Eligibility

Address	Description	Distance*/ Direction	Eligibility	Determination of Effect	Photo Plate
Crane-Phillips House, 124 N. Union Avenue, Cranford, New Jersey	c. mid-19th century, 2-story, vernacular Carpenter Gothic dwelling/house museum with gable roof, clapboard siding and brick foundation, full-width flat roofed front porch with simple Carpenter Gothic style trim work and vergeboards at eaves. Shed roofed additions to rear, and flat roofed wing to side.	1000 feet/SE outside of APE	SR (6/3/97) NR (8/14/97)	n/a	B.5.4
First Presbyterian Church, 11 Springfield Avenue, Cranford, New Jersey	1893, 2 story Romanesque Revival church with copper roofing, wooden shingle siding, rusticated stone foundation. Tiffany designed large stained glass windows on primary facade. Four story square tower on corner of building.	300 feet/SE	Eligible NJSHPO Site Inventory # 200307	No Adverse Effect	B.5.6
123 N. Union Avenue, Cranford, New Jersey	1913, 4 story, institutional building, flat roofed, decorative bands of alternating red brick and stone work, limestone sills, replacement windows, c. 1980 modern style portico constructed on all facades.	Proposed site	Not Eligible	n/a	B.5.1
Manger Building, 108 N. Union Avenue, Cranford, New Jersey	c. 1940, 2-story, Colonial Revival, asphalt shingle roofing, red brick, windows have overscaled drip molding and decorative shutters, portico is 2 full stories and has decorative railing above.	1000 feet/SW	Not Eligible	n/a	B.5.7
117-119 N. Union Avenue, Cranford, New Jersey	c. 1945 one-story commercial building, flat roofed with shingled pent roof, permastone wall cladding, large plate glass storefront windows.	1000 feet/SW	Not Eligible	n/a	B.5.9



PHOTO LOCATOR MAP

**Plate B.5.9:**

117-119 North Union Avenue.
 Photo view: Northwest
 Photographer: Elaine Kiernan Gold
 Date: August 1, 2001

Table 1: Historic Properties within the APE and Determination of Eligibility

Address	Description	Distance*/ Direction	Eligibility	Determination of Effect	Photo Plate
Crane-Phillips House, 124 N. Union Avenue, Cranford, New Jersey	c. mid-19th century, 2-story, vernacular Carpenter Gothic dwelling/house museum with gable roof, clapboard siding and brick foundation, full-width flat roofed front porch with simple Carpenter Gothic style trim work and vergeboards at eaves. Shed roofed additions to rear, and flat roofed wing to side.	1000 feet/SE outside of APE	SR (6/3/97) NR (8/14/97)	n/a	B.5.4
First Presbyterian Church, 11 Springfield Avenue, Cranford, New Jersey	1893, 2 story Romanesque Revival church with copper roofing, wooden shingle siding, rusticated stone foundation. Tiffany designed large stained glass windows on primary facade. Four story square tower on corner of building.	300 feet/SE	Eligible NJSHPO Site Inventory # 200307	No Adverse Effect	B.5.6
123 N. Union Avenue, Cranford, New Jersey	1913, 4 story, institutional building, flat roofed, decorative bands of alternating red brick and stone work, limestone sills, replacement windows, c. 1980 modern style portico constructed on all facades.	Proposed site	Not Eligible	n/a	B.5.1
Manger Building, 108 N. Union Avenue, Cranford, New Jersey	c. 1940, 2-story, Colonial Revival, asphalt shingle roofing, red brick, windows have overscaled drip molding and decorative shutters, portico is 2 full stories and has decorative railing above.	1000 feet/SW	Not Eligible	n/a	B.5.7
117-119 N. Union Avenue, Cranford, New Jersey	c. 1945 one-story commercial building, flat roofed with shingled pent roof, permastone wall cladding, large plate glass storefront windows.	1000 feet/SW	Not Eligible	n/a	B.5.9



PHOTO LOCATOR MAP

**Plate B.5.1:**

123 North Union Avenue - Cleveland Plaza. Orange flag indicates panel antenna placement on this elevation.

Photo view: Northwest

Photographer: Suzanne B. Derrick

Date: June 15, 2001



PHOTO LOCATOR MAP

**Plate B.5.2:**

Rear Elevation of 123 North Union Avenue- Cleveland Plaza from Miln Street.

Photo view: Southeast

Photographer: Suzanne B. Derrick

Date: June 15, 2001

The Crane House
124 Union Avenue N.
Cranford, Union County, New Jersey

⁶
HABS-~~NJ~~-189

HABS
NJ
20-CRANE
1-

PHOTOGRAPHS
WRITTEN HISTORICAL AND DESCRIPTIVE DATA
District of New Jersey

Historic American Buildings Survey
Seymour Williams, A.I.A., District Officer
133 Central Avenue, Rahway, New Jersey

HABS
N.J.
20-CRANE,
1-

The Crane House
124 Union Avenue N.
Cranford, Union County, New Jersey

Owner: Union County Park Commission

Date of Erection: 1867

Architect:

Builder: William C. Wells

Present Condition: Fair

Number of Stories: Two

Materials of Construction: Foundation - fieldstone

Exterior walls - frame
construction, clapboards

Interior walls - plaster

Roof - pitch, asphalt
shingles

Historical Data:

The house was erected in 1867 by a carpenter named William C. Wells, but it is uncertain for whom it was built. The property upon which it was erected was deeded in 1864 to Josiah Crane, Jr. Early in 1867 it was deeded to Henry J. Phillips. It was occupied by the Phillips family until 1902. It was

either vacant or rented until 1931, when the house and property were donated to the Union County Park Commission by the Township of Cranford. Since October, 1931, the house has been rented from the Park Commission and used as a museum by the Cranford Historical Society.

Bibliography:

Field Book and Drawings of Survey

Material collected by Walter D.
Terrill

Walter D. Terrill

Supervising Historian

Approved:

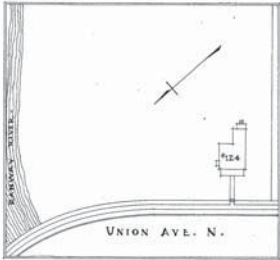
Seymour Williams

SEYMOUR WILLIAMS, A.I.A.
District Officer



THE CRANE HOUSE

124 UNION AVENUE N
CRANFORD NEW JERSEY



PLOT PLAN

ERECTED IN 1867

DISTRICT NO. 6 STATE OF NEW JERSEY
SEYMOUR WILLIAMS AIA DISTRICT OFFICER

HISTORIC AMERICAN BUILDINGS SURVEY
U.S. DEPARTMENT OF THE INTERIOR
NATIONAL PARK SERVICE
BRANCH OF PLANS AND DESIGN

MEASURED: OCT. 10-14-1935
DRAWING: OCT. 24 - NOV. 14-1935
MEASUREMENTS CHECKED: L. M. DENNIS



DRAWINGS APPROVED: *Seymour Williams*
DRAWINGS APPROVED: *L. M. DENNIS*
ACCEPTED FOR LIBRARY OF CONGRESS

DESIGNEE: DE. DENNIS AIA
UNION COUNTY SUPERVISOR
303 N. BROAD ST., ELIZABETH, N.J.
FIELD: R. WATKINS
PARTY: T. BAILEY

SHEET NO. 6-189
INDEX NO. 1-2
SHEETS 14



WEST ELEVATION



EAST ELEVATION

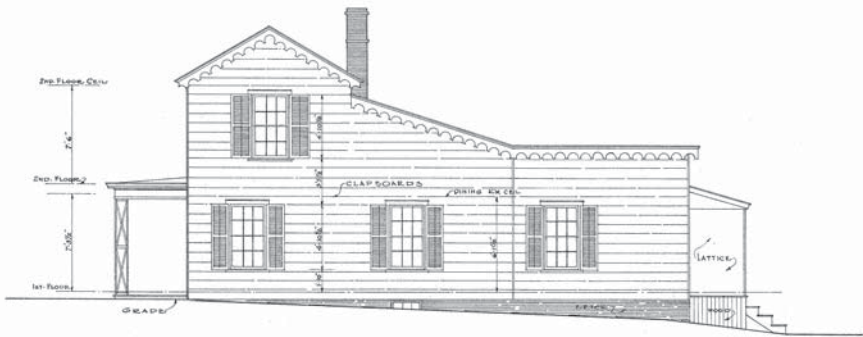
KEYS: VANDERMADE
HARRY MASLOW
NEW JERSEY DIVISION

SCALE: 1/4" = 1'-0"
1" = 4'-0"

U.S. DEPARTMENT OF THE INTERIOR
OFFICE OF NATIONAL PARKS, BUILDINGS, AND RESERVATIONS
BRANCH OF PLANS AND DESIGN

NAME OF STRUCTURE
THE CRANE HOUSE
124 UNION AVE. N. CRANFORD - N.J.

SHEET NO. 6-109
HISTORIC AMERICAN BUILDINGS SURVEY
SHEET 1 OF 4 SHEETS
INDEX NO.



SOUTH ELEVATION

HARRY MASLOW DEL.
NEW JERSEY DIVISION

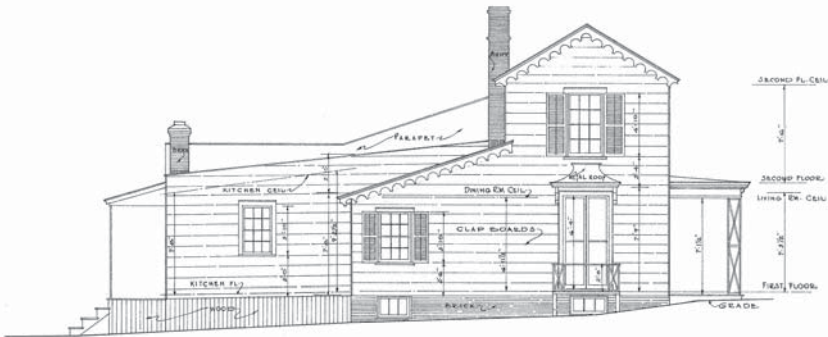
U.S. DEPARTMENT OF THE INTERIOR
OFFICE OF NATIONAL PARKS, BUILDINGS, AND RESERVATIONS
BRANCH OF PLANS AND DESIGN

NAME OF STRUCTURE
THE CRANE HOUSE
124 UNION AVE. - N. CRANFORD - N.J.

Scale bar: 0' 2' 4' 6' 8' 10' 12' 14'

METRIC: 0m 2m 4m 6m 8m 10m 12m 14m

SURVEY NO. 6-169	HISTORIC AMERICAN BUILDINGS SURVEY SHEET 3 OF 4 SHEETS	INDEX NO.
		172-24 <i>Crane</i>



NORTH ELEVATION

HARRY MASLOW DEL.
NEW JERSEY DIVISION

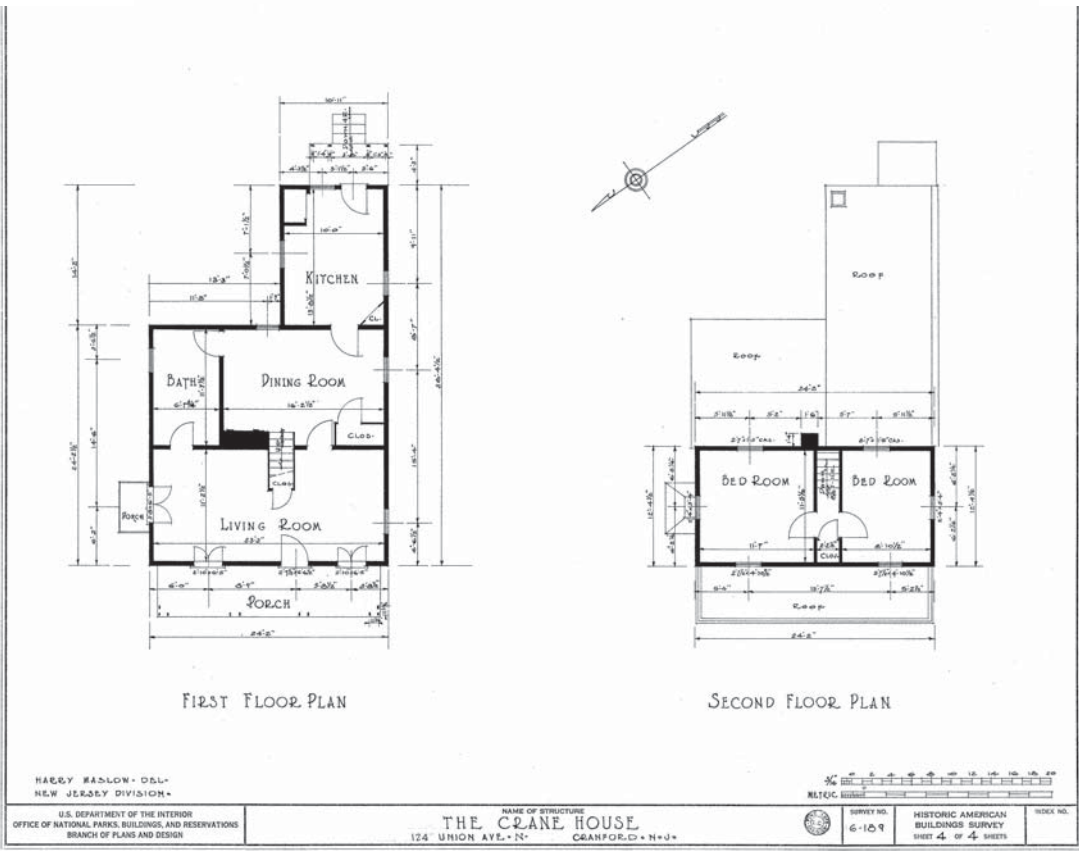
U.S. DEPARTMENT OF THE INTERIOR
OFFICE OF NATIONAL PARKS, BUILDINGS, AND RESERVATIONS
BRANCH OF PLANS AND DESIGN

NAME OF STRUCTURE
THE CRANE HOUSE
124 UNION AVE. - N. CRANFORD - N.J.

Scale bar: 0' 2' 4' 6' 8' 10' 12' 14'

METRIC: 0m 2m 4m 6m 8m 10m 12m 14m

SURVEY NO. 6-169	HISTORIC AMERICAN BUILDINGS SURVEY SHEET 2 OF 4 SHEETS	INDEX NO.
		172-24 <i>Crane</i>



NEW JERSEY OFFICE OF HISTORIC PRESERVATION HISTORIC SITES INVENTORY # 200306
INDIVIDUAL STRUCTURE SURVEY FORM

HISTORIC NAME: Josiah Crane Jr. House
LOCATION: 124 North Union Ave.

COMMON NAME: Cranford Historical Society
BLOCK/LOT 196/2 Museum

MUNICIPALITY: Cranford
USGS QUAD: Roselle
OWNER/ADDRESS: Union County Parks Dept.
Elizabeth, N.J.

COUNTY: Union
UTM REFERENCES:
Zone/Northing/Easting

DESCRIPTION

Construction Date: 1840/1860s

Source of Date: Cranford Historical Society

Architect:

Builder: Henry Phillips

Style: Vernacular Cottage

Form/Plan Type: Narrow "I" house
with two rear lean-to
additions.

Number of Stories: 2

Foundation: Brick

Exterior Wall Fabric: Clapboard

Fenestration: Two bay facade. 6/6 sash windows. Floor to ceiling windows on
first floor.

Roof/Chimneys: Gable roof.

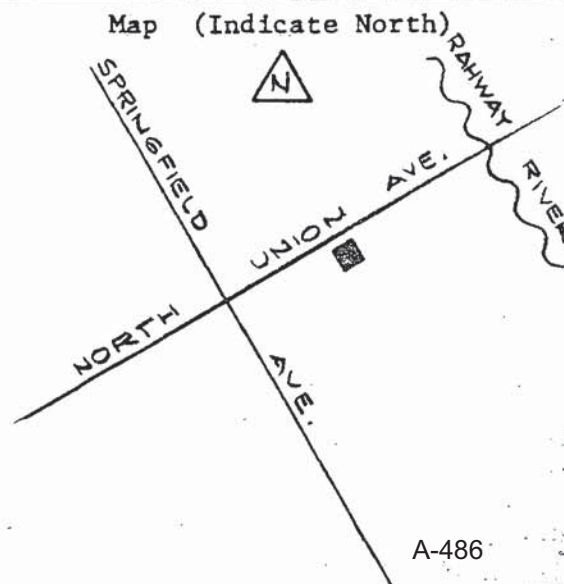
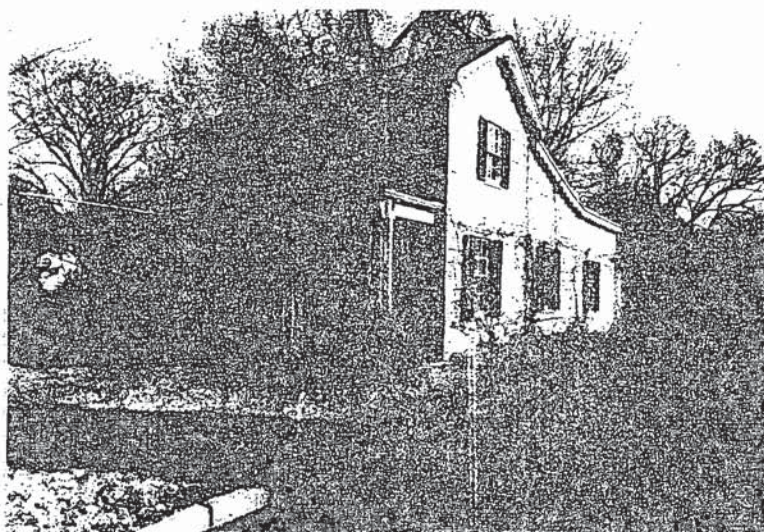
Additional Architectural Description:

Off-center front entry.

Porch across first floor facade - flat roof.

Simple barge board trim along eaves.

PHOTO Negative File # Roll 11 frame 3



A-486

SITING, BOUNDARY DESCRIPTION, AND RELATED STRUCTURES:

Located near the banks of the Rahway River. The house was originally one of the outbuildings to the Josiah Crane farm, located across Union Ave. The site of the Crane farm is now marked by a small park and a plaque.

SURROUNDING ENVIRONMENT: Urban ☐ Suburban ☒ Scattered Buildings ☐
Open Space ☒ Woodland ☐ Residential ☐ Agricultural ☐
Industrial ☐ Downtown Commercial ☐ Highway Commercial ☐ Other ☐

The house is on land owned by Union County, for it is within the Rahway River Parkway, an open space/flood control corridor along the river. The new Cranford Municipal Building is adjacent on the west side.

SIGNIFICANCE:

The little Victorian "gingerbread cottage" which houses the Cranford Historical Society has stood where it is since 1867. It was built as a home by Henry Phillips, a carpenter, who lived there until about 1915, by adding a front-2 story portion to earlier buildings on the site which he bought from Josiah Crane, Jr. Family tradition says that part of the structure was a woodshed brought across the street from the home of Josiah Crane. The Crane farm gave its name to the town of Cranford.

The "property and building" where the museum now stands was given to Josiah, Jr. by his father when he was married in 1842. The woodshed which forms the present kitchen may have been moved at that time to make this a honeymoon cottage. In any case, the center portion is older, and our best guess is that it was a laundry building from the old colonial homestead. It has broad random-width floor boards and a fireplace with the remains of heavy iron pintles (pot hooks) in the cellar, as well as one on the first floor. There is also a big inside cistern, filling half the cellar, with a hand-forged faucet and a top opening (now closed for safety) for drawing water through a trap-door up into the room above.

See attached sheet

ORIGINAL USE:

PHYSICAL CONDITION: Excellent ☒ Good ☐

REGISTER ELIGIBILITY: Yes ☒ Possible ☐

THREATS TO SITE: Roads ☐ Development ☐

No Threat ☒ Other ☐

COMMENTS:

On the New Jersey Register of Historic Places (NJHSI 813.2)

Surveyed by the Historic American Buildings Survey (HABS 189)

PRESENT USE:

Fair ☐ Poor ☐

No ☐ Part of District ☐

Zoning ☐ Deterioration ☐

REFERENCES:

Cranford Historical Society

RECORDED BY: JWF
ORGANIZATION: UCCHPAB

DATE: 1981

page 2

#200306 continued

Significance - continued

In 1852, Josiah Crane, Jr., and his cousin David Miller, built new homes near the railroad crossing. The cottage was sold to Henry Phillips, a skilled carpenter. Mr. Phillips added the second story front part of the building and lived in it himself for over fifty years.

The Cranford Historical Society was organized in 1929 to promote interest in local history, and it acquired this building to exhibit its memorabilia and collections. A new wing was added to the exhibit in 1963 through the generosity of the Cranford Junior Service League.



124 UNION

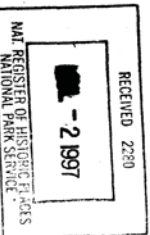
CRANFORD

Hist Soc Bldg,

1840
* 200306

looking S.E.
124 N. Union
Cranford
Union Co.

Roll 11, 3

United States Department of the Interior
National Park ServiceNational Register of Historic Places
Registration Form

This form is for use in nominating or requesting determinations for individual properties and districts. See instructions in *How to Complete the National Register of Historic Places Registration Form* (National Register Bulletin 16A). Complete each item by marking "X" in the appropriate box or by entering the information requested. If an item does not apply to the property being documented, enter "N/A" for "not applicable." For functions, architectural classification, materials, and areas of significance, enter only categories and subcategories from the instructions. Place additional entries and narrative items on continuation sheets (NPS Form 10-900a). Use a typewriter, word processor, or computer, to complete all items.

1. Name of Property

historic name Crane-Phillips Houseother names/site number N/A

2. Location

street & number 125 North Union AvenueN/A ☐ not for publicationcity or town Cranford☐ vicinitystate New Jersey code NJ county Union code 039 zip code 07016

3. State/Federal Agency Certification

As the designated authority under the National Historic Preservation Act, as amended, I hereby certify that this ☒ nomination
☐ request for determination of eligibility meets the documentation standards for registering properties in the National Register of
Historic Places and meets the procedural and professional requirements set forth in 36 CFR Part 60. In my opinion, the property
☒ meets ☐ does not meet the National Register criteria. I recommend that this property be considered significant
☐ nationally ☐ statewide ☐ locally. ☐ See continuation sheet for additional comments.

Signature of certifying official/Title

Date

Assistant Commissioner for Natural & Historic Resources/DSHPO

State of Federal agency and bureau

In my opinion, the property ☐ meets ☐ does not meet the National Register criteria. ☐ See continuation sheet for additional
comments.

Signature of certifying official/Title

Date

State or Federal agency and bureau

4. National Park Service Certification

I hereby certify that the property is:

☒ entered in the National Register.☐ See continuation sheet.☐ determined eligible for the

National Register

☐ See continuation sheet.☐ determined not eligible for the

National Register.

☐ removed from the National

Register.

☐ other, (explain) _____

Signature of the Keeper

Date of Action

Charles Phillips8/14/97

CRANE-PHILLIPS HOUSE

UNION NEW JERSEY

Name of Property

County and State

5. Classification

Ownership of Property
(Check as many boxes as apply)☐ private☒ public-local☐ public-State☐ public-FederalCategory of Property
(Check only one box)☒ building(s)☐ district☐ site☐ structure☐ objectNumber of Resources within Property
(Do not include previously listed resources in the count.)Contributing 1 Noncontributing 0

buildings

sites

structure:

objects

Total

Name of related multiple property listing
(Enter "N/A" if property is not part of a multiple property listing.)

N/A

Number of contributing resources previously listed
in the National Register

0

6. Function or Use

Historic Functions
(Enter categories from instructions)

Domestic/

Single Dwelling

Current Functions
(Enter categories from instructions)

Recreation & Culture/

Historical Museum

7. Description

Architectural Classification
(Enter categories from instructions)

Mid-19th Century

Other - Downingsque (Rural Gothic)

Materials
(Enter categories from instructions)

foundation Stone & Brick

walls Wood

roof Wood Shingle

other Mineral - surfaced roll

roofing

Narrative Description
(Describe the historic and current condition of the property on one or more continuation sheets.)

CRANE-PHILLIPS HOUSE

UNION NEW JERSEY

CRANE-PHILLIPS HOUSE

Union New Jersey

8. Statement of Significance

Applicable National Register Criteria (Mark "x" in one or more boxes for the criteria qualifying the property for National Register listing.)

Areas of Significance (Enter categories from instructions)

Architecture

☐ A Property is associated with events that have made a significant contribution to the broad patterns of our history.

☐ B Property is associated with the lives of persons significant in our past.

☒ C Property embodies the distinctive characteristics of a type, period, or method of construction or represents the work of a master, or possesses high artistic values, or represents a significant and distinguishable entity whose components lack individual distinction.

☐ D Property has yielded, or is likely to yield, information important in prehistory or history.

Criteria Considerations (Mark "x" in all the boxes that apply.)

Significant Dates

ca. 1870

Property is:

☐ A owned by a religious institution or used for religious purposes.

☐ B removed from its original location.

☐ C a birthplace or grave.

☐ D a cemetery.

☐ E a reconstructed building, object, or structure.

☐ F a commemorative property.

☐ G less than 50 years of age or achieved significance within the past 50 years.

Narrative Statement of Significance (Explain the significance of the property on one or more continuation sheets.)

9. Major Bibliographical References

Bibliography (Cite the books, articles, and other sources used in preparing this form on one or more continuation sheets.)

Previous documentation on file (NPS):

☐ preliminary determination of individual listing (36 CFR 67) has been requested

☐ previously listed in the National Register

☐ previously determined eligible by the National Register

☒ designated a National Historic Landmark

HABS. 189. (1936)

recorded by Historic American Buildings Survey

Record #

New Jersey Historical Structural Inventory # 813.2

10. Geographical Data

Acreage of Property ca 1/4 acre

Roselle Quad

UTM References (Place additional UTM references on a continuation sheet.)

1 [1,8] [5,5,8,9,8,0] [4,5] [0,0] [7,8,0]
Zone Easting Northing

3 [] [] [] [] [] [] [] []
Zone Easting Northing

4 [] [] [] [] [] [] [] []
See continuation sheet

Verbal Boundary Description (Describe the boundaries of the property on a continuation sheet.)

Boundary Justification

(Explain why the boundaries were selected on a continuation sheet.)

11. Form Prepared By

name/title Donald M. Widdows

organization Cranford Historical Society date Nov. 12, 1996

street & number 84 Springfield Avenue telephone 908-276-0082

city or town Cranford state NJ zip code 07016

Additional Documentation

Submit the following items with the completed form:

Continuation Sheets

Maps

A USGS map (7.5 or 15 minute series) indicating the property's location.

A Sketch map for historic districts and properties having large acreage or numerous resources.

Photographs

Representative black and white photographs of the property.

Additional Items

(Check with the SHPO or FPO for any additional items)

Property Owner (Complete this item at the request of SHPO or FPO)

name Union County Parks Commission (County of Union)

street & number Administration Building telephone 908-572-4200

city or town Elizabeth state NJ zip code 07207

Paperwork Reduction Act Statement: This information is being collected for applications to the National Register of Historic Places to nominate properties for listing or determine eligibility for listing, to list properties, and to amend existing listings. Response to this request is required to obtain a benefit in accordance with the National Historic Preservation Act, as amended (16 U.S.C. 470 et seq.).

Estimated Burden Statement: Public reporting burden for this form is estimated to average 18.1 hours per response, including time for reviewing instructions, gathering existing data, and completing and reviewing the form. Send comments regarding this burden estimate or any aspect of this form, including instructions, to Washington, DC 20013-7172, and the Office of Management and Budget, Paperwork Reduction Project (1024-0010), Washington, DC 20503.

United States Department of the Interior
National Park Service

RECEIVED
JAN 22 1971
CRS Approved No. 100-4611

National Register of Historic Places
Continuation Sheet

HISTORIC PRESERVATION OFFICE

Page #1
NJ, Union County
Crane-Phillips House

Section number 7 Page 1

Narrative Description

The Crane-Phillips House is a small two story, frame, Downingsque style rural cottage with an "L" shape floor plan. The exact date of construction is not known except that it was before 1867. The front two-story section has a gable roof with hand split cedar shake shingles and the rear one-story section has a nearly flat shed type roof of a composition material. The foundation is a mixture of field stone and brick and the exterior walls are wide clapboard. The wide front porch has large french style windows providing an unique appearance to the house. The house is maintained in excellent condition by the Cranford Historical Society with one alteration in 1963 which matches the original structure.

It is located on the southeast side of North Union Avenue. The front section faces North Union Avenue (Photo #1). It is midway between Springfield Avenue to the southwest and the bridge over the Rahway River to the northwest. It is part of a larger three acre plot of land along the Rahway River presently owned by the County of Union. The Crane-Phillips House is on a parcel of this land, 100 feet wide along Union Avenue and 150 deep, leased to the Cranford Historical Society (Note #5). The house sits on the western corner of the 100 X 150 foot property. The front porch is approximately 30 feet from North Union Avenue.

There is a brick foundation which rests on a random fieldstone base. The front foundation, concealed under the porch is also random fieldstone. This front porch has a continuous brick foundation. The 1963 addition has a concrete block foundation. The kitchen "L" has a concrete foundation that is veneered with brick on the west side.

The floor framing materials visible in the cellar show vertically sawn joists, regularly spaced. On the west side, these joists are continuous members spanning 24 feet from the front foundation wall, across the intermediate bearing wall to the rear foundation. In the northeast corner, the members are a mixture of heavy hewn timbers and modern dimensioned lumber. This could be some of the remains of Josiah, Jr.'s cottage. (Note #6) This may explain this odd 8 X 12 framing in this location. The continuous nature of the first floor joists suggest that the main house was built at one time. This is also confirmed by the lack of any evident joints in the foundation.

All exterior walls are sided with wide clapboard. All the angles are treated with clapboards. The west parapet wall was added to conceal the long slope of the extended shed roof over the kitchen "L". There is a remnant, of a clapboard between the two-story section and the one story rear shed (Photo #7). This feature reveals the original slope of the rear shed as it meets the larger two-story section. The coursing of the siding is continuous below the

United States Department of the Interior
National Park Service

CRS Approved No. 100-4611

National Register of Historic Places
Continuation Sheet

Page #2
NJ, Union County
Crane-Phillips House

Section number 7 Page 2

cornerboard. The other remnant exists between the kitchen "L" and the rear shed portion. A cornerboard marks a transition between the siding. This coursing is not continuous, indicating that the enclosure of the kitchen "L" was clearly added later. The scalloped vergeboard unites all of the components on this side.

The front porch contains an interesting set of vertical supports. These consist of paired 2 X 4 type posts joined at the tops, midpoints and bases, and are diagonally cross-braced within each panel (Photo #12). The shallow cornice of the porch roof is picked up by paired brackets over each of the vertical supports. This decorative column design is repeated on the rails of the small east balcony. The parapet over this balcony is supported by corbelled brackets on each side (Photo #17).

The two-story section of the house roof has sloped gables with hand split cedar shakes. These convey a cruder appearance than would have been typical of the mid-19th century house. The short, rear shed-roof and long rear shed over the kitchen consist of mineral-surfaced roll roofing. The flat roof of this type typically would have been metal at the time of construction. A gabled roof is not guttered while the shed roofs and the porch have wooden box gutters.

The front wall of the first floor has two French doors at each end and a wood four panel door off-set from the center of the French doors. The east side of the main room has a French door opening onto a small porch (Photo #17) while the west wall contains a typical six-over-six double-hung window.

The two-story section consists of a single room on the first floor and two rooms on the second floor. This first floor front parlor (23 X 11 1/2) contains the highest degree of original detail. The entry door, French doors and window to the west side have moulded casings from the third quarter of the nineteenth century. It has wide plank floor boards and plastered walls. The floor boards change direction in the east end of the parlor where the framing direction changes below. The ceiling is modern gypsum board with semi-recessed lighting fixtures. The front door is a four panel door from the nineteenth century (Photo #11). The two front French doors each contain two doors, 18 inches wide with four lights (Photo #12 for detail). The side unit to the east is wider with an opening approximately 4 feet wide and have two lights each (Photo #17).

There is a dining room with a small fireplace under the single story shed section (32 X 11 1/2). The focus of the dining room is the fireplace (Photo #14), located in the middle of the

United States Department of the Interior
National Park Service

National Register of Historic Places Continuation Sheet

Section number 7 Page 3

Page #3
NJ, Union County
Crane-Phillips House

north wall which separates this room from the front parlor. It is a non-functioning fireplace, as the present furnace flue runs up through the firebox. Originally there was a coal stove in this location. The coal stove heating system for the house is consistent with the central location and the era. The dining room door and window casings are simple, unloaded bands. There is a single six-over-six window in the rear exterior wall and one in the west exterior wall. It has wide-plank floor boards similar to the parlor, plastered walls, and a gypsum-board ceiling with lighting fixtures. In the floor toward the west end of the dining room is a hatch door. This door accesses the cistern located in the cellar. The floor hatch may be more of an interpretive device than an access door.

The rear porch has been enclosed to form an "L" shape to the rear of the building, and was converted to a kitchen. The shed roof over the rear has been extended over this kitchen area to form a flat roof. To unite the earlier shed section and the enclosed kitchen section, a parapet wall was built along the west wall to conceal the shallow pitch of the roof line (Photos #6, 7, 8). The kitchen, today, is a support space for the house museum. It has modern finishes. From evidence in the cellar, it appears that the original access to the cellar was through this space, in the northeast corner. It was probably in the form of a floor hatch and stairs. The exact layout of the kitchen in its historical form is unknown. The HABS (Note #4) drawings show a brick chimney rising from the southeast corner of the room, indicates the placement of a cooking stove. The present kitchen "L" has a powder room to the south end near the back door.

There is a partial cellar (with limited head space) under the one-story shed-roofed section. At one time, access to the cellar was through the floor in the kitchen area. Now the cellar is accessed via a modern exterior bulkhead door and stairs on the outside rear wall.

Access to the second floor is from a stair that is located just west of the chimney (Photo #13). This narrow stair begins in the dining room and rises toward the front of the house. There is a closet under the stair in the front parlor. The upper floor consists of two rooms separated by the stairhall. The stair is very steep with treads and risers that greatly exceed what is permitted by modern code standards (Photo #13). The door to each of the two rooms are two panel units from mid-nineteenth century. The casings here are simple beaded bands. Each room has three windows, each of which are different. On the front wall, the units are large six-over-six double-hung wood sash (Photo #1). On the east and west side walls, there are similar double-hung windows of a smaller size. The rear windows on the south side are

United States Department of the Interior
National Park Service

National Register of Historic Places Continuation Sheet

Section number 7 Page 4

Page#4
NJ, Union County
Crane-Phillips House

three-lite awning-type sash (Photo #19). The rooms have wide plank wood floors, plastered walls and ceilings. There is a small closet at the top of the stairs, separating the two main rooms. The east main room is larger (12 X 12) than the west room (9 X 12).

Most of the double hung windows and the French doors contain louvered shutters.

As seen in the open interpretative view panel, the heavy timber wall to the west of the chimney, shows traditional materials and methods employed throughout the second quarter of the nineteenth century. Such braced timber wall construction techniques used in the combination with sawn joists were not uncommon (Photo #15).

In 1963, this basic form was enlarged to include another room (The Junior League Room) on the east side of the house (Photo #10). This addition is noted on the Site diagram. The room (15 X 18) is used for the seasonal and special displays presented by the historical society. This addition was designed to be compatible with the original building.

United States Department of the Interior
National Park Service

OMB Approval No. 1024-0011

National Register of Historic Places Continuation Sheet

Section number 8 Page 1

Page#1
NJ, Union County
Crane-Phillips House

Statement of Significance

The Crane-Phillips House, a small rural cottage built c. 1870, is significant under Criterion C as a good local example of the architectural design characteristics of Andrew Jackson Downing.

This house has been the home of the Cranford Historical Society for over 65 years. For most of the past 30 years the entire first floor has been used to display many of the historical items in the society's vast collection. It is open to the public every Sunday afternoon during September through June and upon special occasions and requests. It is the focal point of local history studies for all third grade school students. Some of the permanent displays in the dining room are related to Cranford's police and fire departments. The front parlor depicts a modest Victorian room. The Junior League Room (opened in 1963) is used for special displays as they might relate to Cranford's history or some other event in American history. This museum is supported entirely by membership and donations.

Mr. Henry Phillips purchased the property in 1867 (Note #1) from the Crane family (the founding family of Cranford) who had owned the land for over 100 years (Note #2).

Although the exact date for the construction of this house is not known, the architectural mode adopted for this dwelling was that of the Downingsque cottage. Andrew Jackson Downing's influential *Victorian Cottage Residences* (New York in 1842), prescribed an appropriate American landscape and architecture based upon picturesque and romantic principles. The Crane-Phillips House embraces these concepts. There is a prominent front veranda (Photo #1), French doors in the front parlor in lieu of windows to visually connect the interior to the exterior (Photos #1, #18). This makes a lighter room and provides for better interior cooling in the hot weather. The addition of a kitchen in the rear and the scalloped vergeboards along the edge of the gables (Photos #6, #7, #8) are also associated with this mode.

In A.J. Downing's book, *Victorian Cottage Residences*, there are several house designs and descriptions.

"Design 1 - (Note #3 - Page 26) A Suburban Cottage for a Small Family: We suppose this cottage to be situated in illustration the treatment of a small portion of

United States Department of the Interior
National Park Service

OMB Approval No. 1024-0011

National Register of Historic Places Continuation Sheet

Section number 8 Page 2

Page#2
NJ, Union County
Crane-Phillips House

ground, we shall also imagine it to be placed on a lot of ground 75 feet front by 150 feet deep. This simple cottage will be a suitable one for a small family, when the mistress wishes to have the management of the domestic affairs directly under her own personal care and supervision. Although this cottage is of very modest size, yet, to a family of small means, leading a comparative retired life, it will afford a great deal of comfort, and even a considerable degree of taste on neatness."

This describes the Crane-Phillips House.

Downing also states (page 29),

"The veranda ornamented by supports which shelters the entrance door (Photo #12), and affords an agreeable place both for walking in damp or inclement weather, and to enjoy a cool shady seat in the hotter portion of the season. The scalloped vergeboards along the edge of the gables are projected which serve to protect the exterior more completely than any other form against the effects of storms, and gives character by its boldness and deep shadows it casts upon the building"

Another Downing feature is the placement of the chimney in the center of the house (Photo #13). He believed when a chimney is built in the outer walls, it seldom continues to stay warm during the entire twenty-four hours. It's heat is displaced rapidly into the cold external air. A good draft depends somewhat on the warmth of the air in the room and the heat of the chimney itself. It is evident that chimneys in the interior of a house must draw better than in the exterior wall. This allows a great deal of heat to be retained in the body of the house.

The Crane-Phillips House is an excellent example of this type of architecture. Even though it is small, it follows the ideals and features of several of Downing's plans for small

United States Department of the Interior
National Park Service

National Register of Historic Places Continuation Sheet

Page#3
NJ, Union County
Crane-Phillips House

Section number 8 Page 3

OMB Approval No. 1050-0042

county or suburban cottages. This is why the Historic American Building Survey chose to recognize this house by recording it in the 1930's. (Note #4)

It is important to say that the name for this house came from the first owners, the Crane family, founders of Cranford and the Phillips family who were also well known in the community.

Henry J. Phillips purchased the house and property from Josiah Crane, Jr. in 1867 (Note#1) and lived in the house until his death in 1911 (Note#7). He was an engraver and an inventor. In 1895 he obtained a patent on a "range shield" (Patent No. 572,715 (Note#5). This device was "designed to prevent the escape of smoke and empyreumatic odors from the cooking into the room". This was a forerunner of our modern rangehood.

In 1884 Henry Phillips deeded the property to his brother Charles H. Phillips (Note#9). Charles is more well known for his patents (Note#10) on Phillips Milk of Magnesia which he marketed in 1872.

United States Department of the Interior
National Park Service

National Register of Historic Places Continuation Sheet

Page#4
NJ, Union County
Crane-Phillips House

Section number 8 Page 4

OMB Approval No. 1050-0042

FOOTNOTES

- #1 Union County New Jersey Deeds, Book 22 pg. 641 Josiah Crane, Jr. to Henry J. Phillips 1867
- #2 Will of John Crane 1722. Trenton, New Jersey Book A pg.283
Essex County New Jersey Deeds, Book M pg. 195 John Crane Sr. to John Crane Jr. et al 1820
Union County New Jersey Deeds, Book 3 pg. 700 Josiah Crane to Josiah Crane, Jr. 1864
Union County New Jersey Deeds, Book 3 pg. 700 Josiah Crane to Josiah Crane, Jr. 1864
- #3 Victorian Cottage Residences by Andrew Jackson Downing (1815-1852) Published 1852
- #4 Historical American Buildings Survey of New Jersey - Josiah Crane, Jr. House NJ189
- #5 Lease - Union County Park Commission to the Cranford Historical Society to the year 2022
- #6 Crane family tradition states that Josiah Crane, Jr. moved his earlier "honeymoon cottage" from across the north side of the street, thus becoming the first component of the Crane-Phillips House.
- #7 Death Certificate of Henry J. Phillips
- #8 United states Patent No. 527,715 - 1896
- #9 Union County New Jersey Deeds, Book 1760 pg. 304 Henry J. Phillips to Charles H. Phillips.
- #10 United States Patent Nos. 138,282 - 141,167 - 159,446 issued in 1873 and 1874.

United States Department of the Interior
National Park Service

OMB Approval No. 1024-0010

**National Register of Historic Places
Continuation Sheet**

Page#1
NJ, Union County
Crane-Phillips House
Section number 9 Page 1

Bibliography

Books and Reports:

Adams, Emmor K. Early Cranford - Hand written notes by E.K. Adams in the files of the Cranford Historical Society.

Bassett, William B. Historical American Buildings Survey of New Jersey. Published by The New Jersey Historical Society 1977. Compiled and edited for HABS National Park Service Department of Interior.

Brideman, Harriet. The Encyclopedia of Victoriana Macmillian Publishing Co. New York 1975

Crane, Ellery Bicknell. Genealogy of the Crane Family Press of Charles Hamilton 1900

Downing, Andrew Jackson. Victorian Cottage Residences First Published in 1842 Reprint by Dover Publishing 1980

Downing, Andrew Jackson. The Architecture of Country Houses D. Appelton & Company 1850 Reprint by Dover Publishing 1980

Githens, Herbert J. Historical Preservation Plan for the Crane-Phillips House. Prepared for the Cranford Historical Society.

Hatfield, Edwin. History of Elizabeth, New Jersey Including The Early History of Union County. Published by Carlton & Lananan New York 1868

United States Department of the Interior
National Park Service

OMB Approval No. 1024-0010

**National Register of Historic Places
Continuation Sheet**

Page#2
NJ, Union County
Crane-Phillips House
Section number 9 Page 2

Honeyman, A. Van Doren. History of Union County, New Jersey 1664-1932 Lewis Historical Publishing Company New York 1923

McKerney, William. Commemorative History of the Presbyterian Church in Westfield, New Jersey 1728-1928 Copyright 1929 by the Presbyterian Church of Westfield, New Jersey

Public Records:

Cranford Property Tax Records - Cranford Historical Society

Essex County, New Jersey Deeds - Newark, New Jersey

Historical American Buildings Survey of New Jersey - Cranford Historical Society.

New Jersey Census Records - Union County 1885, 1905 Westfield Public Library Westfield, New Jersey.

Union County, New Jersey Deeds - Elizabeth, New Jersey.

Union County, New Jersey Mortgages - Elizabeth, New Jersey.

United States Census Records - Union County, New Jersey - 1870, 1880, 1900, 1910 - Westfield Public Library

United States Department of the Interior
National Park Service

National Register of Historic Places Continuation Sheet

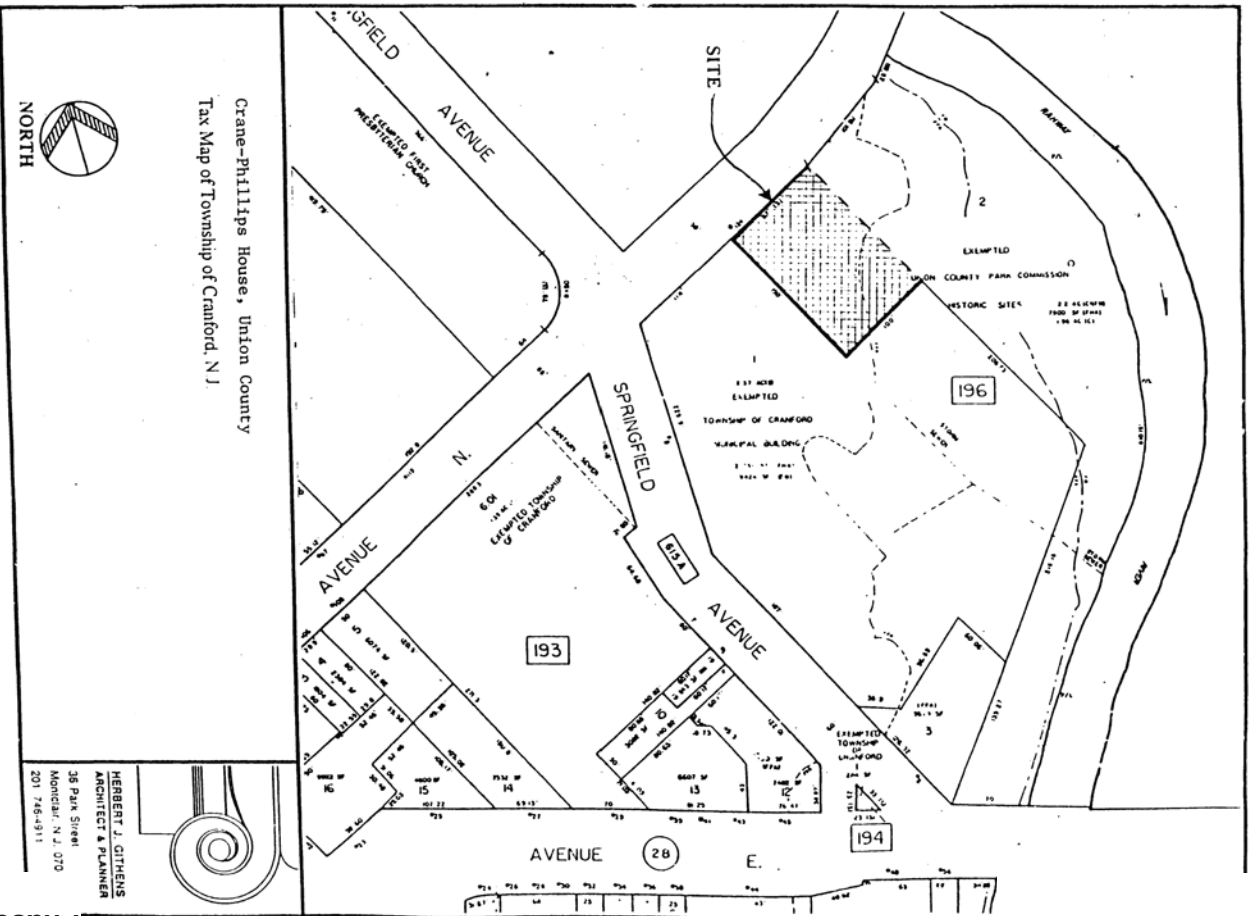
Section number 10 Page 1
NJ, Union County
Crane-Phillips House

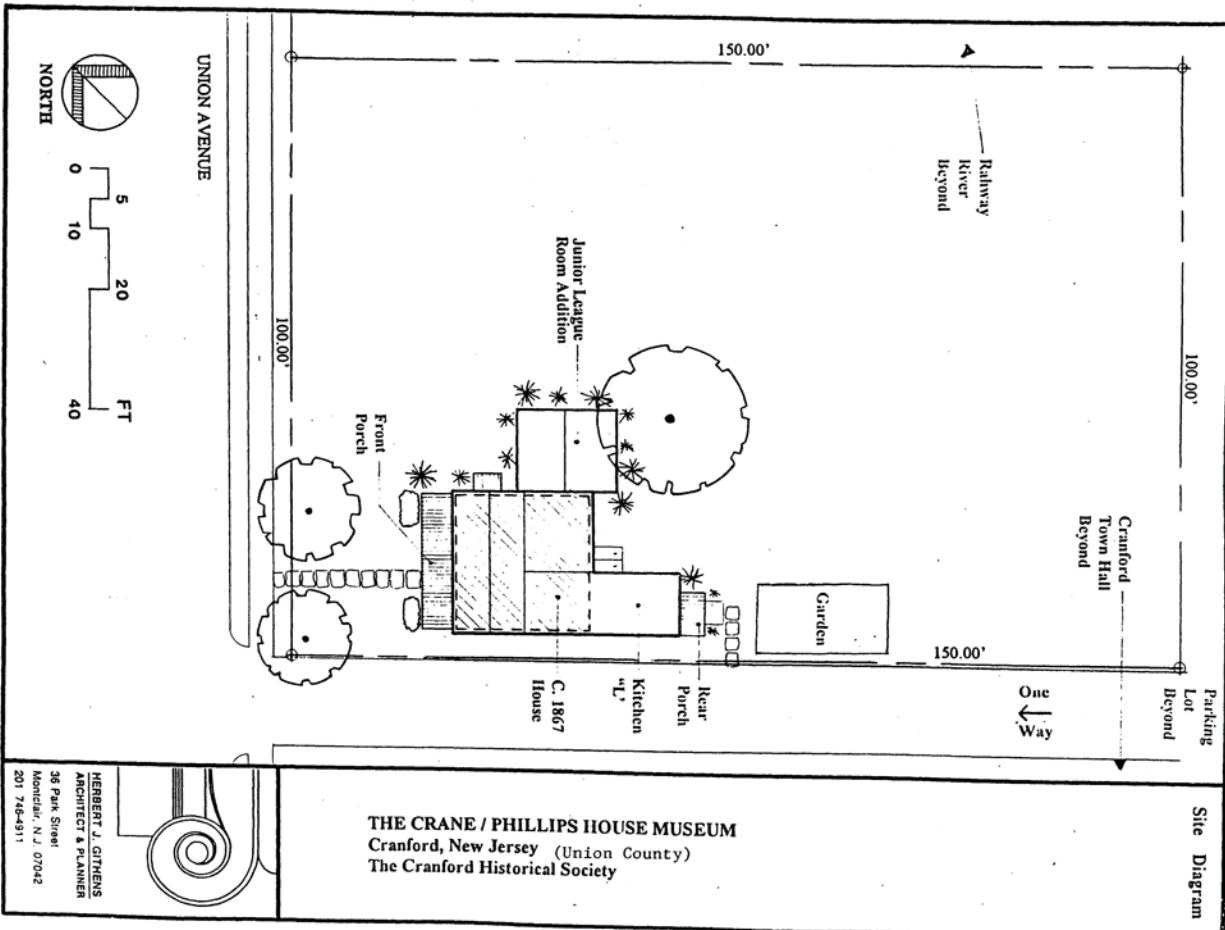
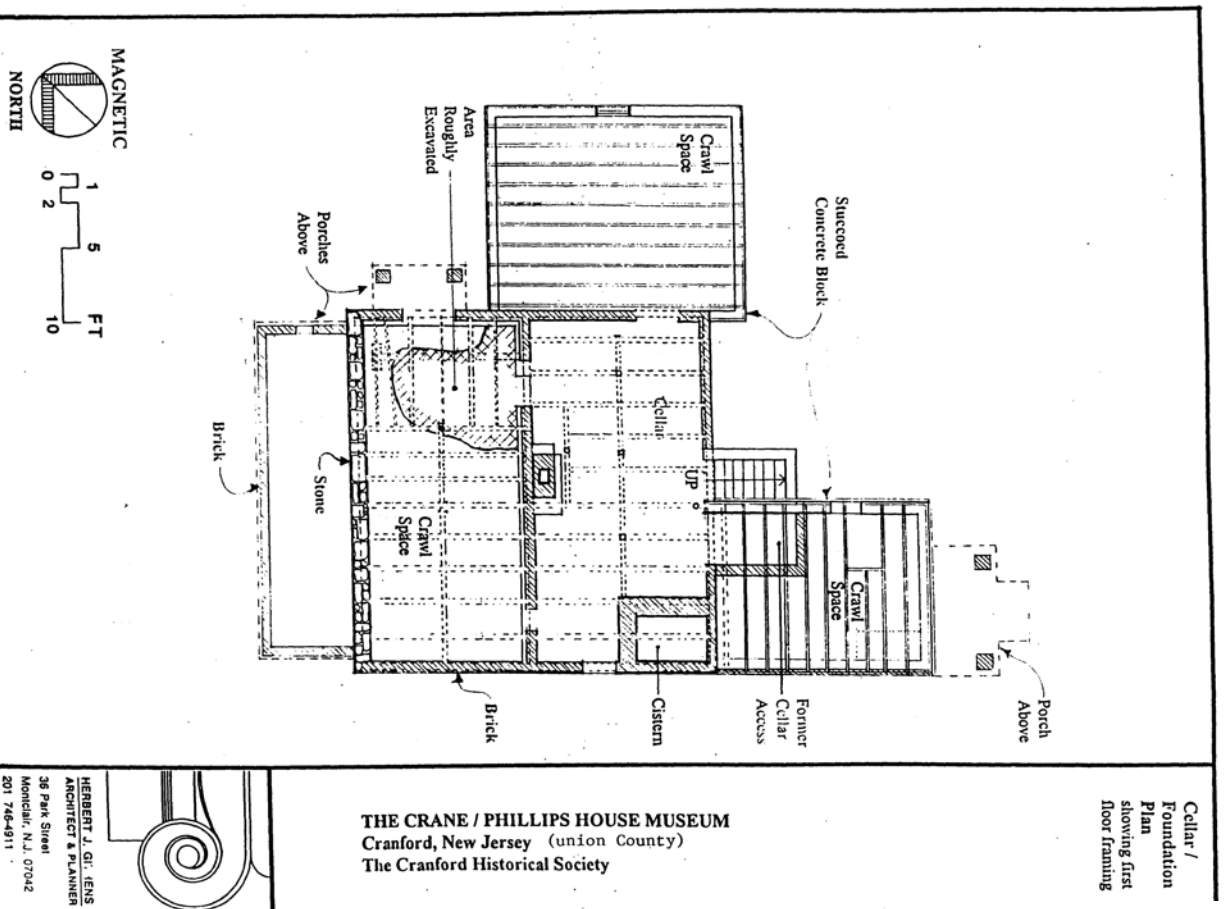
Verbal Boundary Description and Justification

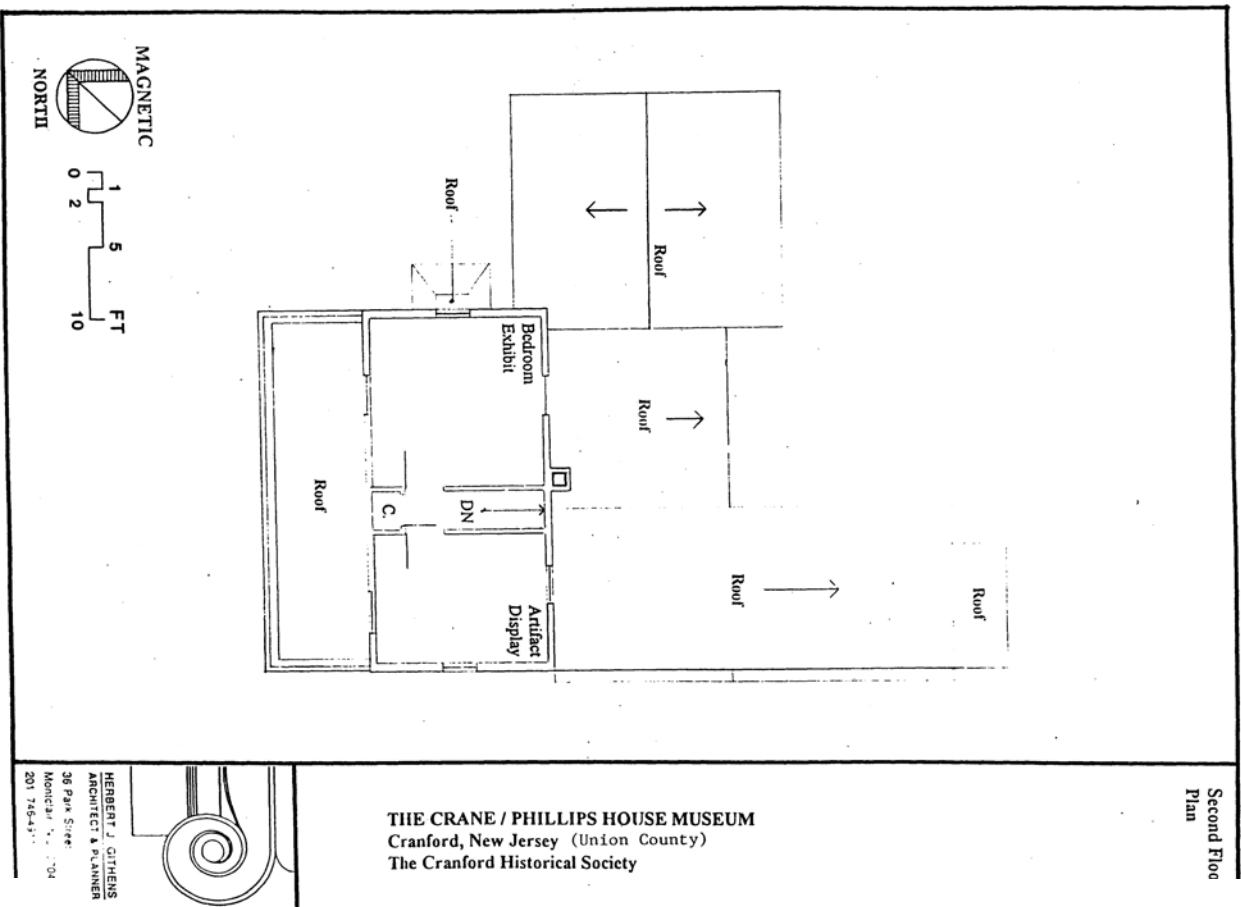
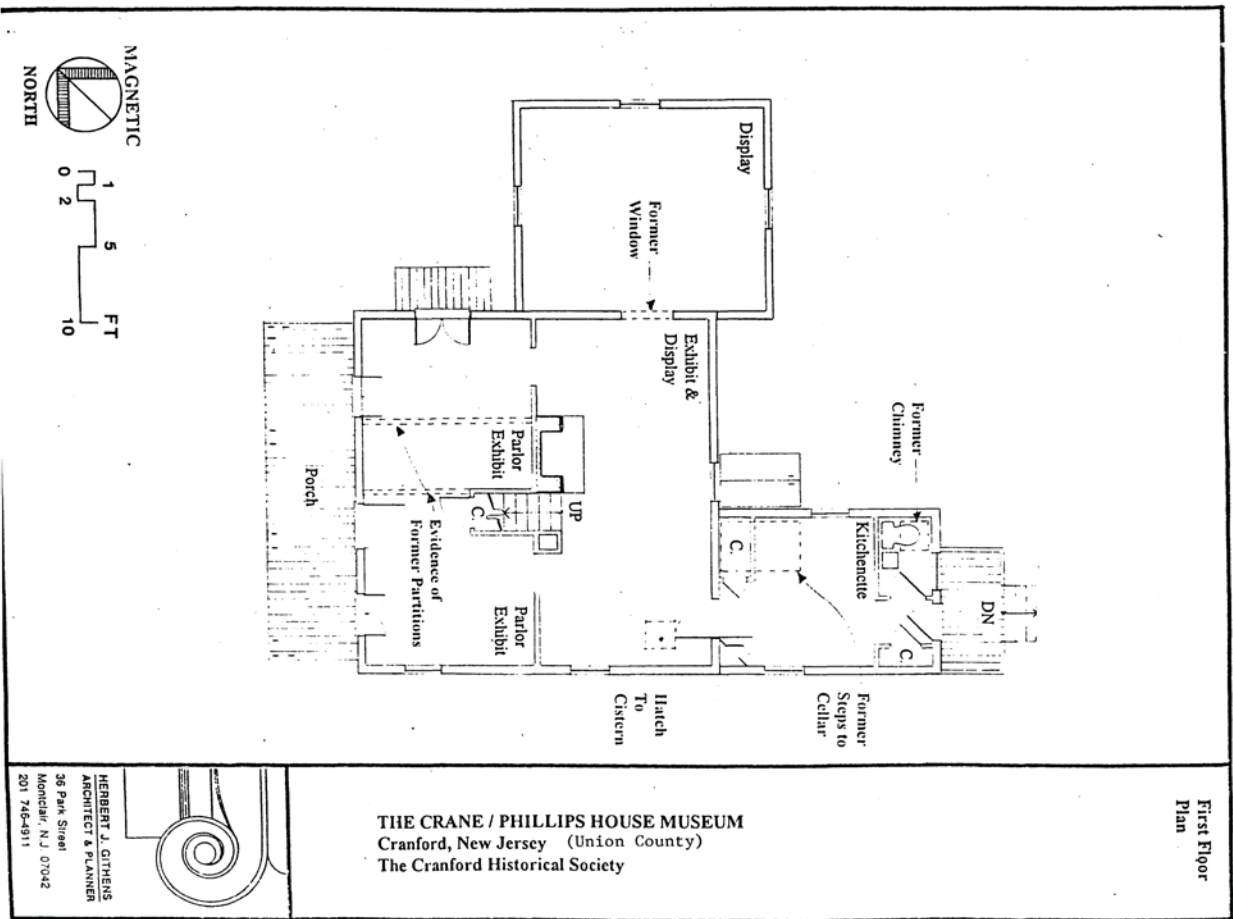
The boundary of this property is described in the Union County lease with the Cranford Historical Society as follows:

"BEGINNING at a point in the southerly side line of North Union Avenue, said point being distance 114 ft. Easterly from the intersection of said line of North Union Avenue with the easterly side line of Springfield Avenue; thence South 48 degrees 34 minutes East 150 ft. to a point; thence North 41 degrees 26 minutes East 100 ft. to a point; thence North 48 degrees 34 minutes West 150 ft. to said side line of North Union Avenue; thence South 41 degrees 26 minutes West along said side line of North Union Avenue 100 ft. to the point or place of BEGINNING."

This property is listed on the Cranford Township tax records as Lot #196, parcel #2.01.
See attached Tax Map of Township of Cranford





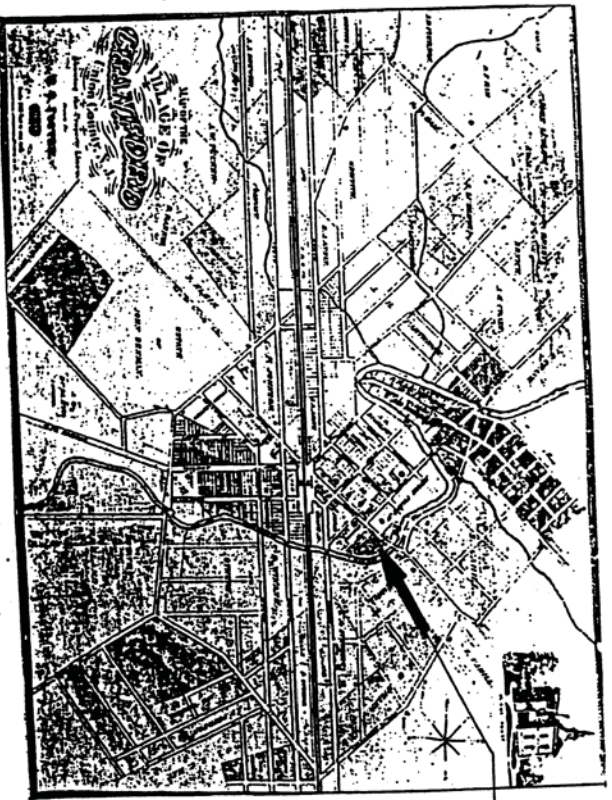


United States Department of the Interior
National Park Service

National Register of Historic Places Continuation Sheet

Section number _____ Page _____

OMB Approval No. 1024-0011



SITE

Crane-Phillips House
Map of the Village of Cranford
Union County, NJ
1870

From: 300 Days at Crane's Ford
A pamphlet written for the Cranford Historical Society, 1964

United States Department of the Interior
National Park Service

National Register of Historic Places Continuation Sheet

Section number _____ Page _____

OMB Approval No. 1024-0011

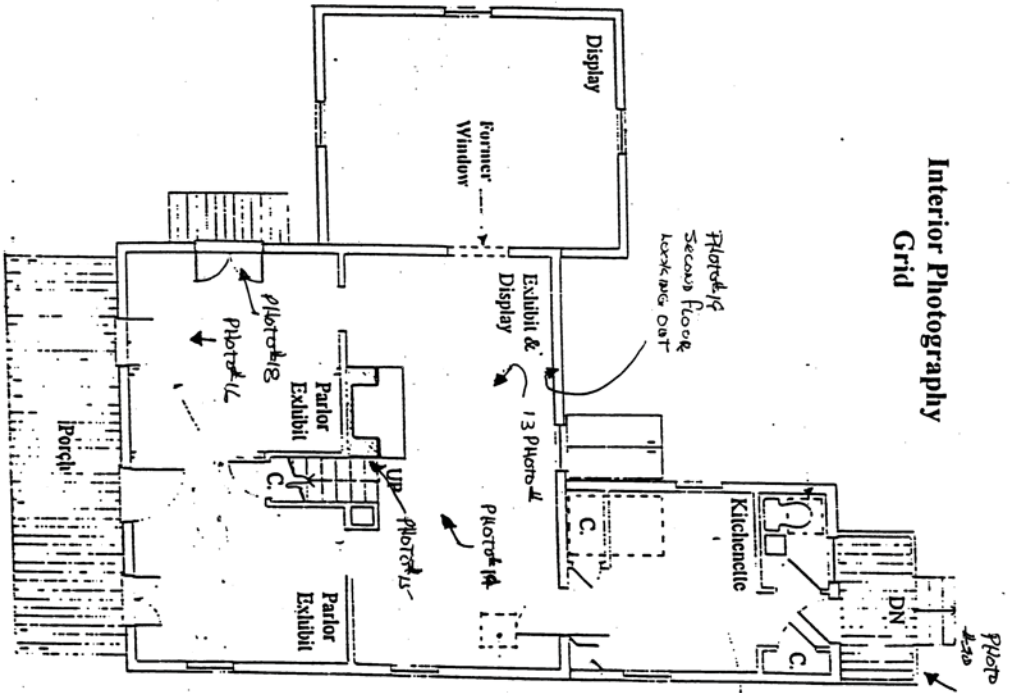
Photograph Identification

Name : Crane-Phillips House
Location : 124 North Union Avenue, Cranford, New Jersey (Union County)
Photographer : Donald H. Widows
Date of Photographs : November 1996
Negative Repository : Cranford Historical Society
38 Springfield Ave.
Cranford, NJ 07016

Photograph :

- # 1 House, View from North Union Avenue
- # 2 " East View
- # 3 " West View
- # 4 " North View
- # 5 " South View
- # 6 Front of West Wall, Second Floor
Showing scalloped vergeboard and clapboards.
- # 7 Rear of West Wall
Showing scalloped vergeboard and clapboards.
- # 8 Detail of upper West Wall
- # 9 South Side (Rear of house) Showing several sections:
Window second floor
Window dining room
Chimney
- # 10 East End of 1963 addition - Note Scalloped vergeboard.
- # 11 Main Entrance Facing North Union Avenue,
12 Right Side Front Porch -
Note French Door
Porch Supports
Sutlers
- # 13 Inside Dining Room - Looking East
Narrow Stairs
Center-wall fireplace
- # 14 Inside Dining Room - Looking West
Center-wall fireplace
- # 15 Interpretative Panel - Inner wall structure (in
stairway)
- # 16 Inside Front Parlor - French door to front porch.
- # 17 East Side - French door east end of parlor
Small balcony
Scalloped vergeboard second floor.
- # 18 Inside View Photo #17
- # 19 Inside View - Window in rear wall second floor.
- # 20 Rear entrance porch - looking North.

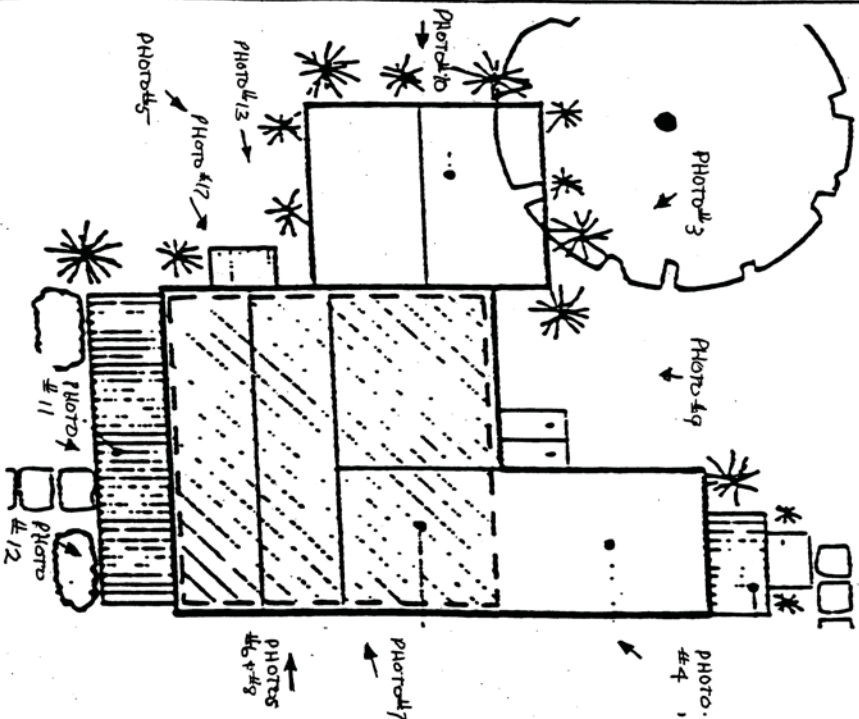
Interior Photography Grid



THE CRANE / PHILLIPS HOUSE MUSEUM
Cranford, New Jersey (Union County)
The Cranford Historical Society

HERBERT J. GITHENS
ARCHITECT & PLANNER
36 Park Street
Morristown, N.J. 07942
201 746-4811

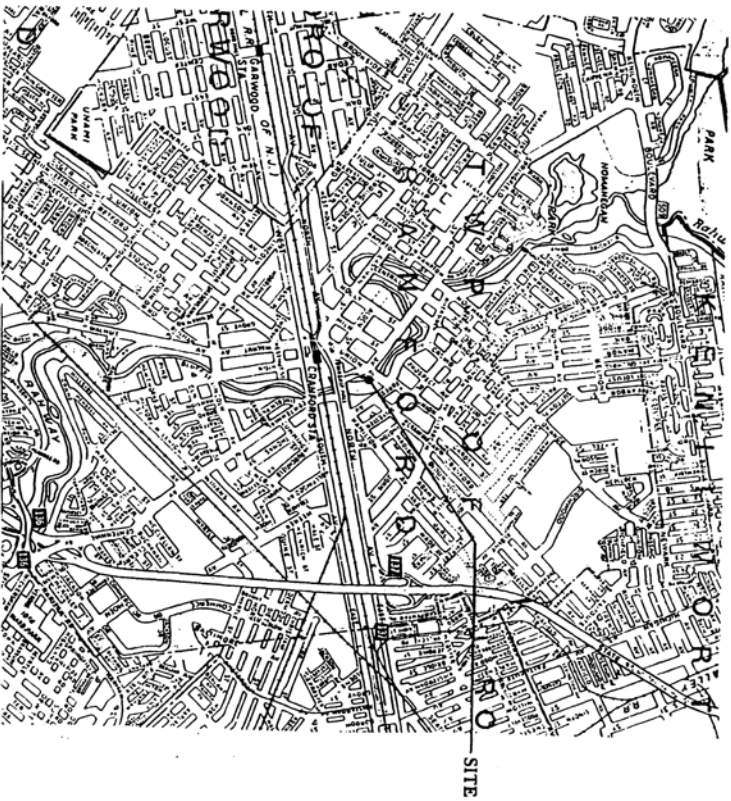
Exterior Photography Grid



THE CRANE / PHILLIPS HOUSE MUSEUM
Cranford, New Jersey (Union County)
The Cranford Historical Society

HERBERT J. GITHENS
ARCHITECT & PLANNER
36 Park Street
Morristown, N.J. 07942
201 746-4811





Location Map
The Hagstrom Map Co.
Crane-Phillips House
Cranford Township
Union County, NJ

HERBERT J. CITRUS
ARCHITECT & PLANNER
35 Park Street
Morristown, N.J. 07942
201-444-9311



PHOTO # 1 of 20
CRANE - PHILLIPS
CRAWFORD, NEW JERSEY
Union County



PHOTO # 2 of 20
CRANE-PITLIPS HOUSE
CRANFORD, NEW JERSEY
Union County
DEC. 1996 N



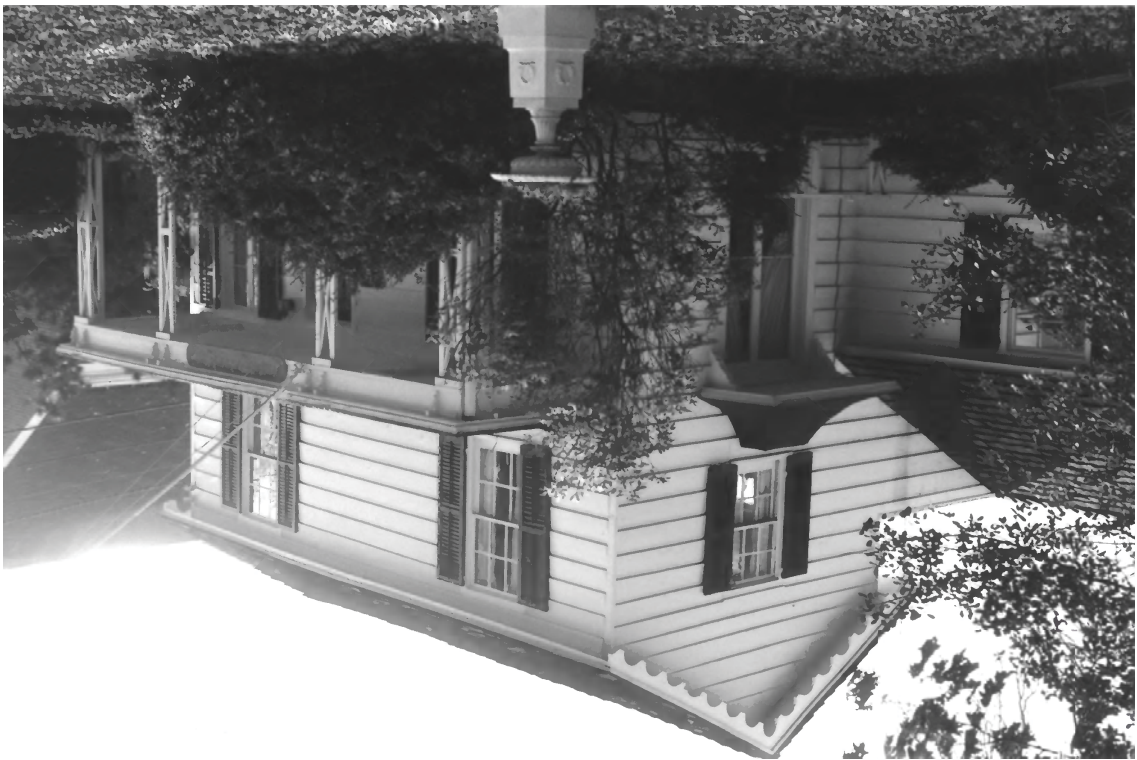
PHOTO #3 of 20
CRANE-PHILLIPS HOUSE
CRANFORD, NEW JERSEY
Union County



PHOTO #4 of 20
CRANE-PHILLIPS HOUSE
CRAUFORD, NEW JERSEY
Union County
DEC. 1996 N



PHOTO #5 of 20
CRANE-PHILLIPS HOUSE
CRANFORD, NEW JERSEY
Union County
Kodak
PAPER



KODAK
PREMIUM
CRANFORD, NEW JERSEY
CRANE-PHILLIPS HOUSE
PHOTO #6 of 20
UNION COUNTY
THIS PHOTO
MANUFACTURED
BY KODAK
PAPER
PICTURED



PHOTO # 7 of 20
CRANFORD, NEW JERSEY
Union County





PHOTO # 8 of 20
CRANE-PHILIPS HOUSE
CRANFORD, NEW JERSEY
Union County



PHOTO #9 of 20
CRANE-PHILLIPS HOUSE
GRANFORD, NEW JERSEY
Union County
Kodak
PREMIUM

PHOTO #10 of 20
CRANE-PHILLIPS HOUSE
CRAUFORD, NEW JERSEY
Union County
Kodak
PREMIUM



PHOTO #11 of 20
CRANE-PHILLIPS HOUSE
CRIMFORD, NEW JERSEY
Union County



PHOTO # 12 of 20
GRAVE-PHILIPS HOUSE
GRANFORD, NEW JERSEY
Union County



PHOTO #13 of 20
CRANE-PHILLIPS HOUSE
CRANFORD, NEW JERSEY
Union County

UNION COUNTY
CRANFORD, NEW JERSEY



PHOTO #14 of 20
CRANE-PHILLIPS HOUSE
CRANFORD, NEW JERSEY
Union County





PHOTO #15 of 20
CRANE-PHILLIPS HOUSE
GRANFORD, NEW JERSEY
Union County

PHOTO #16 of 20
GRAVE-PHILLIPS HOUSE
CRANFORD, NEW JERSEY
Union County



PHOTO #17 of 20
CRANE-PHILIPS HOUSE
CRANFORD, NEW JERSEY
Union County



UNION COUNTY
NEW JERSEY

UNION COUNTY
NEW JERSEY

Photo # 18 of 20
CRANE-PHILLIPS HOUSE
GRANFORD, NEW JERSEY
Union County



PHOTO # 20 of 20
ERDNE-PAILLIPS HOUSE
GRANFORD, NEW JERSEY
Union County



Table 1: Historic Properties within the APE and Determination of Eligibility

Address	Description	Distance*/ Direction	Eligibility	Determination of Effect	Photo Plate
Crane-Phillips House, 124 N. Union Avenue, Cranford, New Jersey	c. mid-19th century, 2-story, vernacular Carpenter Gothic dwelling/house museum with gable roof, clapboard siding and brick foundation, full-width flat roofed front porch with simple Carpenter Gothic style trim work and vergeboards at eaves. Shed roofed additions to rear, and flat roofed wing to side.	1000 feet/SE outside of APE	SR (6/3/97) NR (8/14/97)	n/a	B.5.4
First Presbyterian Church, 11 Springfield Avenue, Cranford, New Jersey	1893, 2 story Romanesque Revival church with copper roofing, wooden shingle siding, rusticated stone foundation. Tiffany designed large stained glass windows on primary facade. Four story square tower on corner of building.	300 feet/SE	Eligible NJSHPO Site Inventory # 200307	No Adverse Effect	B.5.6
123 N. Union Avenue, Cranford, New Jersey	1913, 4 story, institutional building, flat roofed, decorative bands of alternating red brick and stone work, limestone sills, replacement windows, c. 1980 modern style portico constructed on all facades.	Proposed site	Not Eligible	n/a	B.5.1
Manger Building, 108 N. Union Avenue, Cranford, New Jersey	c. 1940, 2-story, Colonial Revival, asphalt shingle roofing, red brick, windows have overscaled drip molding and decorative shutters, portico is 2 full stories and has decorative railing above.	1000 feet/SW	Not Eligible	n/a	B.5.7
117-119 N. Union Avenue, Cranford, New Jersey	c. 1945 one-story commercial building, flat roofed with shingled pent roof, permastone wall cladding, large plate glass storefront windows.	1000 feet/SW	Not Eligible	n/a	B.5.9



PHOTO LOCATOR MAP

**Plate B.5.4:**

Crane-Phillips House - North Union Avenue near Springfield Avenue. This National Register listed site is outside of the APE.

Photo view: Southeast

Photographer: Elaine Kiernan Gold

Date: August 1, 2001

NEW JERSEY OFFICE OF HISTORIC PRESERVATION HISTORIC SITES INVENTORY # 200305
INDIVIDUAL STRUCTURE SURVEY FORM

HISTORIC NAME: Lynwold
LOCATION: 136 North Union Avenue

COMMON NAME:
BLOCK/LOT 312/1

MUNICIPALITY: Cranford

COUNTY: Union

USGS QUAD: Roselle

UTM REFERENCES:

OWNER/ADDRESS: John & Christina Bracco
136 North Union Ave., Cranford, N.J.

Zone/Northing/Easting

DESCRIPTION

Construction Date: 1868/altered 1898

Source of Date: Documentation

Architect:

Builder:

Style: Classical Revival

Form/Plan Type: Irregular shape.

Number of Stories: 2½

Foundation: Fieldstone and brick

Exterior Wall Fabric: Clapboard

Fenestration: Two bay facade. Bay windows on side of house.

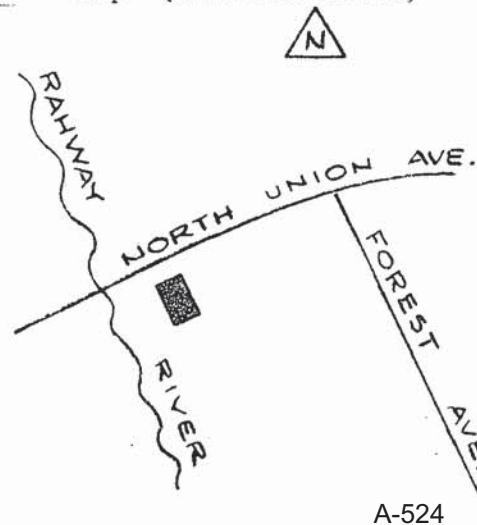
Roof/Chimneys: Gable roof with cross gables; large dormers with classical detailing.

Additional Architectural Description:

Gable end facade. Projecting pediment with heavy moldings and dentil trim.
Two story Corinthian columns and applied pilasters on corners.
One story porch wings flank facade, with paired Doric column supports.

PHOTO Negative File # Roll 32 frames 1.2

Map (Indicate North)



A-524

SITING, BOUNDARY DESCRIPTION, AND RELATED STRUCTURES:

House is located on the east bank of the Rahway River. Lawn slopes down to the Rahway and there are concrete reinforcing walls and slips for boats.

SURROUNDING ENVIRONMENT: Urban ☐ Suburban ☒ Scattered Buildings ☐
Open Space ☐ Woodland ☐ Residential ☒ Agricultural ☐
Industrial ☐ Downtown Commercial ☐ Highway Commercial ☐ Other ☐

House is at the corner of North Avenue and Forst Avenue, marking the center of an area of well-preserved Victorian houses.

SIGNIFICANCE:

The house, known as "Lynwold" was built during Cranford's post-Civil War emergence as a suburban town. When originally built, the house was a simple, frame Victorian house, with bay windows on the side, and it probably faced Forest Avenue.

The house was remodelled in 1898 by the Albertson family. The Albertson's created a grand classical "temple" by changing the orientation of the house to a gable end facade, highlighted by a large portico and wide cornice and frieze all around the house. The Albertson house, with lawns to the Rahway River, was a much-photographed house in the early 20th Century, and it appears much the same today as it did then.

ORIGINAL USE: Residence

PHYSICAL CONDITION: Excellent ☐ Good ☒

REGISTER ELIGIBILITY: Yes ☒ Possible ☒

THREATS TO SITE: Roads ☐ Development ☐

No Threat ☒ Other ☐

COMMENTS:

PRESENT USE: Residence

Fair ☐ Poor ☐

No ☐ Part of District ☐

Zoning ☐ Deterioration ☐

REFERENCES:

Cranford Historical Society

RECORDED BY: JWF
ORGANIZATION: UCCHPAB

DATE: 1981



CRANFORD

Union Co.

*200305

r. 32/1,2

Lynwold
136 North Union Ave.
looking N.E

1981



CRANFORD
Union Co.

200305

r. 32/1, 2

Lyn wold
136 North Union Ave.

looking N.

1981

NEW JERSEY OFFICE OF HISTORIC PRESERVATION HISTORIC SITES INVENTORY # 200337
INDIVIDUAL STRUCTURE SURVEY FORM

HISTORIC NAME: Sperry House
 LOCATION: 319 North Union Avenue
 MUNICIPALITY: Cranford
 USGS QUAD: Roselle
 OWNER/ADDRESS: Joseph & Janet Carroll
 Same

COMMON NAME:
 BLOCK/LOT 271/10
 COUNTY: Union
 UTM REFERENCES: Zone/Northing/Easting

DESCRIPTION

Construction Date: ca. 1890

Source of Date: Visual, documentation

Architect:

Builder:

Style: Queen Anne Cottage

Form/Plan Type: Square shape,

Number of Stories: 2½

Foundation: Not visible.

Exterior Wall Fabric: Clapboard & shingles.

Fenestration: Wide 1/1 sash windows.

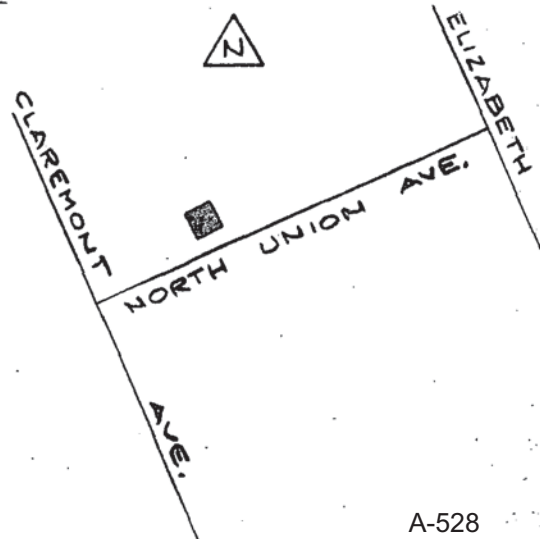
Roof/Chimneys: Gable and hipped roof.

Additional Architectural Description:

Porte-cochere with pyramid roof to side of house.
 Great use of projecting and receding elements on facade.
 House is quite large, but the breaking up of it into parts and small
 projections give the appearance of a small "cottage"-like dwelling.

PHOTO Negative File # Roll 26, Frame 20, 17

Map (Indicate North)



RY DESCRIPTION, AND RELATED STRUCTURES:

In the center of the block of the North Union Avenue Streetscape.
(See 200342).

SURROUNDING ENVIRONMENT: Urban [] Suburban [X] Scattered Buildings []
Open Space [] Woodland [] Residential [X] Agricultural []
Industrial [] Downtown Commercial [] Highway Commercial [] Other []

Located on a street of fine, turn-of-the-century homes.

SIGNIFICANCE:

The house was owned by Thomas Sperry, a New York merchant, who was one of the early suburban commuters. Sperry, and his partner Hutchinson, created the S&H Green Stamp business. Sperry later built a huge house on Prospect Avenue, along the Rahway River, but it was demolished after World War II to erect apartments. Thomas Sperry and his brother William speculated heavily in real estate in Cranford and were active in civic affairs, helping to turn the advertisements of Cranford as a lovely, healthful town into reality.

ORIGINAL USE: Residence	PRESENT USE: Residence
PHYSICAL CONDITION: Excellent [X] Good []	Fair [] Poor []
REGISTER ELIGIBILITY: Yes [X] Possible []	No [] Part of District []
THREATS TO SITE: Roads [] Development []	Zoning [] Deterioration []
No Threat [X] Other []	
COMMENTS:	

Well preserved house in a setting of other well-preserved homes of similar vintage.

REFERENCES:

Cranford Historical Society

RECORDED BY: JWF
ORGANIZATION: UCCHPAB

DATE: 1981



CRANFORD
Union Co.

* 200337
R26/20

Sperry Hse,
319 North Union Ave
looking N.W.

1981

and Co.
Engineering-Science, Inc.

NJ Transit Historic I
Structure Inventory Form

LOCATION

NJ TRANSIT Line	Raritan Valley
Town/City	Cranford
County	Union
USGS Quad	Roselle

Milepost	16.23
Feature Crossed	Walnut Street
Common Name	(243) Walnut Street
UTM Ref.	18.580740.4500520

PHYSICAL SUMMARY

Structure Type	Through Plate Girder
Overall Length	136'
Spans	3
Material(s)	Steel
Skew	71
Inspection Report	No
Bridge Typology Code	XX XX XX XX

Deck Type	Ballasted
Width	78'
Span Length	45'
Design Loading	E-60
Tracks	6
Condition	Good

HISTORICAL SUMMARY

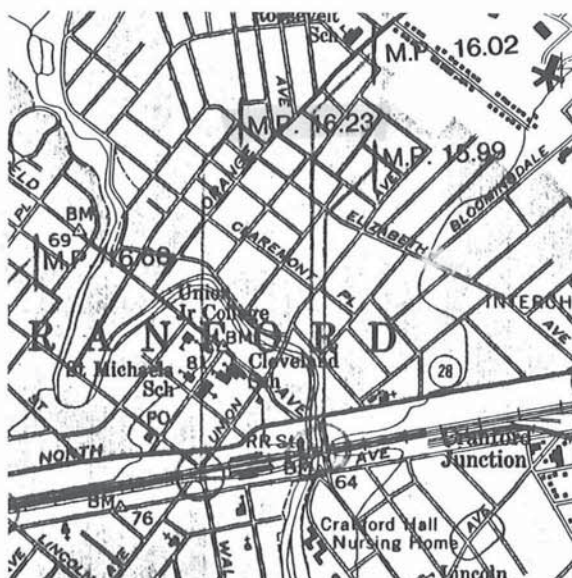
Date	1929
Designer	CNJ
Patent	

Date(s) Rebuilt	
Fabricator	Bethlehem Steel
Historic Rail Line	CNJ Central Div., Main Line

CULTURAL RESOURCE EVALUATIONS

National Register Status	
State Register Status	
Local Landmark Designation	
HAER Doc. #	
Type and Date of HAER documentation	

National Register Date	
State Register Date	

GRAPHICS

DELEUW, GATHER and Co.
Engineering-Science, Inc.

NJ Transit Historic Railroad Bridge Survey
Structure Inventory Form

PHYSICAL DESCRIPTION OF BRIDGE AND SETTING

16.23 Walnut Street abuts Cranford Station and is a wide three-span through plate girder bridge. The bridge is a major physical presence in the town, especially due to the numerous built up cross braced columns (a total of 18 support the bridge) which are braced by arched brackets. The abutments are concrete and are integrated with the stairways to the station.

STATEMENT OF TECHNOLOGICAL MERIT AND HISTORICAL SIGNIFICANCE

16.23 Walnut Street was constructed as part of a grade crossing elimination in the town of Cranford by the CNJ during 1929. The project included four structures: 15.99 Centennial Avenue, 16.02 Rahway River, 16.23 Walnut Street and 16.66 Lincoln Avenue. This grade crossing elimination project does not have historical significance due to its late date and lack of associational characteristics. These projects, which had become a national issue by 1900, were sometimes characterized by highly publicized negotiations between the towns and railroads. The bridges in this project are also not considered technologically significant as railroad structures. The four bridges in Cranford were built on the Main Line of the CNJ's Central Division which provided passenger and freight service.

RECOMMENDATION OF ELIGIBILITY TO NATIONAL REGISTER OF HISTORIC PLACES

Not eligible

PRIMARY AND SECONDARY SOURCES

See New Jersey Transit Historic Railroad Bridge Survey, Report Bibliography.

Survey Team A. Cottrell, J. Galvin
Survey Date 08/01/90

Reviewer A.C.
Review Date 03/12/91

Photographs P18/3-5
Slides S18/1-4

New Jersey Department of Environmental Protection
Historic Preservation Office

Page 1 of 1

CONTINUATION SHEET

Historic Sites #:

Resource Name: Raritan Valley Line over Walnut Street, MP 16.23

Municipality: Cranford

County: Union

Additional Information:

Representation in Existing Surveys:

This bridge was surveyed in 1991 as part of NJ Transit's *Historic Railroad Bridge Survey*.

National Register Eligibility:

The 1991 survey only evaluated the bridge for its individual eligibility, not as a component of a historic district. In that context, the bridge was evaluated as not eligible for inclusion in the National Register of Historic Places.

In 1995, the entire Central Railroad of New Jersey Main Line was determined to be eligible for inclusion in the National Register of Historic Places as a linear historic district. Therefore, the purpose of the current study is to determine if individual resources such as bridges contribute to the historic district's overall significance.

As a resource that falls within the historic district's period of significance, the bridge has been determined to contribute to the CRRNJ Main Line Historic District.

Attachments:

- 1991 NJ Transit Historic Railroad Bridge Survey form

Survey Name: NJ Transit Raritan Valley Line Study

Date: July, 1999

Surveyor: Nancy L. Zerbe

Organization: ARCH², Inc.

NEW JERSEY OFFICE OF HISTORIC PRESERVATION HISTORIC SITES INVENTORY # 200333
INDIVIDUAL STRUCTURE SURVEY FORM

HISTORIC NAME:	COMMON NAME:
LOCATION: 1-15 Walnut Avenue	BLOCK/LOT 479/1
MUNICIPALITY: Cranford	COUNTY: Union
USGS QUAD: Roselle	UTM REFERENCES:
OWNER/ADDRESS: Sunkin, Leonard & John 837 Beckman Dr. No. Bellmore, N.Y.	Zone/Northing/Easting

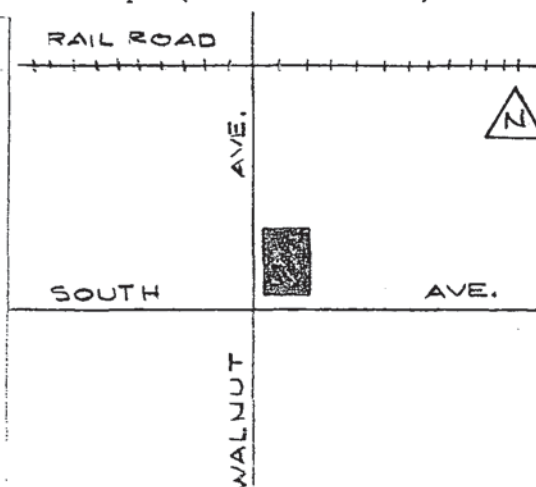
DESCRIPTION

Construction Date: 1930's	Source of Date: Visual, maps
Architect:	Builder:
Style: Art Deco/Commercial	Form/Plan Type: Rectangular block
Number of Stories: One story.	
Foundation: Not visible.	
Exterior Wall Fabric: Pale yellow/tan tiles.	
Fenestration: Five bays - each one a commercial unit, with large glass display windows and central door.	
Roof/Chimneys: Flat roof behind crenellated cornice.	
Additional Architectural Description:	

Flat, smooth surfaced building accented by color. Blue tiles laid in diamond shape above each bay. Roofline and window features outlined in peach colored tiles.

PHOTO Negative File # Roll 33 frame 27A

Map (Indicate North)



A-534

This block of stores occupies most of the east side of the block of Walnut Street between South Avenue and Union Avenue. Both of these streets are main commercial streets in Cranford.

SURROUNDING ENVIRONMENT: Urban ☐ Suburban ☒ Scattered Buildings ☐
Open Space ☐ Woodland ☐ Residential ☐ Agricultural ☐
Industrial ☐ Downtown Commercial ☒ Highway Commercial ☐ Other ☐

Across Walnut Avenue from the late 19th Century Cranford Hotel.

SIGNIFICANCE:

Art Deco is an unusual style within Union County. On this simple building, decorative elements of Art Deco styling have been applied, and are well preserved.

ORIGINAL USE: Commercial

PHYSICAL CONDITION: Excellent ☐ Good ☒

REGISTER ELIGIBILITY: Yes ☐ Possible ☐

THREATS TO SITE: Roads ☐ Development ☐

No Threat ☒ Other ☐

COMMENTS:

PRESENT USE: Commercial

Fair ☐ Poor ☐

No ☒ Part of District ☐

Zoning ☐ Deterioration ☐

REFERENCES:

Cranford Historical Society

RECORDED BY: JWF

DATE: 1981

ORGANIZATION: UCCHPAB



CRANFORD
Union Co

* 200333
R33/27

1-15 Walnut Ave.
looking E.

1981

NEW JERSEY OFFICE OF HISTORIC PRESERVATION HISTORIC SITES INVENTORY # 200334
INDIVIDUAL STRUCTURE SURVEY FORM

HISTORIC NAME: Cranford Hotel
LOCATION: 2 Walnut Avenue

COMMON NAME: "The Porch" Restaurant
BLOCK/LOT 476/1

MUNICIPALITY: Cranford
USGS QUAD: Roselle

COUNTY: Union
UTM REFERENCES:

OWNER/ADDRESS: Cranford Hotel Inc.
2 Walnut Ave., Cranford, N.J.

Zone/Northing/Easting

DESCRIPTION

Construction Date: 1892-93

Source of Date:

Architect:

Builder:

Style: Italianate Commercial

Form/Plan Type: Rectangular block.

Number of Stories: Three

Foundation: Not visible.

Exterior Wall Fabric: Brick. Common Bond. Now painted.

Fenestration: 1/1 under segmental arched lintels

Roof/Chimneys: Flat roof with corbeled cornice.

Additional Architectural Description:

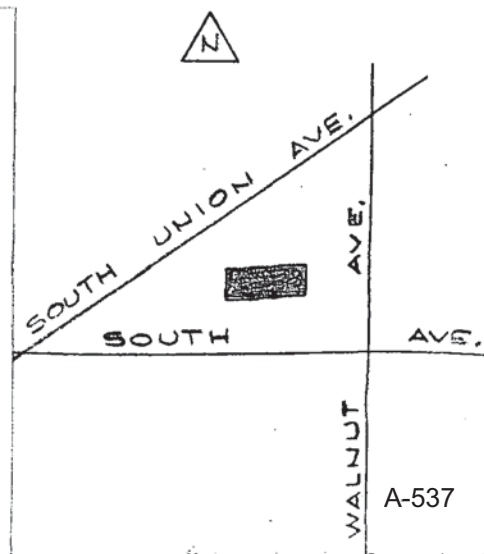
Stone beltcourse marks each floor.

Wooden cornice with small paired brackets along the top of the first floor.

Two story wooden porch across most of the facade. Enclosed on the first floor.

PHOTO Negative File # Roll 33 frame 28A; slide

Map (Indicate North)



Located on the south side of the tracks of the Central Railroad of New Jersey, facing the railroad.

SURROUNDING ENVIRONMENT: Urban ☒ Suburban ☐ Scattered Buildings ☐
Open Space ☐ Woodland ☐ Residential ☐ Agricultural ☐
Industrial ☐ Downtown Commercial ☒ Highway Commercial ☐ Other ☐

SIGNIFICANCE:

The first hotel on this site was built in 1882. In ten years, it was demolished, and the present building, known as the Hess Hotel, was constructed.

ORIGINAL USE: Hotel
PHYSICAL CONDITION: Excellent ☐ Good ☒ Fair ☐ Poor ☐
REGISTER ELIGIBILITY: Yes ☒ Possible ☒ No ☐ Part of District ☐
THREATS TO SITE: Roads ☐ Development ☐ Zoning ☐ Deterioration ☐
No Threat ☒ Other ☐
COMMENTS:

REFERENCES:

Cranford Historical Society

RECORDED BY: JWF
ORGANIZATION: UCCHPAB

DATE: 1981



CRANFORD
Union Co.

* 200334
R33/28

Cranford Hotel
2 Walnut Ave.
looking S.E.

1981

BASE SURVEY FORM

Historic Sites #:

Property Name: Cranford Hotel

Street Address: Street #: 2

(Low) (High)

Apartment #:

Street

Prefix: Name: Walnut

County(s): Union

Municipality(s): Cranford

Local Place Name(s):

Ownership: Private

USGS Quad: Roselle

Block(s): Lot(s):

Zip Code:

Suffix: Type: ST

Description:

The three-story, brick Italianate building is located immediately south of the railroad crossing. The building is three bays wide and eight bays long. A cornice of stepped-brick corbels emphasizes the flat roof. The elliptical arch windows are modern replacements. A two-story porch located along the north elevation is enclosed with brick and metal-framed glass at the first floor. Paired wood brackets divide the first and second floors and a beltcourse of rusticated stone divides the basement and first floor. The entrance is located at an angle at the northwest corner of the building.

Registration and Status

National Historic Landmark:

SHPO Opinion:

Dates:

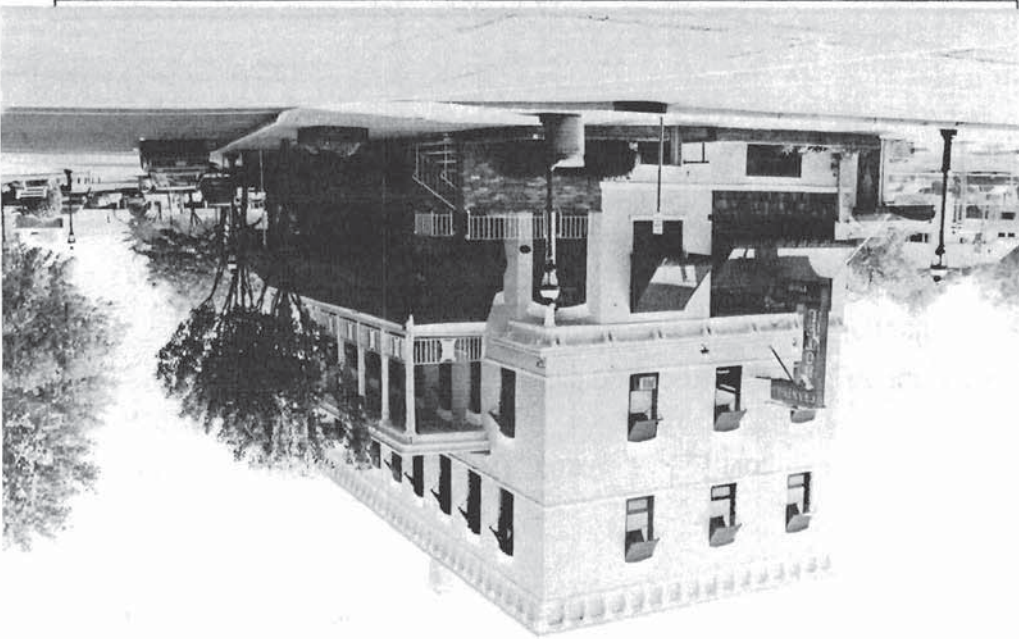
New Jersey Register:

Other Designation:

Determination of Eligibility:

Other Designation Date:

Photograph:



Survey Name:

NJ Transit Raritan Valley Line

Surveyor:

Nancy L. Zerbe, Stacy E. Spies

Organization:

ARCH, Inc.

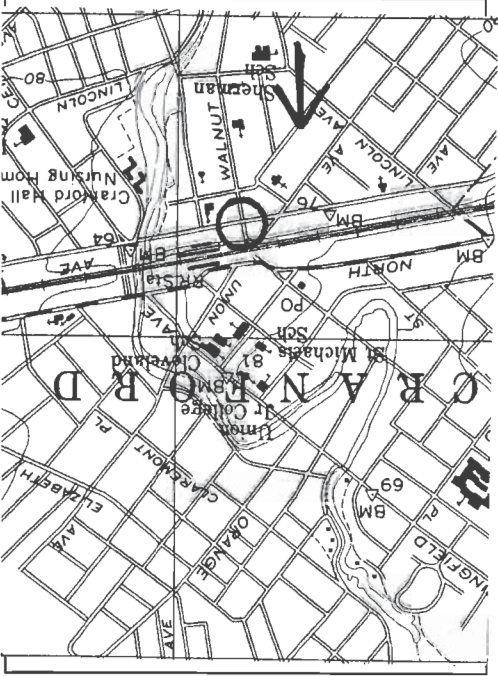
Date:

September, 1999

BASE SURVEY FORM

Historic Sites #:

Location Map:



Bibliography/Sources:

Additional Information:

More Research Needed? ☒ Yes ☐ No

INTENSIVE-LEVEL USE ONLY:

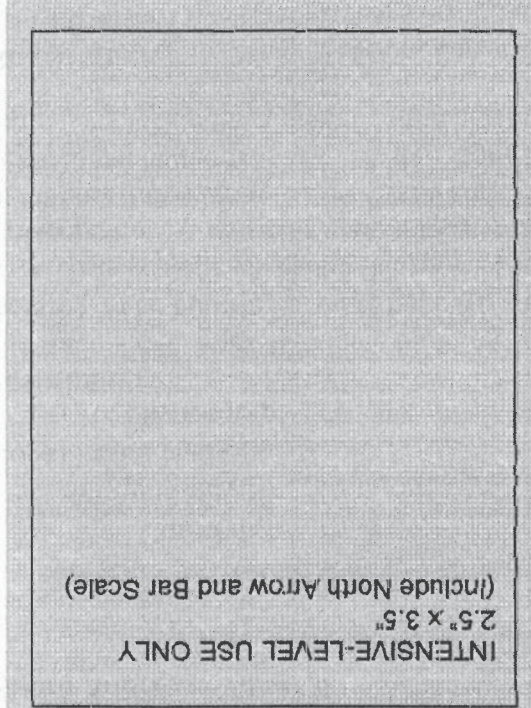
Attachments Included: _____ Building/Element _____ Landscape _____ Farm _____
_____ Bridge _____ Industry _____

Historic District: ☐ Yes ☐ No Historic District Name: _____

Status: _____ Key Contributing _____ Contributing _____ Non-contributing _____

Associated Archaeological Site/Deposits? ☐ Yes ☐ No
(Known or potential sites – If yes, please describe briefly)

Site Map:



Survey Name: NJ Transit Raritan Valley Line
Surveyor: Nancy L. Zerbe, Stacy E. Spies
Organization: ARCH², Inc.
Date: September, 1999

NEW JERSEY OFFICE OF HISTORIC PRESERVATION HISTORIC SITES INVENTORY # 200332
INDIVIDUAL STRUCTURE SURVEY FORM

HISTORIC NAME:

COMMON NAME:

LOCATION: 217 Walnut Avenue

BLOCK/LOT 484/13

MUNICIPALITY: Cranford

COUNTY: Union

USGS QUAD: Roselle

UTM REFERENCES:

OWNER/ADDRESS: Posyton, R & C c/o Mrs. F. Jeannot
217 Walnut Ave., Cranford, N.J.

Zone/Northing/Easting

DESCRIPTION

Construction Date: ca. 1880

Source of Data: Visual

Architect:

Builder:

Style: Second Empire

Form/Plan Type: Symmetrical cube.
Central hall plan.

Number of Stories: 2½

Foundation: Covered with cement.

Exterior Wall Fabric: Clapboard siding.

Fenestration: Three bay facade. Tall 2/2 sash windows.

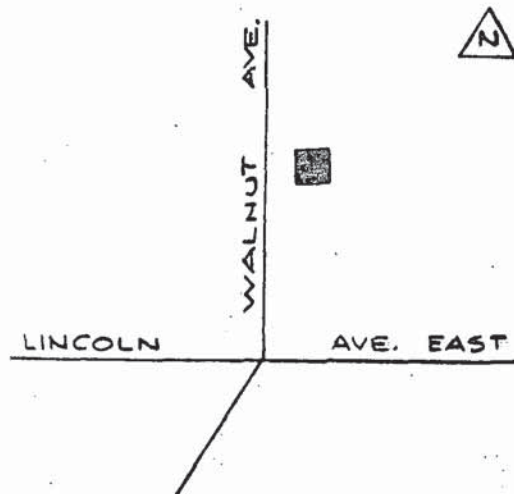
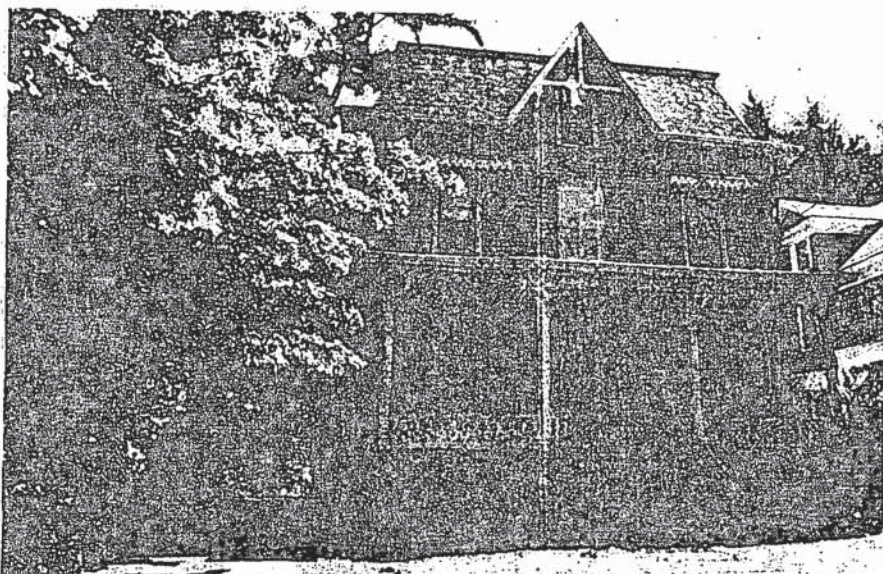
Roof/Chimneys: Mansard roof.

Additional Architectural Description:

Cross gable centered on facade.
 Scroll brackets under the eaves.
 Stick-style type bargeboard in cross gable.
 Double front door with transom light above.
 Bay windows on side of house.
 Ornate wooden trim around front porch.

PHOTO Negative File # Roll 33 frame 22A, 21A

Map (Indicate North)



A-542

Y DESCRIPTION, AND RELATED STRUCTURES:

House is on a large lot. It has a small frame shed-garage at the rear of the property.

SURROUNDING ENVIRONMENT: Urban ☐ Suburban ☒ Scattered Buildings ☐
Open Space ☐ Woodland ☐ Residential ☒ Agricultural ☐
Industrial ☐ Downtown Commercial ☐ Highway Commercial ☐ Other ☐

Walnut Avenue was primarily developed in the second half of the 19th Century as a residential street. Many frame houses on the street date to that time.

SIGNIFICANCE:

Walnut Avenue was developed in the second half of the 19th Century as a suburban, residential street. This house is a good example of the Second Empire Style, and one of the best preserved homes on the street.

ORIGINAL USE: Residence

PHYSICAL CONDITION: Excellent ☐ Good ☒

REGISTER ELIGIBILITY: Yes ☐ Possible ☒

THREATS TO SITE: Roads ☐ Development ☐

No Threat ☒ Other ☐

COMMENTS:

PRESENT USE: Residence

Fair ☐ Poor ☐

No ☐ Part of District ☐

Zoning ☐ Deterioration ☐

REFERENCES:

Cranford Historical Society

RECORDED BY: JWF
ORGANIZATION: UCCHPAB

DATE: 1981



CRANFORD, Union Co.

* 200332

R 33/22

217 Walnut Ave.
looking S.E.

1981



CRANFORD, Union Co.

217 WALNUT AV.

looking E.

* 200332

R 33/21

1981

NEW JERSEY OFFICE OF HISTORIC PRESERVATION HISTORIC SITES INVENTORY # 200331
INDIVIDUAL STRUCTURE SURVEY FORM

HISTORIC NAME:

COMMON NAME:

LOCATION: No. 239 Walnut Ave.

BLOCK/LOT 484/4

MUNICIPALITY: Cranford

COUNTY: Union

USGS QUAD: Roselle

UTM REFERENCES:

OWNER/ADDRESS: Noera, Richard & Sylvia
239 Walnut Ave., Cranford, N.J.

Zone/Northing/Easting

DESCRIPTION

Construction Date: ca. 1875

Source of Date: visual

Architect:

Builder:

Style: Second Empire

Form/Plan Type: Side hall plan

Number of Stories: Three

Foundation: Fieldstone

Exterior Wall Fabric: Aluminum siding.

Fenestration: Three bay facade. 2/2 sash windows. Tall French windows on first floor facade.

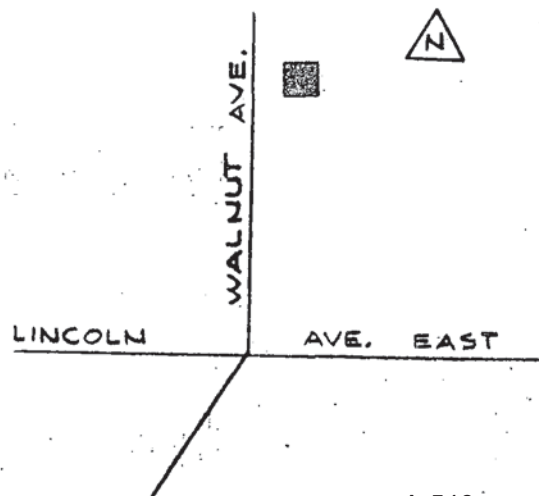
Roof/Chimneys: Mansard roof. Slate shingles. Two brick chimneys.

Additional Architectural Description:

First floor porch with wooden supports and brackets.
Gable roof dormers contain peaked windows.
Double front doors.

PHOTO Negative File # Roll 33 frame 19A, 20A

Map (Indicate North)



Y DESCRIPTION, AND RELATED STRUCTURES:

House sits in center of lot; gabled garage to rear. Original lot larger, as were others on Walnut, but divided for in-fill house construction.

SURROUNDING ENVIRONMENT: Urban ☐ Suburban ☒ Scattered Buildings ☐
Open Space ☐ Woodland ☐ Residential ☒ Agricultural ☐
Industrial ☐ Downtown Commercial ☐ Highway Commercial ☐ Other ☐

Walnut Avenue was developed in the second half of the 19th Century as a residential street. Many frame houses on the street date to that time.

SIGNIFICANCE:

Theodore A. Crane - owner 1905-1909. He was a descendent of the Crane family who first settled in Cranford.

This house has been recently restored, and although aluminum siding was used, much of the other wooden detailing has been preserved. This is one of the best preserved of the homes on Walnut Avenue. Walnut Avenue was developed in the late 19th Century as a residential street at the beginning of Cranford's "suburbanization".

ORIGINAL USE:

PHYSICAL CONDITION: Excellent ☐ Good ☒

REGISTER ELIGIBILITY: Yes ☒ Possible ☒

THREATS TO SITE: Roads ☐ Development ☐

No Threat ☒ Other ☐

COMMENTS:

PRESENT USE:

Fair ☐ Poor ☐

No ☐ Part of District ☐

Zoning ☐ Deterioration ☐

REFERENCES:

Cranford Historical Society

Union County Landmark Inventory, 1976

RECORDED BY: JWF
ORGANIZATION: UCCHPAB

DATE: 1981



CRANFORD
Union Co.

*200331
B 33/19

239 Walnut Ave.
looking N.

1981





CRANFORD
Union Co.

* 200331
R 33/20

239 Walnut Ave.
looking N.E.

1981

Inventory of Identified Architectural Resources

Property	Property Information	Description	NRHP Eligibility
 <p>Photograph Number: 70</p>	<p>Panamerican Resource Number: 70</p> <p>110 Orchard Street</p> <p>Municipality: Cranford, Union County</p> <p>UTM coordinates (1983 Zone 18N): 4500911N, 558291E</p> <p>Date: 1949</p> <p>Other: House backs up to the Rahway River and has a backyard view of McConnell Park.</p>	<p>Style: Minimal Traditional w/Colonial Revival elements</p> <p>Stories: 1</p> <p>Plan: L shape</p> <p>Roof: gable with asphalt shingles</p> <p>Exterior: wood siding and flush board</p> <p>Foundation: concrete</p> <p>Windows: 6/6 wood originals and other original configurations</p> <p>Details: House features the "Early American" decorative elements of dentil molding, front door side lights, and a fanciful "Colonial Cupola" flying a weather vane in a bird shape.</p>	<p>This house is non-contributing to the NRE North Cranford HD (ID #3838; Bary 2012) and is not eligible for the NRHP since it does not meet the criteria.</p>
 <p>Photograph Number: 71</p>	<p>Panamerican Resource Number: 71</p> <p>Garage and house</p> <p>114 Orchard Street</p> <p>Municipality: Cranford, Union County</p> <p>UTM coordinates (1983 Zone 18N): 4500928N, 558271E</p> <p>Date: 1914</p> <p>Other: House backs up to the Rahway River and a small stream runs along its west side. It has a backyard view of McConnell Park.</p>	<p>Style: Four square</p> <p>Stories: 2.5</p> <p>Plan: square; now has an addition</p> <p>Roof: hipped</p> <p>Exterior: stucco</p> <p>Foundation: obscured</p> <p>Windows: various types of originals covered with storms; front windows have upper fan, door has side lights, decorative shutters</p> <p>Details: House has been significantly changed. The porch is new and not sympathetic. The window treatments, the metal balustrades on sidewalks, and the changes to the eaves detract. A flat roofed stucco garage was added at a later date.</p>	<p>The house is eligible as a contributing element to the NRE North Cranford HD (ID #3838; Bary 2012). The garage, a much later addition, is not eligible and non-contributing.</p>

APPENDIX B: HISTORIC DISTRICT DOCUMENTATION

This appendix contains survey forms and other documentation for all baseline historic districts listed above. These documents are extracted and reproduced from the original sources, and are organized by survey ID number as presented in **Table 5: Baseline Inventory, Historic Districts**. As such, documents from the same source may not appear in the same order as originally organized. Phase 1 resource ID numbers have been appended to the original documents in order to aid navigating this section.

(Page intentionally left blank)

2003072

Bibliography: Cranford Board of Trade. Cranford, New Jersey. 1913. Central Railroad of New Jersey. "Why Not Own Your Own Home on the Line of the Central Railroad of New Jersey." 1891.

Physical Description: The 1914 one-span built-up thru girder with floor beams bridge is supported on a concrete substructure. The stringers are encased, and both sides of the girders have knee braces. The top of the girders are set with pipe railings, a once common detail. The cantilevered sidewalks are enclosed with the original decorative iron fence-like railings. The bridge appears to be unaltered.

Historical and Technological Significance: The 1914 thru girder bridge was designed by County Engineer Jacob L. Bauer. It is a representative example of a popular early-20th century bridge type. The span is historically significant because of its location in a locally identified National Register-eligible historic district made up of well-preserved late-19th and early-20th century neighborhoods developed as part of the suburbanization of the greater New York area (criterion A).

Cranford, a township established in 1871, is located on the Central Railroad of New Jersey (CNJ) main line to its terminal in Jersey City and New York City connections. Because of its location on the rail line, the CNJ and local interests promoted Cranford as an ideal situation for successful New York City businessmen who were desirous of a comfortable home in a country setting within commuting distance to the city. With the absence of industry and the advantage of the scenic Rahway River meandering through the town, Cranford was consciously developed as a residential community geared to wealthy New York professionals. Promoted as "The Venice of New Jersey," the town is characterized by large well-preserved picturesque and academic Colonial Revival homes on generous, casually landscaped lots. The river played a large factor in both the physical development of the comfortable community, and its image, as recreational use of the Rahway has been highlighted throughout the town's postbellum history. The river was treated as an amenity to enhance the residential development of the community. In many places the riverbank is lined with stone walls and other ornamentation, and both streets and houses are oriented toward the most dominant physical feature in the potential historic district.

The North Cranford Historic District is a large residential enclave that retains its pre-1930 character. The well-preserved neighborhood ranks as one of the premier railroad-stimulated suburbs in New Jersey. Architecturally it reflects the architectural preferences of the pre-1929 Depression era, and historically it illustrates the strong influence of the railroad on postbellum and early-20th century development in northern New Jersey. Cranford is located about 17 miles west of New York City, and in 1913, the community was serviced by 93 daily trains. The trip, including the ferry connection, was 45 minutes. The initial stimulus for Cranford becoming a "railroad suburb" came in 1864 when the CNJ opened its first bridge over Newark Bay and a new passenger station in the center of what was a crossroads community known as Cranville. Shortly thereafter farms were converted into subdivisions.

The area was identified as a potential National Register historic district after a community wide review by the Cranford Historical Society in 1989. The formal application for National Register listing has not been completed, but the area clearly meets the criteria for inclusion in the Register. This bridge is one of four identified in the potential historic district that has a period of significance from 1838, when train service was introduced to the town, through 1929 (2003025,

DRAFT

2003008, 2003014, 2003072).

Boundary Description and Justification: The bridge is not individually significant, but it is located within and is completely surrounded by an potential architecturally significant late-19th and early-20th century residential historic district with a period of significance through until 1929. Since the 1914 bridge was built within that period of significance and it is not significantly altered, it is a contributing resource. Therefore, the bridge and its surroundings are significant. The limit of the district appears to be beyond the houses adjacent to the west side of the bridge.

4.0 RESULTS OF THE HISTORIC ARCHITECTURAL INVESTIGATION

4.1 North Cranford Historic District

The neighborhood near the Eastman Street Bridge, extending beyond the area of potential effects, was studied to determine whether a potential historic district existed in the area. After conducting background research and field survey, it was determined that a historic district does exist in the area near the Eastman Street Bridge. This district was referred to as the North Cranford Historic District in the Lichtenstein survey of the bridge, and this name has been used in this report to identify the district. The period of significance for the district is 1860 to 1930. This represents the period during which the community transformed from a rural farming village to a suburban railroad community. It also represents the period during which the majority of all houses and structures located within the district were constructed. (See Appendix A for photographs of houses within the district.)

The survey form that was completed by A.G. Lichtenstein and Associates for the Eastman Street Bridge (see Appendix D) described the North Cranford Historic District as follows:

The North Cranford Historic District is a large residential enclave that retains its pre-1930 character. The well-preserved neighborhood ranks as one of the premier railroad-stimulated suburbs in New Jersey. Architecturally it reflects the architectural preferences of the pre-1929 Depression era, and historically it illustrates the strong influence of the railroad on postbellum and early 20th century development in northern New Jersey (A.G. Lichtenstein and Associates 1994).

The North Cranford Historic District is a residential historic district that is roughly bounded by West End Place to the north, Holly Street to the south, Orchard Street to the west, and Springfield Avenue to the east (Figure 8). A few streets are included in the district that extend beyond these street boundaries. The district is comprised of late 19th- and early 20th-century houses. The majority of house styles are Queen Anne Victorian and Colonial Revival, with a few examples of Tudor Revival, Georgian Revival, Shingle Style and Bungalow homes. Most houses are in excellent condition and retain a high degree of integrity of design and materials.

Several houses included within the district were identified during the 1981 Union County Historic Sites Inventory. All houses, except for 400 Springfield Avenue and 20 Pittsfield Street, were recommended as eligible for listing in the National Register by the county survey. These houses are listed in the table on the following page. (See Appendix E for copies of the Union County Historic Sites inventory forms for these buildings.)

Survey Number	Address	Eligibility
200310	400 Springfield Avenue	No
200315	17 Berkeley Place	Yes
200316	20 Pittsfield Street	No
200317	222 Orchard Street	Yes
200318	22 Central Avenue	Yes
200345	Holly Street	Yes

Contributing resources to the historic district include two bridges located on Eastman Street and two bridges located on Springfield Avenue. These bridges were constructed during the period of significance and retain their character-defining features, such as fence-like iron railings. However, the second bridge located on Eastman Street is a modern replacement and does not retain any character-defining features of the bridge that existed there previously.

These four bridges were identified during the 1994 Historic Bridge Survey conducted by A.G. Lichtenstein and Associates. A June 30, 1995 review letter, prepared by the New Jersey Historic Preservation Office, concluded that the bridges are not individually eligible, but are contributing resources to a North Cranford Historic District. (See Appendix F for a copy of the relevant text from the HPO review letter.) The bridges are listed in the table below.

Survey Number	Street Address	Construction Date	Eligibility
2003008	Eastman Street	1918	Yes
2003014	Springfield Avenue	1916, altered 1979	Yes
2003025	Eastman Street	1913, altered 1985	Yes
2003072	Springfield Avenue	1914	Yes

Several small parks, including McConnell Park and Hampton Park also are included as contributing resources to the North Cranford Historic District. The district also includes interesting objects, or street furniture, such as stone walls and concrete obelisks with letter tiles used at intersections to identify street names.

4.2 Eastman Street Bridge (Bridge Cr8, #2003008)

The Eastman Street Bridge was constructed in 1918 and was designed by Jacob L. Bauer, County Engineer. The bridge replaced an earlier bridge that stood in the same location. The bridge carries a two-lane street and two sidewalks over the Rahway River. It is a one-span thru-girder bridge with concrete abutments. It has rolled floorbeams that are encased except for the bottom flanges. Both sides of the girders have knee braces. The bridge has cantilevered sidewalks that have decorative fence-like iron railings. The American Bridge Company fabricated the bridge girders, but the bridge was erected by the Dover Boiler Works (DVB).

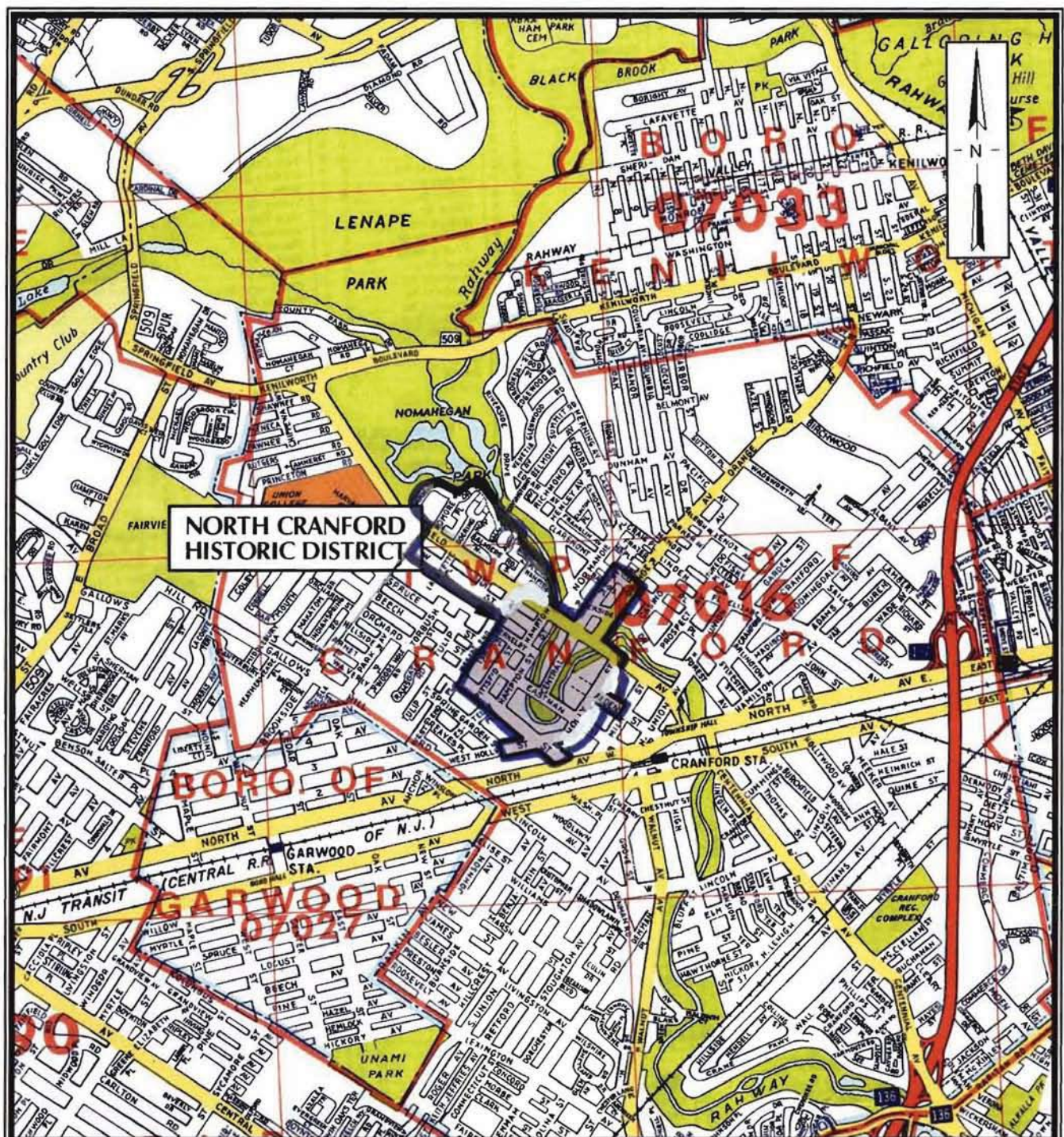


Figure 8: Hagstrom map showing the boundary of the North Cranford Historic District.

0 3520
FEET

Phase II Cultural Resource Investigation
Eastman Street Bridge
Cranford Township, Union County, New Jersey

CULTURAL
RESOURCE
CONSULTING
GROUP



HPO-H2001-208
PROD
01-2588
Page 1 of 3

DONALD T. DiFRANCESCO
Acting Governor

State of New Jersey
Department of Environmental Protection
Division of Parks & Forestry, Historic Preservation Office
PO Box 404, Trenton, NJ 08625-0404
TEL: (609) 292-2023 FAX: (609) 984-0578
www.state.nj.us/dep/hpo

Robert C. Shinn, Jr.
Commissioner

MEMORANDUM

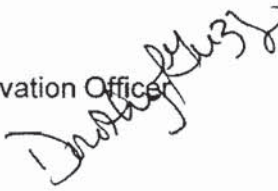
TO: Rob Piel, Manager
Bureau of Inland Regulation, Land Use Regulation Program

ATTN: Kim Kessler

FROM: Dorothy P. Guzzo, Deputy State Historic Preservation Officer
Historic Preservation Office

DATE: August 23, 2001

RE: Union County, Cranford Township
Replacement of Eastman Avenue Bridge over Rahway River
2003-01-0002.1



Thank you for referring the above referenced permit application to the Historic Preservation Office (HPO) for review and comment. These comments were prepared based on the following reports:

"Phase II Cultural Resource Investigation, Eastman Street Bridge, Cranford Township, Union County, New Jersey" by CRCG (July 12, 2001).

"Bridge Alternative Analysis for the Eastman Street Bridge over the Rahway River in Cranford Township, Union County, New Jersey" by Schoor DePalma, Inc. (July 2001).

The information in these reports was supplemented by additional information submitted by CRCG on July 16, 2001.

Identification of Historic Properties

One historic property has been identified within the APE. I concur with the assessment of the reviewed report, that the **North Cranford Historic District is eligible** to be listed in the New Jersey Register of Historic Places and the National Register of Historic Places (NJRHP/NRHP) under Criteria A and C. This neighborhood is a good example of Cranford's residential growth and development during the 19th and 20th

centuries as was transformed from a rural farming village to a suburban railroad community. The period of significance for the historic district is 1860-1930. The boundaries of the historic district extend beyond the area of potential impacts for this bridge replacement project. I have attached a map of approximate boundaries for your reference and use. The following resources within the APE contribute to the North Cranford Historic District:

- 9 Norman Place (B 171, L6)
- 20 Pittsfield Street (B 167, L1)
- 19 Hampton Street (B 172, L 6)
- 18 Hampton Street (B 179, L 1)
- 16 Hampton Street (B 179, L 2)
- 12 Hampton Street (B 179, L 3)
- 8 Hampton Street (B 179, L 4)
- 109 Hampton Street (B 167, L6)
- Hampton Park (B 168, L 1)
- Eastman Street Bridge
- McConnell Park (B 180, L 1)
- 3 Central Avenue (B 169, L 1)
- 5 Central Avenue (B 169, L 2)
- 7 Central Avenue (B 169, L 3)
- 2 Central Avenue (B 184, L 3)
- 126 Eastman Street (B 184, L 2)
- 122 Eastman Street (B 184, L 1)
- 25 Holly Street (B 179, L 17)
- 101 Holly Street (B 185, L 1)
- 28 Holly Street (B 187, L3)

Assessment of Effects

The proposed project will have an **adverse effect** on identified historic properties because it involves the demolition of a contributing resource – the Eastman Street Bridge – in an eligible historic district.

I concur that the Eastman Street Bridge is an extreme state of deterioration. According to the reviewed Alternatives Analysis report, so many of the character defining features would have to be altered or replaced as part of a rehabilitation, that this type of project is not advisable. I concur with this recommendation and agree that replacement is warranted. Therefore I **recommend permit issuance with the following conditions:**

1. The existing slate sidewalk on the southwestern quadrant of the bridge will be maintained as part of the project. If it cannot be retained, it will be replaced in kind with slate of similar dimensions and color.
2. The stone veneer on the parapets and abutments shall be of native stone and shall be no less than 6 inches thick. A test panel of the veneer shall be made for field approval by HPO staff and/or representatives from the local historic preservation commission.
3. Union County shall ensure that the landscape surrounding the new bridge will be compatible with the rural character of the North Cranford Historic District. Each tree 12" in caliper (diameter at breast height) or larger which is to be removed during construction shall be replaced by two trees of the same species, or a locally comparable and appropriate species, which are no less than 2.5" in caliper (diameter at breast height) and which have a two year warranty.

HPO-H2001-208

PROD

01-2588

Page 3 of 3

4. Any riprap used as part of the project shall not be of gabion construction. Instead loose native stone shall be used.
5. Final plans and specifications shall be submitted to the Historic Preservation Office for review and approval.

If you have any questions regarding this memo, please contact Andrea Tingey of my staff at (609-984-0539). Thank you.

cc Patrick Sheppard, NJDEP-LUR (w/ attach)
Dean Talcott, Union County Engineering (w/attach)



The North Cranford Historic District
Boundaries



Phase II Historic Architectural Investigation
Springfield Avenue Bridge
No. 2003014 Over Rahway River
Township of Cranford, Union County, New Jersey

6.0 EVALUATION OF ELIGIBILITY

Resources 50 years of age or older within the A.P.E. were evaluated using the National Register eligibility criteria (36 CFR 60.4) as outlined in the National Park Service's (1995) *National Register Bulletin 15: How to Apply the National Register Criteria for Evaluation*.

North Cranford Historic District

The North Cranford Historic District retains its integrity of location, design, materials, feeling, workmanship, setting, and association as a late-19th-century and early-20th-century suburban residential historic district. The area is comprised of a large number and dense collection of residential buildings that survive relatively intact with few areas of infill. The vast majority of houses retain a high degree of integrity of design, materials, and workmanship. The district still maintains its late-19th to early-20th-century setting, characterized by its wide streets lined with mature trees and sidewalks, similar house setbacks, and historic concrete obelisk street signs. Since the North Cranford Historic District retains its integrity, it is able to convey its historical and architectural significance discussed in the following paragraph.

The North Cranford Historic District was determined eligible for listing on the National Register of Historic Places by the NJDEP-HPO on 8/23/01 and subsequently received a Certification of Eligibility on 5/19/05 that included an extension of the boundaries to include the Hanson House. The historic district meets Criteria A for its contribution to Cranford's residential growth and development during the late 19th and early 20th century as the township transformed from a rural farming community into a suburban neighborhood for wealthy businessmen. The Rahway River and the Central Railroad of New Jersey were key factors in the success of Cranford's development. The river was a strong influence in the community, forming a part of picturesque neighborhoods and providing recreational activities for residents. The railway enabled Cranford to attract New York professional who could live in the community and commute to work in the city. The historic district is also eligible for listing in the National Register under Criterion C as a historic district with representative examples of numerous styles and types of architecture that include Queen Anne and Colonial Revival, with lesser numbers of Tudor Revival, Shingle style and Craftsman. The historic district's period of significance is 1860 to 1930 (CRCG 2001: 24). No evidence was found to suggest that the district is eligible under Criterion B for association with significant individuals, or under Criterion D for having the ability to yield information important to history and/or pre-history. Since the North Cranford Historic District retains its integrity and is able to convey its significance, it remains eligible for listing on the National Register of Historic Places.

New Jersey Department of Environmental Protection
Historic Preservation Office

HISTORIC DISTRICT OVERLAY**Historic Sites #****District Name:** North Cranford Historic District**County(s):** Union**District Type:** Residential**Municipality(s):** Cranford Twp. 2003**USGS QUAD:** Roselle**Local Place Name(s):** _____**Development Period:** 1860 To 1930**Physical Condition:** Excellent**Remaining Historic Fabric:** High**Registration
and Status****National Historic Landmark:** _____**SHPO Opinion:** 8/23/01**Dates:****National Register:** _____**Local Designation:** _____**New Jersey Register:** _____**Other Designation:** _____**Determination of Eligibility:** _____**Other Designation Date:** COE: 5/19/05**Description:**

The North Cranford Historic District is a residential historic district comprised of late-19th-century and early-20th-century single-family homes. Prevalent house styles include mostly Queen Anne and Colonial Revival, with lesser numbers of Tudor Revival, Shingle style and Craftsman. The historic district's period of significance is 1860 to 1930 and represents a period during which the community was transformed from a rural farming village to a suburban railroad community. When originally evaluated in 2001 the district also included four contributing bridges: two on Eastman Street and two on Springfield Avenue; however, three bridges have been replaced within the last 10 years. The new bridges on Eastman Street and one on Springfield Avenue were constructed in 2003 and 2004 and no longer contribute to the historic district. The North Cranford Historic District is roughly bounded by Park Drive to the north, Riverside Drive and Springfield Avenue to the northeast (including Central and Orange Avenues between Springfield Avenue and Day Place), Holly Street to the southeast (including Alden Street between Holly and Miln Streets), Orchard and Willow Streets to the southwest, and Brookside Place to the northwest (A.D. Marble 2004: 26).

Setting:

The district is characterized by wide suburban boulevards lined with mature trees and sidewalks and featuring concrete obelisk-shaped street markers. The suburban district also boasts several municipal parks that connect the neighborhood to the Rahway River, which served as the primary means of leisure and entertainment during the community's heyday.

Survey Name: Vanessa Zeoli**Date:** September 2008**Surveyor:** Phase II Springfield Ave. Bridge, Cranford, NJ**Organization:** Cultural Resource Consulting Group

industrial roots and can be almost mistaken for natural land forms as a result of their isolation and the mature trees and other flora that cover them. They have lost their integrity, and without integrity they cannot be eligible for the NRHP. Panamerican recommends PRN 103 (a series of eight earthen explosives storage structures associated with American Can Company) not eligible for the National Register.

5.1.4 Cranford Township. The largest number of resources recorded is in Cranford. This includes two historic districts—North Cranford Historic District and the Central Railroad of New Jersey (CNJ) Mainline Corridor Historic District—as well as the continuation of the Rahway River Parkway and Union County Parks historic districts. Five Union County parks are located in Cranford: Nomahegan, McConnell, Sperry, Lincoln, and Droescher's Mill, all of which are eligible for the NRHP as contributing elements to both the Rahway River Parkway Historic District and the Union County Park System Historic District. The Cranford municipal parks include: Memorial Park, Hampton Park, Girl Scout Park (with the Cranford Clay Tennis Courts), and Hanson Park, all of which are eligible for the NRHP as contributing elements to the Rahway River Parkway Historic District (see the large fold-out map at the back of the report for location of these resources).

All of the surveyed houses and their related outbuildings and garages are located in Cranford with the majority of them located in the North Cranford Historic District, which encompasses a gathering of homes from the period 1850 to 1929. These homes were built during the period of Cranford's early expansion, which was linked to the railroad and the arrival of commuters primarily from New York seeking the bucolic virtues of the area offered by the Rahway River.

In order to best discuss the various aspects of the resources surveyed in Cranford, the report presents those resources located in a historic district first, followed by a discussion of those resources not in a historic district, which are presented by street beginning geographically at the northwestern-most point in the survey area. While many of the resources are located in multiple, overlapping districts, a resource will be discussed primarily within the one historic district with which it is most identified. This report will note any other historic district(s) in which it may also be included. Section 6.0, Inventory of Identified Architectural Resources, lists all resources surveyed and all the historic districts of which they are a part.

5.1.4.1 North Cranford Historic District. The North Cranford Historic District (ID #3838) was determined eligible for the NRHP by the New Jersey HPO on August 23, 2002. The district has not formally submitted a nomination form to the National Register and is still in its formative state some ten years after its Determination of Eligibility (DOE). The North Cranford Historic District (NCHD) is being created and eventually will be regulated in some form by the Cranford Historic Preservation Board.

The NCHD encompasses those neighborhoods on the Rahway River where the town began and contains many of the houses built between approximately 1850 and 1929 that “provide the character and historical feel of [the] town” (Bary 2010). After the first trains connected New York and Craneville (Cranford's earlier name), New Yorkers began to flood the area, drawn to its rural beauty and the Rahway River, which offered a plethora of recreational activities for Manhattanites who had tired of fighting city life. Sylvester Cahill and the Bigelows, Alden, William, and Charles, arrived on the scene in the early 1860s and saw an opportunity. They purchased land and laid out a village in 1865 in a two-square-block area between Holly and Union avenues where they built more than 30 residences. This development was followed

by Dr. Phinaes P. Lounsbury's development of Central Avenue and Cahill's development of the area around Forest and Elizabeth avenues (Bary 2010).

The NCHD has undergone a number of boundary changes since the Preservation Board first began creating it. A 2009 Cranford Master Planning Map (T&M Associates) shows the district's northwestern-most point beginning at Park Avenue, outside the boundaries of Nomahegan Park, and flowing through the neighborhoods bounded by Springfield Avenue to the south and the Rahway River to the north. The district then jumped Springfield Avenue and continued south along West End Place and east roughly along Orchard Street to Holly Street and north to Springfield Avenue. The district jumped Springfield Avenue extending along Casino Avenue to Day Place and east along Day Place to Normandie Street and then south back to Springfield Avenue.

By 2012 the boundary has changed dramatically (Figure 5.2). The northwestern-most point is West End Place one block north of Springfield Avenue and extending along West End Place to southwesterly to approximately one block south of Orchard Street. Eliminating the neighborhoods north to Nomahegan Park, the boundary roughly follows the 2009 district line to the south. However, the NCHD greatly expanded to the northeast, almost doubling in size on the east side of the Rahway River. The district extends southeast along Springfield Avenue past Hampton Road to Highway 28, North Avenue/Westfield Street. To the northeast, the district is roughly bounded approximately by Elizabeth and Manor avenues and the northwest by Orange Avenue (see Figure 5.2). This is a huge area, encompassing thirteen full blocks and thirteen partial blocks of housing, three Union County parks (the Rahway River Parkway, McConnell, and Sperry), and four municipal parks (Memorial, Hampton, Girl Scout, and Hanson). The NCHD also includes the original site of Crane's Ford and the memorial that marks that site, as well as the National Register Listed (NRL) Crane-Phillips House (Reference #97000842) at 124 Union Avenue within Sperry Park.

According to the Preservation Board, the NCHD occupies one-third square mile of Cranford's total 4.8 square miles, and includes 1,100 of the approximate 8,000 houses in Cranford. Further, it encompasses more than 50 percent (about 570) of all houses built in Cranford between 1800 and 1929. The district includes the Lounsbury Tract (Central Avenue), the Dayton, Eastman & Bigelow Company tract (Alden Street, Eastman Street, and Holly Street), and the Cahill Tract (Forest Street). The board also believes that the boundaries may change again (Docstop.com nd).

The houses within the district tend to be extremely large and many are situated on large, deep lots. The houses lining Central Avenue, the west side of Holly Street, the east side of Hampton Street, and the north side of Orchard Street are typical. Most measure more than 4,000 square feet and have deep back lots that end at the Rahway River. When these houses were first constructed, many had boathouses or landings on the river which are no longer extant. Even today, most of these properties still have racked canoes in their backyards. The houses were designed to take advantage of the river, one of the original, primary draws of the area (Figure 5.3).

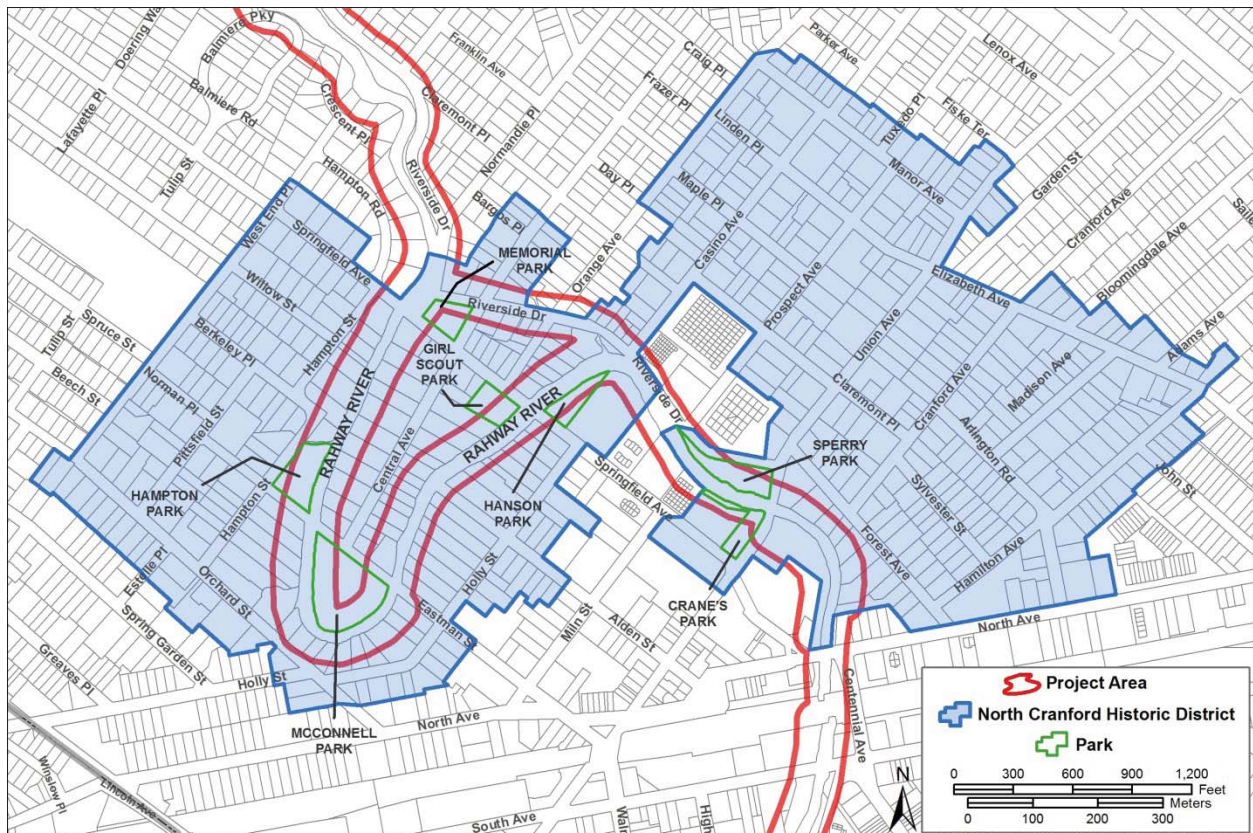


Figure 5.2. Street map of Cranford showing extent of the North Cranford Historic District and the parks within the district in relation to the Project Area.

House styles in the district run the gamut of late Victorian and early twentieth-century styles with very few true high style residences. Most, instead are exuberant interpretations and blendings of Queen Anne, Stick, Shingle, Colonial Revival, Dutch Colonial, Tudor, Romanesque, Gothic Revival, Italianate, Eastlake, and Second Empire. Certainly, vernacular stylings are found, but they are the exception to the rule in this district. Smaller houses are also found but they too are exceptions. In the NCHD, bigger is better. In keeping with the exuberance of interpretation of style, ornamentation tends to be applied with a heavy hand; in the NCHD, excess of ornamentation is best (Figure 5.4). Many more houses within the district likely had extensive ornamentation at one time, but have lost it during the twentieth century as owners applied new siding, made changes to modernize the look of the house, or tired of painting the detailed carpentry work.



Figure 5.3. Rear of houses on Central Avenue as seen from the Rahway River between Springfield Avenue and Eastman Street, showing the deep back yards, racked canoes, and a rock decorated landing, ca. 1910 (Fridlington and Fuhro 1995:15).

Within the APE, Panamerican surveyed resources (e.g., houses, garages/outbuildings, bridges, parks, park elements, monuments, and infrastructure) on all or parts of the following streets within the NCHD: Hampton Street, Orchard Street, Eastman Street, Holly Street, Central Avenue, Springfield Avenue, Riverside Drive, Orange Avenue, Union Avenue, Forest Street, and North Avenue/Highway 28. In addition, the three Union County parks (Rahway River Parkway, and McConnell and Sperry parks) and the four municipal parks (Memorial, Hampton, Girl Scout, and Hanson) within the district are also within the APE and were surveyed. The monument at Crane's Ford and the rock wall on Riverside Drive at the Rahway River were also inventoried. Since the APE follows the Rahway River, Panamerican did not survey parts of the historic district away from the river (i.e., outside the APE), which included all areas in the northeastern part of the district north of Forest Street, Central Avenue north of Riverside Drive, and the connecting streets west of Hampton Street to West End Place, including West End Place itself.

Because of the size of the APE, many of the houses themselves were not surveyed since they were outside the APE, but their outbuildings or separate garages were surveyed since they were situated within the APE. In determining National Register eligibility for outbuildings or separate garages, Panamerican looked to the eligibility status of the primary structure, unless it was abundantly clear that the ancillary buildings were modern. In Section 6.0, Inventory of Identified Architectural Resources, when only an ancillary building was surveyed, the primary building is noted and its NRHP eligibility status given.



Figure 5.4. House at the corner of Holly Street and Alden Street, within the NCHD but not the APE, which illustrates the blending of styles and the exuberant use of ornamentation within the district (Ms. Kelly Nolte, Panamerican 2012).

As noted, the NCHD is still very much in the planning stages. Most of its buildings have been surveyed, but a list of non-contributing buildings has apparently not been created. In speaking with a representative of the Preservation Board, they believe that the slate sidewalks, the granite curbs and the remaining horse hitching posts within the district are contributing elements to the district (Figures 5.5, 5.6, 5.7, and 5.8). However, there is less immediate consensus about the contributing status of the parks, the road network, the bridges, and other hardscape such as rock walls and that associated with the channeling of brooks into the Rahway River (Figure 5.9). Nevertheless, the New Jersey HPO has determined some bridges within the district as contributing elements. These bridges are noted in the text.

The following is a list of buildings and structures surveyed within the NCHD by streets, in alphabetical order (see Section 6.0, Inventory of Identified Architectural Resources for a full discussion of all buildings and structures).

Central Avenue

- PRN 52, 3 Central Avenue (ca. 1925), garage only, recommended eligible for listing to the National Register as a contributing element to the NCHD
- PRN 53, 7 Central Avenue (ca. 1941), garage only, recommended not eligible for listing to the National Register and is a non-contributing element to the district (Bary 2012)
- PRN 54, 11 Central Avenue (ca. 1950), garage only, recommended not eligible for listing to the NRHP and is a non-contributing element to the district (Bary 2012)



Figure 5.5. The original slate sidewalks are considered contributing elements to the NCHD; none are within the APE (Ms. Kelly Nolte, Panamerican 2012).



Figure 5.6. The original horse hitching posts are considered contributing elements to the NCHD; none are within the APE (Ms. Kelly Nolte, Panamerican 2012).



Figure 5.7. The original street markers are considered contributing elements to the NCHD; none are within the APE (Ms. Kelly Nolte, Panamerican 2012).



Figure 5.8. The original granite curbs are considered contributing elements to the NCHD; none are within the APE (Ms. Kelly Nolte, Panamerican 2012).

Table 8: NCHD Property Inventory
(Sorted by street address)

Property Name	Address	NCHD Status	Block	Lot	ID Number
St. Michaels Roman Catholic Church	40 ALDEN ST	Key contributing	191	1	-631310371
St. Michaels School	100 ALDEN ST	Contributing	188	1	-1491211440
109 ALDEN ST	109 ALDEN ST	Contributing	187	12	-1140310631
111 ALDEN ST	111 ALDEN ST	Contributing	187	11	-1643492890
Mount Carmel Guild	112 ALDEN ST	Non contributing	188	2	-55315
113 ALDEN ST	113 ALDEN ST	Contributing	187	10	-2118934161
114 ALDEN ST	114 ALDEN ST	Contributing	188	3	1074209217
115 ALDEN ST	115 ALDEN ST	Contributing	187	9	349459694
116 ALDEN ST	116 ALDEN ST	Contributing	188	4	-2095005225
117 ALDEN ST	117 ALDEN ST	Contributing	187	8	-39240500
118 Alden Street	118 ALDEN ST	Contributing	188	5	1869402046
119 Alden Street	119 ALDEN ST	Contributing	187	7	-409807802
1 ARLINGTON RD	1 ARLINGTON RD	Contributing	307	1	463609818
3 ARLINGTON RD	3 ARLINGTON RD	Non contributing	307	22	-535803062
8 ARLINGTON RD	8 ARLINGTON RD	Contributing	308	5	2030348044
10 ARLINGTON RD	10 ARLINGTON RD	Contributing	308	6	-1271362526
12 ARLINGTON RD	12 ARLINGTON RD	Contributing	308	7	-1276818052
16 ARLINGTON RD	16 ARLINGTON RD	Contributing	308	8	1484553495
18 ARLINGTON RD	18 ARLINGTON RD	Contributing	308	9	-384085855
20 ARLINGTON RD	20 ARLINGTON RD	Contributing	308	10	-866645527
22 ARLINGTON RD	22 ARLINGTON RD	Contributing	308	11	1671720433
23 ARLINGTON RD	23 ARLINGTON RD	Contributing	309	25	-1202334204
24 ARLINGTON RD	24 ARLINGTON RD	Contributing	308	12	-279753808
25 ARLINGTON RD	25 ARLINGTON RD	Contributing	309	24	-1197640365
26 ARLINGTON RD	26 ARLINGTON RD	Contributing	308	13	-1488757506
29 ARLINGTON RD	29 ARLINGTON RD	Contributing	309	23	-228706635
37 ARLINGTON RD	37 ARLINGTON RD	Contributing	316	3	-1633233699
102 ARLINGTON RD	102 ARLINGTON RD	Contributing	315	2	-335081864
104 ARLINGTON RD	104 ARLINGTON RD	Non contributing	315	3	-32137746
1 BALMIERE PKWY	1 BALMIERE PKWY	Contributing	200	15	-522833512
3 BALMIERE PKWY	3 BALMIERE PKWY	Contributing	200	14	410513067
4 BALMIERE PKWY	4 BALMIERE PKWY	Contributing	199	1	-803131807
5 BALMIERE PKWY	5 BALMIERE PKWY	Contributing	200	8	-1284460044
6 BALMIERE PKWY	6 BALMIERE PKWY	Contributing	199	16	-896870710
7 BALMIERE PKWY	7 BALMIERE PKWY	Contributing	200	7	2136430343
8 BALMIERE PKWY	8 BALMIERE PKWY	Contributing	199	15	-132755315
9 BALMIERE PKWY	9 BALMIERE PKWY	Contributing	200	6	586923889
11 BALMIERE PKWY	11 BALMIERE PKWY	Contributing	200	5	-1041042357

Table 8: NCHD Property Inventory

Property Name	Address	NCHD Status	Block	Lot	ID Number
12 BALMIERE PKWY	12 BALMIERE PKWY	Contributing	199	14	1681213369
13 BALMIERE PKWY	13 BALMIERE PKWY	Contributing	200	4	641818953
14 BALMIERE PKWY	14 BALMIERE PKWY	Contributing	199	13	-1928474234
15 BALMIERE PKWY	15 BALMIERE PKWY	Contributing	200	3	372119887
16 BALMIERE PKWY	16 BALMIERE PKWY	Contributing	199	12	-1754621853
17 BALMIERE PKWY	17 BALMIERE PKWY	Non contributing	200	2	-1746846333
18 BALMIERE PKWY	18 BALMIERE PKWY	Contributing	199	11	-312212994
19 BALMIERE PKWY	19 BALMIERE PKWY	Contributing	200	1	438497372
30 BALMIERE PKWY	30 BALMIERE PKWY	Contributing	199	2	-658486002
31 BALMIERE PKWY	31 BALMIERE PKWY	Non contributing	198	46	426873198
32 BALMIERE PKWY	32 BALMIERE PKWY	Contributing	199	3	924804183
34 BALMIERE PKWY	34 BALMIERE PKWY	Contributing	199	4	2032941199
35 BALMIERE PKWY	35 BALMIERE PKWY	Key Contributing	198	52	1421007163
36 BALMIERE PKWY	36 BALMIERE PKWY	Non contributing	199	5	-221865246
38 BALMIERE PKWY	38 BALMIERE PKWY	Non contributing	199	6	362702957
40 BALMIERE PKWY	40 BALMIERE PKWY	Contributing	199	7	1525838316
42 BALMIERE PKWY	42 BALMIERE PKWY	Contributing	199	8	-1149227810
43 BALMIERE PKWY	43 BALMIERE PKWY	Contributing	198	53	-1604949212
46 BALMIERE PKWY	46 BALMIERE PKWY	Contributing	199	9	-114411198
47 BALMIERE PKWY	47 BALMIERE PKWY	Contributing	198	54	1413329020
50 BALMIERE PKWY	50 BALMIERE PKWY	Non contributing	199	10	118782730
203 BALMIERE RD N	203 BALMIERE RD N	Contributing	198	47	211026064
205 BALMIERE RD N	205 BALMIERE RD N	Contributing	198	48	968316790
207 BALMIERE RD N	207 BALMIERE RD N	Contributing	198	49	-2105181871
209 BALMIERE RD N	209 BALMIERE RD N	Contributing	198	50	1933177874
211 BALMIERE RD N	211 BALMIERE RD N	Contributing	198	51	-991531706
102 BALMIERE RD S	102 BALMIERE RD S	Contributing	200	9	-655734486
103 BALMIERE RD S	103 BALMIERE RD S	Contributing	200	13	-1876552135
104 BALMIERE RD S	104 BALMIERE RD S	Contributing	200	10	-395104939
105 BALMIERE RD S	105 BALMIERE RD S	Contributing	200	12	1475932842
107 BALMIERE RD S	107 BALMIERE RD S	Contributing	200	11	-1377255913
5 BARGOS PL	5 BARGOS PL	Contributing	223	2	-1019652622
1 BEECH ST	1 BEECH ST	Contributing	170	7	-1496373441
2 BEECH ST	2 BEECH ST	Contributing	155	8	1185732854
6 BEECH ST	6 BEECH ST	Contributing	155	9	-477867230
7 BEECH ST	7 BEECH ST	Contributing	170	6	-68678225
10 BEECH ST	10 BEECH ST	Contributing	155	10	382437259
11 BEECH ST	11 BEECH ST	Contributing	170	5	-1568464373
13 BEECH ST	13 BEECH ST	Contributing	170	4	586567462
17 BEECH ST	17 BEECH ST	Non contributing	170	3	1951726808
18 BEECH ST	18 BEECH ST	Non contributing	155	11	-636809269

Table 8: NCHD Property Inventory

Property Name	Address	NCHD Status	Block	Lot	ID Number
22 BEECH ST	22 BEECH ST	Contributing	155	12	575954488
23 BEECH ST	23 BEECH ST	Contributing	170	2	-505453820
26 BEECH ST	26 BEECH ST	Contributing	155	13	-1198002691
27 BEECH ST	27 BEECH ST	Contributing	170	1	-2000385116
28 BEECH ST	28 BEECH ST	Non contributing	155	14	1453213955
30 BEECH ST	30 BEECH ST	Contributing	159	9	878805537
31 BEECH ST	31 BEECH ST	Contributing	161	8	1426187221
33 BEECH ST	33 BEECH ST	Contributing	161	7	222163286
34 BEECH ST	34 BEECH ST	Contributing	159	10	645057400
36 BEECH ST	36 BEECH ST	Contributing	159	11	809921138
37 BEECH ST	37 BEECH ST	Contributing	161	6	-1075241466
38 BEECH ST	38 BEECH ST	Contributing	159	12	-312287760
39 BEECH ST	39 BEECH ST	Contributing	161	5	-1230432055
40 BEECH ST	40 BEECH ST	Contributing	159	13	887151599
41 BEECH ST	41 BEECH ST	Contributing	161	4	-453068891
42 BEECH ST	42 BEECH ST	Contributing	159	14	1236336492
44 BEECH ST	44 BEECH ST	Contributing	159	15	-665347785
46 BEECH ST	46 BEECH ST	Contributing	159	16	-1203748549
49 BEECH ST	49 BEECH ST	Contributing	161	3	-1724359075
53 BEECH ST	53 BEECH ST	Contributing	161	2	1659526409
57 BEECH ST	57 BEECH ST	Contributing	161	1	-708266594
2 BERKELEY PL	2 BERKELEY PL	Contributing	168	3	244463146
3 BERKELEY PL	3 BERKELEY PL	Contributing	167	5	1976546848
4 BERKELEY PL	4 BERKELEY PL	Contributing	165	9	-407883875
5 BERKELEY PL	5 BERKELEY PL	Contributing	167	4	1040879485
6 BERKELEY PL	6 BERKELEY PL	Contributing	165	10	733828595
7 BERKELEY PL	7 BERKELEY PL	Contributing	167	3	992199585
12 BERKELEY PL	12 BERKELEY PL	Contributing	165	11	645884486
14 BERKELEY PL	14 BERKELEY PL	Contributing	165	12	-764679548
16 BERKELEY PL	16 BERKELEY PL	Contributing	165	13	-2126714917
17 BERKELEY PL	17 BERKELEY PL	Key Contributing	166	5	-1634375778
18 BERKELEY PL	18 BERKELEY PL	Contributing	165	14	1267708271
19 BERKELEY PL	19 BERKELEY PL	Contributing	166	4	1606648704
20 BERKELEY PL	20 BERKELEY PL	Contributing	165	15	-617418639
21 BERKELEY PL	21 BERKELEY PL	Contributing	166	3	-578609791
22 BERKELEY PL	22 BERKELEY PL	Non contributing	165	16	793044956
23 BERKELEY PL	23 BERKELEY PL	Contributing	166	2	2138253563
24 BERKELEY PL	24 BERKELEY PL	Contributing	165	17	1275739367
25 BERKELEY PL	25 BERKELEY PL	Contributing	166	1	-1304483475
203 CASINO AVE	203 CASINO AVE	Non contributing	264	12	-2043792671
205 CASINO AVE	205 CASINO AVE	Contributing	264	11	1985018748

Table 8: NCHD Property Inventory

Property Name	Address	NCHD Status	Block	Lot	ID Number
209 CASINO AVE	209 CASINO AVE	Contributing	264	10	470024628
307 CASINO AVE	307 CASINO AVE	Non contributing	267	13.01	1887917918
308 CASINO AVE	308 CASINO AVE	Contributing	269	2	718882411
309 CASINO AVE	309 CASINO AVE	Contributing	267	11.01	1418203624
310 CASINO AVE	310 CASINO AVE	Contributing	269	3	1661225093
311 CASINO AVE	311 CASINO AVE	Non contributing	267	10	834197051
312 CASINO AVE	312 CASINO AVE	Contributing	269	4	-2127199536
316 CASINO AVE	316 CASINO AVE	Contributing	270	1	704188162
317 CASINO AVE	317 CASINO AVE	Contributing	268	10	-1743590158
319 CASINO AVE	319 CASINO AVE	Non contributing	268	9	-1095429493
320 CASINO AVE	320 CASINO AVE	Contributing	270	2	-202370758
321 CASINO AVE	321 CASINO AVE	Contributing	268	8	-1666186801
325 CASINO AVE	325 CASINO AVE	Contributing	268	7	1431325540
403 CASINO AVE	403 CASINO AVE	Contributing	272	5	-1137763390
406 CASINO AVE	406 CASINO AVE	Contributing	276	1	-764110712
408 CASINO AVE	408 CASINO AVE	Non contributing	276	2	1844371089
409 CASINO AVE	409 CASINO AVE	Contributing	272	4	2117297534
410 CASINO AVE	410 CASINO AVE	Contributing	277	1	1228307725
[Open Lot]	CENTRAL AVE	Unknown	222	13	-1572760913
2 CENTRAL AVE	2 CENTRAL AVE	Contributing	184	3	-2057850947
3 CENTRAL AVE	3 CENTRAL AVE	Contributing	169	1	-1880825893
4 CENTRAL AVE	4 CENTRAL AVE	Contributing	184	4	728133038
5 CENTRAL AVE	5 CENTRAL AVE	Contributing	169	2	-1014152038
6 CENTRAL AVE	6 CENTRAL AVE	Contributing	184	5	620704633
7 CENTRAL AVE	7 CENTRAL AVE	Contributing	169	3	173720134
8 CENTRAL AVE	8 CENTRAL AVE	Contributing	184	6	268372559
9 CENTRAL AVE	9 CENTRAL AVE	Contributing	169	4	1003997668
10 CENTRAL AVE	10 CENTRAL AVE	Contributing	184	7	136618028
11 CENTRAL AVE	11 CENTRAL AVE	Non contributing	169	5	-576763692
12 CENTRAL AVE	12 CENTRAL AVE	Contributing	184	8	1907581262
14 CENTRAL AVE	14 CENTRAL AVE	Contributing	184	9	624275081
15 CENTRAL AVE	15 CENTRAL AVE	Contributing	169	6	560874821
16 CENTRAL AVE	16 CENTRAL AVE	Contributing	184	10	1936993975
17 CENTRAL AVE	17 CENTRAL AVE	Non contributing	169	7	-2105080760
18 CENTRAL AVE	18 CENTRAL AVE	Contributing	184	11	2055128086
21 CENTRAL AVE	21 CENTRAL AVE	Contributing	169	8	-1697297665
Moore House	22 CENTRAL AVE	Key contributing	184	12	-1495841428
25 CENTRAL AVE	25 CENTRAL AVE	Contributing	169	9	1265922243
26 CENTRAL AVE	26 CENTRAL AVE	Contributing	184	13	-408743550
30 CENTRAL AVE	30 CENTRAL AVE	Contributing	184	14	-602268800
32 CENTRAL AVE	32 CENTRAL AVE	Contributing	184	15	1112416507

Table 8: NCHD Property Inventory

Property Name	Address	NCHD Status	Block	Lot	ID Number
33 CENTRAL AVE	33 CENTRAL AVE	Contributing	169	10	-376147799
34 CENTRAL AVE	34 CENTRAL AVE	Non contributing	184	16	509049761
35 CENTRAL AVE	35 CENTRAL AVE	Contributing	169	11	160408502
102 CENTRAL AVE	102 CENTRAL AVE	Contributing	222	12	19766477
104 CENTRAL AVE	104 CENTRAL AVE	Contributing	222	1	-354482527
201 CENTRAL AVE	201 CENTRAL AVE	Contributing	223	6	-431066598
[Open Corner Lot]	202 CENTRAL AVE	Unknown	226	1	16571445
205 CENTRAL AVE	205 CENTRAL AVE	Contributing	223	5	-290840507
206 CENTRAL AVE	206 CENTRAL AVE	Contributing	226	2	-917571450
208 CENTRAL AVE	208 CENTRAL AVE	Contributing	226	3	-2081145889
209 CENTRAL AVE	209 CENTRAL AVE	Contributing	223	4	2088296500
211 CENTRAL AVE	211 CENTRAL AVE	Contributing	223	3	1092616580
212 CENTRAL AVE	212 CENTRAL AVE	Contributing	226	4	-1993851550
214 CENTRAL AVE	214 CENTRAL AVE	Contributing	226	5	-955076169
215 CENTRAL AVE	215 CENTRAL AVE	Contributing	224	5	-267386172
216 CENTRAL AVE	216 CENTRAL AVE	Contributing	226	6	-981727311
217 CENTRAL AVE	217 CENTRAL AVE	Contributing	224	4	-462328769
218 CENTRAL AVE	218 CENTRAL AVE	Contributing	226	7	1153409314
219 CENTRAL AVE	219 CENTRAL AVE	Contributing	224	3	-2085740251
220 CENTRAL AVE	220 CENTRAL AVE	Contributing	226	8	-276261413
302 CENTRAL AVE	302 CENTRAL AVE	Contributing	227	1	-1072262689
304 CENTRAL AVE	304 CENTRAL AVE	Contributing	227	2	-2058627711
305 CENTRAL AVE	305 CENTRAL AVE	Contributing	225	6	1414359459
306 CENTRAL AVE	306 CENTRAL AVE	Contributing	227	3	441105803
307 CENTRAL AVE	307 CENTRAL AVE	Non contributing	225	5	-1353029782
308 CENTRAL AVE	308 CENTRAL AVE	Contributing	227	4	1472880092
401 CENTRAL AVE	401 CENTRAL AVE	Contributing	225	4	1172445900
402 CENTRAL AVE	402 CENTRAL AVE	Contributing	228	1	-599904806
403 CENTRAL AVE	403 CENTRAL AVE	Contributing	225	3	1084975521
404 CENTRAL AVE	404 CENTRAL AVE	Contributing	228	2	755735447
405 CENTRAL AVE	405 CENTRAL AVE	Contributing	225	2	2056853531
406 CENTRAL AVE	406 CENTRAL AVE	Contributing	228	3	-1953234036
407 CENTRAL AVE	407 CENTRAL AVE	Contributing	225	1	1398123185
408 CENTRAL AVE	408 CENTRAL AVE	Contributing	228	4	-482020523
499 CENTRAL AVE	499 CENTRAL AVE	Non contributing	229	8	-1422628963
500 CENTRAL AVE	500 CENTRAL AVE	Contributing	231	6	-203318354
501 CENTRAL AVE	501 CENTRAL AVE	Contributing	229	7	-268997869
502 CENTRAL AVE	502 CENTRAL AVE	Contributing	231	7	1527851263
503 CENTRAL AVE	503 CENTRAL AVE	Contributing	229	6	510714968
504 CENTRAL AVE	504 CENTRAL AVE	Contributing	231	8	1270578283
505 CENTRAL AVE	505 CENTRAL AVE	Contributing	229	5	1097144704

Table 8: NCHD Property Inventory

Property Name	Address	NCHD Status	Block	Lot	ID Number
506 CENTRAL AVE	506 CENTRAL AVE	Contributing	231	9	1457113751
507 CENTRAL AVE	507 CENTRAL AVE	Non contributing	229	4	-1986224471
509 CENTRAL AVE	509 CENTRAL AVE	Contributing	229	3	-487345120
511 CENTRAL AVE	511 CENTRAL AVE	Contributing	229	2	1593040635
1 CLAREMONT PL	1 CLAREMONT PL	Contributing	310	1	1135715941
3 CLAREMONT PL	3 CLAREMONT PL	Contributing	310	2	838652594
5 CLAREMONT PL	5 CLAREMONT PL	Contributing	310	3	-1682336844
6 CLAREMONT PL	6 CLAREMONT PL	Non contributing	306	24	1698989032
7 CLAREMONT PL	7 CLAREMONT PL	Contributing	266	5	529021725
8 CLAREMONT PL	8 CLAREMONT PL	Contributing	271	15	1486805219
9 CLAREMONT PL	9 CLAREMONT PL	Contributing	310	4	-540017628
10 CLAREMONT PL	10 CLAREMONT PL	Contributing	271	16	230173979
12 CLAREMONT PL	12 CLAREMONT PL	Contributing	306	23	-974268066
110 CLAREMONT PL	110 CLAREMONT PL	Contributing	269	11	-1719679812
112 CLAREMONT PL	112 CLAREMONT PL	Contributing	269	12	-506933369
114 CLAREMONT PL	114 CLAREMONT PL	Contributing	269	1	1766937877
207 CLAREMONT PL	207 CLAREMONT PL	Non contributing	267	14	689759425
209 CLAREMONT PL	209 CLAREMONT PL	Non contributing	267	15	1548681720
211 CLAREMONT PL	211 CLAREMONT PL	Contributing	267	16.01	82398330
214 CLAREMONT PL	214 CLAREMONT PL	Contributing	264	9	1866204325
314 CLAREMONT PL	314 CLAREMONT PL	Contributing	226	9	-1800796386
400 CLAREMONT PL	400 CLAREMONT PL	Non contributing	225	7	1776691117
402 CLAREMONT PL	402 CLAREMONT PL	Non contributing	225	8	1959030467
403 CLAREMONT PL	403 CLAREMONT PL	Contributing	224	2	-1589840565
1 CRAIG PL	1 CRAIG PL	Non contributing	232	1	-414650472
3 CRAIG PL	3 CRAIG PL	Non contributing	232	12	964316936
4 CRAIG PL	4 CRAIG PL	Contributing	233	3	1218440019
5 CRAIG PL	5 CRAIG PL	Contributing	232	11	609144629
7 CRAIG PL	7 CRAIG PL	Contributing	232	10	-2138689002
8 CRAIG PL	8 CRAIG PL	Contributing	233	4	-1753148311
9 CRAIG PL	9 CRAIG PL	Contributing	232	9	72595894
11 CRAIG PL	11 CRAIG PL	Contributing	232	8	200412815
12 CRAIG PL	12 CRAIG PL	Contributing	233	5	-245537236
16 CRAIG PL	16 CRAIG PL	Contributing	233	6	726452621
20 CRAIG PL	20 CRAIG PL	Contributing	233	7	1106689926
24 CRAIG PL	24 CRAIG PL	Contributing	233	8	-69985334
27 CRAIG PL	27 CRAIG PL	Contributing	229	1	600484424
28 CRAIG PL	28 CRAIG PL	Contributing	233	9	600964573
29 CRAIG PL	29 CRAIG PL	Contributing	229	19	1438670467
32 CRAIG PL	32 CRAIG PL	Contributing	233	10	-1419704028
33 CRAIG PL	33 CRAIG PL	Contributing	229	18	-665747776

Table 8: NCHD Property Inventory

Property Name	Address	NCHD Status	Block	Lot	ID Number
36 CRAIG PL	36 CRAIG PL	Contributing	233	11	775145409
37 CRAIG PL	37 CRAIG PL	Contributing	229	17	-1456445659
40 CRAIG PL	40 CRAIG PL	Non contributing	233	12	487894457
41 CRAIG PL	41 CRAIG PL	Contributing	229	16	-1567042294
7 CRANFORD AVE	7 CRANFORD AVE	Non contributing	310	5	-1267490504
10 CRANFORD AVE	10 CRANFORD AVE	Non contributing	311	19	-822601330
14 CRANFORD AVE	14 CRANFORD AVE	Contributing	308	1	-426949330
16 CRANFORD AVE	16 CRANFORD AVE	Contributing	308	2	1078244167
18 CRANFORD AVE	18 CRANFORD AVE	Contributing	308	3	-437076076
22 CRANFORD AVE	22 CRANFORD AVE	Contributing	308	4	-1779693875
91 CRANFORD AVE	91 CRANFORD AVE	Contributing	306	22	1437142307
93 CRANFORD AVE	93 CRANFORD AVE	Contributing	306	21	-2033339752
95 CRANFORD AVE	95 CRANFORD AVE	Contributing	306	20	-901463768
101 CRANFORD AVE	101 CRANFORD AVE	Non contributing	306	19	2107222407
103 CRANFORD AVE	103 CRANFORD AVE	Non contributing	306	18	-846356535
104 CRANFORD AVE	104 CRANFORD AVE	Contributing	307	2	1091494430
105 CRANFORD AVE	105 CRANFORD AVE	Non contributing	306	17	407605810
106 CRANFORD AVE	106 CRANFORD AVE	Contributing	307	3	989633107
107 CRANFORD AVE	107 CRANFORD AVE	Contributing	306	16	1825700058
108 CRANFORD AVE	108 CRANFORD AVE	Contributing	307	4	1732701302
109 CRANFORD AVE	109 CRANFORD AVE	Contributing	306	15	2016165613
110 CRANFORD AVE	110 CRANFORD AVE	Contributing	307	5	795448511
111 CRANFORD AVE	111 CRANFORD AVE	Non contributing	306	14	1482689160
112 CRANFORD AVE	112 CRANFORD AVE	Contributing	307	6	1682065642
115 CRANFORD AVE	115 CRANFORD AVE	Contributing	306	13	-785668391
116 CRANFORD AVE	116 CRANFORD AVE	Non contributing	307	7	499121965
117 CRANFORD AVE	117 CRANFORD AVE	Contributing	306	12	-1824463077
120 CRANFORD AVE	120 CRANFORD AVE	Contributing	307	8	-1877097636
121 CRANFORD AVE	121 CRANFORD AVE	Contributing	306	11	1908761747
4 DAY PL	4 DAY PL	Contributing	228	11	-187064025
1 DOERING WAY	1 DOERING WAY	Non contributing	198	18	250686898
2 DOERING WAY	2 DOERING WAY	Contributing	198	39	-619619173
3 DOERING WAY	3 DOERING WAY	Contributing	198	19	915124448
4 DOERING WAY	4 DOERING WAY	Contributing	198	38	160959493
5 DOERING WAY	5 DOERING WAY	Contributing	198	20	-1376252291
6 DOERING WAY	6 DOERING WAY	Contributing	198	37	988828464
7 DOERING WAY	7 DOERING WAY	Contributing	198	21	563839496
8 DOERING WAY	8 DOERING WAY	Contributing	198	36	-1186294212
9 DOERING WAY	9 DOERING WAY	Contributing	198	22	1538595134
10 DOERING WAY	10 DOERING WAY	Contributing	198	35	-2023043764
11 DOERING WAY	11 DOERING WAY	Contributing	198	23	-1299261028

Table 8: NCHD Property Inventory

Property Name	Address	NCHD Status	Block	Lot	ID Number
12 DOERING WAY	12 DOERING WAY	Contributing	198	34	-600085082
13 DOERING WAY	13 DOERING WAY	Contributing	198	24	-359643717
14 DOERING WAY	14 DOERING WAY	Non Contributing	198	33	-2085584923
15 DOERING WAY	15 DOERING WAY	Contributing	198	25	-507362336
16 DOERING WAY	16 DOERING WAY	Non contributing	198	32	370814111
18 DOERING WAY	18 DOERING WAY	Non contributing	198	31	-2087569419
22 DOERING WAY	22 DOERING WAY	Non contributing	198	30	-1830083011
24 DOERING WAY	24 DOERING WAY	Key contributing	198	29	711434305
106 EASTMAN ST	106 EASTMAN ST	Contributing	187	2	-97573079
108 EASTMAN ST	108 EASTMAN ST	Key contributing	187	3	-1215722760
122 EASTMAN ST	122 EASTMAN ST	Contributing	184	1	-779587533
McConnell Park	125 EASTMAN ST	Contributing	180	1	-558106999
126 EASTMAN ST	126 EASTMAN ST	Contributing	184	2	915255866
145 EASTMAN ST	145 EASTMAN ST	Non contributing	172	5.01	618479971
149 EASTMAN ST	149 EASTMAN ST	Non contributing	172	5.02	-251640886
Eastman Street Bridge (STR# 2003008)	Eastman Street over Rahway River (NW crossing)	Non contributing	NA	NA	550685363
Eastman Street Bridge (STR# 2003025)	Eastman Street over Rahway River (SE crossing)	Contributing	NA	NA	1458838553
317 ELIZABETH AVE	317 ELIZABETH AVE	Contributing	306	10	930885313
319 ELIZABETH AVE	319 ELIZABETH AVE	Contributing	306	9	-573653032
1 ESTELLE PL	1 ESTELLE PL	Non contributing	173	18	-2099043792
2 ESTELLE PL	2 ESTELLE PL	Non contributing	178	1.01	-1629315466
4 ESTELLE PL	4 ESTELLE PL	Non contributing	178	1.02	1322963680
6 ESTELLE PL	6 ESTELLE PL	Non contributing	178	1.03	-1899017128
7 FOREST AVE	7 FOREST AVE	Non contributing	310	9	66039183
9 FOREST AVE	9 FOREST AVE	Contributing	310	8	685264371
11 FOREST AVE	11 FOREST AVE	Non contributing	310	7	328918797
12 FOREST AVE	12 FOREST AVE	Contributing	312	2	631201530
14 FOREST AVE	14 FOREST AVE	Contributing	312	3	1713572865
15 FOREST AVE	15 FOREST AVE	Non contributing	310	6	1772423027
16 FOREST AVE	16 FOREST AVE	Contributing	312	4	-339559424
18 FOREST AVE	18 FOREST AVE	Contributing	312	5	-966733221
22 FOREST AVE	22 FOREST AVE	Contributing	312	6	-137044478
26 FOREST AVE	26 FOREST AVE	Contributing	312	7	1793028005
28 FOREST AVE	28 FOREST AVE	Contributing	312	8	455247455
30 FOREST AVE	30 FOREST AVE	Contributing	312	9	1053267209
32 FOREST AVE	32 FOREST AVE	Contributing	312	10	1230607907
34 FOREST AVE	34 FOREST AVE	Non contributing	312	11	86426341
101 FOREST AVE	101 FOREST AVE	Non contributing	311	18	-453628525

Table 8: NCHD Property Inventory

Property Name	Address	NCHD Status	Block	Lot	ID Number
103 FOREST AVE	103 FOREST AVE	Non contributing	311	17	838341848
105 FOREST AVE	105 FOREST AVE	Non contributing	311	16	-1453999623
109 FOREST AVE	109 FOREST AVE	Contributing	311	15	711419590
111 FOREST AVE	111 FOREST AVE	Contributing	311	14	-1375537969
113 FOREST AVE	113 FOREST AVE	Contributing	311	13	-1489567132
115 FOREST AVE	115 FOREST AVE	Contributing	311	12	1976648597
117 FOREST AVE	117 FOREST AVE	Contributing	311	11	1958788914
4 FRAZER PL	4 FRAZER PL	Contributing	232	4	-1322372240
7 FRAZER PL	7 FRAZER PL	Contributing	231	11	-976923835
9 FRAZER PL	9 FRAZER PL	Contributing	231	10	-734171747
10 FRAZER PL	10 FRAZER PL	Contributing	232	5	-992374078
12 FRAZER PL	12 FRAZER PL	Contributing	232	6	-1777946217
14 FRAZER PL	14 FRAZER PL	Contributing	232	7	283110297
4 GREAVES PL	4 GREAVES PL	Contributing	174	24	641143229
6 GREAVES PL	6 GREAVES PL	Contributing	174	25	-1657746900
8 GREAVES PL	8 GREAVES PL	Contributing	174	26	202710435
9 GREAVES PL	9 GREAVES PL	Contributing	175	11	-1494856732
10 GREAVES PL	10 GREAVES PL	Contributing	174	27	-1549550675
11 GREAVES PL	11 GREAVES PL	Contributing	175	9.01	681929578
12 GREAVES PL	12 GREAVES PL	Contributing	174	28	-701114498
14 GREAVES PL	14 GREAVES PL	Contributing	174	29	-1744437499
15 GREAVES PL	15 GREAVES PL	Contributing	175	8.01	-1181175701
16 GREAVES PL	16 GREAVES PL	Contributing	174	30	-1625103475
17 GREAVES PL	17 GREAVES PL	Contributing	175	7.01	1298236895
19 GREAVES PL	19 GREAVES PL	Contributing	175	6	-1432590199
20 GREAVES PL	20 GREAVES PL	Contributing	174	31	1359713246
21 GREAVES PL	21 GREAVES PL	Contributing	175	5	488921233
22 GREAVES PL	22 GREAVES PL	Contributing	174	32	-1691335284
23 GREAVES PL	23 GREAVES PL	Non contributing	175	4	1332658652
24 GREAVES PL	24 GREAVES PL	Contributing	174	33	869445066
25 GREAVES PL	25 GREAVES PL	Contributing	175	3	-1364974991
26 GREAVES PL	26 GREAVES PL	Contributing	174	34	494949692
1 HAMILTON AVE	1 HAMILTON AVE	Contributing	308	15	1249497521
2 HAMILTON AVE	2 HAMILTON AVE	Contributing	316	4	254835937
3 HAMILTON AVE	3 HAMILTON AVE	Non contributing	308	14	-1018895343
4 HAMILTON AVE	4 HAMILTON AVE	Contributing	316	5	1345750294
5 HAMILTON AVE	5 HAMILTON AVE	Contributing	309	22	603339730
6 HAMILTON AVE	6 HAMILTON AVE	Contributing	316	6	-1634693552
7 HAMILTON AVE	7 HAMILTON AVE	Contributing	309	21	1786558055
8 HAMILTON AVE	8 HAMILTON AVE	Contributing	316	7	-68527008
10 HAMILTON AVE	10 HAMILTON AVE	Contributing	316	8	-72990059

Table 8: NCHD Property Inventory

Property Name	Address	NCHD Status	Block	Lot	ID Number
11 HAMILTON AVE	11 HAMILTON AVE	Contributing	309	20	-2117204552
12 HAMILTON AVE	12 HAMILTON AVE	Contributing	316	9	1173131323
14 HAMILTON AVE	14 HAMILTON AVE	Contributing	316	10	926956289
15 HAMILTON AVE	15 HAMILTON AVE	Contributing	309	19	-1067822759
16 HAMILTON AVE	16 HAMILTON AVE	Contributing	316	11	2015780196
17 HAMILTON AVE	17 HAMILTON AVE	Contributing	309	18	1113810897
21 HAMILTON AVE	21 HAMILTON AVE	Contributing	309	17	2067309993
22 HAMILTON AVE	22 HAMILTON AVE	Contributing	318	39	1025613515
24 HAMILTON AVE	24 HAMILTON AVE	Contributing	318	40	-1164968534
25 HAMILTON AVE	25 HAMILTON AVE	Contributing	309	16	2075379961
26 HAMILTON AVE	26 HAMILTON AVE	Contributing	318	41	-934770689
27 HAMILTON AVE	27 HAMILTON AVE	Contributing	309	15	-975079066
28 HAMILTON AVE	28 HAMILTON AVE	Contributing	318	42	1312178854
29 HAMILTON AVE	29 HAMILTON AVE	Contributing	309	14	-385404140
7 HAMPTON RD	7 HAMPTON RD	Non contributing	201	7	1528472576
9 HAMPTON RD	9 HAMPTON RD	Contributing	201	6	481583995
10 HAMPTON RD	10 HAMPTON RD	Contributing	200	47	1413729856
12 HAMPTON RD	12 HAMPTON RD	Contributing	200	46	-1042653586
14 HAMPTON RD	14 HAMPTON RD	Contributing	200	45	-1600888244
18 HAMPTON RD	18 HAMPTON RD	Non contributing	200	44	1676132698
20 HAMPTON RD	20 HAMPTON RD	Contributing	200	43	342535960
21 HAMPTON RD	21 HAMPTON RD	Contributing	201	5	645777180
22 HAMPTON RD	22 HAMPTON RD	Non contributing	200	42	17458994
23 HAMPTON RD	23 HAMPTON RD	Contributing	201	4	-1154766146
25 HAMPTON RD	25 HAMPTON RD	Contributing	201	3	-128621627
27 HAMPTON RD	27 HAMPTON RD	Contributing	201	2	574009375
28 HAMPTON RD	28 HAMPTON RD	Contributing	200	41	592642558
29 HAMPTON RD	29 HAMPTON RD	Contributing	201	1	2145527984
30 HAMPTON RD	30 HAMPTON RD	Contributing	200	40	1266343897
[Open Lot]	HAMPTON ST	Unknown	168	6	-29359766
1 HAMPTON ST	1 HAMPTON ST	Contributing	172	12	1793776431
5 HAMPTON ST	5 HAMPTON ST	Contributing	172	11	-1262252482
6 HAMPTON ST	6 HAMPTON ST	Contributing	179	5	1370207463
8 HAMPTON ST	8 HAMPTON ST	Contributing	179	4	-1770629476
9 HAMPTON ST	9 HAMPTON ST	Contributing	172	10	1604149613
11 HAMPTON ST	11 HAMPTON ST	Contributing	172	9	-1985408779
12 HAMPTON ST	12 HAMPTON ST	Contributing	179	3	1278482706
13 HAMPTON ST	13 HAMPTON ST	Contributing	172	8	2049749476
15 HAMPTON ST	15 HAMPTON ST	Contributing	172	7	-1641114581
16 HAMPTON ST	16 HAMPTON ST	Contributing	179	2	473179031
18 HAMPTON ST	18 HAMPTON ST	Contributing	179	1	453946768

Table 8: NCHD Property Inventory

Property Name	Address	NCHD Status	Block	Lot	ID Number
19 HAMPTON ST	19 HAMPTON ST	Contributing	172	6	203073506
101 HAMPTON ST	101 HAMPTON ST	Non contributing	167	8	-496211539
103 HAMPTON ST	103 HAMPTON ST	Non contributing	167	1.01	-22359755
Hampton Park	104 HAMPTON ST	Non contributing	168	1	113978792
107 HAMPTON ST	107 HAMPTON ST	Contributing	167	7	-79074043
108 HAMPTON ST	108 HAMPTON ST	Non contributing	168	2	-319504060
109 HAMPTON ST	109 HAMPTON ST	Contributing	167	6	-214316578
200 HAMPTON ST	200 HAMPTON ST	Contributing	168	4	-2054954415
204 HAMPTON ST	204 HAMPTON ST	Contributing	168	5	-83526749
208 HAMPTON ST	208 HAMPTON ST	Contributing	168	7	1473123758
209 HAMPTON ST	209 HAMPTON ST	Non contributing	164	8.02	559717419
210 HAMPTON ST	210 HAMPTON ST	Contributing	168	8	728857404
211 HAMPTON ST	211 HAMPTON ST	Contributing	164	8.01	512650454
Concrete Culvert, Stone Parapets	Hampton Street over Tributary Rahway River	Non contributing	NA	NA	-2005103866
7 HOLLY ST	7 HOLLY ST	Contributing	179	12	440758717
8 HOLLY ST	8 HOLLY ST	Unknown	181	1	-919492716
9 HOLLY ST	9 HOLLY ST	Contributing	179	13	-2136683462
11 HOLLY ST	11 HOLLY ST	Contributing	179	14	418644531
14 HOLLY ST	14 HOLLY ST	Contributing	181	2	1788753144
16 HOLLY ST	16 HOLLY ST	Contributing	181	3	1197875097
18 HOLLY ST	18 HOLLY ST	Contributing	181	4	-157875226
19 HOLLY ST	19 HOLLY ST	Contributing	179	15	-987318402
20 HOLLY ST	20 HOLLY ST	Contributing	181	5	25973615
22 HOLLY ST	22 HOLLY ST	Contributing	181	6	1231053956
23 HOLLY ST	23 HOLLY ST	Contributing	179	16	222315317
24 HOLLY ST	24 HOLLY ST	Contributing	181	7	-1796379054
25 HOLLY ST	25 HOLLY ST	Contributing	179	17	-111538069
26 HOLLY ST	26 HOLLY ST	Contributing	181	8	55903440
28 HOLLY ST	28 HOLLY ST	Contributing	181	9	474265745
101 HOLLY ST	101 HOLLY ST	Contributing	185	1	1512666407
103 HOLLY ST	103 HOLLY ST	Contributing	185	2	-548757028
105 HOLLY ST	105 HOLLY ST	Contributing	185	3	375621015
106 HOLLY ST	106 HOLLY ST	Non contributing	187	4	-1604511100
107 HOLLY ST	107 HOLLY ST	Contributing	185	4	205792652
108 HOLLY ST	108 HOLLY ST	Contributing	187	5	146170269
109 HOLLY ST	109 HOLLY ST	Contributing	185	5	2046965402
110 HOLLY ST	110 HOLLY ST	Contributing	187	6	-1812859629
111 HOLLY ST	111 HOLLY ST	Contributing	185	6	-1988680104
115 HOLLY ST	115 HOLLY ST	Contributing	185	7	342150521
203 HOLLY ST	203 HOLLY ST	Contributing	186	1	306526311

Table 8: NCHD Property Inventory

Property Name	Address	NCHD Status	Block	Lot	ID Number
207 HOLLY ST	207 HOLLY ST	Contributing	186	2	-1114147812
208 HOLLY ST	208 HOLLY ST	Contributing	188	6	-521105171
209 HOLLY ST	209 HOLLY ST	Contributing	186	3	113141674
211 HOLLY ST	211 HOLLY ST	Contributing	186	4	1494938851
214 HOLLY ST	214 HOLLY ST	Non contributing	188	7	-1779684818
215 HOLLY ST	215 HOLLY ST	Contributing	186	5	-343867928
216 HOLLY ST	216 HOLLY ST	Non contributing	188	8	764497148
217 HOLLY ST	217 HOLLY ST	Non contributing	186	6	1003164105
218 HOLLY ST	218 HOLLY ST	Non contributing	188	9	313429702
219 HOLLY ST	219 HOLLY ST	Contributing	186	7	1231331511
2 HOLLY ST W	2 HOLLY ST W	Contributing	176	18	484802190
3 HOLLY ST W	3 HOLLY ST W	Contributing	178	7	-1804660878
4 HOLLY ST W	4 HOLLY ST W	Contributing	176	17	-458224912
5 HOLLY ST W	5 HOLLY ST W	Contributing	178	8	902650028
6 HOLLY ST W	6 HOLLY ST W	Contributing	176	16	-1191976761
7 HOLLY ST W	7 HOLLY ST W	Contributing	178	9	-1108497762
8 HOLLY ST W	8 HOLLY ST W	Contributing	176	15	-74387001
9 HOLLY ST W	9 HOLLY ST W	Contributing	178	10	-860069708
10 HOLLY ST W	10 HOLLY ST W	Contributing	176	14	402675480
11 HOLLY ST W	11 HOLLY ST W	Contributing	178	11	-733028551
12 HOLLY ST W	12 HOLLY ST W	Contributing	176	13	-1616219386
13 HOLLY ST W	13 HOLLY ST W	Contributing	178	12	1473092623
14 HOLLY ST W	14 HOLLY ST W	Contributing	176	12	572123300
16 HOLLY ST W	16 HOLLY ST W	Contributing	176	11	53799760
18 HOLLY ST W	18 HOLLY ST W	Contributing	176	10	982596821
19 HOLLY ST W	19 HOLLY ST W	Non contributing	174	15	-1844009048
20 HOLLY ST W	20 HOLLY ST W	Contributing	176	9	-835809070
21 HOLLY ST W	21 HOLLY ST W	Non contributing	174	16	-1845478016
23 HOLLY ST W	23 HOLLY ST W	Contributing	174	17	1867208346
25 HOLLY ST W	25 HOLLY ST W	Contributing	174	18	518030468
27 HOLLY ST W	27 HOLLY ST W	Contributing	174	19	1220836133
28 HOLLY ST W	28 HOLLY ST W	Non contributing	176	8	-491667559
29 HOLLY ST W	29 HOLLY ST W	Contributing	174	20	-1196853738
30 HOLLY ST W	30 HOLLY ST W	Non contributing	176	7	1471405759
31 HOLLY ST W	31 HOLLY ST W	Contributing	174	21	31859467
32 HOLLY ST W	32 HOLLY ST W	Contributing	176	6	1793409207
33 HOLLY ST W	33 HOLLY ST W	Contributing	174	22	883041254
34 HOLLY ST W	34 HOLLY ST W	Contributing	176	5	-1929620232
36 HOLLY ST W	36 HOLLY ST W	Contributing	176	4	560784258
38 HOLLY ST W	38 HOLLY ST W	Contributing	176	3	964542868
40 HOLLY ST W	40 HOLLY ST W	Contributing	176	2	-398396561

Table 8: NCHD Property Inventory

Property Name	Address	NCHD Status	Block	Lot	ID Number
42-44 HOLLY ST W	42-44 HOLLY ST W	Contributing	176	1	-1316625611
43 HOLLY ST W	43 HOLLY ST W	Non contributing	175	12	-258861785
45 HOLLY ST W	45 HOLLY ST W	Non contributing	175	13	303506640
49 HOLLY ST W	49 HOLLY ST W	Contributing	175	14	1757776945
502 LINDEN PL	502 LINDEN PL	Contributing	279	7	928992495
504 LINDEN PL	504 LINDEN PL	Contributing	279	8	-544692502
505 LINDEN PL	505 LINDEN PL	Contributing	271	7	-635022080
506 LINDEN PL	506 LINDEN PL	Contributing	279	9	731856900
507 LINDEN PL	507 LINDEN PL	Contributing	271	6	1524210670
508 LINDEN PL	508 LINDEN PL	Contributing	279	10	2097216613
510 LINDEN PL	510 LINDEN PL	Contributing	279	1	2060282515
600 LINDEN PL	600 LINDEN PL	Contributing	276	6	1339571415
601 LINDEN PL	601 LINDEN PL	Non contributing	270	4	1769943090
602 LINDEN PL	602 LINDEN PL	Contributing	276	7	-1271394957
604 LINDEN PL	604 LINDEN PL	Non contributing	276	8	-574833848
605 LINDEN PL	605 LINDEN PL	Contributing	270	3	722802574
705 LINDEN PL	705 LINDEN PL	Contributing	268	6	572833290
722 LINDEN PL	722 LINDEN PL	Contributing	272	6	1522757467
723 LINDEN PL	723 LINDEN PL	Contributing	268	5	-1146208148
724 LINDEN PL	724 LINDEN PL	Contributing	272	7	-922461270
725 LINDEN PL	725 LINDEN PL	Contributing	268	4	-509202496
728 LINDEN PL	728 LINDEN PL	Contributing	272	1	1026368307
1 MADISON AVE	1 MADISON AVE	Non contributing	307	21	403772012
2 MADISON AVE	2 MADISON AVE	Contributing	309	1	-1916129976
3 MADISON AVE	3 MADISON AVE	Non contributing	307	20	277693693
5 MADISON AVE	5 MADISON AVE	Non contributing	307	19	-623288310
6 MADISON AVE	6 MADISON AVE	Contributing	309	2	-1774048081
7 MADISON AVE	7 MADISON AVE	Non contributing	307	18	352247235
10 MADISON AVE	10 MADISON AVE	Contributing	309	3	1957587386
11 MADISON AVE	11 MADISON AVE	Contributing	307	17	-473008386
14 MADISON AVE	14 MADISON AVE	Non contributing	309	4	-1314843658
15 MADISON AVE	15 MADISON AVE	Contributing	307	16	-743235929
16 MADISON AVE	16 MADISON AVE	Contributing	309	5	-892965540
17 MADISON AVE	17 MADISON AVE	Contributing	307	15	1515662843
18 MADISON AVE	18 MADISON AVE	Contributing	309	6	-1312104000
19 MADISON AVE	19 MADISON AVE	Contributing	307	14	-85580515
20 MADISON AVE	20 MADISON AVE	Contributing	309	7	903039526
22 MADISON AVE	22 MADISON AVE	Contributing	309	8	-1702061540
24 MADISON AVE	24 MADISON AVE	Contributing	309	9	-819465595
6 MANOR AVE	6 MANOR AVE	Contributing	278	14	772614236
9 MANOR AVE	9 MANOR AVE	Contributing	279	4	1585644266

Table 8: NCHD Property Inventory

Property Name	Address	NCHD Status	Block	Lot	ID Number
10 MANOR AVE	10 MANOR AVE	Contributing	278	15	1236367870
38 MANOR AVE	38 MANOR AVE	Contributing	278	16	1240346481
40 MANOR AVE	40 MANOR AVE	Contributing	278	17	766785002
42 MANOR AVE	42 MANOR AVE	Contributing	278	1	-1944291800
101 MANOR AVE	101 MANOR AVE	Contributing	276	4	-609736551
110 MANOR AVE	110 MANOR AVE	Contributing	277	22	271614037
112 MANOR AVE	112 MANOR AVE	Contributing	277	23	-382211548
113 MANOR AVE	113 MANOR AVE	Non contributing	276	3	-1616953853
114 MANOR AVE	114 MANOR AVE	Contributing	277	24	-89501912
206 MANOR AVE	206 MANOR AVE	Contributing	273	7	622780124
208 MANOR AVE	208 MANOR AVE	Contributing	273	8	-914144466
209 MANOR AVE	209 MANOR AVE	Contributing	272	3	1954802814
210 MANOR AVE	210 MANOR AVE	Contributing	273	9	152760577
211 MANOR AVE	211 MANOR AVE	Contributing	272	2	1663591627
216 MANOR AVE	216 MANOR AVE	Contributing	273	10	724157991
220 MANOR AVE	220 MANOR AVE	Contributing	273	11	234984905
101 MAPLE PL	101 MAPLE PL	Non contributing	269	7	640709072
104 MAPLE PL	104 MAPLE PL	Contributing	270	9	131668117
105 MAPLE PL	105 MAPLE PL	Contributing	269	6	-98189533
106 MAPLE PL	106 MAPLE PL	Contributing	270	8	1580812316
109 MAPLE PL	109 MAPLE PL	Contributing	269	5	1614619760
201 MAPLE PL	201 MAPLE PL	Contributing	267	9	1559650334
207 MAPLE PL	207 MAPLE PL	Contributing	267	8	-457988308
208 MAPLE PL	208 MAPLE PL	Contributing	268	11	893836391
210 MAPLE PL	210 MAPLE PL	Contributing	268	12	1233093566
214 MAPLE PL	214 MAPLE PL	Contributing	268	1	-1971168515
303 MAPLE PL	303 MAPLE PL	Contributing	228	6	-1928636584
304 MAPLE PL	304 MAPLE PL	Contributing	231	5	360374668
305 MAPLE PL	305 MAPLE PL	Contributing	228	5	1773405329
416 MAPLE PL	416 MAPLE PL	Contributing	229	9	104234429
Cranford Post Office	3 MILN ST	Key contributing	181	10	1345608344
211 Miln Street	211 MILN ST	Non contributing	188	13	18404046
215 Miln Street	215 MILN ST	Non contributing	188	12	1830645164
4 NORMAN PL	4 NORMAN PL	Contributing	166	6	359124506
9 NORMAN PL	9 NORMAN PL	Contributing	171	6	-1964337517
10 NORMAN PL	10 NORMAN PL	Contributing	166	7	-2025570660
15 NORMAN PL	15 NORMAN PL	Contributing	171	5	-1145144063
16 NORMAN PL	16 NORMAN PL	Contributing	166	8	537583406
19 NORMAN PL	19 NORMAN PL	Contributing	171	4	-873689321
20 NORMAN PL	20 NORMAN PL	Contributing	166	9	-203173620
21 NORMAN PL	21 NORMAN PL	Contributing	171	3	-1814264035

Table 8: NCHD Property Inventory

Property Name	Address	NCHD Status	Block	Lot	ID Number
2 NORMANDIE PL	2 NORMANDIE PL	Contributing	223	10	297591750
4 NORMANDIE PL	4 NORMANDIE PL	Contributing	223	1	-1512257692
8 NORMANDIE PL	8 NORMANDIE PL	Contributing	224	6	2068843570
10 NORMANDIE PL	10 NORMANDIE PL	Contributing	224	1	876906968
40 NORMANDIE PL	40 NORMANDIE PL	Contributing	229	10	83619379
41 NORMANDIE PL	41 NORMANDIE PL	Contributing	219	29	1475205318
44 NORMANDIE PL	44 NORMANDIE PL	Contributing	229	11	568916604
48 NORMANDIE PL	48 NORMANDIE PL	Contributing	229	12	1741028916
52 NORMANDIE PL	52 NORMANDIE PL	Non contributing	229	13	174940034
57 NORMANDIE PL	57 NORMANDIE PL	Contributing	220	11	-173357980
59 NORMANDIE PL	59 NORMANDIE PL	Contributing	220	12	1763856690
60 NORMANDIE PL	60 NORMANDIE PL	Contributing	229	14	-398545051
61 NORMANDIE PL	61 NORMANDIE PL	Contributing	220	13	2049188528
64 NORMANDIE PL	64 NORMANDIE PL	Contributing	229	15	-169751369
[Open Riverfront Lot]	NORTH AVE E	Unknown	312	18	1292383861
181 NORTH AVE E	181 NORTH AVE E	Non contributing	312	17	-6377326
183 NORTH AVE E	183 NORTH AVE E	Contributing	312	16	1275606443
185 NORTH AVE E	185 NORTH AVE E	Non contributing	312	15	713494731
187 NORTH AVE E	187 NORTH AVE E	Contributing	312	14	-1455252034
189 NORTH AVE E	189 NORTH AVE E	Contributing	312	13	292098320
191 NORTH AVE E	191 NORTH AVE E	Non contributing	312	12	1758224337
Trinity Church	205 NORTH AVE E	Contributing	315	1	1227151100
215 NORTH AVE E	215 NORTH AVE E	Contributing	315	6	-883405685
217 NORTH AVE E	217 NORTH AVE E	Contributing	315	5	-1004394149
221 NORTH AVE E	221 NORTH AVE E	Contributing	315	4	-2111458893
301 NORTH AVE E	301 NORTH AVE E	Contributing	316	2	1795880118
309 NORTH AVE E	309 NORTH AVE E	Contributing	316	1	371793762
North Union Avenue Bridge (STR# 2003016)	North Union Avenue over Rahway River	Contributing	NA	NA	-469504210
6 ORANGE AVE	6 ORANGE AVE	Contributing	261	2	512381778
7 ORANGE AVE	7 ORANGE AVE	Contributing	222	7	-1183083741
8 ORANGE AVE	8 ORANGE AVE	Contributing	261	3	1559852027
9 ORANGE AVE	9 ORANGE AVE	Contributing	222	3.01	268606929
10 ORANGE AVE	10 ORANGE AVE	Contributing	261	4	698076632
12 ORANGE AVE	12 ORANGE AVE	Contributing	261	5	1739566904
16 ORANGE AVE	16 ORANGE AVE	Contributing	261	6	-1130924769
101 ORANGE AVE	101 ORANGE AVE	Non contributing	226	17	875803950
[Open Corner Lot]	102 ORANGE AVE	Unknown	264	1	1081502465
103 ORANGE AVE	103 ORANGE AVE	Contributing	226	16	1668500431
104 ORANGE AVE	104 ORANGE AVE	Contributing	264	2	62500824
105 ORANGE AVE	105 ORANGE AVE	Contributing	226	15	966785273

Table 8: NCHD Property Inventory

Property Name	Address	NCHD Status	Block	Lot	ID Number
106 ORANGE AVE	106 ORANGE AVE	Contributing	264	3	-1493166323
107 ORANGE AVE	107 ORANGE AVE	Contributing	226	14	975195492
108 ORANGE AVE	108 ORANGE AVE	Contributing	264	4	77005362
109 ORANGE AVE	109 ORANGE AVE	Non contributing	226	13	-1538507856
110 ORANGE AVE	110 ORANGE AVE	Contributing	264	5	2060490508
111 ORANGE AVE	111 ORANGE AVE	Contributing	226	12	-112178850
112 ORANGE AVE	112 ORANGE AVE	Contributing	264	6	-285604164
113 ORANGE AVE	113 ORANGE AVE	Contributing	226	11	1636169472
114 ORANGE AVE	114 ORANGE AVE	Contributing	264	7	-66160186
115 ORANGE AVE	115 ORANGE AVE	Contributing	226	10	-2023871209
116 ORANGE AVE	116 ORANGE AVE	Contributing	264	8	-158901881
201 ORANGE AVE	201 ORANGE AVE	Contributing	227	7	-2057349859
203 ORANGE AVE	203 ORANGE AVE	Contributing	227	6	-1573555563
204 ORANGE AVE	204 ORANGE AVE	Non contributing	267	1	-1766270629
205 ORANGE AVE	205 ORANGE AVE	Contributing	227	5	-1904606182
206 ORANGE AVE	206 ORANGE AVE	Contributing	267	2	-1348332397
208 ORANGE AVE	208 ORANGE AVE	Contributing	267	3	1291870466
210 ORANGE AVE	210 ORANGE AVE	Contributing	267	4	1288213805
212 ORANGE AVE	212 ORANGE AVE	Contributing	267	5	-1687229731
213 ORANGE AVE	213 ORANGE AVE	Contributing	228	10	-771939342
214 ORANGE AVE	214 ORANGE AVE	Contributing	267	6	1614828022
215 ORANGE AVE	215 ORANGE AVE	Contributing	228	9	1610552190
216 ORANGE AVE	216 ORANGE AVE	Non contributing	267	7	1332768255
217 ORANGE AVE	217 ORANGE AVE	Contributing	228	8	-1052928887
219 ORANGE AVE	219 ORANGE AVE	Contributing	228	7	-666631256
301 ORANGE AVE	301 ORANGE AVE	Contributing	231	4	-241611052
303 ORANGE AVE	303 ORANGE AVE	Contributing	231	3	867072897
305 ORANGE AVE	305 ORANGE AVE	Contributing	231	2	92078288
306 ORANGE AVE	306 ORANGE AVE	Contributing	268	2	-1184848911
307 ORANGE AVE	307 ORANGE AVE	Contributing	231	1	-1270609326
308 ORANGE AVE	308 ORANGE AVE	Contributing	268	3	879237552
409 ORANGE AVE	409 ORANGE AVE	Contributing	232	3	-1966348230
411 ORANGE AVE	411 ORANGE AVE	Non contributing	232	2	-834150426
417 ORANGE AVE	417 ORANGE AVE	Contributing	233	2	418764759
419 ORANGE AVE	419 ORANGE AVE	Contributing	233	1	-2055304685
9 ORCHARD ST	9 ORCHARD ST	Contributing	176	20	1593340691
11 ORCHARD ST	11 ORCHARD ST	Contributing	176	19	-1068077578
102 ORCHARD ST	102 ORCHARD ST	Contributing	179	11	599299820
104 ORCHARD ST	104 ORCHARD ST	Contributing	179	10	-1247994689
106 ORCHARD ST	106 ORCHARD ST	Contributing	179	9	-1618670615
110 ORCHARD ST	110 ORCHARD ST	Non contributing	179	8	-2057955952

Table 8: NCHD Property Inventory

Property Name	Address	NCHD Status	Block	Lot	ID Number
114 ORCHARD ST	114 ORCHARD ST	Contributing	179	7	1013172276
201 ORCHARD ST	201 ORCHARD ST	Contributing	178	6	984256909
207 ORCHARD ST	207 ORCHARD ST	Contributing	178	5	1077699666
215 ORCHARD ST	215 ORCHARD ST	Contributing	178	4	-1518577601
221 ORCHARD ST	221 ORCHARD ST	Non contributing	178	3	-1840244
222 ORCHARD ST	222 ORCHARD ST	Key Contributing	179	6	612318602
225 ORCHARD ST	225 ORCHARD ST	Contributing	178	2	656109755
312 ORCHARD ST	312 ORCHARD ST	Contributing	172	1	1523128544
395 ORCHARD ST	395 ORCHARD ST	Contributing	173	17	285459423
397 ORCHARD ST	397 ORCHARD ST	Contributing	173	16	1783950032
399 ORCHARD ST	399 ORCHARD ST	Contributing	173	15	-1498538731
401 ORCHARD ST	401 ORCHARD ST	Contributing	173	14	-488366828
403 ORCHARD ST	403 ORCHARD ST	Contributing	173	13	443770499
404 ORCHARD ST	404 ORCHARD ST	Non contributing	171	1.01	1268224127
406 ORCHARD ST	406 ORCHARD ST	Non contributing	171	1.02	1138755269
407 ORCHARD ST	407 ORCHARD ST	Contributing	173	12	-410064218
408 ORCHARD ST	408 ORCHARD ST	Non contributing	171	1.03	-860475742
409 ORCHARD ST	409 ORCHARD ST	Contributing	173	11	-1507674360
410 ORCHARD ST	410 ORCHARD ST	Non contributing	171	1.04	1513521236
411 ORCHARD ST	411 ORCHARD ST	Contributing	173	10	108028719
412 ORCHARD ST	412 ORCHARD ST	Non contributing	170	8	1026584916
413 ORCHARD ST	413 ORCHARD ST	Non contributing	173	9	-1694536550
414 ORCHARD ST	414 ORCHARD ST	Contributing	170	9	589593477
416 ORCHARD ST	416 ORCHARD ST	Contributing	170	10	1602666705
417 ORCHARD ST	417 ORCHARD ST	Contributing	173	8	-24421271
418 ORCHARD ST	418 ORCHARD ST	Contributing	170	11	-870678023
419 ORCHARD ST	419 ORCHARD ST	Contributing	173	7	2041749345
420 ORCHARD ST	420 ORCHARD ST	Contributing	170	12	1506190129
422 ORCHARD ST	422 ORCHARD ST	Contributing	170	13	921012130
426 ORCHARD ST	426 ORCHARD ST	Non contributing	161	9	1886715087
430 ORCHARD ST	430 ORCHARD ST	Contributing	161	10	1934460366
434 ORCHARD ST	434 ORCHARD ST	Contributing	161	11	749898645
436 ORCHARD ST	436 ORCHARD ST	Contributing	161	12	-724173152
438 ORCHARD ST	438 ORCHARD ST	Contributing	161	13	662680979
440 ORCHARD ST	440 ORCHARD ST	Contributing	161	14	-1684818073
442 ORCHARD ST	442 ORCHARD ST	Contributing	161	15	427353471
Concrete Culvert, Concrete Parapets	Orchard Street over Rahway River Trib.	Non Contributing	NA	NA	1475265165
Orchard Street Bridge (STR# 2003071)	Orchard Street over Rahway River Tributary	Non Contributing	NA	NA	-1280747029
4 PITTSFIELD ST	4 PITTSFIELD ST	Contributing	172	2	-1828354310

Table 8: NCHD Property Inventory

Property Name	Address	NCHD Status	Block	Lot	ID Number
6 PITTSFIELD ST	6 PITTSFIELD ST	Contributing	172	3	570724027
7 PITTSFIELD ST	7 PITTSFIELD ST	Contributing	171	9	-1612437871
8 PITTSFIELD ST	8 PITTSFIELD ST	Contributing	172	4	-1144427498
11 PITTSFIELD ST	11 PITTSFIELD ST	Contributing	171	8	1535191633
13 PITTSFIELD ST	13 PITTSFIELD ST	Non contributing	171	7	-520538562
20 PITTSFIELD ST	20 PITTSFIELD ST	Contributing	167	1	431312636
26 PITTSFIELD ST	26 PITTSFIELD ST	Contributing	167	2	-245329329
Concrete Culvert, Stone Parapets	Pittsfield/Eastman Streets over Rahway River Trib.	Non Contributing	NA	NA	-992311034
209 PROSPECT AVE	209 PROSPECT AVE	Non contributing	265	3	-1332043693
214 PROSPECT AVE	214 PROSPECT AVE	Non contributing	266	2	-1966740803
216 PROSPECT AVE	216 PROSPECT AVE	Contributing	266	3	-1557538489
[Condominiums]	217 PROSPECT AVE	Contributing	265	2	-1838425140
218 PROSPECT AVE	218 PROSPECT AVE	Contributing	266	4	1974905440
301 PROSPECT AVE	301 PROSPECT AVE	Contributing	269	10	-328675250
302 PROSPECT AVE	302 PROSPECT AVE	Contributing	271	1	1230759180
305 PROSPECT AVE	305 PROSPECT AVE	Contributing	269	9	-180174589
307 PROSPECT AVE	307 PROSPECT AVE	Contributing	269	8	-2015060088
308 PROSPECT AVE	308 PROSPECT AVE	Contributing	271	2	2079329053
310 PROSPECT AVE	310 PROSPECT AVE	Contributing	271	3	1768817178
315 PROSPECT AVE	315 PROSPECT AVE	Contributing	270	7	-2071708951
316 PROSPECT AVE	316 PROSPECT AVE	Contributing	271	4	-491144557
319 PROSPECT AVE	319 PROSPECT AVE	Contributing	270	6	90943734
320 PROSPECT AVE	320 PROSPECT AVE	Contributing	271	5	-1106911784
325 PROSPECT AVE	325 PROSPECT AVE	Contributing	270	5	-1536483521
405 PROSPECT AVE	405 PROSPECT AVE	Contributing	276	5	-1792169654
406 PROSPECT AVE	406 PROSPECT AVE	Contributing	279	2	-5783839
410 PROSPECT AVE	410 PROSPECT AVE	Contributing	279	3	-1187335282
Flood Control Dam	Rahway River at North Union Avenue	Contributing	NA	NA	1560442659
Rock Wall	RIVERSIDE DR	Contributing	NA	NA	-2007369938
[Open Lot]	RIVERSIDE DR	Unknown	226	18	-1929097525
[Open Lot, RRPW]	RIVERSIDE DR	Unknown	261	8	-743283773
Riverside Condominiums	4-8 RIVERSIDE DR	Contributing	266	1.03	965279721
12-18 RIVERSIDE DR	12-18 RIVERSIDE DR	Non contributing	265	4	532797146
[Condominiums]	22 RIVERSIDE DR	Non contributing	265	1	-1699204426
96 RIVERSIDE DR	96 RIVERSIDE DR	Contributing	264	13	-1270459553
98 RIVERSIDE DR	98 RIVERSIDE DR	Contributing	264	14	-270137306
100 RIVERSIDE DR	100 RIVERSIDE DR	Non contributing	264	15	314731193
102 RIVERSIDE DR	102 RIVERSIDE DR	Contributing	264	16	-1101631532
104 RIVERSIDE DR	104 RIVERSIDE DR	Contributing	264	17	-1392983912

Table 8: NCHD Property Inventory

Property Name	Address	NCHD Status	Block	Lot	ID Number
107 RIVERSIDE DR	107 RIVERSIDE DR	Contributing	261	7	672787030
121 RIVERSIDE DR	121 RIVERSIDE DR	Contributing	222	5	-1991033095
123 RIVERSIDE DR	123 RIVERSIDE DR	Contributing	222	4	-842210016
126 RIVERSIDE DR	126 RIVERSIDE DR	Contributing	226	19	-620323343
127 RIVERSIDE DR	127 RIVERSIDE DR	Non contributing	222	2	-1888106554
300 RIVERSIDE DR	300 RIVERSIDE DR	Contributing	223	9	327885862
Pierson House	420 RIVERSIDE DR	Key contributing	223	7	1228947263
Sperry Park	Riverside Drive at North Union Avenue	Contributing	263	1	-1555635752
Crane's Ford Monument	Riverside Drive at Springfield Avenue	Contributing	NA	NA	-1541740288
Culvert, Metal Railings	Riverside Drive over Casino Brook	Contributing	NA	NA	-2120718822
[Open Lot]	SPRING GARDEN ST	Unknown	174	23	-1195466189
4 SPRING GARDEN ST	4 SPRING GARDEN ST	Non contributing	178	13	-1344410566
5 SPRING GARDEN ST	5 SPRING GARDEN ST	Non contributing	174	14	-1051996287
6 SPRING GARDEN ST	6 SPRING GARDEN ST	Non contributing	178	14	1291321757
7 SPRING GARDEN ST	7 SPRING GARDEN ST	Contributing	174	13	1686934803
9 SPRING GARDEN ST	9 SPRING GARDEN ST	Contributing	174	12	838950830
10 SPRING GARDEN ST	10 SPRING GARDEN ST	Contributing	178	15	1835622219
11 SPRING GARDEN ST	11 SPRING GARDEN ST	Contributing	174	11	733793897
16 SPRING GARDEN ST	16 SPRING GARDEN ST	Contributing	178	16	416676686
19 SPRING GARDEN ST	19 SPRING GARDEN ST	Contributing	174	10	196355479
101 SPRING GARDEN ST	101 SPRING GARDEN ST	Contributing	174	9	543508056
102 SPRING GARDEN ST	102 SPRING GARDEN ST	Non contributing	173	19	-949849593
103 SPRING GARDEN ST	103 SPRING GARDEN ST	Contributing	174	8	48926290
105 SPRING GARDEN ST	105 SPRING GARDEN ST	Non contributing	174	7	-2073275934
106 SPRING GARDEN ST	106 SPRING GARDEN ST	Contributing	173	20	-1792351692
107 SPRING GARDEN ST	107 SPRING GARDEN ST	Contributing	174	6	1688995971
108 SPRING GARDEN ST	108 SPRING GARDEN ST	Contributing	173	21	329462024
109 SPRING GARDEN ST	109 SPRING GARDEN ST	Contributing	174	5	-1929351815
110 SPRING GARDEN ST	110 SPRING GARDEN ST	Contributing	173	22	-840150687
111 SPRING GARDEN ST	111 SPRING GARDEN ST	Non contributing	174	4	394974287
112 SPRING GARDEN ST	112 SPRING GARDEN ST	Contributing	173	23	1735861426
113 SPRING GARDEN ST	113 SPRING GARDEN ST	Contributing	174	3	-324144058
114 SPRING GARDEN ST	114 SPRING GARDEN ST	Contributing	173	24	-1012257003
117 SPRING GARDEN ST	117 SPRING GARDEN ST	Contributing	174	2	-1962227237
Culvert, Concrete Parapets	Spring Garden St. over Rahway River Trib.	Contributing	NA	NA	1475434534
Girl Scout Park	SPRINGFIELD AVE	Contributing	184	18	-1742473672
[9-11 Memorial Park]	10 SPRINGFIELD AVE	Non contributing	262	9	1605128555
First Presbyterian Church	11 SPRINGFIELD AVE	Key contributing	191	3	1227184794

Table 8: NCHD Property Inventory

Property Name	Address	NCHD Status	Block	Lot	ID Number
Gray Memorial Funeral Home	12 SPRINGFIELD AVE	Contributing	262	8	602972436
Cranford Towers	18 SPRINGFIELD AVE	Non contributing	262	7	-1706518298
First Church of Christ Scientist	21 SPRINGFIELD AVE	Contributing	188	11	-1990268328
Apartments	24 SPRINGFIELD AVE	Contributing	262	6	1126356236
29 SPRINGFIELD AVE	29 SPRINGFIELD AVE	Non contributing	188	10	1981976159
[Condominiums]	30 SPRINGFIELD AVE	Non contributing	262	5	-951182119
Hanson House (Hanson Park)	38 SPRINGFIELD AVE	Contributing	262	3.01	1465671797
Hanson Park	50 SPRINGFIELD AVE	Unknown	262	1	-194012247
Cranford Canoe Club	250 SPRINGFIELD AVE	Contributing	261	1	1504318351
300 SPRINGFIELD AVE	300 SPRINGFIELD AVE	Contributing	222	8	921503504
Cranford Clay Courts	301 SPRINGFIELD AVE	Contributing	184	18	1897745952
304 SPRINGFIELD AVE	304 SPRINGFIELD AVE	Contributing	222	9	593410039
305 SPRINGFIELD AVE	305 SPRINGFIELD AVE	Non contributing	184	17	-1428280542
306 SPRINGFIELD AVE	306 SPRINGFIELD AVE	Contributing	222	10	-1597709935
320 SPRINGFIELD AVE	320 SPRINGFIELD AVE	Contributing	222	11	-2116365075
Memorial Park	350 SPRINGFIELD AVE	Contributing	221	1	-414057606
Dykeman Farmhouse	400 SPRINGFIELD AVE	Contributing	200	48	609964933
405 SPRINGFIELD AVE	405 SPRINGFIELD AVE	Contributing	169	12	-299494766
411 SPRINGFIELD AVE	411 SPRINGFIELD AVE	Contributing	168	9	1309011975
501 SPRINGFIELD AVE	501 SPRINGFIELD AVE	Non contributing	164	7	-1831392754
503 SPRINGFIELD AVE	503 SPRINGFIELD AVE	Non contributing	164	6	499767541
504 SPRINGFIELD AVE	504 SPRINGFIELD AVE	Contributing	201	8	1150019632
505 SPRINGFIELD AVE	505 SPRINGFIELD AVE	Contributing	164	5	-1526657666
506 SPRINGFIELD AVE	506 SPRINGFIELD AVE	Contributing	201	9	1541435307
507 SPRINGFIELD AVE	507 SPRINGFIELD AVE	Non contributing	164	4	-598645345
508 SPRINGFIELD AVE	508 SPRINGFIELD AVE	Contributing	201	10	1281943845
509 SPRINGFIELD AVE	509 SPRINGFIELD AVE	Contributing	164	3	-585435017
511 SPRINGFIELD AVE	511 SPRINGFIELD AVE	Contributing	164	2	-911785710
512 SPRINGFIELD AVE	512 SPRINGFIELD AVE	Contributing	201	11	-1224246407
513 SPRINGFIELD AVE	513 SPRINGFIELD AVE	Contributing	164	1	2636162
605 SPRINGFIELD AVE	605 SPRINGFIELD AVE	Contributing	163	8	1794872345
607 SPRINGFIELD AVE	607 SPRINGFIELD AVE	Contributing	163	7	-517240952
609 SPRINGFIELD AVE	609 SPRINGFIELD AVE	Contributing	163	6	339345333
610 SPRINGFIELD AVE	610 SPRINGFIELD AVE	Contributing	200	21	-1589471728
611 SPRINGFIELD AVE	611 SPRINGFIELD AVE	Contributing	163	5	-1460540014
613 SPRINGFIELD AVE	613 SPRINGFIELD AVE	Contributing	163	4	81109444
614 SPRINGFIELD AVE	614 SPRINGFIELD AVE	Contributing	200	20	739266077
615 SPRINGFIELD AVE	615 SPRINGFIELD AVE	Contributing	163	3	1605844241
617 SPRINGFIELD AVE	617 SPRINGFIELD AVE	Contributing	163	2	1236359771

Table 8: NCHD Property Inventory

Property Name	Address	NCHD Status	Block	Lot	ID Number
618 SPRINGFIELD AVE	618 SPRINGFIELD AVE	Contributing	200	19	-9300980
619 SPRINGFIELD AVE	619 SPRINGFIELD AVE	Contributing	163	1	1387341000
622 SPRINGFIELD AVE	622 SPRINGFIELD AVE	Contributing	200	18	-217656319
626 SPRINGFIELD AVE	626 SPRINGFIELD AVE	Non contributing	200	17	-390185494
630 SPRINGFIELD AVE	630 SPRINGFIELD AVE	Contributing	200	16	497676380
700 SPRINGFIELD AVE	700 SPRINGFIELD AVE	Contributing	198	45	1086972996
701 SPRINGFIELD AVE	701 SPRINGFIELD AVE	Contributing	153	11	-1251127283
702 SPRINGFIELD AVE	702 SPRINGFIELD AVE	Contributing	198	44	-965383633
703 SPRINGFIELD AVE	703 SPRINGFIELD AVE	Contributing	153	10	-1501287801
704 SPRINGFIELD AVE	704 SPRINGFIELD AVE	Contributing	198	43	427256441
705 SPRINGFIELD AVE	705 SPRINGFIELD AVE	Contributing	153	9	1733248753
706 SPRINGFIELD AVE	706 SPRINGFIELD AVE	Contributing	198	42	2063632704
707 SPRINGFIELD AVE	707 SPRINGFIELD AVE	Contributing	153	8	435168346
708 SPRINGFIELD AVE	708 SPRINGFIELD AVE	Contributing	198	41	2096632619
709 SPRINGFIELD AVE	709 SPRINGFIELD AVE	Non contributing	153	7	-865748120
710 SPRINGFIELD AVE	710 SPRINGFIELD AVE	Contributing	198	40	-114750615
711 SPRINGFIELD AVE	711 SPRINGFIELD AVE	Contributing	153	6	-1952057160
713 SPRINGFIELD AVE	713 SPRINGFIELD AVE	Contributing	153	5	780332275
715 SPRINGFIELD AVE	715 SPRINGFIELD AVE	Non contributing	153	4	1552424823
717 SPRINGFIELD AVE	717 SPRINGFIELD AVE	Contributing	153	3	1503096036
719 SPRINGFIELD AVE	719 SPRINGFIELD AVE	Contributing	153	2	-1725619564
810 SPRINGFIELD AVE	810 SPRINGFIELD AVE	Non contributing	198	17	472231894
812 SPRINGFIELD AVE	812 SPRINGFIELD AVE	Contributing	198	16	1067583577
814 SPRINGFIELD AVE	814 SPRINGFIELD AVE	Contributing	198	15	-1871512555
816 SPRINGFIELD AVE	816 SPRINGFIELD AVE	Contributing	198	14	-1635302640
818 SPRINGFIELD AVE	818 SPRINGFIELD AVE	Contributing	198	13	1608440314
820 SPRINGFIELD AVE	820 SPRINGFIELD AVE	Contributing	197	7	1828922343
822 SPRINGFIELD AVE	822 SPRINGFIELD AVE	Contributing	197	8	-313629761
Springfield Avenue Bridge (STR# 2003072)	Springfield Avenue over Rahway River (NW crossing)	Non contributing	NA	NA	-1970502225
Springfield Avenue Bridge (STR# 2003014)	Springfield Avenue over Rahway River (SE crossing)	Non contributing	NA	NA	1878835061
1 SPRUCE ST	1 SPRUCE ST	Contributing	155	7	1709410165
5 SPRUCE ST	5 SPRUCE ST	Contributing	155	6	-1584737545
9 SPRUCE ST	9 SPRUCE ST	Contributing	155	5	1643342730
15 SPRUCE ST	15 SPRUCE ST	Non contributing	155	4	1297464872
17 SPRUCE ST	17 SPRUCE ST	Contributing	155	3	1664342131
21 SPRUCE ST	21 SPRUCE ST	Contributing	155	2	-670619183
25 SPRUCE ST	25 SPRUCE ST	Contributing	155	1	-525012915
27 SPRUCE ST	27 SPRUCE ST	Contributing	159	8	276015887

Table 8: NCHD Property Inventory

Property Name	Address	NCHD Status	Block	Lot	ID Number
30 SPRUCE ST	30 SPRUCE ST	Contributing	157	6	802285613
31 SPRUCE ST	31 SPRUCE ST	Contributing	159	7	-56413086
34 SPRUCE ST	34 SPRUCE ST	Contributing	157	5	1147961364
35 SPRUCE ST	35 SPRUCE ST	Contributing	159	6	2058821306
36 SPRUCE ST	36 SPRUCE ST	Contributing	157	4.02	-2052687520
39 SPRUCE ST	39 SPRUCE ST	Contributing	159	5	-938970986
40 SPRUCE ST	40 SPRUCE ST	Non Contributing	157	4.01	1475455723
41 SPRUCE ST	41 SPRUCE ST	Contributing	159	4	1215457892
42 SPRUCE ST	42 SPRUCE ST	Contributing	157	3	512196198
43 SPRUCE ST	43 SPRUCE ST	Contributing	159	3	347650102
44 SPRUCE ST	44 SPRUCE ST	Contributing	157	2	-112761885
45 SPRUCE ST	45 SPRUCE ST	Contributing	159	2	232114712
46 SPRUCE ST	46 SPRUCE ST	Contributing	157	1	-1538555213
47 SPRUCE ST	47 SPRUCE ST	Contributing	159	1	-2024827423
1 SYLVESTER ST	1 SYLVESTER ST	Contributing	311	10	782038201
3 SYLVESTER ST	3 SYLVESTER ST	Contributing	311	9	-1548811820
4 SYLVESTER ST	4 SYLVESTER ST	Contributing	308	16	-1089841552
5 SYLVESTER ST	5 SYLVESTER ST	Contributing	311	8	-2034943354
6 SYLVESTER ST	6 SYLVESTER ST	Contributing	308	17	-1045015837
7 SYLVESTER ST	7 SYLVESTER ST	Contributing	311	7	-2011947074
8 SYLVESTER ST	8 SYLVESTER ST	Contributing	308	18	141643506
9 SYLVESTER ST	9 SYLVESTER ST	Contributing	311	6	615754838
10 SYLVESTER ST	10 SYLVESTER ST	Contributing	308	19	-1364591189
11 SYLVESTER ST	11 SYLVESTER ST	Contributing	311	5	1382533701
11A SYLVESTER ST	11 SYLVESTER ST	Non contributing	311	4	-710941687
12 SYLVESTER ST	12 SYLVESTER ST	Contributing	308	20	1080858770
14 SYLVESTER ST	14 SYLVESTER ST	Non contributing	308	21	-681132427
15 SYLVESTER ST	15 SYLVESTER ST	Contributing	311	3	-918869098
16 SYLVESTER ST	16 SYLVESTER ST	Contributing	308	22	-1994937771
17 SYLVESTER ST	17 SYLVESTER ST	Non contributing	311	2	-940929249
18 SYLVESTER ST	18 SYLVESTER ST	Contributing	308	23	-561572668
25 SYLVESTER ST	25 SYLVESTER ST	Non contributing	311	1	554833596
711 TULIP ST	711 TULIP ST	Contributing	153	12	610222035
Josiah Crane Jr. House	124 N UNION AVE	Key contributing	196	2.01	609237062
[Parkland adjacent Crane House / Municipal Building] RRPW	124 N UNION AVE	Contributing	196	2.01	-678809955
Lynwold	136 N UNION AVE	Key contributing	312	1	-69062684
203 N UNION AVE	203 N UNION AVE	Non contributing	266	1.02	353229321
204 N UNION AVE	204 N UNION AVE	Non contributing	310	10	1712460279
205 N UNION AVE	205 N UNION AVE	Non contributing	266	1.01	982739350

Table 8: NCHD Property Inventory

Property Name	Address	NCHD Status	Block	Lot	ID Number
206 N UNION AVE	206 N UNION AVE	Non contributing	310	11	1011039
207 N UNION AVE	207 N UNION AVE	Contributing	266	7	1230554683
211 N UNION AVE	211 N UNION AVE	Contributing	266	6	-1735417119
303 N UNION AVE	303 N UNION AVE	Contributing	271	14	559598333
304 N UNION AVE	304 N UNION AVE	Contributing	306	1	73246711
305 N UNION AVE	305 N UNION AVE	Contributing	271	13	-1434059405
306 N UNION AVE	306 N UNION AVE	Non contributing	306	2	-804418343
308 N UNION AVE	308 N UNION AVE	Non contributing	306	3	416187055
309 N UNION AVE	309 N UNION AVE	Contributing	271	12	291701877
310 N UNION AVE	310 N UNION AVE	Non contributing	306	4	-547900501
312 N UNION AVE	312 N UNION AVE	Contributing	306	5	283119057
313 N UNION AVE	313 N UNION AVE	Contributing	271	11	421645415
316 N UNION AVE	316 N UNION AVE	Non contributing	306	6	-957397779
Sperry House	319 N UNION AVE	Key Contributing	271	10	1810213042
320 N UNION AVE	320 N UNION AVE	Contributing	306	7	1230588781
321 N UNION AVE	321 N UNION AVE	Contributing	271	9	1070347107
324 N UNION AVE	324 N UNION AVE	Contributing	306	8	797471817
325 N UNION AVE	325 N UNION AVE	Contributing	271	8	698578847
405 N UNION AVE	405 N UNION AVE	Contributing	279	6	647590004
407 N UNION AVE	407 N UNION AVE	Contributing	279	5	-1181857689
412 N UNION AVE	412 N UNION AVE	Contributing	280	2	1600864154
414 N UNION AVE	414 N UNION AVE	Contributing	280	3	-485734772
416 N UNION AVE	416 N UNION AVE	Contributing	280	4	-909163492
420 N UNION AVE	420 N UNION AVE	Contributing	280	5	731914477
423 N UNION AVE	423 N UNION AVE	Contributing	278	13	1160096290
424 N UNION AVE	424 N UNION AVE	Contributing	280	6	-1381575507
425 N UNION AVE	425 N UNION AVE	Contributing	278	12	1917621365
426 N UNION AVE	426 N UNION AVE	Contributing	280	7	-486885462
427 N UNION AVE	427 N UNION AVE	Contributing	278	11	1066730723
428 N UNION AVE	428 N UNION AVE	Contributing	280	8	425053672
429 N UNION AVE	429 N UNION AVE	Contributing	278	10	2098438601
431 N UNION AVE	431 N UNION AVE	Contributing	278	9	-452065706
433 N UNION AVE	433 N UNION AVE	Contributing	278	8	2031564434
130 WEST END PL	130 WEST END PL	Non contributing	171	1.05	-244798442
140 WEST END PL	140 WEST END PL	Non contributing	171	1.06	1268790921
150 WEST END PL	150 WEST END PL	Contributing	171	2	595079753
Cranford High School	201 WEST END PL	Key contributing	154	16	-1671249846
204 WEST END PL	204 WEST END PL	Contributing	165	1	-857751925
301 WEST END PL	301 WEST END PL	Contributing	163	10	-362085655
303 WEST END PL	303 WEST END PL	Non contributing	163	9	-1044177347
304 WEST END PL	304 WEST END PL	Contributing	164	13	1000188228

Table 8: NCHD Property Inventory

Property Name	Address	NCHD Status	Block	Lot	ID Number
801 WEST END PL	801 WEST END PL	Contributing	200	22	1032920611
803 WEST END PL	803 WEST END PL	Contributing	200	23	-761737356
805 WEST END PL	805 WEST END PL	Contributing	200	24	876961904
807 WEST END PL	807 WEST END PL	Contributing	200	25	-1696079037
809 WEST END PL	809 WEST END PL	Contributing	200	26	457951514
1 WILLOW ST	1 WILLOW ST	Contributing	165	8	-918524488
3 WILLOW ST	3 WILLOW ST	Contributing	165	7	1387742535
5 WILLOW ST	5 WILLOW ST	Contributing	165	6	-2025894293
6 WILLOW ST	6 WILLOW ST	Contributing	164	9	1459354290
7 WILLOW ST	7 WILLOW ST	Contributing	165	5	-312250264
8 WILLOW ST	8 WILLOW ST	Contributing	164	10	1557041959
9 WILLOW ST	9 WILLOW ST	Non contributing	165	4	417765721
10 WILLOW ST	10 WILLOW ST	Contributing	164	11	-1060793226
11 WILLOW ST	11 WILLOW ST	Contributing	165	3	1893110705
12 WILLOW ST	12 WILLOW ST	Contributing	164	12	-710345698
15 WILLOW ST	15 WILLOW ST	Non contributing	165	2	1611070795
601 WILLOW ST	601 WILLOW ST	Contributing	154	15	-1089830680
603 WILLOW ST	603 WILLOW ST	Non contributing	154	14	1086727034
604 WILLOW ST	604 WILLOW ST	Contributing	163	11	-1362353941
607 WILLOW ST	607 WILLOW ST	Contributing	154	13	1110492092
608 WILLOW ST	608 WILLOW ST	Non contributing	163	12	-756970073
609 WILLOW ST	609 WILLOW ST	Contributing	154	12	703331934
614 WILLOW ST	614 WILLOW ST	Contributing	163	13	-1979415038
615 WILLOW ST	615 WILLOW ST	Contributing	154	11	1630861180
701 WILLOW ST	701 WILLOW ST	Non contributing	154	10	531163341
705 WILLOW ST	705 WILLOW ST	Contributing	154	9	1142495754
707 WILLOW ST	707 WILLOW ST	Contributing	154	8	582514504

Table 8: NCHD Property Inventory



The Mill Wheel

News and Notes from the Cranford Historical Society

Winter 2016

Union County, New Jersey

New Series, Number 87

President's Message

Dear Members,

Happy New Year!

We hope everyone had a wonderful Holiday season.

In 2016, we will be having new programs to look forward to and two fundraisers.

On May 21, 2016 by request from so many, we will be having our second "Hat Box Tea" at the Hanson House from 2:00pm to 4:00pm. Also on June 4, 2016, we will have our Garage Sale at the Hanson House. The proceeds will benefit the upkeep of the gardens at the museum, Hanson House and the Cranford High School History award Scholarship fund. Flyers for both will sent out in our next Mill Wheel.

If you are starting to down size and getting rid of items, please save them for our garage sale. Please no electrical items. We will be accepting garage sale items starting May 9th through May 12th, 2016 from 3:30pm to 5:30pm (Monday-Thursday).

Please mark your calendars:

For our "Hat Box Tea", we have seating for 30 people. So please make your reservations early. Call our office at 908-276-0082. Price is \$25.00 per person. We are thinking about having costume curator, Gail Alterman do a program on Downton Abbey.

We hope you will join us for both fundraisers.

Our 2016 membership forms have been sent out. Please continue to be a member so we

can continue to preserve and perpetuate the history of Cranford, continue our third grade school tours and maintain the Crane-Phillips House Museum and Hanson House. We can't do it without your support. I would also like to thank all our member volunteers who have helped us in 2015. You're greatly appreciated. And hope more members will become volunteers to help out with the fundraising and garage sale, etc.

Until our next Mill Wheel, take care, stay well and always be happy!

Your President,

Margaret Gerlach

The Birth and Growth of Cranford

Part VI (Final): Sunny Acres

By Vic Bary

The announcement in the June 6, 1940 *Citizen and Chronicle* that the Modern Homes Division of Sears Roebuck & Company had submitted plans to build at least 200 homes on property that had been part of the old Thomas Sperry Osceola Farm estate was attention-getting in several respects. First, it was an unusually large residential building project for an economy still emerging from the Great Depression. More importantly, while Sears Modern Homes Division had been in existence for 45 years, during which it had sold an estimated 100,000 pre-cut homes, it had never before bought land and built homes on it at its own expense in a development Sears, itself, would manage.

Sears Modern Homes Department History

Sears sold home building materials from 1895 to 1907 through the Modern Homes Department. In the face of flagging sales, Sears appointed its china department manager Frank W. Kushel to close down the failing department in 1906. Instead of closing the department, Kushel came up with a plan to sell complete homes, from pre-cut lumber down to 750 pounds of nails for joining these pieces, accompanied by a construction manual of up to 75 pages. The materials would be delivered by rail to the purchaser in a phased series of deliveries intended to get the materials on site as needed. In this era before power tools, it was estimated that pre-cutting the lumber would reduce on-site carpentry hours by 40%. Sears boasted that "a man of average abilities could assemble a Sears kit home in about 90 days."

The first catalogue was published in 1908, with 22 home styles, priced from \$650 to \$2,500. The construction catalogue was accompanied by a catalogue of interior furnishing ideas - all illustrated with Sears products. In 1918 Sears began offering financing for these purchases. In 1929, Sears added contractor services (either to build the house itself, or to consult to the purchaser planning to personally erect the home). Over 100,000 such homes were eventually built, in 447 different styles. Although architects for these styles were usually not identified, one who was is Randolph Evans, whose name would appear on the blueprints for the Sunny Acres numbered Cape Cod designs. (Evans, along with architect Albert E. Olson, were prominent members of the "Small House Movement".)

Sears was not alone in offering pre-cut homes - Aladdin, Harris Brothers, The Hodgson Company, Lewis Homes, and Montgomery Ward were also in the business - but Sears was arguably the largest supplier. During

WWI, Sears also built pre-cut hospitals for the Red Cross for shipment to and assembly in Europe. After the War, using its pre-cut home designs, Sears built worker housing for factory towns for corporations such as Standard Oil (Carlinville, IL) and American Magnesia (Plymouth Meeting, PA).

As sales began to flag during the Great Depression, Sears withdrew from pre-cut home sales and from financing in 1933. (When some borrowers proved unable to keep up payments, Sears generally absorbed the losses, not wanting to appear heartless.) In 1934, Sears' annual report stated that the Modern Homes Department had been closed, and \$11MM in mortgages liquidated (written off as a loss). In spite of the closure, Sears kept its major Modern Homes Department sales offices in place - including the one in Port Newark.

However, Sears began selling pre-fabricated homes again in 1935. But now the houses were made by General Homes, Inc. of Chicago and featured steel framing members and roofs, and plywood walls. (The relationship seems to have ended by 1940.)

Sears Modern Homes Division becomes a Developer - the Home Club Plan

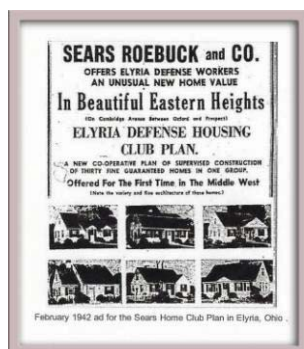
Sunny Acres represented an extension of the activities engaged in by the Modern Homes Department, as Sears was now buying the land, building the dwellings on it, and offering them for sale through the Home Club Plan. The Home Club Plan, operated out of its Port Newark offices, allowed Sears to work with local bonded contractors who would build large numbers of new Sears standardized homes at a single time to realize economies of scale.

The July 7, 1940 *New York Times* and the July 18, 1940 *Citizen and Chronicle* stated that 63 one-family dwelling permits had been

issued to Sears, Roebuck & Company's Home Club Builders, Inc. According to the *Citizen and Chronicle*, "Work was scheduled to get underway yesterday, and the homes are expected to be completed and ready for occupancy within six months. ... All of the homes in the tract were sold within five weeks" (even before the first spade full of earth was turned).

There seem to have been two driving forces behind this shift in Sears Modern Homes approach from simply being a manufacturer, to being a supplier and developer under the new Home Club Plan. Initially, the advent of FHA mortgages under FDR's second "New Deal" in 1934-1935 made 90% financing for extended periods of time (up to 30 years) available for the first time. Under the Home Club Plan, FHA financing customers would not have to pay legal fees or title and survey fees, and could get into a new home with a modest 10% down payment. This was the driving force behind Sunny Acres, Sears **first** Home Club development, which was laid out according to FHA subdivision plans and specifications.

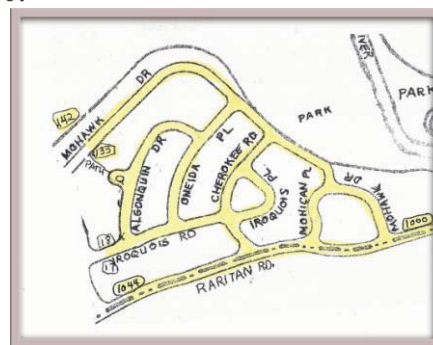
Later, as the fourth decade of the 20th Century unfolded, it became increasingly clear that the U.S. would be drawn into WW II. With this in mind, the federal government foresaw the need for increased housing for defense industry workers. This additional factor influenced new Sears Home Club Plan developments as early as February, 1942.



Sears responded to either or both of these driving forces by initiating Home Club Plan developments in Cranford (started mid-1940) and North Plainfield ("Green Acres" started September, 1940) New Jersey; Elyria, Ohio; and in Briarcliff Manor, Grandyle Village, North Tonowanda, and Sidney, New York. Ultimately, 10,000 - 20,000 Home Club Plan houses would be built. With its proximity to manufacturing centers in Elizabeth and Newark, and excellent train service, Cranford was a natural choice in which to locate the first of these developments. It didn't hurt that Sears had Modern Homes sales staff and a lumber mill in Newark.

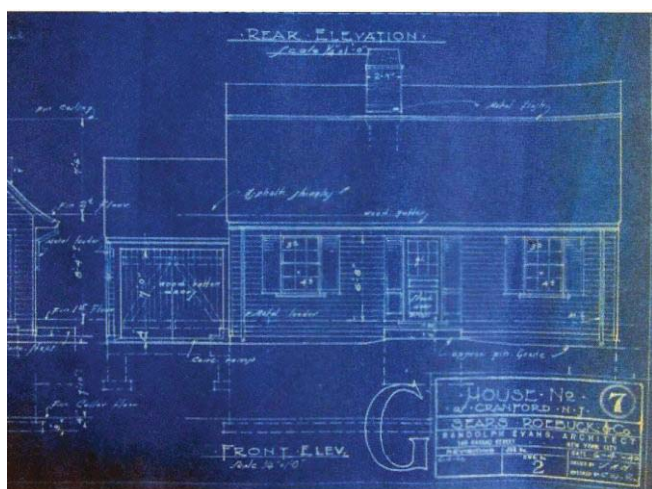
Sunny Acres - Phase I

Sunny Acres (which at its inception didn't have a formal name yet) was built over a period of slightly more than two years in three rounds of construction, all accomplished by Phillip J. Bowers & Company of Newark. The first round of building consisted of 63 homes erected roughly between July, 1940 and July, 1941. The new homes all followed a similar standardized Cape Cod design, but by placing the main body of the house lengthwise or endwise to the street, moving the placement of the garage to either side of the house, set forward or back, and adding a breezeway to detached garages, 12 numbered designs of the basic house were offered. The first floor alone was finished in the basic offering, priced at \$4,335 providing four rooms. The second floor could be finished to produce a 6-room house for a total cost of \$5,250.





Near the end of the first building period, Sears held a contest to name its new development, awarding \$25 to the winner. A committee comprised of Cranford's Mayor Osterheldt, Chairman of the Board of Education Plummer, and *Citizen and Chronicle* editor Ray made the selection. The winner was Mrs. Helen Cederholm of 4 Mohican Place. She later admitted that her submission of the name "Sunny Acres" was in part an oblique complaint that hardly a tree stood in the new development.



All 63 homes were completed according to these plans (nine of which we have records for), and potential purchasers bought them by responding to a newspaper ad which required answering a series of questions on a qualifying application. Descriptions of the houses and floor plans could be viewed at a Sears office at 15 North Avenue West, but no model home was yet available for inspection. The first round of houses occupied the northwest side of Raritan Road between Mohawk Drive and just west of Cherokee Road, Mohican Place, Mohawk Drive to Cherokee Road, Cherokee Road itself and the tiny Iroquois Place. No. 5 Cherokee Road was the first of these houses to be occupied. All property transfers were from Sears' Home Club Plan to the purchaser.



The new name was announced at a meeting held at the Cranford Casino. At that meeting, the newly formed Sunny Acres Civic and Improvement Association voted to petition the Union County Parks Commission for sidewalks to be built on that portion of Raritan Road owned by the Commission. The Association would become increasingly active and vocal as the development progressed.

Sunny Acres - Phase II

The second round of building most likely commenced before all the homes in the first phase had been completed and resulted in 52 additional houses. Again, all were the standardized Cape Cod designs, however, by now; a model home was available for inspection. The new homes were to be found on Oneida Place, Algonquin Drive and much

of Iroquois Road. Property transfer records, now between Sears Port Newark Lumber and Materials Company and the new owners, ran from July, 1941 through May, 1942.

As the second phase of constructing Sunny Acres progressed, relations between the Sunny Acres Civic and Improvement Association and Sears became decidedly less "sunny". The October 2, 1941 *Citizen and Chronicle* reported on an association meeting attended by approximately 50 residents. Sunny Acres residents complained about building delays caused by a trucking strike, and listed un-remediated complaints made against Sears, its builder, and subcontractors. A letter from Sears to the Association the following year indicates that wet basements had also become a source of contention. In the letter Sears acknowledged the problem and secured the services of a contractor to remedy the problems (apparently at its own expense).

Throughout its history, the Association (which would file with the State of New Jersey as a not-for-profit association March 1, 1950) was a voice for the needs of the development and a source of social action and community activity. The Association pressed for sidewalks, street lights, street signs, extension of bus service to cover the development, and a footbridge across the Rahway River so that children could get safely to school. Social activities included, but were not limited to, fielding children's and adult sports teams, dances and social outings, and perhaps most notably, the annual Baby Parade.



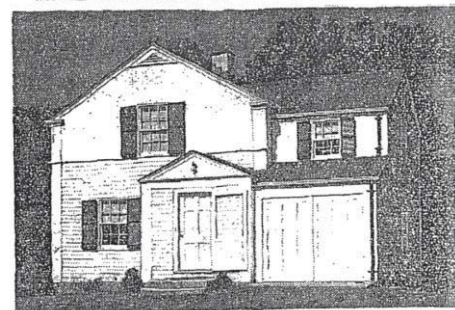
Initially charging members 25 cents a month (collected at their homes), the Association remains an active force in town to this day.

Sunny Acres - Phase III

The last building phase resulted in 57 additional homes with the final transfers (again between Sears Port Newark Lumber and Materials Company and the new owners) occurring in May, 1943. More than half of the houses were on Oneida Place, with the balance on Mohawk Drive west of Oneida Place and on parts of Iroquois Road. Responding to requests for more variety in house designs, Sears added a Colonial design, again moving the garage to provide several variations. Most of these can be found on the western portion of Mohawk Drive.

HOME PURCHASED IN NEW JERSEY COLONY
New York Times (1923 Current file); Oct 27, 1941;
ProQuest Historical Newspapers New York Times (1851-2006) w/ Index (1851-1993)
pg. 32

HOME PURCHASED IN NEW JERSEY COLONY



Edward J. Kohler bought this residence from the homes division of Sears, Roebuck & Co. Erected in the company's development at Cranford from plans by Randolph Evans, it will be duplicated in Green Acres, new Sears, Roebuck colony on Greenbrook Road, North Plainfield.

Final Thoughts

With the completion of phase III, Sunny Acres reached a total of 172 homes (171 built by Sears and one built by a private contractor when Sears deemed the lot too steep to build on). Why this house count fell short of the 200+ Sears originally announced it planned to build could not be determined from available records. It's quite possible that, by 1943, the required manpower and materials had been diverted to the War effort, but that is conjecture, rather than verifiable fact.

What is undeniable is that Sunny Acres holds a unique place in Sears history (the first housing development it built for itself) and an important place in Cranford's history and present. The Sunny Acres Civic and Improvement Association has played, and continues to play, a vital role in the family and social life of Cranford.

(The author wishes to acknowledge a profound debt of gratitude to Carole Esposito, whose knowledge and records of Sunny Acres and its Civic and Improvement Association were invaluable in the preparation of this article.)

Sources:

1. www.bizpedia.com/nj/SUNNY-ACRES-CIVIC-AND-IMPROVEMENT-ASSOCIATION.html.
2. *Citizen and Chronicle*, various.
3. Esposito, Carole, "From Farmland to Active Community", unpublished manuscript on Sunny Acres development, 2015.
4. Lyons, Chuck, "Assembly Required", *The History Channel Magazine*, September/October, 2013.
5. *Newark Evening News*, September 14, 1940.
6. www.searsarchive.com/homes/history.htm.
7. Solonickne, Lara, www.sears-homes.com/2012/yes-virginia-sears-homes-were-built.html.
8. Stevenson, Katherine Cole & Jandl, H. Ward, Houses by Mail: A Guide to Houses from

Sears, Roebuck and Company, National Trust for Historic Preservation, John Wiley & Sons, Inc., New York, NY, 1986.

9. Strazdon, Maureen, "Know Your Neighbor(hood)", presentation at Cranford Public Library, April 14, 2014.
10. "Suburban Buying Rising in New Jersey", *New York Times*, July 7, 1940.



Martin Luther King Program

**Sunday, January 10, 2016
2:00-4:00pm**

Using primary sources, Christine Glazer will take a look back at the summer of 1963 and the state of race relations in Cranford at the time of Martin Luther King's "I Have a Dream Speech." Several local citizens and prominent local clergymen were involved in this historic event. Join us at the Crane-Phillips House Museum on Sunday, January 10, 2016 from 2:00-4:00pm. Program begins at 2:15pm. Reservations are required, so please call 908-276-0082 or email us at cranfordhistoricalsociety@verizon.net.

Table 9: SAHD Property Inventory
(Sorted by street address)

Property Name	Address	SAHD Status	Block	Lot	ID Number
1 ALGONQUIN DR	1 ALGONQUIN DR	Contributing	622	17	-265490737
2 ALGONQUIN DR	2 ALGONQUIN DR	Non-contributing	619	12	-2088983652
3 ALGONQUIN DR	3 ALGONQUIN DR	Contributing	622	16	-57736910
4 ALGONQUIN DR	4 ALGONQUIN DR	Contributing	619	13	195391845
6 ALGONQUIN DR	6 ALGONQUIN DR	Non-contributing	619	14	-187146740
7 ALGONQUIN DR	7 ALGONQUIN DR	Contributing	622	14	1945676017
8 ALGONQUIN DR	8 ALGONQUIN DR	Contributing	619	15	-2076696033
9 ALGONQUIN DR	9 ALGONQUIN DR	Contributing	622	13	1601009633
10 ALGONQUIN DR	10 ALGONQUIN DR	Contributing	619	16	-1037023176
11 ALGONQUIN DR	11 ALGONQUIN DR	Contributing	622	12	1008960759
12 ALGONQUIN DR	12 ALGONQUIN DR	Contributing	619	17	2036269529
13 ALGONQUIN DR	13 ALGONQUIN DR	Contributing	622	11	-175131165
14 ALGONQUIN DR	14 ALGONQUIN DR	Non-contributing	619	18	1668621098
15 ALGONQUIN DR	15 ALGONQUIN DR	Non-contributing	622	10	-1570035056
16 ALGONQUIN DR	16 ALGONQUIN DR	Contributing	619	19	833546851
17 ALGONQUIN DR	17 ALGONQUIN DR	Contributing	622	9	-271093725
18 ALGONQUIN DR	18 ALGONQUIN DR	Non-contributing	619	20	-1249423331
19 ALGONQUIN DR	19 ALGONQUIN DR	Contributing	622	8	1935640774
20 ALGONQUIN DR	20 ALGONQUIN DR	Contributing	619	1	-1855132952
21 ALGONQUIN DR	21 ALGONQUIN DR	Contributing	622	7	1666300508
23 ALGONQUIN DR	23 ALGONQUIN DR	Non-contributing	587	23	-394210803
25 ALGONQUIN DR	25 ALGONQUIN DR	Non-contributing	587	22	-479914409
27 ALGONQUIN DR	27 ALGONQUIN DR	Non-contributing	587	21	2074772033
29 ALGONQUIN DR	29 ALGONQUIN DR	Contributing	587	20	-701491194
31 ALGONQUIN DR	31 ALGONQUIN DR	Contributing	587	19	535702226
33 ALGONQUIN DR	33 ALGONQUIN DR	Contributing	587	18	783535509
35 ALGONQUIN DR	35 ALGONQUIN DR	Non-contributing	587	17	-1529923527
37 ALGONQUIN DR	37 ALGONQUIN DR	Contributing	587	16	-308865204
5 Algonquin St	5 Algonquin ST	Contributing	622	15	1265865439
1 CHEROKEE RD	1 CHEROKEE RD	Contributing	620	9	2070409213
2 CHEROKEE RD	2 CHEROKEE RD	Contributing	616	16	-1085423600
3 CHEROKEE RD	3 CHEROKEE RD	Contributing	620	8	-1468379550
4 CHEROKEE RD	4 CHEROKEE RD	Contributing	616	17	396786516
5 CHEROKEE RD	5 CHEROKEE RD	Non-contributing	620	7	-1738820095
7 CHEROKEE RD	7 CHEROKEE RD	Non-contributing	618	11	-1975507669
8 CHEROKEE RD	8 CHEROKEE RD	Contributing	617	3	393045025
9 CHEROKEE RD	9 CHEROKEE RD	Contributing	618	10	500032184
10 CHEROKEE RD	10 CHEROKEE RD	Contributing	617	1	381748338
11 CHEROKEE RD	11 CHEROKEE RD	Non-contributing	618	9	1851171500

Table 9: SAHD Property Inventory

Property Name	Address	SAHD Status	Block	Lot	ID Number
13 CHEROKEE RD	13 CHEROKEE RD	Contributing	618	8	1409018971
15 CHEROKEE RD	15 CHEROKEE RD	Contributing	618	7	-1311362325
17 CHEROKEE RD	17 CHEROKEE RD	Contributing	618	6	128293635
19 CHEROKEE RD	19 CHEROKEE RD	Contributing	618	5	1871596343
20 CHEROKEE RD	20 CHEROKEE RD	Non-contributing	616	25	-1787594331
21 CHEROKEE RD	21 CHEROKEE RD	Contributing	618	4	-1989132338
22 CHEROKEE RD	22 CHEROKEE RD	Contributing	616	26	1469969787
23 CHEROKEE RD	23 CHEROKEE RD	Contributing	618	3	-1650681270
25 CHEROKEE RD	25 CHEROKEE RD	Non-contributing	618	2	1664303589
2 IROQUOIS PL	2 IROQUOIS PL	Non-contributing	616	18	792273617
4 IROQUOIS PL	4 IROQUOIS PL	Contributing	616	19	-1379112403
6 IROQUOIS PL	6 IROQUOIS PL	Contributing	616	20	1365125160
8 IROQUOIS PL	8 IROQUOIS PL	Non-contributing	616	21	297678942
10 IROQUOIS PL	10 IROQUOIS PL	Contributing	616	22	982806084
12 IROQUOIS PL	12 IROQUOIS PL	Contributing	616	23	1260782409
14 IROQUOIS PL	14 IROQUOIS PL	Contributing	616	24	-637403041
15 IROQUOIS PL	15 IROQUOIS PL	Contributing	617	2	-235565636
1 IROQUOIS RD	1 IROQUOIS RD	Contributing	620	6	-1272333324
3 IROQUOIS RD	3 IROQUOIS RD	Contributing	620	5	-1687197004
5 IROQUOIS RD	5 IROQUOIS RD	Contributing	620	4	-240194922
7 IROQUOIS RD	7 IROQUOIS RD	Contributing	620	3	877230266
8 IROQUOIS RD	8 IROQUOIS RD	Contributing	619	10	206285427
9 IROQUOIS RD	9 IROQUOIS RD	Contributing	620	2	-1573705536
10 IROQUOIS RD	10 IROQUOIS RD	Contributing	619	11	-724669610
11 IROQUOIS RD	11 IROQUOIS RD	Contributing	620	1	405149008
13 IROQUOIS RD	13 IROQUOIS RD	Non-contributing	621	5	469392431
14 IROQUOIS RD	14 IROQUOIS RD	Contributing	622	18	26404030
15 IROQUOIS RD	15 IROQUOIS RD	Contributing	621	4	2139068735
16 IROQUOIS RD	16 IROQUOIS RD	Contributing	622	19	-1296686645
17 IROQUOIS RD	17 IROQUOIS RD	Contributing	621	3	-871094936
18 IROQUOIS RD	18 IROQUOIS RD	Contributing	622	20	-219747731
1 MOHAWK DR	1 MOHAWK DR	Contributing	626	6	1525495107
2 MOHAWK DR	2 MOHAWK DR	Contributing	627	8	-1458316463
3 MOHAWK DR	3 MOHAWK DR	Contributing	626	5	1524843914
4 MOHAWK DR	4 MOHAWK DR	Contributing	627	7	1065983247
5 MOHAWK DR	5 MOHAWK DR	Contributing	626	4	35025485
6 MOHAWK DR	6 MOHAWK DR	Contributing	627	6	970254695
7 MOHAWK DR	7 MOHAWK DR	Contributing	626	3	1855373043
8 MOHAWK DR	8 MOHAWK DR	Contributing	627	5	-927398130
9 MOHAWK DR	9 MOHAWK DR	Non-contributing	626	2	-324586502
10 MOHAWK DR	10 MOHAWK DR	Contributing	627	4	-1453175296

Table 9: SAHD Property Inventory

Property Name	Address	SAHD Status	Block	Lot	ID Number
12 MOHAWK DR	12 MOHAWK DR	Contributing	627	3	1759765550
14 MOHAWK DR	14 MOHAWK DR	Contributing	627	2	-2145709983
15 MOHAWK DR	15 MOHAWK DR	Contributing	616	6	-334420539
16 MOHAWK DR	16 MOHAWK DR	Contributing	627	1	1583616125
17 MOHAWK DR	17 MOHAWK DR	Contributing	616	5	319831038
19 MOHAWK DR	19 MOHAWK DR	Non-contributing	616	4	-1700713324
21 MOHAWK DR	21 MOHAWK DR	Contributing	616	3	1301358037
23 MOHAWK DR	23 MOHAWK DR	Contributing	616	2	-1188713957
25 MOHAWK DR	25 MOHAWK DR	Contributing	616	1	1499318645
101 MOHAWK DR	101 MOHAWK DR	Non-contributing	587	14	-113292434
102 MOHAWK DR	102 MOHAWK DR	Unknown	586	17	2134111332
103 MOHAWK DR	103 MOHAWK DR	Contributing	587	13	-1432598234
105 MOHAWK DR	105 MOHAWK DR	Contributing	587	12	1008470824
106 MOHAWK DR	106 MOHAWK DR	Contributing	586	16	-990648823
107 MOHAWK DR	107 MOHAWK DR	Contributing	587	11	-724859498
108 MOHAWK DR	108 MOHAWK DR	Non-contributing	586	15	961232735
109 MOHAWK DR	109 MOHAWK DR	Contributing	587	10	-841507169
110 MOHAWK DR	110 MOHAWK DR	Contributing	586	14	-370573900
111 MOHAWK DR	111 MOHAWK DR	Contributing	587	9	-839200603
112 MOHAWK DR	112 MOHAWK DR	Non-contributing	586	13	-1141632557
113 MOHAWK DR	113 MOHAWK DR	Contributing	587	8	1019931394
114 MOHAWK DR	114 MOHAWK DR	Contributing	586	12	1071731891
115 MOHAWK DR	115 MOHAWK DR	Contributing	587	7	894376795
116 MOHAWK DR	116 MOHAWK DR	Contributing	586	11	-438469030
117 MOHAWK DR	117 MOHAWK DR	Contributing	587	6	759316917
118 MOHAWK DR	118 MOHAWK DR	Contributing	586	10	1932671118
119 MOHAWK DR	119 MOHAWK DR	Contributing	587	5	-1308111377
120 MOHAWK DR	120 MOHAWK DR	Contributing	586	9	-646094755
121 MOHAWK DR	121 MOHAWK DR	Contributing	587	4	1858304102
122 MOHAWK DR	122 MOHAWK DR	Contributing	586	8	1471034176
123 MOHAWK DR	123 MOHAWK DR	Contributing	587	3	126727705
124 MOHAWK DR	124 MOHAWK DR	Contributing	586	7	1887118176
125 MOHAWK DR	125 MOHAWK DR	Contributing	587	2	-408243176
126 MOHAWK DR	126 MOHAWK DR	Non-contributing	586	6	664862473
127 MOHAWK DR	127 MOHAWK DR	Contributing	587	1	-1703627400
128 MOHAWK DR	128 MOHAWK DR	Contributing	586	5	-728731625
129 MOHAWK DR	129 MOHAWK DR	Contributing	622	6	-2115884586
130 MOHAWK DR	130 MOHAWK DR	Contributing	586	4	-1054199242
131 MOHAWK DR	131 MOHAWK DR	Non-contributing	622	5	-319844846
132 MOHAWK DR	132 MOHAWK DR	Non-contributing	586	3	2027044229
133 MOHAWK DR	133 MOHAWK DR	Contributing	622	4	970432273

Table 9: SAHD Property Inventory

Property Name	Address	SAHD Status	Block	Lot	ID Number
134 MOHAWK DR	134 MOHAWK DR	Contributing	586	2	588902043
136 MOHAWK DR	136 MOHAWK DR	Contributing	586	1	1641843220
138 MOHAWK DR	138 MOHAWK DR	Non-contributing	582	41	-1634527254
140 MOHAWK DR	140 MOHAWK DR	Contributing	582	40	2034286503
142 MOHAWK DR	142 MOHAWK DR	Contributing	582	39	830932976
1 MOHICAN PL	1 MOHICAN PL	Contributing	616	10	1029819909
2 MOHICAN PL	2 MOHICAN PL	Contributing	626	11	-202313826
3 MOHICAN PL	3 MOHICAN PL	Contributing	616	9	1326278566
4 MOHICAN PL	4 MOHICAN PL	Non-contributing	626	1	-1723745300
5 MOHICAN PL	5 MOHICAN PL	Non-contributing	616	8	1060282786
7 MOHICAN PL	7 MOHICAN PL	Non-contributing	616	7	-860125294
2 ONEIDA PL	2 ONEIDA PL	Contributing	618	12	1289890393
3 ONEIDA PL	3 ONEIDA PL	Contributing	619	9	763385319
4 ONEIDA PL	4 ONEIDA PL	Non-contributing	618	13	645155180
5 ONEIDA PL	5 ONEIDA PL	Non-contributing	619	8	-1657685303
6 ONEIDA PL	6 ONEIDA PL	Contributing	618	14	-1325194470
7 ONEIDA PL	7 ONEIDA PL	Contributing	619	7	-1899474114
8 ONEIDA PL	8 ONEIDA PL	Contributing	618	15	-1440607545
9 ONEIDA PL	9 ONEIDA PL	Contributing	619	6	1768091104
10 ONEIDA PL	10 ONEIDA PL	Non-contributing	618	16	1596189956
11 ONEIDA PL	11 ONEIDA PL	Contributing	619	5	1031164738
12 ONEIDA PL	12 ONEIDA PL	Contributing	618	17	-963355513
13 ONEIDA PL	13 ONEIDA PL	Non-contributing	619	4	1678267126
14 ONEIDA PL	14 ONEIDA PL	Contributing	618	18	352577240
15 ONEIDA PL	15 ONEIDA PL	Contributing	619	3	-1345674007
16 ONEIDA PL	16 ONEIDA PL	Contributing	618	19	1930939562
17 ONEIDA PL	17 ONEIDA PL	Contributing	619	2	-1323981831
18 ONEIDA PL	18 ONEIDA PL	Contributing	618	20	-779504414
20 ONEIDA PL	20 ONEIDA PL	Non-contributing	618	21	-1973346243
21 ONEIDA PL	21 ONEIDA PL	Non-contributing	587	15	1924202415
22 ONEIDA PL	22 ONEIDA PL	Contributing	618	22	320926088
24 ONEIDA PL	24 ONEIDA PL	Contributing	618	1	-600320069
1000 RARITAN RD	1000 RARITAN RD	Contributing	627	9	-1818487044
1006 RARITAN RD	1006 RARITAN RD	Contributing	626	7	-649733772
1008 RARITAN RD	1008 RARITAN RD	Contributing	626	8	1373504817
1010 RARITAN RD	1010 RARITAN RD	Contributing	626	9	-2107846281
1012 RARITAN RD	1012 RARITAN RD	Contributing	626	10	-275502140
1014 RARITAN RD	1014 RARITAN RD	Contributing	616	11	-1631794903
1016 RARITAN RD	1016 RARITAN RD	Non-contributing	616	12	-865324021
1018 RARITAN RD	1018 RARITAN RD	Contributing	616	13	-1995396960
1020 RARITAN RD	1020 RARITAN RD	Contributing	616	14	54788267

Table 9: SAHD Property Inventory

Property Name	Address	SAHD Status	Block	Lot	ID Number
1022 RARITAN RD	1022 RARITAN RD	Non-contributing	616	15	-937405776
1026 RARITAN RD	1026 RARITAN RD	Contributing	620	10	-1368665285
1028 RARITAN RD	1028 RARITAN RD	Contributing	620	11	118232484
1030 RARITAN RD	1030 RARITAN RD	Contributing	620	12	1531743944
1032 RARITAN RD	1032 RARITAN RD	Contributing	620	13	-187260926
1034 RARITAN RD	1034 RARITAN RD	Contributing	620	14	-586721409
1036 RARITAN RD	1036 RARITAN RD	Contributing	620	15	-517833350
1038 RARITAN RD	1038 RARITAN RD	Contributing	621	6	182423836
1040 RARITAN RD	1040 RARITAN RD	Contributing	621	7	-1281980525
1042 RARITAN RD	1042 RARITAN RD	Contributing	621	8	-2136233491
1044 RARITAN RD	1044 RARITAN RD	Contributing	621	9	-467109387

Table 9: SAHD Property Inventory

(Page intentionally left blank)



State of New Jersey
DEPARTMENT OF TRANSPORTATION

THOMAS M. DOWNS
COMMISSIONER

1035 PARKWAY AVENUE
CN 600
TRENTON, NEW JERSEY 08625

Multiple Bridge
Projects

DNJH 691-153

IN REPLY PLEASE REFER TO

June 6, 1991

Ms. Nancy L. Zerbe
Deputy State Historic Preservation Officer
Office of New Jersey Heritage
NJ Department of Environmental Protection
CN 404
Trenton, New Jersey 08625

RECEIVED
JUN 13 1991
OFFICE OF THE HISTORIC PRESERVATION

Dear Ms. Zerbe:

In accordance with 36 CFR 800: "Protection of Historic Properties" as published in the Federal Register 2 September 1986 (52, 169, 31115-31125), staff of the State Historic Preservation Office met with New Jersey Department of Transportation staff on May 7, 1991 to determine if State or National Register eligible or listed resources persist and will be affected by eleven new bridge projects. In addition, five projects which had previously been the subject of consultation were again discussed. All consultation was based on the submission and review of summary documents and original photographs, and a subsequent discussion on the architecture of each project and on the archeology. Details of the results of those discussions are documented in an enclosed memorandum of record and summarized in the attached table. These consultation comments list only those projects for which a determination of the National Register eligibility (or lack thereof) has been made for the bridge in question, or the conclusion that no National Register eligible resources persist within the project has been reached. I am therefore seeking your concurrence with the following consultation comments.

800.4 Identifying Historic Properties

As the result of the review of documentation submitted by the New Jersey Department of Transportation on behalf of the Federal Highway Administration, no properties listed in or eligible for listing in the National Register of Historic Places exist within the areas of impact of the following projects:

1. Route 31 (8L)
Clinton, Hunterdon County
Project No.: F0370109

Ms. Nancy L. Zerbe

Page 2

June 6, 1991

2. Route 47 (6A, 7A, 8E)
Dennis Twp., Cape May County/Eldora, Cumberland County
Project No.: BR-NBIS309
3. Route 24 over the Musconetcong
Hackettstown, Warren County/Washington, Morris County
Project No.: RS-352(101)

Consultation resulted in the finding that the following bridge is eligible for listing in the National Register of Historic Places and questions pertinent to the archeological investigations remain:

1. Routes 1, 1&9 (5A) over Rt. 35
Woodbridge, Middlesex County
Project No.: BR-NBIS282

Consultation resulted in the finding that the following bridge is a contributing element of the Glen Ridge Historic District, which is listed in the National Register of Historic Places:

- * 1. Baldwin Street-Highland Avenue
Glen Ridge, Essex County
Project No.: BRZ-0708(102)
- (New Jersey Register En-
croachment review also
required.)

Consultation resulted in the finding that the following bridges are not eligible for listing in the National Register of Historic Places, but questions pertinent to other, proximate resources or the archeological investigations remain to be answered prior to reaching a finding for the project:

- * 1. Route 1, 1&9 (5B) over Conrail
Edison, Middlesex County
Project No.: BR-NBIS282
 2. Route 1, 1&9 (5B) over Reading RR
Woodbridge, Middlesex County
Project No.: BR-NBIS309
 3. Route 46 (4H)
Independence, Warren County
Project No.: BRF50(107)
- } To be reviewed as
part of the larger
road project.

Ms. Nancy L. Zerbe

Page 3

June 6, 1991

Consultation resulted in the finding that although questions pertinent to the National Register eligibility of the bridges in question exist, no significant and eligible archeological resources persist within the areas of impact of the following projects:

1. Pearl Street
Burlington City, Burlington County
Project No.: BHM-4397(101)PE
2. Route 50 (2D)
Upper Township, Cape May County
Project No.: BR-NBIS394

Consultation resulted in the finding that questions pertinent to the National Register eligibility of the bridges and pertinent to the archeology exist for the following bridge:

1. Route 1, 1&9 (5C) over Rahway River
Rahway, Union County
Project No.: BR-NBIS161

800.5 Assessing Effects

Consultation resulted in the finding that although the bridge is not eligible, the railroad corridor which the bridge spans is. The proposed bridge and track elimination project will have an adverse effect. However, there will be no adverse effect if either one of the below mentioned proposals is adopted for the following project:

1. Warren Glen - Bloomsbury Road Bridge
Pohatcong, Warren County
Project No.: RSG-366(103)

Proposals:

1. The bridge be replaced on existing alignment and the tracks remain
 2. The bridge be replaced using the existing railroad grade with the tracks remaining on either side of the road
-

Ms. Nancy L. Zerbe

Page 4

June 6, 1991

Consultation resulted in the realization that a portion of the following project lies within the Mullica River-Chestnut Neck Historic District; therefore coordination in accordance with the NJ Register of Historic Places Act must be initiated:

1. Pleasant Mills Road
Washington Twp. etc., Burlington County
Project No.: BRRS-197(104)PE

Additional Comments

It is intended that concurrence with these comments will complete the consultation process for the three projects listed in the preceding where no significant resources were identified within the area of potential impact. Consultation on ways to minimize the effects of the proposed construction and appropriate mitigation measures as needed will be initiated for the project containing the National Register eligible bridge. Additional consultation to determine National Register eligibility and assess effects which may result from the proposed construction will be initiated for all other projects for which these issues remain unresolved as the requested information becomes available.

As always, the time your staff has spent on the historic bridge effort is greatly appreciated. Receipt of your concurrence with these comments as soon as possible and in no more than 30 days would be greatly appreciated.

Very Truly Yours,



Andras Fekete
Manager

Bureau of Environmental Analysis

Attachments

 I concur

 ✓ I concur with the exceptions noted (see asterisks,*)

Nancy L. Zerbe
Nancy L. Zerbe
Deputy State Historic Preservation
Officer

7-19-91
Date

Note: Minor comments on Memo to Record to follow.

LR-M

cc: L. Jacobs, FHWA w/attachments

F. Hajtovik, BLA
R. Goslin
S. Barton
T. Cutrona

M. Funari, BSM
S. Park
H. Capers

J. McQuillan, BEA
R. Crum
M. Crum
J. Mole
T. Carbone
C. Ashton
J. Fittipaldi
S. Szczepkowski

Page 2

The HPO fully recognizes that the identification, evaluation and management of historic transportation districts (corridors) is a significant concern of federal agencies such as FHWA and state DOTs. In addressing this concern, we have sought to evaluate this and other transportation corridors cautiously, consistently, objectively, and with full consideration and understanding of existing National Register listings, as well as the evaluation activities of other state DOTs and SHPOs. To this end the HPO has consulted with other states and, at times, National Register staff such as Beth Savage. In discussions with Pennsylvania, for example, we have learned that the Pennsylvania SHPO, in consultation with the Pennsylvania DOT and FHWA, has found ten (10) railroad lines eligible for the National Register. Most significant is agreement between the Pennsylvania DOT and SHPO on the National Register eligibility of the Pennsylvania Railroad Main Line (Philadelphia-Pittsburgh). This cooperative effort to delineate the Commonwealth's namesake railroad as a linear National Register eligible historic district, while permitting rehabilitation of the line for increased rail freight capacity and service, is an example that we hope we can ultimately emulate in New Jersey.

We recognize the need for more thorough and detailed exploration of specific corridors to fully evaluate and delineate the contributing and non-contributing features of these complex historic resources. However, this need does not preclude the current evaluation of eligibility of the resource as a whole.

In conclusion, the HPO after much research, evaluation, and consultation with knowledgeable historians, has concluded that the CRRNJ possesses the significance and integrity to make the Elizabeth to Phillipsburg Main Line of the CRRNJ, the State's namesake railroad, deserving of recognition and a Determination of Eligibility. We trust that you will agree that our delineation of the CRRNJ Main Line as a National Register of Historic Places eligible historic district is consistent with and supported by the historical record and the existing listings of comparable resources in the National Register of Historic Places.

If you have any questions regarding this letter, please contact either Charles Scott or Andrea Tingey of my staff at (609-292-2023). Thank you.

Sincerely,



Dorothy P. Guzzo
Deputy State Historic
Preservation Officer



State of New Jersey

Department of Environmental Protection
DIVISION OF PARKS AND FORESTRY
HISTORIC PRESERVATION OFFICE

CN-404

TRENTON, N.J. 08625-0404

TEL: (609) 292-2023

FAX: (609) 984-0578

August 2, 1995

HPO-G95-139

Ms. Carol Shull, Chief
National Register of Historic Places
800 North Capitol Street, NW
Suite 250
Washington, DC 20002
ATTN: Mr. Patrick Andrus

RE: Determination of Eligibility
Central Railroad of New Jersey Main Line Historic District
(Phillipsburg to Elizabeth)
Federal Highway Administration
New Jersey Department of Transportation

Dear Ms. Shull:

As Deputy State Historic Preservation Officer (DSHPO) for New Jersey, I am pleased to have this opportunity to provide comments supporting the National Register eligibility of the CRRNJ Main Line Historic District in response to those already prepared by the Federal Highway Administration (FHWA) and the New Jersey Department of Transportation (NJDOT). I have enclosed my detailed comments as Attachment A.

The HPO has been working successfully with New Jersey Transit, the property owner, on projects along the Central Railroad of New Jersey (CRRNJ) Main Line with their acceptance of the SHPO Opinion of National Register eligibility. We have completed one design project which was initiated through Section 106 as a result of the corridor identification. This productive consultation has resulted in an executed MOA (Appendix A).

We are currently reviewing with New Jersey Transit, the owner of the majority of the CRRNJ Main Line and numerous other publicly owned New Jersey railroad lines, the concept of National Register eligible railroad historic districts (corridors). In the past we have actively sought the participation of the NJDOT and FHWA in efforts to identify and evaluate both rail and highway corridors. We deeply regret that despite our efforts for cooperative evaluation, the NJDOT and FHWA have requested a Determination of Eligibility for a New Jersey Transit historic property.

- c David Koenig, NJTransit (with attachments)
 Andras Fekete, NJDOT (with attachments)
 Lance Metz, Canal Museum, Hugh Moore Park (with Attachment A only)
 Thomas Flagg (with Attachment A only)
 Ed Rutsch (with Attachment A only)
 Howard Green, NJ Historical Commission (with Attachment A only)
 Ben Shiriak, Friends of Sleepy Hollow (with Attachment A only)
 Denise Nichol, Plainfield Dept. of Community Dev., (with Attachment A only)
 Frank Reilly, CRRNJ Historical Society (with Attachment A only)
 Walter Grosselfinger, United Railway Historical Society (with Attachment A only)
 Mitch Dakelman (with Attachment A only)
 Pete Schaver, NYSHPO (with Attachment A only)
 Bruce Bomberger, PASHPO (with Attachment A only)
 Susan Shearer, Preservation Pennsylvania (with Attachment A only)
 Phillipsburg Riverview Organization (with Attachment A only)
 William Jayne (with Attachment A only)

DPG:CS:AT

C:\wpwin\crrnj

ATTACHMENT A

JUSTIFICATION OF THE LINEAR HISTORIC DISTRICT CONCEPT

The HPO has evaluated the CRRNJ Mail Line in accordance with the National Register of Historic Places Criteria for Evaluation, and in conformance with the past cultural resource review practice established by FHWA and NJDOT. An appropriate standard for the review of historically significant transportation properties was established by FHWA and NJDOT in evaluating the Camden and Amboy Railroad in 1983:

Ideally, to properly assess the historic and archaeological value of the physical resources of the Camden and Amboy Railroad, the route in its entirety and all associated structures needs to be considered. It is difficult therefore in studying only a small part of the whole, to do justice to an historically significant linear feature that extends across a considerable portion of the state and which deserves to be evaluated in the widest possible context of landscape history. (Cultural Resource Survey for U.S. Route 130/N.J. Route 33, FHWA and NJDOT, March 1983, page 104, included as Appendix B).

The HPO feels strongly that this framework for evaluating the Camden and Amboy Railroad, established over twelve years ago, is equally appropriate to evaluating the CRRNJ Main Line.

In order to firmly ground the discussion of the eligibility of this resource, the legitimacy of the concept of historic districts based on linear transportation features must be established. Several of this type of resource have already been recognized in New Jersey:

Delaware & Raritan Canal (SR 11/30/72, NR 5/11/73);
 Morris Canal (SR 11/26/73, NR 10/1/74);
 Old Mine Road (SR 10/2/75, NR 12/3/80);
 Camden & Amboy Railroad (SHPO Opinion 6/26/75);
 Delaware, Lackawanna & Western Cutoff (SHPO Opinion 3/22/94);
 Palisades Park (which includes the Palisades Parkway, NR 10/15/66, NHL);
 Appalachian Trail (DOE 10/2/78);
 Dundee Canal (DOE 11/7/80);
 Pulaski Skyway (SHPO Opinion 8/4/83);
 and Raritan & Delaware Bay Railroad (SHPO Opinion 8/18/94).

Other states have similarly recognized the validity of this resource type and listed examples in the National Register of Historic Places:

Cumbres and Toltec Scenic Railroad, Colorado;
 Branford Electric Railway Historic District, Connecticut;
 Merit Parkway, Connecticut;

Attachment A, Page 2

New Castle and Frenchtown Railroad Right-of-Way,

Delaware;

Wilmington & Western Railroad, Delaware;

Saint Charles Streetcar Line, Louisiana;

Baltimore-Washington Parkway, Maryland;

Tremont Street Subway, Massachusetts;

Butte Anaconda and Pacific Railway Historic District,

Montana;

Bronx River Parkway, New York;

Hocking Valley Railroad, Ohio;

Mt. Hood Railroad Linear Historic District, Oregon;

Horseshoe Curve, Pennsylvania;

East Broad Top Railroad, Pennsylvania;

Pennsylvania Railroad District, Pennsylvania;

Ashley River Historic District, South Carolina;

and Cass Scenic Railroad, West Virginia.

Indeed FHWA and NJDOT have accepted the Delaware, Lackawanna & Western Cutoff Historic District as eligible, please see attached Historic Bridge Survey forms (Appendix C). Furthermore, the National Park Service has dedicated an entire CRM issue to historic transportation corridors (volume 16, no. 11 1993 - please see Appendix D). Prominent American historians have explored the concept in print. The following excerpts from John R. Stilgoe's 1983 publication Metropolitan Corridor: Railroads and the American Scene demonstrate how a respected academic historian has viewed the tangible broad impacts of railroad corridors, and establish a foundation for the importance of the property type:

In the half-century following 1880, the railroad industry reshaped the American built environment and reoriented American thinking. The luxury passenger express booming over grade crossings and hurtling past small-town depots, the slow freight chugging through industrial zones, even the morning and evening commuter locals shuttling back and forth between suburban stations and subterranean urban terminals operated in a unique environment. Certainly the actual railroad right-of-way of roadbed and tracks, signals and depots, bridges and junctions no longer represented the tentative beginnings of a new transportation pathway. Instead it reflected the power of sophisticated engineering, of heavy industry unknown two generations before. Trains and right-of-way transformed adjacent built environments, modifying them in novel, sometimes startling ways. They nurtured factory complexes, electricity generating stations, and commuter suburbs while enfolding Main streets and other traditional places. "Railroad iron is a magician's rod," mused Emerson in the 1840s, "in its power to evoke the sleeping energies of land and water." By the turn of the century, Americans knew that railroad iron meant

Attachment A, Page 3

far more; train travel provided a distinctive, almost cinematic vision of the built environment beyond the plate glass windows. Trains, right-of-way, and adjacent built form had become part environment, part experience, a combination perhaps best called metropolitan.

Today the trains are fewer, the steam locomotives retired, and the once impeccably maintained right-of-way littered with rubbish and overgrown with weeds. But the trains and right-of-way that molded space, structure, and thinking in nearly forgotten ways remain somewhat active nonetheless. Almost but not quite ruins, they stir now with quickened energy. Understanding their half century of importance indeed facilitate understanding rural, suburban, and urban environments, for trains and right-of-way created a fourth distinctive environment-the metropolitan corridor. (Preface)

Metropolitan corridor designates the portion of the American built environment that evolved along railroad right-of-way in the years between 1880 and 1935. No traditional spatial term, not urban, suburban, or rural, not cityscape or landscape, adequately identifies the space that perplexed so many turn-of-the-century observers. Reaching from the very hearts of great cities across industrial zones, suburbs, small towns, and into mountain wilderness, the metropolitan corridor objectified in its unprecedented arrangement of space and structure a wholly new lifestyle. Along it flowed the forces of modernization, announcing the character of the twentieth century, and abutting it sprouted new clusters of building. Its peculiar juxtaposition of elements attracted the scrutiny of photographers and advertising illustrators; its romance invigled poets and novelists; its energy challenged architects, landscape architects, and urban designers. Always it resisted definition in traditional terminology. And suddenly, in the years of the Great Depression, in the ascendancy of the automobile, it vanished from the national attention. Yet the corridor remains, although now often screened by sumac and other jungle-like trees and avoided by highways, still snaking from one well-known, often-studied sort of space to another.

Where is the corridor? To find it one must drive far from the interstate highway cloverleaf, away from Main Street and Second Street; one drives downhill, for the corridor follows the gentle gradients of river valleys, or to the rundown part of town, for the corridor no longer enriches the structures snugged along it. In the city, one drives away from the tall glass-skinned office towers to the grimy factories

still watched by thrusting red-brick smoke stacks. In the suburb, one drives along the old parkway, along the streetcar route leading to the commuter station. In the small town, one drives toward the grain elevator, the coal trestle, the creek bed. In the country one drives toward the line of telegraph poles. Always one drives toward the railroad right-of-way, the energizing spine of the corridor. (Introduction)

This recognition of the tangible significance of railroads was expressed as early as 1852 when The Report of the Superintendent of the Census asserted:

Railroads in the United States - In no other particular can the prosperity of a country be more strikingly manifested than by the perfection of its roads and other means of internal communications. The system of railroads, canals, turnpike, post routes, river navigation and telegraphs possessed by the United States, presents an indication of its advancement in power and civilization more wonderful than any other feature of its progress among the nations of the world. . . . The usefulness and comparative economy of railroads as channels of commerce and travel have become so evident that they have in some measure superseded canals and are likely to detract seriously from the importance of navigable rivers for like purposes.

Recognizing the validity of the linear transportation corridor as historic district is vital to our increasing understanding of our built environment. It fosters the recognition that such features as bridges, stations, industrial buildings, tracks, and tunnels are historically and geographically linked together. To falsely isolate them, is to see a hand, a toe, a torso, rather than a human being.

RAILROADS IN NEW JERSEY

New Jersey's historical role as a corridor (between Philadelphia and New York, between the anthracite coal mines of Pennsylvania and the Port of New York, between the manufacturing centers of eastern New Jersey and western markets) is the major aspect of its transportation history. This has deeply penetrated our state's historical consciousness. One of our most popular history books is John Cunningham's New Jersey: America's Main Road (1966). New Jersey has always been a thoroughfare for canals, railroads, and highways. The cost of transportation and the routes were greatly determined by these outside forces. This context is one which defines the character of New Jersey history and the landscape it created.

The importance of railroads to New Jersey has also been recognized, both historically and contemporarily. Assessments of the importance of railroads to the State of New Jersey frequently appeared in the United States Census and Census of Manufactures. For example, the 1910 Census of Manufactures stated:

The industrial prominence of New Jersey is due largely to its exceptionally favorable geographic position and its splendid transportation facilities, since a large part of the materials used in the manufacturing industries of the state are produced beyond its borders and most of the manufactured products are shipped to outside markets. The proximity of the state to anthracite coal fields of Pennsylvania which supply a large portion of the fuel used in its manufacturing industries and to the markets of New York City and Philadelphia have been powerful factors of the industrial development. (p. 739)

In 1900, for example, of the ten (10) largest New Jersey cities listed in the United States Census, the CRRNJ served four (4) and three (3) of them were on the Jersey City to Phillipsburg Main Line. In 1928, the New Jersey State Chamber of Commerce published New Jersey: Life, Industries, and Resources of a Great State. The chapter on "Transportation and Commerce" began with the statement:

It is the railroads of New Jersey that have put the State in the forefront among the great industrial centers of the United States. (Page 175).

The extensive rail network in New Jersey is a testament to the importance of the state's location to regional and international commerce and passenger transportation (Heritage Studies Station Survey).

I agree with FHWA and NJDOT that there are many rail lines in New Jersey. In fact, during the "golden age" of railroading (1890-1915), New Jersey was the most densely tracked state in the country (John T. Cunningham, Railroading in New Jersey, p. 73). However, responsible application of the National Register Criteria of Eligibility has not resulted in finding all examples of this frequently occurring property type eligible. Indeed, several lines have been evaluated as ineligible by the NJ SHPO: Central Railroad of New Jersey Coastal Spur (10/27/93);

Amtrak Northeast Corridor Line, former Pennsylvania Railroad (9/12/94);

former Sussex Branch of the Delaware, Lackawanna and Western Railroad (4/18/95);

and Winchester and Western Railroad (4/30/95). This clearly demonstrates the responsible application of the National Register Criteria by the NJ SHPO.

The Central Railroad of New Jersey (CRRNJ) right-of-way between Jersey City and Phillipsburg, and contributing

adjacent, abutting and intersecting features, is a linear historic district (corridor) eligible for the State and National Registers of Historic Places under Criteria A and C.

CRITERION A SIGNIFICANCE

The eligibility of the CRRNJ under Criterion A has two components: industrial freight and passenger service. This duality is exemplified by the company's nickname - 'the BIG Little Railroad' - which meant that it was big enough to handle all the transportation needs of the great industrial, commercial, farming and mining areas it served, yet small enough to give personalized service to its passengers (Elaine Anderson, The Central Railroad of New Jersey's First 100 Years: a historical survey, p. 214).

"Only two ports in the world, Hamburg and Rotterdam, rivaled but did not surpass New York in tonnage handled, but none remotely approached it in the multiplicity and complexity of its coordinated rail and marine operations. An analysis of the port traffic made in 1914 by the New York and New Jersey Port and Harbor Commission revealed that the total volume of freight conveyed through, into, and out of the harbor by all modes of transportation reached nearly 133,000,000 tons for the year, of which 66.2 percent was carried by rail and the balance of 33.8 percent by vessels. The importance of this commerce to the city and to the nation may be gauged from two massive facts: more than one-fifth of the entire foreign commerce of the United States passed through the port, and somewhat more than 40,000,000 tons of the total, or 30.3 percent were consumed in the port area, for an average consumption of 5.6 tons per capita." (Condit, The Port of New York: A History of the Rail and Terminal System from the Grand Central Electrification to the Present, p.108-9).

Three types of industrial freight are particularly notable: coal, iron, and cement.

Jersey City's economic health balanced on the success or failure of the marine facilities located on its waterfront, which after 1875 were completely under railroad company control, the largest share of which was owned by the CRRNJ.

The Morris Canal and the Delaware and Raritan Canal are both listed in the State and National Registers of Historic Places. The CRRNJ was originally conceived as direct competition for the Morris Canal in tapping the coal trade of the Lehigh Valley. In fact, the line was chartered in 1831, the same year that the Morris Canal opened. The relative important and competitive relationship of the canals and the railroads is partially illustrated by a comparison of the

annual freight tonnage transported by each system. The high point of the Morris Canal was in 1866 when 870,000 tons of freight was transported. The Delaware and Raritan (D&R) Canal reached its zenith in 1871 when it transported 2,800,000 tons of freight. The CRRNJ began overtaking the

D&R in 1870, one year before the Canal's peak year for freight, and thereafter canal tonnage rapidly declined while CRRNJ tonnage rapidly increased. In the year 1900, 25 times more freight was hauled on the CRRNJ as was hauled on the D&R Canal. A more detailed examination of impacts on both the State and the municipalities served by the CRRNJ could be obtained from a review of population, employment, industrial production, and industrial power usage statistics in the municipalities along the CRRNJ's Main Line. These cities would include the industrial and commercial municipalities of: Jersey City, Bayonne, Elizabeth, Plainfield, Bound Brook, Somerville, and Phillipsburg.

As the New York Harbor developed into the nation's leading port in the early nineteenth century, surrounding land remained largely undeveloped. The arrival of the railroad set the pattern of Jersey City as a terminal to the New York Harbor and Manhattan's shipping facilities. (Ed Rutsch, Ellis Island Report)

The CRRNJ was the first railroad to make a complete connection between the anthracite coal mines of Pennsylvania and the Port of New York. Prominent New Jersey historian John T. Cunningham maintains that the primacy resulted in a fifteen year domination of this type of traffic: "Jersey Central's quick roll to the edge of anthracite gave it a lead it didn't lose for 15 years. . . . Fortunately for [CRRNJ President] Johnston two equally-dynamic figures in the Pennsylvania coal fields -- John I. Blair and Asa Packer -- saw in Jersey Central the answer to their own dreams of getting to tidewater. These were the golden days of the Jersey Central. In October, 1855, the Lehigh Valley-or-Packer Road was complete from the Mauch Chunk coal region to Phillipsburg. Jersey Central had a tight hold on the anthracite traffic." (Railroading in New Jersey, p. 23). "The early importance of coal traffic to the Central Railroad of New Jersey is revealed by the fact that in 1860, after little more than a decade of operation, the road transported 1,061,502 tons of freight, of which 845,769 tons, or 80.5 percent, represented coal traffic. The proportion dropped to 77.1 percent in 1864 but remained in the 60-80 percent range throughout much of the company's history until the drastic collapse of traffic following World War II. (Condit, The Port of New York: A History of the Rail and Terminal System for the Beginnings of the Pennsylvania Station, p.365)

"For 22 years the C.N.J. operated only in New Jersey, which for the time proved to be no handicap in gaining its share of the traffic. Since its future competitors, the Lackawanna and the Lehigh Valley, lay entirely west of the New Jersey state line, the road enjoyed an unchallenged rail monopoly in the transportation of anthracite tonnage into the harbor area." (Condit, p. 65).

The CRRNJ leased rolling stock to the Lehigh Valley and laid down a third rail from Happort Junction to Elizabethport so that the extra wide coal cars of the Lackawanna Railroad could go all the way to Elizabethport without change. Most coal headed for tidewater went over the CRRNJ. During the 1850s-1860s, the CRRNJ had a tight hold on the anthracite traffic. Despite the fact that not all of the coal hauled on the CRRNJ belonged to the CRRNJ, maps of trackage of the period clearly show that both the Delaware Lackawanna & Western and the Lehigh Valley used the CRRNJ mainline to transport coal from their western termini to the Port of New York. The other companies used the CRRNJ because it was the only continuous east-west link across New Jersey. Clearly these facts do not diminish the significance of the CRRNJ, but bolster it. In 1868, the Lackawanna Railroad built their own east-west line across New Jersey. The competition which ensued is known as 'the railroad wars'.

That the volume of coal traffic was extraordinary, is demonstrated by the facilities the CRRNJ built at its eastern terminus, where only the National Register listed passenger terminal survives. The CRRNJ was among the three of the seven coal ports in the New York area which had two McMyler car dumpers. This type of dumper picked up the entire railroad car and dumped its contents into a barge for further transportation. It was much more efficient than earlier methods of transfer which operated by gravity, draining the coal out of the railroad cars and letting it fall down a trestle onto the barge. One McMyler car dumper could empty two cars every minute.

The CRRNJ was/is significant as an interstate railroad and carrier of anthracite coal between Pennsylvania mines and the industry and ports of North Jersey. West of Phillipsburg, the CRRNJ continued into Pennsylvania, through Easton, Bethlehem and Allentown, up the Lehigh River Valley through the coal center of Jim Thorpe and coal mining cities of Wilkes-Barre and Scranton. The Pennsylvania section of the CRRNJ began life as the Lehigh and Susquehanna in 1838 before becoming a part of the CRRNJ in 1871.

Today, the Pennsylvania section of the CRRNJ, both the active railroad operated by Conrail and the abandoned right-of-way, is all within the Delaware and Lehigh Canal

National Heritage Corridor. This 150 mile long National Historic Corridor, created in 1988 with the assistance of the National Park Service is formed from the historic routes of the Lehigh and Susquehanna Railroad (the Pennsylvania corporate identity of the CRRNJ), and the Delaware Canal. The National Heritage Corridor Management Action Plan (January 1993) describes the corridor:

This extraordinary early 19th century system of mountain railroads, rivers, dams and canals, devised to move anthracite coal from mine to market, forms the visible heart of the corridor. This system was remarkable in its time for its engineering, daring and vision. Today, it is equally remarkable for its endurance, its integrity and its role as the centerpiece of a system of sites, facilities and tours that collectively tell the corridor's stories. (p.ix)

In discussing the National Significance of this National Heritage Corridor, the Management Action Plan highlights the associative significance of the railroad, the physical transportation link between resource extraction and commerce and heavy industry. (p. 14).

The significance statement continues: "Anthracite coal broke the dependence on wood and water power as an energy source for manufacturing -- no other fuel could compete in price or quantity (p. 21). . . . By 1838, the 150 mile long transportation system was in place between the almost limitless supplies of anthracite and the burgeoning industrial centers of Philadelphia, Trenton and Camden (p. 22). . . . The discovery of anthracite and the drive to transport, market and use it were responsible for the sudden and accelerated industrial and urban development of the corridor. This reliable and inexpensive fuel allowed the concentration of industry in growing urban areas along coal transportation routes, where steel textiles, manufacturing and mineral processing were the basis of a new diversified industrial economy." (p. 74)

The CRRNJ brought anthracite coal and its associated commercial and industrial prosperity to both Northern Central New Jersey and the ports of New Jersey and New York. In New Jersey, the CRRNJ was the earliest east-west interstate railroad corridor, the most direct physical transportation link between the Pennsylvania anthracite coal fields and the port of New Jersey and New York, and primary outlet for the anthracite mined in the Lehigh Corridor. Without the existence of the CRRNJ across New Jersey, where its track is in place to a far greater degree than in Pennsylvania, we cannot fully understand or appreciate the full impact and the significance of the history of "coal transportation and

industry" (p. 318) that is the focus of the Delaware and Lehigh National Heritage Corridor.

One precedent for the eligibility of a rail line linking coal fields to industry and ports is the Hocking Valley Railroad, in Ohio, which is listed in the National Register of Historic Places as the Hocking Valley Railway Historic District. A review of the history and significance of the Hocking Valley Railroad highlights its Criteria A and C significance as an example of a successful railroad built to connect eastern Ohio's bituminous coal fields to Lake Erie ports and the cities of northern Ohio ("Historic Railway Districts and the National Register of Historic Places," Locomotive and Railway Preservation, January-February 1990). By substituting 'CRNJ' for 'Hocking Valley Railroad', 'Pennsylvania's anthracite' for 'Ohio's bituminous', and 'Port of New York' for 'Lake Erie ports and the cities of northern Ohio', an equivalent statement of significance for the CRNJ is produced.

Coal was not the only mineral commodity significant in the history of New Jersey and the CRNJ. Since the 18th century the New Jersey Highlands were known as an abundant source of iron ore. Mining activities occurred in the region as early as 1710. The industry was retarded by poor transportation facilities which greatly limited the distribution of the ore. The industry was greatly boosted by the arrival of the railroads in the mid-nineteenth century, which first supplemented, and then quickly surpassed the Morris Canal as the prime mover of coal and iron. Much of the ore was shipped to Pennsylvania on the CRNJ. It was consumed by the great furnaces of the Allentown and Bethlehem region -- the nation's center of processing at the time. Regional iron production reached its peak in the 1880s. (Richard L. Porter Historical Background of the Mount Hope Iron Mining Complex and telephone conversation with Lance Metz 7/26/95). By the 1920s the CRNJ had absorbed the Mount Hope Mineral Railroad as a spur line, assuring the flow of iron. (Ed Rutsch, Ellis Island Report)

The limestone, "cement rock", formations of eastern Pennsylvania and Northeastern New Jersey have always been an important natural resource. During the late 19th and early 20th centuries increased usage of this material for roads, viaducts, tunnels, and bridges revolutionized American transportation means and the landscape. Alpha, NJ was one important center of the extraction and processing industries. During the late 1940s the CRNJ was a pioneer in providing cement to the harbor of New York for construction. Previously cement was conveyed in small quantities, usually bagged. The CRNJ innovated a system of dumping cement directly from a rail car to a barge for bulk transport to

construction sites making the process cheaper and easier. (phone conversation with Thomas Flagg, 7/26/95). Bulk cement was loaded directly into covered hopper cars at the production site. The hopper cars were unloaded in a weather tight shed, at the eastern terminus, where it was conveyed via twin-bucket elevators directly to specially designed barges.

The CRNJ's role as an inter- and intra-state passenger railroad also shaped the broad patterns of New Jersey's residential growth and development. While the CRNJ was not the largest passenger carrying railroad in the State, it was a most important avenue for transporting passengers to and from New York, Jersey City and Newark and dozens of suburban and rural communities in Essex, Union, Somerset, Hunterdon, and Warren counties. The passenger traffic on the CRNJ epitomizes one facet of New Jersey's most significant twentieth century demographic trend: suburbanization.

The CRNJ was a vital commuter line. One of the first official recognitions of commuters as a distinguishable type of passenger is contained in the 1854 annual report of the Jersey Central: "The increase in local passenger and freight business is very large . . . owing to the fact that the easterly end of the line is peopling with parties residing in the country with their families but doing business in New York." (Railroading in New Jersey, p. 96). Until the opening of Penn Station (NYC), the CRNJ was able to offer superior accommodations, faster schedules on limiteds, and a more rural and scenic ride -- an especially important attraction in the era before air conditioning (Heritage Studies Station Survey). They capitalized on the attractiveness of their route with the publication of the magazine Within the Suburban Limits, which encouraged the purchase of homes along its lines -- suburbanization.

Passenger service was extremely important to the CRNJ. One particularly interesting innovation made on the central line was the introduction of uniforms for railroad employees by President John Taylor Johnson (Elaine Anderson, The Central Railroad of New Jersey's First 100 Years: a historical survey, p.2). No doubt increased recognition facilitated the ability to speedily address passenger needs. The success of this measure is borne out by the fact that it has become an industry standard which today we take for granted.

The CRNJ was also an important carrier of interregional passengers. In 1862, the CRNJ began advertising the "Allentown Route" to Chicago as the "shortest link to the West." And it was. A passenger could travel from the shores of the Hudson to the shores of Lake Michigan in 36 hours --

an amazing feat at the time (Cunningham, Railroading in New Jersey, p 28).

Due to the location of the prominent Jersey City passenger terminal, the CRRNJ was the primary artery of westward travel for immigrants entering at Ellis Island. Journeys to Cincinnati, Chicago, Pittsburgh, and St. Louis would all likely start on the CRRNJ. The CRRNJ ran at least one, sometimes two trains daily from the Jersey City terminal from 1864 until the 1940s specifically to convey recent immigrants westward (telephone conversation with Frank Reilly 7/28/95).

In 1978, this line, provided the last passenger service into Northwestern New Jersey. This service included daily trips with observation car #1178, the last observation car operating on scheduled service in the United States.

The impact of the CRRNJ on the adjacent landscape, and its consequent Criterion A significance, is further borne out by the county and municipal surveys which have been done.

Union County: "... the railroads eventually helped transform rural Union County into a suburban/industrial section of metropolitan New York." The CRRNJ formed the 'Central Land Improvement Company' in 1872 to market lots in lower Westfield, and by 1877 in Fanwood as well.

Elizabeth: The initial intent for the railroad was to connect with agriculture, but this goal quickly became secondary as its potential was realized. Rail connection spurred the city's industrial growth, especially the ceramic industry, oil cloth manufacturing, and the attraction of large manufacturing interests such as the National Register listed Singer factory.

Roselle Park: "The railroad was the principal contributing to the development of Roselle Park. . . Although farms remained in Roselle Park well into the 1880s, the community was becoming suburbanized."

Westfield: "The village of Westfield was to retain its rural character throughout most of the nineteenth century until the railroad caused it to develop into a suburban commuter community. . . By 1890, twenty-two trains made daily stops in Westfield, and the town was increasing by two hundred new residents per year. The promotion was escalated throughout the following decade by a Central Railroad advertising campaign. Their

magazine, Suburban Life, often showcased the homes of commuting Westfielders."

Fanwood: The existence and character of this community was shaped by the railroad. Although the CRRNJ first reached Fanwood in 1838, it did not have much of an impact until 1874 when the station was relocated and rebuilt. Development was stimulated by speculative housing construction in the vicinity of the station.

Cranford: "Cranford is a residential township largely developed in the 1880s to 1930s as a suburban community along the CRRNJ." After the Civil War, Cranford -- the Venice of New Jersey -- was promoted to New York businessmen as a place to summer, "... but faster trains made daily commuting to New York City a reality, and suburban type, single family residential development began to grow in concentric circles out from the railroad station."

Plainfield: "Plainfield was incorporated as a city in 1867, shortly after the rail connection with New York City was completed. The latter event catalyzed Plainfield's transformation, within fifteen years, from a rural village into a fashionable commuter suburb of 12,000 inhabitants."

Dunellen: "Dunellen was founded because the Jersey Central president, John Taylor Johnston, wanted to create residential communities along the route of the railroad. The Central began assembling land for a town in New Market in 1866, and within two years had acquired 300 acres. In 1867 the railroad secured a charter for a subsidiary corporation, the 'Central New Jersey Land Improvement Company,' and deeded the land to it the following year. Johnston provided the name 'Dunellen' (the prefix 'dun' added for euphony to the name of a family friend); the land company laid out the town and drew a map of it."

Somerset County: "The first major railroad to have been established through Somerset County, and the most influential in the county's history, was the [CRRNJ]. . . it became a main passenger thoroughfare and a freight route. . . the [CRRNJ] dominated rail travel through Somerset County throughout the 19th and into the 20th centuries. . . In general, a significant shift in population distribution occurred in the late 19th and early 20th centuries, much of the population came to be concentrated in discrete areas, rather than being relatively dispersed as it had been before the advent of the railroads, when the economy had been more strictly rural."

Somerville: "The coming of the railroad gave great impetus to Somerville's growth and prosperity . . . Somerville's link with the railroad was instrumental in attracting new commercial enterprises to town and also in making an attractive community for businessmen who wished to live in the suburbs while commuting to New York City offices." By 1909, Somerville was receiving daily service from fifty CRRNJ trains.

Warren County: "Despite the increased capacity for the transportation of goods, the impact of the (Morris) canal through the county was limited. The real impetus for industrial development in Warren County came with the construction of the railroad." The text goes on to specifically cite the importance of the CRRNJ as the first major rail link in the county. Specific industries spurred by the railroad include: Andover Iron Co. (1868) in Phillipsburg and the Asbury Graphite Mill (1895) in Asbury.

Alpha: This municipality is solely the product of modern industry. In 1891, A.B. Bonneville recognized the existence in the Alpha area of large supplies of a type of blue rock necessary for making Portland Cement. Shortly thereafter, he established the Alpha Portland Cement Company. It was the first company in the nation devoted to the production of Portland Cement. It had an annual output of 100,000 barrels. After several name and administration changes the company closed in 1914. It briefly reopened during World War I, but finally ceased all production quickly thereafter.

Phillipsburg: "The beginnings of residential and industrial expansion in Phillipsburg is dated to the 1850s, attributable to the selection of Phillipsburg as a railroad point . . . the town was to become a gateway to the antebellum west. . . the first planned subdivision occurred in 1853 in expectation of growth, when the Phillipsburg Land Company purchased the Roseberry Farm and laid out 1,130 residential lots."

CRITERION C SIGNIFICANCE

The CRRNJ passes through or very close to the following eighteen related resources whose individual eligibility has already been recognized:

Phillipsburg Main Street Historic District (SHPO Opinion 7/21/92);

Central Alpha Historic District (staff opinion 9/30/94);

Asbury Historic District (SHPO Opinion 8/27/92, SR 11/2/92, NR 3/19/93);
Annandale Historic District (SR 8/1/94, NR 9/8/94);
Whitehouse Railroad Station (SR 3/17/84, NR 6/22/84);
Wallace House (SR 9/11/70, NR 12/2/70);
Old Dutch Parsonage (SR 9/11/70, NR 1/25/71);
Somerville Railroad Station (SHPO Opinion 2/21/79);
Bound Brook Railroad Station (SR 3/17/84, NR 6/22/84);
Grant Avenue-Plainfield South Railroad Station (SHPO Opinion 2/21/79);
Plainfield Railroad Station (SR 3/17/84, NR 6/22/84);
North Avenue Commercial Historic District (SR 2/9/84, NR 3/29/84); Freight building (DOE 8/26/77);
Netherwood Railroad Station (SR 3/17/84, NR 6/22/84);
Fanwood-Scotch Plains Railroad Station (SR 11/7/79, NR 7/17/80);
Roselle Railroad Station (SHPO Opinion 9/29/77);
Westfield-Garwood Railroad Station (SHPO Opinion 9/29/77);
Staten Island Railroad Historic District (SHPO Opinion 6/11/91);
Mid-Town Elizabeth Historic District (SR 9/29/94);
Elizabeth Station (SR 3/17/84, NR 9/29/84);
Jersey City Terminal (SR 8/27/75, NR 9/12/75).

The NUTRANSIT bridge survey recommended no less than fifteen bridges along the line as individually eligible to be listed in the National Register of Historic Places. The NUTRANSIT Bridge survey recommended no less than four bridges along the line as individually eligible. There are also bridges which, although not individually eligible, contribute to the significance of the line due to their associations and period and type of construction. The CRRNJ used lots of stone -- there is a proliferation of stone arch bridges on this line, there are also many girder bridges with stone abutments which survive intact and help to make this line distinguishable from many others.

CRRNJ (Raritan Valley Line of NJ Transit) inventory of bridges and culverts (1989, Office of Engineering) lists: 167 undergrade or overhead bridges and culverts between Aldene (Cranford) and Phillipsburg (west end of CNJ in NJ) in a distance of 57.2 miles; 143 of the 167 constructed before 1950, and 50 of the 167 were constructed before 1900. Of the 50 from before 1900, 42 are (undergrade) stone arches dating from 1839-1865, and 8 are iron/steel (undergrade) dating from 1891-1899. A majority of the 1800s stone arches (38 of 42) are in the western 35 miles of CRRNJ between Raritan and Phillipsburg. This density is notable: for example, the National Register listed Hocking Valley Railroad inventory showed only four bridges in a 13 mile stretch.

Attachment A, Page 16

In contrast, the Camden to Atlantic City line's first 6.3 miles of track, east of Camden (to Haddonfield), were completely rebuilt in the 1950's when the line became the Port Authority Transit (PATCO) electrified high speed rail transit line. In the 52.7 miles between Haddonfield and Atlantic City, there is a total of 53 undergrade or overhead bridges and culverts. Of these 53, only 34 are pre-1950 and only two (2) are pre-1900 (both of which are 1898 iron/steel girder bridges). Thus, the CRRNJ, in the same amount of mileage has 3 times the total number of bridges and culverts, 4 times the number of pre-1950 structures, and 25 times the number of 1800's structures. The CRRNJ mainline has all but 1 of its 50 pre-1900 crossings, a total of 49 bridges and culverts, older than the oldest Camden-Atlantic City line structure. This comparative evaluation of bridges does not include structures from the Elizabeth to Aldene (eastern) section of the CRRNJ now owned by Conrail.

Here, the Consolidated Rail Corporation (Conrail) bridge inventory lists 34 undergrade or overhead structures in 6.4 miles. Eight (8) of these structures are stone arches from the period 1870-1895, including two (2) listed as contributing properties in the Mid-Town Elizabeth Historic District (Union County) listed in the New Jersey Register of Historic Places on 9/29/94.

One of the most notable bridges on the western end of the line is the 1865 multi-span stone arch "Greens Bridge" over the abandoned (National Register listed) Morris Canal located west of the Warren-Glen Bloomsbury Road in the out-of-service section of the CRRNJ Main Line and evaluated as eligible by the NTH Bridge survey. Also in this section of the line, the Main Street-Phillipsburg bridge, an overhead bridge built by the CRRNJ and given a SHPO Opinion of eligibility, was fully rehabilitated as a result of successful NJDOT and NJSHPO Section 106 Consultation.

Conversations with prominent New Jersey historian John T. Cunningham revealed that at the western end of the line, there are some prominent engineering features -- such as the High Bridge fill and Black Dan's cut -- which also make the line a distinguishable entity.

CRRNJ Main Line Significant Accomplishments*

In the early 1860s, the world's first railroad dining car travelled on this route.

There is abundant evidence that the CRRNJ stood in the front rank of technical innovation in the last decade of the nineteenth century. (Condit, The Port of New York: A History

Attachment A, Page 17

of the Rail and Terminal System for the Beginnings of the Pennsylvania Station, p.147)

In 1889-1890, the CRRNJ installed a pioneer system of automatic block signaling between the Jersey City terminal and Bergen Point, which was unique for its time. The system, which applied the principles of electropneumatic interlocking, was developed by Westinghouse Electric and Manufacturing Company, and was an important step in the direction of fully automatic signalling. (Condit, The Port of New York: A History of the Rail and Terminal System for the Beginnings of the Pennsylvania Station, p. 146-7)

The CRRNJ continued its role as a leader in the development of signaling technology when in 1893, the world's first automatic motor-operated semaphore was installed at Black Dan's Cut near Phillipsburg. This signalling and safety innovation was a significant engineering achievement and was soon copied by many other companies. This was the predecessor to the automatic block system now in operation throughout the world's railroads. For more details on this corridor component, please see HAER Survey form attached.

Also in 1893, the CRRNJ was the first to have a passenger train break the 100 mph mark: west of Fanwood a train reached 105 mph.

Also in 1893, the first railroad car to use electric lights in a coach, travelled on the CRRNJ.

In 1910, the first heavy steel rails were used on this line. This increased speed capacity. Heavy steel rails later became the world's standard.

In 1925, the CRRNJ was the first railroad to acquire and operate a diesel locomotive. This locomotive is now on display at the B&O Museum in Baltimore.

On October 7, 1948, the first practical demonstration of television on a moving train. They watched the second game of the World Series.

In 1966, the Aldene Plan diverted CRRNJ trains from Jersey City to Newark. The Aldene Plan, implemented in 1967, was an attempt to salvage rail passenger service in New Jersey after the CRRNJ filed for bankruptcy. It was the first use of Federal Transit funds by New Jersey.

(*Most of the information in this section came from a telephone conversation with Frank Reilly, President of the CRRNJ Historical Society on 7/28/95)

INTEGRITY

The NJDOT has raised concerns about the integrity of the resource. They have stated that the absence of rails and ties from the period of significance destroys the integrity of the resource. This flies in the face of established practice. Like paint on a clapboard house, or the deck on a truss bridge, or the paving material on a historic roadway (such as the Merrit Parkway, the Baltimore-Washington Parkway, or the Blue Ridge Parkway), rails and ties are frequently replaced as part of cyclical maintenance to assure the continued use of the resource for its original function. It is ironic that NJDOT would raise this issue while proffering the Atlantic City railroad as a potentially eligible example of this property type: all of its rails and ties were replaced in the 1980s.

Even the total absence of these features does not preclude the eligibility of this type of resource, as a railroad right-of-way and its associated features may remain as a distinguishable entity of the landscape. Quoting from a National Register nomination for a rail line in Maryland: "Removal of the tracks and ties upon the line's abandonment compromised the integrity of the roadbed, but the route retains essential continuity and most of it may be travelled by vehicle or on foot. The bridges, tunnels, and other structural features retain high integrity." Similarly, the National Register listed Hocking Valley Railroad had some of its multiple tracking removed in the 1930s. NJDOT proffers the Camden & Amboy as potentially eligible while portions have no rails or ties and the remaining sections operated by Conrail have had rails and ties replaced including the installation of 'welded rail'.

REBUTAL OF SPECIFIC POINTS MADE IN NJDOT/FHWA CORRESPONDENCE

Evaluation of 63 miles of the CRRNJ Main Line as eligible for the National Register of Historic Places is not, as NJDOT and FHWA assert, "arbitrary and indefensible" (Introduction, page 2) but justified by National Register of Historic Places evaluation procedures and criteria and consistent with numerous transportation properties currently listed on the National Register.

Significance:

The statements "all but a few railroads in New Jersey contributed more or less (emphasis added) equally to the development of either their particular region or the entire state" (Introduction, page 2, paragraph 2) and "In terms of local impact the CRRNJ did have an effect, an effect that all

other railroads in the state had in over 90% of the state's municipalities" (page 3, paragraph 3) are ahistorical, unsubstantiated, and incorrect. Every New Jersey community adjacent to a railroad line did not reach an identical or uniform level or density of development. Railroad helped shape economic development in conjunction with distinct topography, geography, geology and natural resources. The origin and destination of the railroad line as well as the on-line communities also mattered. The Camden and Amboy, although significant as the first railroad in New Jersey, did not concentrate industrial development in Middlesex or Mercer counties. The C & A, operating between the Raritan Bay at South Amboy and the Delaware River at Bordentown, served communities such as Jamesburg, Hightstown, and Cranbury as agricultural centers. Quite distinctly, in Plainfield, Bound Brook, Somerville, and Manville, industrial development was concentrated along the CRRNJ. The CRRNJ's Southern Division (to Bridgeton and Port Norris, Cumberland County), though important for linking northern and southern New Jersey, was constructed through relatively sparsely settled portions of Ocean and Burlington counties, places that today, generally retain a low density, rural, forest covered character known as the New Jersey Pinelands. Although the CRRNJ's Southern Division contributed heavily to the development of the Delaware Bay oyster industry (NPS Survey of Commercial Township, 1994), the glass and construction sand mining industries, and the communities of Lakehurst, Vineland and Bridgeton, most communities along the line remained agricultural. Clearly not all railroads had the same effect on the State and not every community with a railroad developed similarly. Indeed many railroads were abandoned and removed years ago, testimony to their transitory relation to the landscape and the local economy. The HPO knows of no credible historical analysis has ever asserted that "the overall influences of railroad good and bad were realized by almost every community." This premise implies that railroad routes and proximity to railroad routes did not matter, even in the early days of transportation before the highway system extended the reach of transportation to more distant and underdeveloped communities. This NJDOT assertion is contrary to the historic recognition that communities fought furiously to lure the railroad and keep it from bypassing them.

The CRRNJ did have many spur lines. This does not detract from the significance of the mainline. Indeed it was the financial success of the mainline which made expansion viable. The spur lines are a testament to the success and financial solvency of the mainline due to the amount of passengers and freight carried. It is also important to note that the spur lines contributed traffic which eventually moved over part of the main line.

The NJDOT statements that "while the CRRNJ can be shown to have carried larger amounts than the Morris Canal, that freight can not be shown to have carried over the entire length of the mainline" and "winning a 'tonnage' contest does not confer eligibility itself" (Page 2, paragraph 4) requires examination. The HPO never asserted that tonnage alone equaled significance. The HPO used a comparison of canal and railroad tonnages to illustrate the importance of the railroad relative to two other National Register listed transportation resources. A review of tonnage statistics highlights the competitive advantage that the railroad developed relative to canals and of the impact of the railroad on the economy and hence the broad patterns of history at the national, state, and local levels. A review of the tonnage statistics demonstrates that the CRRNJ was a significant component of the railroad network in New Jersey and the eastern United States. The statement (Page 2, Paragraph 4) "For example the Delaware Lackawanna and Western Railroad which used only a section of the CRRNJ Main Line track was the true originator of at least half of the tonnage reported by the CRRNJ" makes no reference to a specific period of time and is unsubstantiated and undocumented by historical reference. As we have indicated previously, with its initial connection to the Lehigh Canal and Lehigh Valley Railroad and later acquisition of the Lehigh and Susquehanna Railroad, the CRRNJ had substantial traffic that traveled the full length of the Main Line throughout its history. Regardless of the origin of the commodity, the CRRNJ Main Line was a critical link in the transport between mine and market. As Appendix — illustrates, the CRRNJ was a significant avenue for freight.

The HPO sincerely appreciates the NJDOT and FHWA recognition of the historic significance of both the Camden and Amboy (C&A) and the Camden and Atlantic railroads. We agree that both lines are important in the history of New Jersey and each contributed to the distinctive development of a portion of the state. While we agree with the contribution of the Camden and Atlantic to the development of the Atlantic Ocean seashore resort industry in the state, we find the lack of discussion of this line's integrity most disingenuous. As I pointed out in our discussion of bridges as contributing features to the CRRNJ Main Line, the Camden and Atlantic lacks both the density and quality of these resources. Likewise, while the NJDOT and FHWA have highlighted recent rail and tie replacement on the CRRNJ, they have neglected to acknowledge that a 6-mile portion of the Camden and Atlantic line was completely transformed into the PATCO high speed rail transit line and that the remainder of the line has been fully rehabilitated with new ties and continuous welded rail as part of a multi-million dollar improvement for New Jersey Transit and (formerly) AMTRAK rail passenger service to

Atlantic City. Likewise, this formerly two track railroad now has only one continuous track with numerous signaled passing sidings.

In responding to the comment on Page 5, Paragraph 2, we also find considerable engineering significance in the western end of the line. We have previously mentioned the heavy concentration of 1800's vintage stone arch bridges, with spectacular construction such as Greens Bridge, and the High Bridge fill. The distinctive landscape, alignment and contours of the CRRNJ are not mimicked by "every other railroad in the state." The CRRNJ southern Division traverses a relatively flat, open and frequently coastal topography. The Lehigh Valley, despite being parallel to the CRRNJ Main Line, was forced to drive a tunnel through Musconetcong Mountain. The Delaware Lackawanna and Western Morris and Essex link was forced to ascend to Summit and the Warren Railroad forced to build two tunnels to reach the Delaware from its junction with the CRRNJ at Hampton. Clearly these lines all have distinctive engineering features associated with their particular and unique routes across the state. The comment (page 8, paragraph 3) "...without a map, the casual observer cannot discern whose tracks are whose" could be applied to almost any historic railroad. What makes the CRRNJ distinctive and identifiable is its surrounding topography, bridges, station, abutting communities. All of these features have been identified in previous sections of this letter.

We have expanded and elaborated our discussion of bridges as contributing features. The original number of bridges referred by the letter related to those bridges surveyed by New Jersey Transit. NJT did not, however, have the times or funds to initially survey all bridges. A more thorough review of bridges based on the full inventory or undergrade, overgrade, and culverts (see Appendix) produced the bridge statistics that we cited earlier in this letter.

The question (page 2) "An extreme example would be a railroad that only carried coal from Pennsylvania directly to New York. What is the effect to New Jersey?" is not applicable to the CRRNJ. Although the CRRNJ carried tremendous volumes of coal for shipment across the Port of New York, huge volumes were also hauled for distribution in New Jersey. And coal destined for shipment to New York was the reason for building and sustaining the largest port facilities in New Jersey in the later 19th and early 20th centuries.

The failure to assign significance to the CRRNJ during the Interstate Route 78 cultural resources review reflects the past focus of both the federal agency and SHPO on

in the Nation Register. Numerous additional examples of this continuous use of a listed property through the use of "in kind" rehabilitation materials could be cited.

In defining the eligible historic district as Elizabeth to Phillipsburg, the HPO modified the initial opinion of eligibility for the easternmost portion of the CRRNJ Main Line after a very detailed review of that portion of the line completed as part of the Section 106 consultation for the federally funded New Jersey Transit Bergen-Hudson Light Rail Transit (LRT) project.

The description of the CRRNJ Main Line as "abandoned" in the vicinity of the Warren Glen-Bloomsbury Road Bridge (Cover Letter, page 2 and Page 10, paragraph 2) is inaccurate. The line was transferred by Conrail to New Jersey Transit, the state's rail transit operating agency, and has never been the subject of Interstate Commerce Commission abandonment proceedings. Consequently, a Section of 106 evaluation was never undertaken. "Out of service" with track in place would be a more precise description of the status of the line. As the enclosed letter from Mr. William Jayne, an owner of property adjacent to right-of-way west of the Warren Glen-Bloomsbury Road Bridge and "interested party," indicates, there is a continuing interest in this right-of-way for its historic function, railroad transportation. This letter is enclosed as Appendix I. A second letter from the New Jersey Association of Railroad Passengers, requesting "interested party" status relates to the historic and contemporary transportation significance of the CRRNJ at the eastern end of the line and is enclosed as Appendix J.

In evaluating the CRRNJ Main Line, NJDOT/FHWA utilize the context for railroad bridges of statewide highway bridge survey. While this is an important tool, it is important when using it to note the scope of that survey: roadway bridges. It did not include any of the bridges already inventoried by NJTransit, it also excluded bridges under fifty years in age, and under twenty feet in length. A substantial number of bridges previously evaluated as not individually eligible are contributing resources to the MACRRNJ Main Line Historic District by period, method of construction, and designer/builder. This relationship to the district as contributing resources also applies to some previously evaluated stations and related structures.

Regarding the cultural resource evaluation guidelines of the New Jersey Pinelands Commission, telephone conversations with Barry Brady, Cultural Resources Coordinator for the New Jersey Pinelands Commission reveal that his agency's position regarding the evaluation of railroad corridors was misrepresented in the FHWA letter to you. Mr. Brady

discrete or individual sites and traditional residential or commercial historic districts. The current evaluation of the CRRNJ as a linear transportation district with significance and integrity reflects the evolution of cultural resource evaluation highlighted by CRM Volume 16, Number 11, 1993, "Historic Transportation Corridors" and the 1993 national Historic Transportation Corridors Conference. E. Blaine Cliver, Chief of the Preservation Assistance Division of the National Park Service, succinctly expressed this evolution of identification and evaluation in his article "Historic Transportation Corridors: A New and Dynamic Element of Heritage Preservation" (CRM, Vol 16, #11, page 8): "Over the past decades, as our view of historic preservation has matured to include not just buildings and monuments, we are becoming more comprehensive in our view of what constitutes the physical remnants of our past." While recognizing historic transportation corridors as a "new and dynamic" focus of historic preservation, we have in the CRRNJ Main Line essentially a long linear variant of the long affirmed historic district concept.

The statement (page 7, paragraph 2) "The Camden and Amboy always (NJDOT/FHWA emphasis) moved more people than the CRRNJ," is neither accurate nor substantiated by references. The third generation of the C & A, the line between New Brunswick and Trenton, what is now the 'Northeast Corridor' line (with an SHPO opinion of not eligible), did move more passengers. But this line is not the original C & A from South Amboy through Bordentown to Camden.

Integrity:

NJDOT and FHWA have seriously distorted the evaluation of integrity. Throughout the NJDOT/FHWA letter reference is made to the lack of track and ties older than fifty years. As we have indicated earlier, the National Register contains dozen of historic transportation properties, which in the course of operation, have been rehabilitated with the use of modern ties, rails, and paving, without a loss of integrity. Repaving has not diminished the significance or integrity of the Merrit Parkway, Bronx River Parkway, or Baltimore-Washington Parkway or resulted in their delisting from the National Register. The absence of rails has not precluded the Western Maryland Railway or Frenchtown and New Castle Railroad rights-of way from being added to the National Register. Horseshoe Curve, originally had four tracks and now only has three. Portions of the Hocking Valley Railway had two tracks and now only have one track. The three surviving tracks at Horseshoe Curve or the tracks in the Pennsylvania Railroad District are modern continuous welded rail on less than fifty year old ties and yet both properties possess sufficient integrity to retain a listing

Attachment A, Page 24

states that the position ascribed to him by NJDOT relates to the evaluation of small segments of railroad related resources on a small parcel of land. This position does not relate to the evaluation of railroad corridors as a property type.

We have also indicated where, consistent with other National Register listed railroad properties, the associated building and even rolling stock survives. Only two pieces of equipment were part of the Hocking Valley National Register nomination, because they were seen to exemplify the type of equipment historically used. CRRNJ locomotives and rolling stock from both the steam and diesel eras survive both in- and out-of-state. The Baltimore and Ohio Railroad Museum (located in Baltimore, Maryland) possesses a CRRNJ steam locomotive used in suburban commuter service and diesel locomotive #1000 (built in Phillipsburg, New Jersey by the Ingersoll-Rand Company). New Jersey Transit owns (and operates in special service) one of two surviving CRRNJ observation cars. Other CRRNJ equipment -- boxcars, coal hoppers, cabooses, and 1920 vintage passenger cars -- are scattered throughout the United States at operating railroad museums and excursion lines.

C:\wpwin\crnj.ata

E.O. 11593**DETERMINATION OF ELIGIBILITY NOTIFICATION**

**National Register of Historic Places
National Park Service**

Name of Property: Central Railroad of New Jersey Main Line Corridor

Location: Warren County

State: NEW JERSEY

Request submitted by: FHWA/Gary N. Hamby

Date received: 4/14/95

Additional Information received: 8/4/95; 11/13/95

Opinion of the State Historic Preservation Officer:

☒ Eligible

☐ Not Eligible

☐ No Response

☐ Need More Information

Comments:

The Secretary of the Interior has determined that this property is:

☒ Eligible

Applicable criteria: A,C

☐ Not Eligible

Comments:

See attached comment.

☐ Documentation Insufficient

(Please see accompanying sheet explaining additional materials required)

for Patrick Andrews
Keeper of the National Register

Date: 11/30/95

**NATIONAL REGISTER OF HISTORIC PLACES
DETERMINATION OF ELIGIBILITY NOTIFICATION**

**Central Railroad of New Jersey Main Line Corridor (Phillipsburg to Elizabeth)
Warren County, NEW JERSEY**

The Central Railroad of New Jersey (CRRNJ) Main Line Corridor is eligible for the National Register under National Register Criteria A and C for its historical significance in the development of transportation and commerce in New Jersey and for its architectural and engineering importance as a linear historic district associated with important transportation trends in the 19th and 20th centuries.

A major aspect of the State's history is the role of New Jersey as a corridor linking the anthracite coal mines of Pennsylvania with the industries of New Jersey and the Port of New York, and connecting the manufacturing centers of eastern New Jersey with its western markets. The CRRNJ played a vital role in the industrial and commercial development of New Jersey; it was the first railroad to cross the State to connect with the anthracite coal mines of Pennsylvania and was the primary outlet for the anthracite mined in the Lehigh Corridor. During its history the CRRNJ transported massive amounts of coal to the industries and ports of North Jersey. The CRRNJ also was a major shipper of New Jersey iron ore to the Pennsylvania iron furnaces in the Allentown and Bethlehem region.

Documentation presented demonstrates that the CRRNJ directly affected the residential growth and development of northern New Jersey in its role as an inter and intrastate passenger carrier. The railroad directly impacted the growth of numerous communities (including, among others, Jersey City, Bayonne, Elizabeth, Plainfield, Bound Brook, Somerville, and Phillipsburg) as it brought coal and its associated industrial and commercial growth. Another impact of the CRRNJ is that it increased the spread of suburbanization along its route. It also served as a major carrier of newly arrived immigrants heading west to Pittsburgh, Chicago, Cincinnati, and St. Louis.

The CRRNJ also possesses architectural and engineering significance for the numerous buildings and structures (bridges) constructed by the railroad. The CRRNJ corridor retains integrity as a linear historic district. The replacement of original railroad ties and track over time does not destroy the ability of this district to convey its significance. The importance of the CRRNJ is conveyed through the retention of the roadbed and its right-of-way and the associated buildings and bridges.

The boundary of the eligible historic district is formed by the historic right-of-way of the CRRNJ.

New Jersey Department of Environmental Protection
Historic Preservation Office

Page 1 of 2

BASE SURVEY FORM

Historic Sites #:

Property Name: CRRNJ Main Line Historic District Corridor

Street Address: *Street #:* _____ *Apartment #:* _____
(Low) (High) (Low) (High)

Prefix: _____ *Street Name:* _____ *Suffix:* _____ *Type:* _____

County(s): Multiple (see below) **Zip Code:** _____

Municipality(s): Multiple (see below)

Local Place Name(s): _____ **Block(s):** _____ **Lot(s):** _____

Ownership: Multiple **USGS Quad:** Multiple (see below)

Description:

The Central Railroad of New Jersey Main Line Historic District extends from Elizabeth in Union County to Phillipsburg in Warren County; however, the eastern end of the current study area is Aldene in Roselle, Union County. The rail line runs through five counties and twenty-four municipalities, listed below in order proceeding east to west: Roselle, Cranford, Garwood, Westfield, Fanwood, and Plainfield (all in Union County); Dunellen and Middlesex (in Middlesex County); (Cont'd, See Continuation Sheet)

**Registration
and Status
Dates:**

National Historic Landmark: _____

SHPO Opinion: 7 / 19 / 91

National Register: / /

Local Designation: / /

New Jersey Register: / /

Other Designation: _____

Determination of Eligibility: 11 / 30 / 95 Other Designation Date: / /

Photograph:



Survey Name: NJ Transit Raritan Valley Line Study

Date: July, 1999

Surveyor: Nancy L. Zerbe

Organization: ARCH², Inc.

New Jersey Department of Environmental Protection
Historic Preservation Office

Page 2 of 2

BASE SURVEY FORM

Historic Sites #:

Location Map:

2.5" x 3.5"
(Include North Arrow and Bar Scale)

Site Map:

INTENSIVE-LEVEL USE ONLY
2.5" x 3.5"
(Include North Arrow and Bar Scale)

Bibliography/Sources:

Baer, Christopher. Unpublished research, 1998.
CNJ Company records, including annual reports.
Drew University. *HAER Northwest New Jersey Inventory*. 1978.
ICC Valuation Reports, 1923.

Additional Information:

See Attached Continuation Sheet

More Research Needed? ☐ Yes ☒ No

INTENSIVE-LEVEL USE ONLY:

Attachments Included: ☐ Building/Element ☐ Landscape ☐ Farm
☐ Bridge ☐ Industry

Historic District: ☒ Yes ☐ No Historic District Name: CRRNJ Main Line

Status: ☐ Key Contributing ☒ Contributing ☐ Non-contributing

Associated Archaeological Site/Deposits? ☐ Yes ☒ No
(Known or potential sites – If yes, please describe briefly)

Survey Name: NJ Transit Raritan Valley Line Study
Surveyor: Nancy L. Zerbe
Organization: ARCH², Inc.

Date: July, 1999

New Jersey Department of Environmental Protection
Historic Preservation Office

Page 1 of 4

CONTINUATION SHEET

Historic Sites #:

CRRNJ Main Line Historic District Corridor Base Form Cont'd...

Description:

Bound Brook, Somerville, Raritan, Bridgewater, North Branch (all in Somerset County); Readington, Lebanon, Clinton, High Bridge, Glen Gardner, Hampton, Bethlehem, and Bloomsbury (all in Hunterdon County); and Greenwich, Pohatcong, and Phillipsburg (all in Warren County). The corridor is shown on the following quad sheets (listed in east-to-west order): Roselle, Chatham, Plainfield, Bound Brook, Raritan, Flemington, Califon, High Bridge, Bloomsbury, and Easton.

The Determination of Eligibility for the CRRNJ Main Line Historic District defined the boundaries as the rail line's right-of-way. Based on the current study, it is recommended that these boundaries be expanded to include the following facilities:

- Cranford Junction Coach Yard (including the roundhouse and roundhouse office)
- Cranford Station complex
- Westfield Station complex
- Fanwood Station complex
- Netherwood Station complex
- Plainfield Freight Station
- Plainfield Station complex
- Dunellen Boiler House
- Middlesex Station Grounds
- Bound Brook Station complex
- Somerville Station complex
- "RA" Tower, Raritan
- Raritan Station
- Raritan Yard
- White House Station
- Lebanon Station
- High Bridge Station complex
- Hampton Yard
- Phillipsburg Yard (including "PU" Tower, coal chutes, and retaining wall)
- Phillipsburg Station

The character of the line dramatically changes west of Somerville to more rural with greater areas of fill and cut due to the increased elevation. Although active service currently ends at High Bridge (MP 52.24), the area between High Bridge and Station Road in Ludlow (just west of MP 60.03) consists of a single track and is weeded by NJ Transit. For approximately the next 6 ½ miles, the track is overgrown, and not easily accessible. Just to

Description Cont'd:

the west of Pine Hollow Road (MP 66.51) is the "Alpha Bypass" where NJ Transit switches to the former Lehigh Valley Rail line. The original CRRNJ rail bed from just of west MP 66.51 until the creek at MP 70.96 is dormant. Approximately the last 1 ½ miles of the old CRRNJ Main Line is in active freight service.

Areas of cuts and fills which have particular significance include:

- Cut at Fanwood. The CNJ's April 1, 1855 annual report indicated that "great progress has been made in reducing the grade at Scotch Plains to 21' per mile." As shown in the elevation chart, Fanwood has the highest elevation in the eastern end of the CRRNJ Main Line.
- Vanderveer's Cut. In 1901, the grade through Vanderveer's Cut west of Raritan was reduced from 414 of 1% to 271 of 1% in order to allow the hauling of heavier tonnage. The CNJ's December 21, 1924 report stated that the cut had been widened and deepened at the cost of \$65,858.45.
- Fill at High Bridge. During the 1860s, the CNJ's annual reports summarized the progress of the filling in of the "high bridge" for which the town was named:
 - The January 1, 1861 report states that the work to replace the high bridge with an embankment had begun.
 - The January 1, 1862 report states that there was continued progress on the filling in of the high bridge.
 - The January 1, 1863 report states that they were progressing with the filling in of the high bridge.
 - The January 1, 1865 report states that "The high bridge is now so entirely filled up that the tracks rest on earth the entire distance, and the embankment has assumed its permanent character."
 - The January 1, 1866 report states that the embankment at high bridge is finished.
- Black Dan's Cut, Phillipsburg. This area is important as the site where the CNJ "installed the world's first electrically operated semaphore" in 1893.

Additional Information:

The February 9, 1831 charter for the Elizabethtown and Somerville Railroad charter provides some specific language for the path of the new line west of Somerville:

“from one or more suitable place or places in the village of Somerville, northerly, in the most direct and feasible route, to the Musconetcong valley, passing to the north, or within two miles of the village of Clinton, in the county of Hunterdon; thence passing down the valley of the Musconetcong, or any valley northward, to the valley of the river Delaware; thence, up the Delaware, to one or more suitable place or places within two miles of the Easton Delaware Bridge, opposite to the village or borough of Easton, in the State of Pennsylvania.”

The authors of the above description were already aware of the mountains which served as obstacles for the new line in its goal to reach Pennsylvania. The route outlined above eases the obstacles by following the river valley north to Glen Gardner where there is a gap in the mountain range, and then down the Musconetcong valley. Elevations of the area, shown on the attached chart, clearly indicate why the rail line loops north to Glen Gardner and Hampton before winding back south again. A key phrase in the above quoted charter is “the most direct and feasible route”. By avoiding steep inclines, the Elizabethtown and Somerville was not taking the most “direct” route, but certainly the most “feasible” route.

One of the first documented references to the line and its important qualities is included in the company’s 1852 report to stockholders. Describing the portion of the line built in 1848, the report stated that this segment preserved “the same character of road, namely, straight lines, easy curves, and grades of not more than 20 feet to the mile, coming east.” The report proceeds, however, that the segment was more expensive as the “country was more rolling” (CNJ 1852 Annual Report:7). The western most portion of the road was described as presenting “great natural difficulties; as the country is crossed by several ranges of hills, running north and south.” On this portion there were no grades over 21 feet to the mile, coming east and none but easy curves. Cuttings and fillings are very heavy: 1,660,000 cubic yards of earth or rock to be moved; 50,000 cubic yards of masonry built on 28 miles of road (CNJ 1852 Annual Report:7).

In the 1852 report, the company described the portion of the road from Elizabethport to Somerville as having come to them through purchase and that its work was “originally very light and the grades were without difficulty kept very low.” The report proceeded to indicate that the company wanted to remove the only “heavy grade on the whole line” which was 40’ to the mile near Scotch Plains coming east (CNJ 1852 Annual Report:6).

In 1921, the Interstate Commerce Commission described the line’s characteristics as “Going west from Elizabeth, the ground with few local exceptions, gradually raises and may be

Additional Information Cont'd:

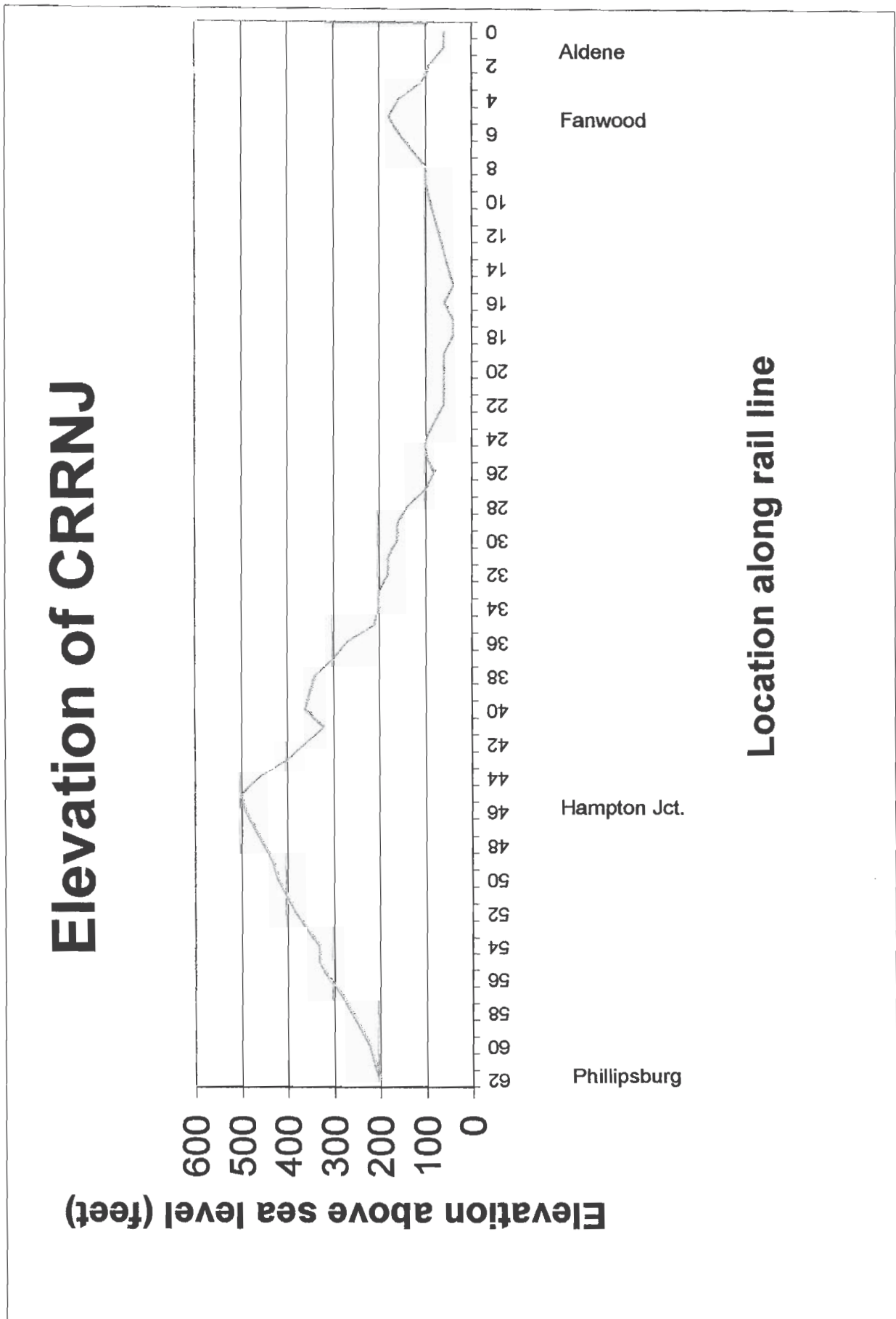
called a light medium rolling country. The country becomes rougher and more broken a short distance west of Somerville, or about half way from Jersey City to Phillipsburg. Westerly from about Lebanon, the general topography is quite rugged and we encounter some heavy fills in approaching the larger streams. This is particularly true at High Bridge. Going from Phillipsburg to Easton, the two principal streams are crossed, the Delaware and Lehigh Rivers. The line crosses these streams very close to their junction." (ICC, February, 1921, p. 13). ... "Common excavation is largely earth and reddish clay through New Jersey, considerably intermixed with small rock fragments in the eastern portion. In the vicinity of Hampton there is considerable granular materials mixed with the clay.... The rock encountered between Elizabeth and High Bridge is largely red shale, medium to hard. High Bridge westerly to around Springtown, medium sandstone predominates. Near Vulcanite the cement rock, medium in hardness, is encountered much of which is thinly laminated. Considerable of the sandstone west of High Bridge is quite massive." (ICC, February, 1921, p. 14).

The ICC report also discusses the grade: "Across New Jersey the maximum eastbound as well ruling grade is about 0.45%. Westbound, the ruling grade is about 1% with a maximum of 1.16%. There are no pusher grades used on westbound freight trains, but on eastbound freights pushers are used from Dunellen to Fanwood summit, over a distance of about 6.5 miles of about 0.40% grade. Also from Phillipsburg to Hampton about 15.5 miles of grade averaging close to 0.40%." (ICC, February, 1921, p. 16).

The ICC report indicates the amount of curve on the line. From Jersey City to Bound Brook Junction, 10% of the line is curved, the maximum degree of center is 2 – 15', and the average degree of curve is 1 – 07'. From Bound Brook Junction to Phillipsburg, 30% of the line is curved, the maximum degree of center is 4 – 00', and the average degree of curve is 3 – 40'. (ICC, February, 1921, p. 17).

Survey Name: NJ Transit Raritan Valley Line Study
Surveyor: Nancy L. Zerbe
Organization: ARCH², Inc.

Date: July, 1999



The railroad experienced great success until the 1920s, when the popularity of the automobile and construction of new roads resulted in a decrease in rail traffic. The onset of the Great Depression worsened the financial condition of both the West Jersey and Seashore and the Atlantic City Railroad, a competing line created in 1889, resulting in the discontinuation of service to some areas. As a result, in 1932 the Pennsylvania, West Jersey and Seashore, Reading, and Atlantic City Railroads were consolidated to form the Pennsylvania Reading Seashore Lines. The consolidation benefitted rail travelers, who enjoyed improved service, and the parent companies, which received one-fourth of the line's profits. The new line suffered from the profit-sharing with the parent companies, however, posting losses in every year save one (1943). By the mid-1950s, construction of the New Jersey Turnpike, GSP, and other highways in the region further reduced traffic on the railroad. In 1976, the Pennsylvania Reading Seashore Line became part of Conrail with passenger service by New Jersey Transit (Heritage Studies, Inc. 1981: 105-110).

According to the cultural resource survey upon which the determination of eligibility was made, the West Jersey and Atlantic Railroad complex consists of "two parallel sites of tracks, seven stations, two power plants, seven sidings, and the connection in Pleasantville with the Shore Fast Line" (Wilson 1980b:27). Specifically, the eligible resources include: the West Jersey and Atlantic Railroad, Reega Station, Reega Power Plant, McKee City Station, McKee City Power Plant, Spruce Street Station in Cardiff, Mount Calvary Cemetery Station in Cardiff, Atlantic City Cemetery Station in Pleasantville, Pleasantville Freight Yard and Station, and the Main Street Junction with the Shore Fast Line in Pleasantville (Wilson 1980b: 67).

9.4 Garden State Parkway

The GSP is a 173-mile, limited-access, parkway extending from the New York State line to Cape May (Plates 9.11 and 9.12). The GSP connects to Route 87 in New York to the north and heads southward through the wooded suburbs of northern New Jersey, roughly paralleling the Hudson River twelve miles to the east. Between Route 4 in Paramus and the Driscoll Bridge over the Raritan River (MP 127.2), the GSP's "Metropolitan Region" -- so named by its designers -- follows a gradually curving route through densely-populated urban and suburban areas. South of the Raritan River, the route of the "Shore Section" heads eastward near Raritan Bay and then turns southward through Monmouth County. Beginning near Red Bank, the route parallels the Atlantic Ocean

shoreline and Route 9 southward toward Cape May and retains an approximate four-mile distance from the shoreline for the duration of the route.

The resource boundary was determined based on the integrity of the resource and its ability to convey its historic significance. The boundary has been chosen to include all the land and features historically associated with the GSP and is defined by the entire right-of-way acquired and developed for the GSP, including all adjoining or contiguous parkway-related features such as the roadway, vegetative and wooded screening areas, service areas, toll plazas, culverts, bridges, picnic areas, maintenance areas, and state police barracks. The boundary is contiguous with the outer (east and west) rights-of-way. The north and south boundaries for the GSP as a whole are the New York state line and the parkway terminus at milepost 0.

The GSP right-of-way is a long, narrow, serpentine ranging from 150 feet to 1,200 feet. North of Route 4 (the original northern terminus), the dual roadways follow the rolling terrain and are generally shielded from the surrounding development by wooded buffers within the right-of-way. A large wooded median separates the roadways. Intersecting roadways and natural features are carried over or under the GSP. Between Route 4 and Union County, the GSP is separated from the surrounding landscape by engineered effects instead of natural effects. In this section the roadway is largely located in a cut with intersecting roadways and pedestrian walkways carried over the roadway. The median contains only guardrails and concrete retaining walls form the walls of the cut. In two sections, Bloomfield and Cranford, the GSP itself is carried above the towns. In both urban instances, the GSP remains disconnected from the surrounding landscape. South of Cranford, the rolling terrain and wooded buffers return. South of the Raritan River through Monmouth County, the terrain is largely flat and open. The GSP travels through the grassy wetlands of Monmouth County. Beginning in Ocean County and continuing southward, the GSP travels through dense pine forests on the coastal plain.

The GSP retains its original status as a scenic pleasure drive, however, at present, it more typically serves as a transportation route for vacationers heading to the shore and as the hoped-for commuter route. These seemingly disparate, dual identities provide the rationale behind the GSP's design. Efforts to keep the GSP scenic co-exist with the efforts to maintain current standards for efficiency and safety.

9.4.1 Garden State Parkway Right-of-Way

The GSP provides a controlled visual experience through the design of the roadway as well as through details such as signage. The vegetative screening sets the GSP apart from the surrounding landscape. The uninterrupted, well-defined linear corridor provides visual and spatial control through the distancing of the driver from the landscape surrounding the GSP (Plates 9.13- 9.15). In the Shore Section and north of Route 4, the driver is generally separated from the surrounding landscape by a dense buffer of vegetation. One notable exception to this rule are the wide-open wetlands of Monmouth County near Keyport, and the Bass River and Mullica River crossings. Here, the raised bridges provide a commanding view of the surrounding landscape. In the Metropolitan Section, the driver is generally separated from outside world by a designed system of retaining walls articulated with reveals (in the cut) that match the bridge piers or by being carried over the surrounding landscape (the multi-span overpasses in Cranford and Bloomfield, for example).

The plantings in the median and in the buffer are also character-defining features of the GSP. A grassy median varies in width from 0 to 600 feet is located in rural and suburban areas. Only a very slim, grassy median remains in limited locations in the urban areas between Union County and Route 4. In certain constrained areas along the length of the GSP the median no longer exists. Roadside and median plantings include trees, shrubs and ground covers indigenous to the Northeast. Trees planted include: American Ash, American Beech, Gray Birch, River Birch, Crabapple, White Cedar, Cherry, Elm, Ginkgo, Golden-rain, Hawthorne, Hemlock, Honeylocust, American Holly, Ironwood, American Linden, Magnolia, Norway Maple, Red Maple, Sugar Maple, Red Oak, Spanish Oak, Willow Oak, White Oak, Peach, Pear, Persimmon, Red Pine, Poplar, Beech Plum, Redbud, Sassafras, Shadbush, Japanese Snowball, Sourwood, Tulip, Weeping Willow. Shrubs include Azalea, Chokeberry, Coralberry, Dogwood, Forsythia, Hazel, Honeysuckle, Inkberry, Laurel, Lilac, Japanese Quince, Rhododendron, Rose, Spicebush, Sumac, Summersweet, Viburnum, and Winterberry. A concerted effort continues to be made by the NJHA to retain or replant pre-existing trees at the time of construction wherever possible.

The coherent, arboreous character of the GSP right-of-way has been diminished in some areas with the intrusions of concrete noise barriers and cellular phone towers. The noise barriers have appeared in locations where traffic is concentrated, such as toll plazas, and where recent residential construction abuts the right-of-way. Efforts have been made to conceal cellular phone towers and

their attendant buildings by placing them within the wooded corridor and by camouflaging them as trees themselves. However, undisguised cellular phone towers and their buildings are also being placed in the median and in plain sight. The service buildings do, however, have a consistent brick construction with low, asphalt-shingled gable roofs.

The visual experience is further controlled through the prohibition of advertising (i.e. billboards), through controlled access, and through standardized signage. Controlled access, a defining feature of parkways and other higher class roadways in general, is an integral part of the GSP. Access is allowed only at designated, non-signalized interchanges. The exceptions to the rule are signalized intersections at Shell Bay Boulevard north of Interchange 8, Stone Harbor Boulevard/Interchange 10, and Crest Haven Road north of MP 11. The standardized signage consists of non-illuminated, reflective green metal signs on Cor-Ten steel poles, on overhead steel structures, or on wood signposts (Plates 9.16-9.18). The rustic design detail of the wood signposts is continued in the placement of timber light poles at service areas and the Arts Center drives.

Cor-Ten guardrails are located throughout the GSP (Plate 9.19). This design provides a “rustic” feel to a vital safety feature. Remnants of the original rustic post and rail guardrails are scattered throughout the GSP alignment, largely in unused or non-safety-critical areas (in picnic areas and to block access from small pre-existing roadways and pathways, for example). These guardrails have low, cast concrete posts and timber rails (Plates 9.20 and 9.21). These original guardrails are also located at intersections with small, pre-existing trails, pathways, and unpaved roads to visually deter pedestrian access.

The uninterrupted, well-defined linear corridor makes it clear that the driver is located on the GSP. The visual control also extends to those who view the GSP from outside the corridor. For drivers on roadways that intersect with the GSP, the GSP is perceived as a distinct, recognizable entity that is readily-distinguished from non-GSP crossings. In most cases, the same bridge treatments used for spans that carry the GSP are used for spans that cross the GSP (Plate 9.22).

The GSP features widely separated, independent roadways with opposing lanes at different levels, yet contained within the same overall right-of-way. In Monmouth County express and local lanes located on separate roadways have been constructed to meet traffic needs. The surface treatment of the two (or more) twelve-foot lanes in each direction is paved and regular. The vertical alignment

of the superelevated roadways is rolling hills through Monmouth County, Union County, and north of Route 4. The vertical alignment is generally flat through the shore region. Throughout the length of the GSP, steep grades and vertical curves have been eliminated. Most grades have a maximum of 3 percent. The horizontal alignment is comprised of gentle curves; straight-aways are minimized.

The earth-moving undertaken during construction of the roadway was undertaken to compensate for steep grades and hollows. In general, the curvature and grade of the roadway vary in response to local conditions. The earth-moving was also undertaken for safety reasons. The earthen mounds or swales present in the median are so designed to reduce headlight glare from oncoming traffic and the possibility of head-on collisions (Plate 9.23 and 9.24). If a car careens off the road into the median, the swale or mound will slow down or stop the vehicle from crossing into oncoming traffic.

Safety was also considered in the construction of the shoulders. The unobstructed, sloping, stabilized, vegetated shoulders with a clear zone allow drivers of disabled cars to pull off the roadway and out of traffic (See Plates 9.13-9.15, 9.19). To aid these motorists, the roadway, for most of the length of the GSP, does not have curbs. Low, mountable concrete curbs are located near guardrails, bridge crossings, interchanges, toll plazas and service areas. Some of these curbs retain their original hand-crafted vertical fluting. Others have been obscured with asphalt paving. While necessary for safety reasons, concrete Jersey barriers comprise a visual intrusion. "Singing" shoulders, better known as "rumble strips," are located along the edges of the roadway to alert motorists straying from the roadway (Plate 9.25).

The roadway is designed for safety, comfort, and speed. The surface treatment is paved and regular. Acceleration and deceleration lanes are provided for entrances, exits, and service facilities in order to smooth traffic flow. Additional, more subtle, safety measures are also tied into the design of the GSP. The minimal curbing acts as a visual cue when a feature warrants the driver's attention, such as a curve in the roadway or a change of grade.

9.4.2 Bridges

In addition to the significant design attention given to the roadway corridor, the GSP's built environment is also a source of character-defining features (Figures 9.2-9.3). Twenty overpass



HPO-G2001-152
PROD
01-2272, Page 1

State of New Jersey

DONALD T. DiFRANCESCO
Acting Governor

Department of Environmental Protection
Division of Parks & Forestry, Historic Preservation Office
PO Box 404, Trenton, NJ 08625-0404
TEL: (609) 292-2023 FAX: (609) 984-0578
www.state.nj.us/dep/hpo

Robert C. Shinn, Jr.
Commissioner

October 12, 2001

Ryan J. Scerbo, Esq.
DeCotiis, Fitzpatrick, Gluck, Hayden & Cole, L.L.P.
Glenpointe Centre West
500 Frank W. Burr Boulevard
Teaneck, NJ 07666

Dear Mr. Scerbo:

As Deputy State Historic Preservation Officer for New Jersey, in accordance with 36 CFR Part 800: Protection of Historic Properties, as published on December 12, 2000, in the *Federal Register* 65(239):77725-77739, I am providing **Consultation Comments** for the following proposed undertaking:

**New Jersey Highway Authority
Garden State Parkway Widening, Interchange 30 to 80
Cultural Resources Investigation
Atlantic County – Somers Point City, Egg Harbor Township, Galloway Township,
Port Republic City, Linwood City
Burlington County – Bass River Township
Ocean County – Little Egg Harbor Township, Eagleswood Township, Stafford
Township, Barnegat Light Borough, Ocean Township, Lacey Township, Berkley
Township, Beachwood Borough, South Toms River Borough**

SUMMARY: The Garden State Parkway is eligible for listing in the National Register of Historic Places (new SHPO Opinion). Efforts to identify archaeological properties remain on going. Although a final effects assessment cannot be offered until all identification efforts have been completed, based on available information it appears that the project as proposed will have an adverse effect on historic properties that have been identified so far.

These comments were prepared in reply to your request for HPO review and comment on the following report:

"Technical Memorandum No. 18, Cultural Resources Investigation, Widening of the Garden State Parkway, Interchange 30 to Interchange 80, Contract 133-572D" prepared for the New Jersey Highway Authority by T&M Associates in conjunction with Richard Grubb & Associates, Inc. (April 2000). HPO accession no. MULT Z 78.

Please note that Attachments A & B referred to in the reviewed report (199 sheets of project plans and a bridge study report respectively) were not transmitted for review and comment.

HPO-G2001-152

PROD

01-2272, Page 2

800.3 Initiating Consultation

I concur with the APE delineated in your submission for both architecture and archaeology. However, if wetlands mitigation areas are required through the permitting process, then the APE for the project should be expanded to include these areas. Once specific locations have been chosen, then a qualified team of professionals should determine whether or not they contain historic properties. If historic properties are identified, then they should be dealt with according to 36 CFR Part 800.

Your standard public involvement plan (as represented by Resolution 87-138) should be tailored to the specifics of the Section 106 process and the Interchange 30 to 80 widening project. A list of suggested members of the interested public was appended to my December 30, 1999 memorandum to NJDEP-Land Use Regulation and is included in Appendix F in the reviewed Grubb report.

This project will involve permitting, or permitting oversight, by several federal agencies: Army Corps of Engineers (USACOE), Environmental Protection Agency (EPA), and Coast Guard. It is my understanding that the USACOE has agreed to act as lead federal agency for this undertaking.

800.4 Identification of Historic Properties

Archaeological Resources

I concur with the submitted report that the following previously identified historic property is within the Area of Potential Effect (APE) for the proposed project: Mullica River/Chestnut Neck Archaeological Historic District (Atlantic County – Galloway Township, and Burlington County – Bass River Township), listed in the New Jersey Register of Historic Places on 10/1/76.

The report presents an acceptable level of background investigation, field inspections, and site location modeling to support its recommendations for Phase IB field survey. I agree with the plan presented in the report to focus the effort to identify archaeological historic properties in the selected high potential areas listed on pp. 8-3 to 8-6. The plan to conduct a Phase IB field investigation with shovel testing at 95 locations for Native American archaeological remains and seven locations for historic remains is acceptable. The intensity of shovel testing proposed at each of the locations is also acceptable. Consultation regarding the identification of NJRHP/NRHP eligible archaeological resources should continue when the field survey findings are available.

Architectural Resources

I concur with the submitted report that the following previously identified historic resources are within the APE for the proposed project:

- Port Republic Historic District (Atlantic County – Port Republic City), listed in the New Jersey Register of Historic Places 4/1/91, listed in the National Register of Historic Places 5/16/91; and
- West Jersey and Atlantic Railroad Historic District (Atlantic County – Hamilton Township to Pleasantville City), SHPO Opinion 8/28/96; and
- U.S. Route 9 Bridge over Bass River, Str. #0302150 (Burlington County – Bass River Township), SHPO Opinion 10/29/92.

HPO-G2001-152

PROD

01-2272, Page 3

It is my opinion, in concurrence with the submitted report, that the **Garden State Parkway (GSP) is eligible to be listed in the National Register of Historic Places** as a historic district under Criteria A and C. The boundaries of the eligible Garden State Parkway Historic District include all land and features historically associated with the GSP and is defined as the entire right-of-way acquired and developed for the GSP within the period of significance, including all contiguous parkway-related features such as: the roadway, vegetative and screening areas, service areas, toll plazas, culverts, bridges, picnic areas, maintenance areas, and state police barracks. The boundary is contiguous with the outer (east and west) rights-of-way (150 to 1,200 feet variable). Contributing and non-contributing resources outside of the 30 to 80 project would require additional work to delineate as part of future projects. The north and south boundaries for the GSP as a whole are the New York state line and the GSP terminus at milepost 0. The period of significance for the Garden State Parkway Historic District is 1945-1957. For additional information about the eligible Garden State Parkway Historic District, such as an expanded statement of significance and a list of character defining features, please consult Attachment A.

In addition to the West Jersey and Atlantic Railroad Historic District, the GSP 30 to 80 project intersects four other railroad rights of way:

1. The Camden and Atlantic Railroad (currently active as the NJT Atlantic City Line), which has been previously identified as potentially eligible for National Register listing
2. The Philadelphia and Atlantic City Railroad (currently inactive) which has been previously identified as potentially eligible for National Register listing. However, an exhaustive examination of topographic maps has not lead to a precise identification of the location of this crossing. Therefore it seems unlikely that there are remnants of this line with the integrity to convey significance. No further efforts regarding this railroad line are recommended as part of this project.
3. the Tuckerton Railroad (crossing at approximately MP 69.7), currently unevaluated
4. The eastern extension of the Camden and Burlington County Railroad, the former Pennsylvania Railroad line from Camden to Seaside Park and Bay Head Junction (crossing at MP 80), also currently unevaluated.

800.5 Assessment of Effects

Although a final effects assessment cannot be offered until all identification efforts have been completed, based on available information it appears that the project as proposed will have an adverse effect on the Garden State Parkway Historic District for the following reasons:

- The project proposes the demolition, or substantial alteration through widening, of 81 contributing bridges within the APE. The reviewed Grubb report characterizes contributing bridges within the GSP Historic District according to the following types:
 - (1) the major waterway crossings, 2/4 of which are in the APE – GSP over the Mullica River (an 8 span riveted plate girder bridge) and the GSP over the Bass River (a 7 span rived plated girder bridge);
 - (2) stone faced reinforced-concrete rigid-frame bridges – all 25 bridges of this type lay outside the APE for the widening project;
 - (3) reinforced-concrete rigid-frame bridges without stone facing – both bridges of this type lay within the APE;
 - (4) rolled steel girder bridges – of which there are 271 in the HD overall (one list containing all of the rolled steel girder, riveted plate girder, and welded plate girder bridges within the APE can be found on pp. 9-47 and 9-54 of the reviewed report);
 - (5) riveted plate girder bridges – of which there are 17 in the HD overall (one list containing all of the rolled steel girder, riveted plate girder, and welded

HPO-G2001-152
 PROD
 01-2272, Page 4

- plate girder bridges within the APE can be found on pp. 9-47 and 9-54 of the reviewed report);
- (6) welded plate girder bridges – of which there are 34 in the HD over all (one list containing all of the rolled steel girder, riveted plate girder, and welded plate girder bridges within the APE can be found on pp. 9-47 and 9-54 of the reviewed report);
 - (7) pre-stressed concrete girder bridges (a listing of all of the concrete bridges within the APE can be found on pp. 9-54 and 9-55 of the reviewed report);
 - (8) reinforced concrete beam bridges (a listing of all of the concrete bridges within the APE can be found on pp. 9-54 and 9-55 of the reviewed report);
 - (9) small concrete slab bridges – 6/7 of which are within the APE (a list of the concrete slab bridges within the APE can be found on page 9-55 of the reviewed report);
 - (10) concrete pipes, corrugated metal pipes and reinforced concrete box culverts – 24 of which are located within the APE (see list on page 9-58 of the reviewed report).
 - (11) concrete box girder – these are largely replacement structures and do not contribute to the character of the historic district.
- The project also proposes widening the Garden State Parkway into the median. The median is an original design feature of the GSP, and is a character-defining feature of the GSP Historic District. The median, as well as the plantings and berms that it contains, exemplifies the scenic pleasure drive character of the GSP which distinguishes it from many of its contemporary expressways. The precise extent and impact of this loss can only be quantified after a detailed review of the preliminary project plans.

The Interchange 30 to 80 widening project will have no adverse effect on the Port Republic Historic District. Although the project will not result in any physical or visual changes to the district, the increased width and traffic can be presumed to result in additional noise. However, this audible effect will not be adverse to the district as there is already substantial noise generated from the existing GSP.

The proposed project will have no adverse effect on the Mullica River/Chestnut Neck Archaeological Historic District. One known contributing element to this historic district is several hundred feet distant from the margin of the APE. Within the APE, previous disturbance to the river bed and river banks indicate low potential for surviving contributing elements.

The Interchange 30 to 80 widening project will have no adverse effect on the West Jersey and Atlantic Railroad Historic District. The GSP will continue to span this eligible railroad corridor, as shown on plan sheet 24 of 199 dated April 2000.

The Interchange 30 to 80 widening project will have no adverse effect on the potentially eligible Camden and Atlantic Railroad. The GSP will continue to span this eligible railroad corridor, as shown on plan sheet 37 of 199 dated April 2000.

Additional information is required to assess effects to the Tuckerton Railroad and the Camden and Burlington County Railroad. Please send the appropriate plan sheets at your earliest possible convenience.

The Interchange 30 to 80 widening project will have an adverse effect on the U.S. Route 9 Bridge over the Bass River. However, it is anticipated that the New Jersey Department of Transportation (NJDOT) will complete their planned bridge replacement project before GSP construction begins in this vicinity. The NJDOT has already completed Section 106 consultation on this project which resulted in a fully executed Memorandum of Agreement.

HPO-G2001-152
PROD
01-2272, Page 5

All efforts to avoid or reduce harm should be explored and documented for future consultation.

Proposed Mitigation Measures

The reviewed report proposes several mitigation measures, including:

- The establishment of a Programmatic Agreement to establish parameters for future projects, thereby facilitating future permits.
- Design mitigation – this would ensure that new bridges, as well as the additions to existing bridges, would be compatible with the character of the historic district in terms of design, scale, massing, and materials. Design mitigation may also address landscape issues through a planting plan.
- Recordation – HAER documentation for the structures and setting/character of the GSP similar to the work done for the Merritt Parkway or Taconic State Parkway.
- Installation of historical markers along the parkway and or informational exhibits in the service areas.
- The production of a television ready video production about the history of the GSP including: its design, construction, and impact on the development of the New Jersey shore region.

All of these measures merit further exploration as part of continued consultation.

Report Comments

I would like to take this opportunity to compliment your cultural resource consultant on an extremely well researched and well-written report. The 21-page bibliography is particularly impressive and will be a valuable tool for future researchers. No changes or corrections are recommended.

If you have any questions regarding this letter, please contact Andrea Tingey regarding architecture at (609-984-0539) or Mike Gregg regarding archaeology at (609-633-2395). Thank you.

Sincerely,



Dorothy P. Guzzo
Deputy State Historic
Preservation Officer

cc Charles McManus, NJ Highway Authority
Michael Luchkiw, DeCotiis, Fitzpatrick, Gluck, Hayden & Cole, L.L.P.
Andrew Bayer, DeCotiis, Fitzpatrick, Gluck, Hayden & Cole, L.L.P.
Sam Reynolds, USACOE
Michael Hayduk, USACOE
Ann Deaton, USCG

HPO-G2001-152
PROD
01-2272, Page 6

Waverly Gregory, USCG
Bob Montgomerie, EPA
Barry Brady, Pinelands Commission
Miriam Crum, NJDOT-CPM
Janet Wolfe, New Jersey Coastal Heritage Trail
Andy Heyl, NJDEP-LUR
Michael Garrity, NJDEP-LUR
Andras Fekete, NJDOT-BES
Amy Fox, FHWA

c:\sikra\GSP.106.doc

HPO-G2001-152
PROD
01-2272, Page 7

ATTACHMENT A

Statement of Significance for the Garden State Parkway Historic District

The Garden State Parkway (GSP) is significant in the areas of transportation, engineering and landscape architecture. It meets National Register Criteria A and C.

Criterion A – for its association with events that have made a significant contribution to the broad patterns of our history – New Jersey Shore Development

The GSP was largely responsible for the transformative suburban and commercial development of the New Jersey shore region. Indeed one of its intended purposes was to stimulate economic development in this region as well as attract increasing numbers of shore visitors.

1948 marked an economic downturn for shore businesses, this was reversed in 1954 by the opening of the GSP to Cape May.

In 1958 the New Jersey Highway Authority commissioned a study of the economic effect of the highway. The report concluded that retail sales, population, tax rates, home construction, and other measures of economic growth had increased more rapidly in the region contiguous to the GSP than in other parts of the State. Indeed between 1955 and 1957 all ten counties through which the GSP traveled enjoyed an increase in rates which was four times that experienced by other counties.

Ocean County saw its population double every ten years from 1950 to 1970.

Criterion C – as the embodiment of the distinctive characteristics of a type, period or method of construction – the transition in highway design from landscaped parkway to interstate highway

It is a well-defined example of the evolution of parkway design that by the 1950s blended the original 1920s/1930s concept with the needs of the modern superhighway.

The dual nature of the GSP is indicative of its transitional design. It is both scenic pleasure drive and high-speed expressway. It is distinguished from its parkway predecessors by its high speed orientation, as much as it is distinguished from its interstate counterparts by its artfully designed landscape.

The GSP is placed in the context of the 'second toll road era' in the United States. From 1940-1958, many toll roads were built across the county by special authorities created by state governments. Revenue bonds and toll income were substituted for monies absent from public coffers after a decade of economic depression and world wide war.

Buildings for service areas, police barracks, and toll plaza administration facilities are generally one-story domestic design that was compatible with the "New Jersey Colonial" design principle. Predominately of brick or wood frame construction.

List of Character Defining Features for the Garden State Parkway Historic District

- Bridges and Culverts
- Retaining Walls
- Grassy Median (0-600 feet variable)

HPO-G2001-152
 PROD
 01-2272, Page 8

- Controlled access, with non-signalized interchanges
- Prohibition of advertising
- Standardized signage
- Surviving examples of original guiderail system – cast concrete posts with timber rails
- Picnic Areas
- Vertical alignment of roadway, maximum 3% grade
- Gently curving horizontal roadway alignment
- Horizontal curves are super-elevated
- Earthen berms in median
- Shoulders
- Extremely limited use of curbing
- Dull sage green paint color
- Pull overs with roadside telephone booths
- Acceleration and deceleration lanes provided for entrances, exits, and service facilities
- Toll plazas
- Toll plaza administration facilities
- State Police barracks and maintenance areas
- Predominantly Indigenous Vegetative Screening; in median and buffer areas
 - *Trees:* American Ash, American Beech, Gray Birch, River Birch, Crabapple, White Cedar, Cherry, Elm, Gingko, Golden-rain, Hawthorne, Hemlock, Honeylocust, American Holly, Ironwood, American Linden, Magnolia, Norway Maple, Red Maple, Sugar Maple, Red Oak, Spanish Oak, Willow Oak, White Oak, Peach, Pear, Persimmon, Red Pine, Poplar, Beech Plum, Redbud, Sassafras, Shadbush, Japanese Snowball, Sourwood, Tulip, Weeping Willow.
 - *Shrubs:* Azalea, Chokeberry, Coralberry, Dogwood, Forsythia, Hazel, Honeysuckle, Inkberry, Laurel, Lilac, Japanses Quince, Rhododendron, Rose, Spicebush, Sumac, Summersweet, Viburnum, Winterberry.

List of Non-Contributing but Compatible Features in the Garden State Parkway Historic District

- Cor-Ten guiderails
- Wildflower beds

List of Intrusions in the Garden State Parkway Historic District

- Loss of median in congested areas
- Concrete noise barriers
- Cellular telephone towers and their service buildings
- Jersey barriers

5.0 IDENTIFICATION OF PROPERTIES

During June 2003, Richard Grubb & Associates conducted an identification/evaluation-level architectural survey for the proposed collocation site. The results of the survey are discussed below.

5.1 Historic Context

The Garden State Parkway Historic District is located 200 feet west of the proposed collocation site at 25 Commerce Drive. Significant in the areas of transportation, engineering, and landscape architecture (National Register Criteria A and C), the State Historic Preservation Officer (SHPO) recommended the Garden State Parkway as eligible for listing on the National Register of Historic Places as a linear historic district in October 2001 (SHPO Opinion 10-12-01: 7).

Opened to Cape May in 1954, the Garden State Parkway greatly contributed to the suburban and commercial development in the New Jersey shore region. Better access helped to reverse the economic downturn of shore businesses that began in 1948 by stimulating economic development and attracting shore visitors. A study of the economic effect of the highway commissioned in 1958 by the New Jersey Highway Authority concluded that economic growth increased more rapidly, as much as four times more than other parts of the state, in the ten counties through which the Garden State Parkway traveled. Additionally, the population of Ocean County doubled every ten years from 1950 to 1970 (SHPO Opinion 10-12-01: 7).

As a toll road, the Garden State Parkway falls into the "second toll road era" (1940-1958). During this time many toll roads were built in the United States by special authorities to generate income for state governments with diminished monies following years of economic depression and World War Two (SHPO Opinion 10-12-01: 7). Moreover, the Garden State Parkway represents a transitional period in the evolution of parkway design. It combined the 1920s/1930s scenic pleasure drive concept with the needs of a high-speed expressway. The designed landscape included predominately brick or wood frame service buildings that followed the "New Jersey Colonial" design principles, as well as gently curving roadways lined with indigenous vegetative screening in medians and as buffers (SHPO Opinion 10-12-01: 7).

5.2 Architecture

The objective of the architectural survey was to locate and identify through research and in-field investigation all structures and sites over 50 years of age, to evaluate their eligibility for listing on the National Register of Historic Places, to identify those buildings or sites already listed on the State and National Registers of Historic Places, and to assess the effects of the project on any eligible resources located within the APE according to the Criteria of Adverse Effect (36 CFR 800.5(a)(1)). The survey included delineation of an APE that encompasses the area in which effects related to the project can be reasonably expected. Research was conducted at the HPO to locate known historic resources, to review cultural resources surveys, and to identify previously

surveyed resources in the New Jersey Historic Sites Inventory: Township of Cranford, Union County (Office of Cultural and Heritage Affairs, County of Union, New Jersey 1986).

A review of the files at the HPO revealed that one linear historic district exists within the 500-foot suggested search radius that is eligible for listing on the State and National Registers of Historic Places. The Garden State Parkway Historic District, recommended as eligible for listing on the National Register, is located within the viewshed and approximately 200 feet west of the proposed collocation site (SHPO Opinion 10/12/01). Three additional National Register eligible linear historic districts are located within one mile of the proposed collocation site but do not lie within the APE and include the Rahway River Park Historic District approximately 2000 feet to the southwest, the Staten Island Railroad Historic District approximately 4000 feet to the north, and the Central Railroad of the New Jersey Mainline Historic District approximately 5000 feet to the north (SHPO Opinion 9/18/02; 6-1-91; 2/27/95). No cultural resource surveys have been conducted within the suggested search radius. Additionally, no historic resources were identified by the Union County historic sites survey within the APE or within the suggested search radius (Union County Office of Cultural and Heritage Affairs 1986).

The function, commuter use, was the basis of the design for the northern section of the Garden State Parkway Historic District. Known as the "metropolitan section," the northern New Jersey segment of the Parkway is comparable to other expressways built in the 1950s. Roughly one half of one mile (Mile 137.59 to just beyond mile 138) of the Garden State Parkway lies within the viewshed of the proposed collocation site. At this point, ten 12-foot wide lanes (five in each direction) follow rolling terrain at grade as the roadway curves through the densely populated suburban area of Cranford Township. Indigenous wooded buffers separate the Parkway from adjacent communities while a concrete Jersey barrier takes the place of the landscaped median found north and south of this section (T&M Associates 2000: 3, 7) (Plate C.5.1 and C.5.2). The stone bridge that carries Centennial Avenue over the Garden State Parkway at Mile 137.59 is a character-defining feature of the historic district (Plate C.5.3).

During the current architectural survey, the majority of the properties identified within the APE are less than 50 years of age. The area surveyed is mainly east of the Garden State Parkway Historic District and principally consists of large-scale office buildings and warehouses constructed of modern materials, up to four stories in height, separated by expansive paved parking lots, and manicured grounds and medians (Plates C.5.4-C.5.6). Developed in the last 50 years, the office park encompasses buildings on Commerce Drive, Jackson Drive, and Moen Avenue and includes a hotel and a nursing home as well.

Where Commerce Drive and Moen Avenue intersect with Raritan Road, the gap created by the cross streets allows for a view of 25 Commerce Drive. The building stock along Raritan Road is mainly comprised of single-family dwellings constructed in the early to mid-twentieth century. One modern brick bank and a commercial building that is more than 50 years of age are also located within the APE along Raritan Road (Plate C.5.7).

Residences fronting Raritan Road that lie within the viewshed of the proposed collocation site are vernacular interpretations of the following architectural styles: Ranch, Minimal Traditional,

Craftsman, Split Level, and Gothic Revival (Plates C.5.8-C.5.14). Minimal Traditional brick dwellings are, by far, the most common housing style along Raritan Road.

The architectural survey revealed a collection of buildings more than 50 years of age along Raritan Road that represents an area of circa 1945 development. However, these Minimal Traditional dwellings are located along a street also lined with an eclectic mix of early and mid-twentieth century houses that do not maintain the continuity of post-war development. Overall, the buildings lack the scale, age, and quality of architectural stock that would make them eligible for listing on the National Register of Historic Places either individually or as part of a district. The proposed collocation will not have an effect on these properties because they are not National Register eligible.

A substantial portion of the APE, mainly along the Garden State Parkway Historic District corridor, will only fall within the viewshed of the proposed collocation site during the winter months when the dense deciduous trees in the area lose their leaves. Still the undertaking will not cause an adverse effect on the Garden State Historic District because the proposed collocation of antennas will take place upon an existing modern and non-contributing office building, 25 Commerce Drive, which already lies within the viewshed of the historic district.

NEW JERSEY DEPT. OF ENVIRONMENTAL PROTECTION
HISTORIC PRESERVATION OFFICE
HISTORIC DISTRICT SURVEY FORM

HISTORIC SITES INVENTORY NO. 47

DISTRICT NAME: Lehigh Valley Railroad Line
MUNICIPALITY: South Plainfield
COUNTY: Middlesex
TYPE OF DISTRICT: Railroad corridor
USGS QUAD: Plainfield

UTM REFERENCES:

Zone/Northing/Easting

A
B
C
D

DESCRIPTION: (General description of district as a whole and boundaries)

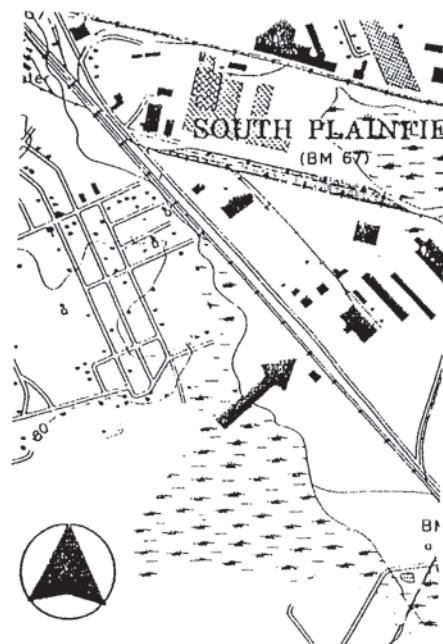
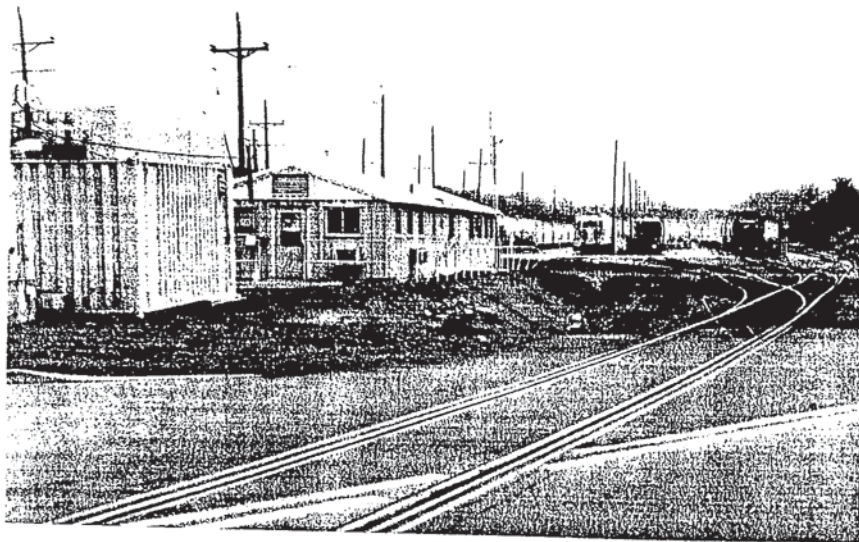
The Lehigh Valley Railroad Line consists of several components within South Plainfield, including: portions of the original main line which ran between Phillipsburg, New Jersey and Perth Amboy, New Jersey; portions of the 1888 line which connected the main line in South Plainfield with the Central Railroad of New Jersey in Roselle; the interlocking tracks between these two lines; the South Plainfield freight and coal yard (located at the intersections of Metuchen Road and Kentile Road near the juncture of the two lines); and numerous industrial sidings. The Lehigh Valley's lines through South Plainfield are indicated on the attached 1958 County road map.

The freight yard, running parallel to Kentile Road, is still in use by Conrail. The only structure on the site is a modern metal clad structure with a gable roof (see photograph).

The coal storage yard, which ran east from the freight yard for about a mile (see attached map), was abandoned in 1933. The circular pattern of the coal storage area can be seen in early twentieth century aerial photographs.

Character defining features include:

- numerous at grade crossings throughout the APE
- industrial sidings
- multi-track yard



APPROXIMATE NUMBER OF BUILDINGS: 1
PHYSICAL CONDITION OF STRUCTURES: Excellent 1 % Good 0 % Fair 100 % Poor 0 %
REGISTER ELIGIBILITY: Yes ☒ Possible ☐ No ☐
THREATS TO DISTRICT/LOCAL ATTITUDES:

COMMENTS:

SIGNIFICANCE:

The Lehigh Valley Railroad is eligible for inclusion in the National Register of Historic Places under National Register Criterion A for its statewide significance in transporting coal from the Pennsylvania coal fields to the New York market and for its local significance in leading to the industrial development of both Perth Amboy and South Plainfield.

The Lehigh Valley Railroad Company was incorporated April 21, 1846 by a Special Act of the Legislature of Pennsylvania as the Delaware Lehigh Schuylkill and Susquehanna Railroad, began constructing its line in 1852, and ran its first coal train in 1855. Its line ran as far east as Phillipsburg, New Jersey where it connected with three other rail lines: the Central Railroad of New Jersey, the Bel-Del, and later the Morris and Essex.

However, the December 31, 1868 merger of the Delaware Lackawanna and Western and the Morris and Essex Railroads alarmed both the rivaling Central Railroad and the Lehigh Valley Railroads. Asa Packer, president of the Lehigh Valley Railroad, quickly initiated steps to develop his own line to the New York port. In the 1871 Annual Report of the Board of Directors, Packer reported that "Our Coal Trade has suffered for some years from the want of an independent outlet to tide water."

To remedy this problem, the Lehigh Valley leased the Morris Canal, attempted to buy the New Jersey West Line Railroad, acquired the unbuilt Bound Brook & Perth Amboy charter, and obtained a new charter for the Easton-to-Bound Brook line. The company's 1876 annual report indicated that on May 28, 1875, the Lehigh Valley Railroad made its first shipment of 125 cars of coal from Phillipsburg to Perth Amboy over its new line, and that on June 28, the line was formally opened for traffic and travel as the New Jersey Division. The report also indicated that to date they had shipped to the New Jersey Division 397,371 tons of coal, of which 348,992 tons went to Amboy for shipment. Only a year later, the Company's annual report (dated January 6, 1877) reported that they had shipped 881,459 tons of anthracite coal and that they had completed double tracking the line, and building sidings, station-houses etc., "furnished for the proper working of the line."

The Company's annual reports continued to reflect their prominent role in transporting Pennsylvania coal to the New York market. In January, 1878, the Company reported that their business had greatly increased and that it had "equaled our most sanguine expectations - 1,405,508 tons of coal having been carried over that road last year, being a gain of 524,049 tons, or about 59.5 per cent over 1876." Ten years later, the coal tonnage reports show another significant increase: 6,824,321 tons of anthracite coal and 59,636 tons of bituminous coal for a total of 6,883,957 tons. Within another four years, the numbers had again significantly increased: 10,332,954 tons of anthracite coal; 265,847 tons of bituminous coal; and 5,230,913 tons of miscellaneous freight.

SEE ATTACHED CONTINUATION SHEETREFERENCES: (Include representation in existing surveys)

- Lehigh Valley Railroad Company minutes and records, Hagley Library, Greenville, DE, and Alexander Library, Rutgers Un.
- Archer, Robert F. The History of the Lehigh Valley Railroad, "The Route of the Black Diamond", 1977.
- Baird, D.G. "A Narrative of Some of the Events Connected with the Building by the Lehigh Valley Railroad Company of its Railroad Lines to, and its Terminal at, New York Harbor", 1915.
- DeLeuw, Cather and Company. Coordination and Consolidation of Freight Services in the Northern New Jersey Area for New Jersey Department of Transportation, Volume I, Preliminary Report, July, 1973.
- Greenberg, William T., Jr. and Robert F. Fischer. The Lehigh Valley East of Mauch Chunk, 1997.
- Mead, Charles A. New Jersey's Relation to the Port of New York. Fourth Preliminary Report of the New Jersey Harbor Commission, February, 1914.
- Middlesex County Industrial Department. "Middlesex County", 1941.
- 1930/32, 1940, 1951, 1954, and 1962 aerial photography
- Middlesex County road map, 1958

ATTACHMENTS: (Indicate number)

MAPS: 2 PHOTOS: 1 SLIDES: _____

OTHER: (Specify)

RECORDED BY: Nancy L. Zerbe

ORGANIZATION: Nancy L. Zerbe Historic Preservation Consulting, Inc.

DATE: August, 1998 / revised July, 2001

LEHIGH VALLEY RAILROAD LINE STATEMENT OF SIGNIFICANCE – CON'T

As the line through South Plainfield grew, so did South Plainfield's role. In 1876, the Company built "a small passenger station at South Plainfield." Within 13 years of building the main line to Perth Amboy, the Lehigh Valley reported that "in order to reduce the distance by our line between the Delaware River and Jersey City, and to carry traffic a larger part of the distance on our own rails, we have promoted the construction of a new line, under the charter of the Roselle and South Plainfield Railroad Company, the capital stock of which is controlled by this Company." The new line would run 10 miles between South Plainfield on the Easton & Amboy Railroad to Roselle on the Central Railroad of New Jersey, and then the Lehigh Valley would use the Central to Jersey City under a "traffic contract" with the Central. In January, 1889, the Company reported that the new line was open to freight traffic as of December 17, 1888.

In 1887, the Company reported that "a new station building, comprising passenger waiting-room, freight house, and dwelling, has been erected, at South Plainfield." This station building was replaced by a new structure in 1895. But, the most significant step in terms of South Plainfield's role in the Lehigh Valley Railroad came in 1891 when the Board of Directors reported that "Owing to the value of lands and cost of storage facilities at tide shipping points a large tract of land has been procured at South Plainfield, New Jersey, adjacent to our tracks, at the junction of the Easton and Amboy line leading to Perth Amboy with our new line leading to New York Harbor."

A major change in the administration of the rail line occurred in 1892. The 1/17/1893 annual report of the Lehigh Valley Railroad Company indicated that "On February 11, 1892, the railroads, public works, transportation lines, and appurtenances of this company were leased and transferred to the Philadelphia and Reading Railroad Company for the full period of 999 years from the first day of December, 1891." The Philadelphia and Reading reported that acquisition of the Lehigh Valley "enables the Reading Company to compete with other trunk lines for the large volume of west-bound traffic from the eastern centres of trade and industry."

The Lehigh Valley Railroad Company's January, 1897 Annual Report listed "the coal yards and stocking grounds owned and controlled by our railroad and coal companies, together with their respective tonnage capacities". Of the 25 yards listed, only four had a tonnage capacity of 100,000 tons or more: South Plainfield (310,000); Perth Amboy (200,000); Buffalo, Cheektowaga Trestle (163,000); and West Superior Dock (100,000).

In 1927, the Lehigh Valley constructed additional tracks to facilitate efficient handling of business installed at numerous locations, including South Plainfield. The Lehigh Valley Railroad Company was reported to have "barely survived the depression. Increased revenues brought on by World War II only put off the inevitable. In 1960, passenger service was ended and in the 1970s, the Lehigh Valley became one of the six bankrupt railroads joined together to form the Conrail system."

The Lehigh Valley Railroad was instrumental in the industrial development of both Perth Amboy and South Plainfield. Wall and Pickersgill's county history stated that "The real industrial life of Perth Amboy began with the decision of the Lehigh Valley Railroad Company to make the city its tidewater terminus. Coal wharves were erected, and in 1876 the shipment of anthracite coal to eastern and foreign ports was commenced. After a few years the shipments of coal aggregated more than 2 million tons annually, and for a long time the total amount handled has been in excess of that amount. The coming and going of coal carriers brought other industries to the awakened city."

The Lehigh Valley Railroad's impact on South Plainfield was even more prominent. Local histories generally acknowledge the dramatic impact that the railroads had on developing South Plainfield.

Larry Randolph wrote "...if anything has left its mark upon this town, it is the railroad. Because of the railroad, South Plainfield is what it is today." (Randolph, 1981). A 1951 Courier News article on South Plainfield's industries stated "To the Lehigh Valley Railroad has been given much credit for aiding industrial development and job opportunities. In the beginning the railroad itself provided jobs. Later its main line brought materials to be fabricated, and it carried away the finished product. Along its right-of-way, the railroad acquired desirable parcels of land which later were sold to manufacturing companies and home developers. South Plainfield became an important stop for freight and express." (Courier News, 1951).

In 1912, the Spicer Manufacturing Company moved from Plainfield to South Plainfield, attracted by the proximity of the Lehigh Valley Railroad. Other industries which followed include the Rock Wool Corporation and Harris Steel (South Plainfield Jaycee-ettes, 1977, p.5). Throughout the twentieth century, areas in South Plainfield located near the Lehigh Valley Railroad (along Hamilton Boulevard, Metuchen Road, Kentile Road, and Park Avenue) continued to experience industrial development. A 1941 publication by the Middlesex County Industrial Department described South Plainfield as "Among its industrial advantages are an unlimited water supply, rapid rail facilities, labor of all types in large numbers and consistently low tax and insurance rates." A 1963 article in the Courier-News described the continued industrial development in South Plainfield in an article entitled "South Plainfield Big Contributor to Middlesex Development." (Courier-News, 1963). The new and/or expanded industries were in close proximity to the Lehigh Valley Railroad, along S. Clinton Avenue, Oak Tree Avenue, Kentile Road, Metuchen Road, Teeple Street, and Hamilton Boulevard.

In a 1973 freight study, South Plainfield Yard is described as "a receiving yard for traffic bound for Perth Amboy as well as industries on the Main Line between Read Valley and Clark...Crews also switch 15 local industries within yard limits." (DeLeuw, Cather)

In summary, the Lehigh Valley Railroad is eligible for inclusion in the National Register of Historic Places under Criterion A for its statewide significance in transporting coal from the Pennsylvania coal fields to the New York market and for its local significance in leading to the industrial development of both Perth Amboy and South Plainfield. The line's period of significance runs from 1875 when the first shipment was sent to Perth Amboy to 1951. Although the line was in service beyond 1951, its use past that time does not meet the test for "exceptional significance" for resources less than fifty years old.



State of New Jersey

es E. McGreevey
GovernorDepartment of Environmental Protection
Division of Parks & Forestry, Historic Preservation Office
PO Box 404, Trenton, NJ 08625
TEL: (609) 292-2023 FAX: (609) 984-0578
www.state.nj.us/dep/hpoBradley M. Campbell
CommissionerMarch 15, 2002
HPO-C2002-151 PROD
Log #02-1100Mr. Andras Fekete
Manager
Bureau of Environmental Services
New Jersey Department of Transportation
1035 Parkway Avenue
P.O. Box 600
Trenton, New Jersey 08625-0600

Dear Mr. Fekete:

As Deputy State Historic Preservation Officer for New Jersey, in accordance with 36 CFR Part 800: Protection of Historic Properties, as published in the *Federal Register* on December 12, 2000 (65 FR 77725-77739), I am providing Continuing Consultation Comments for the following proposed undertaking:

**Helen Street Extension Project
Borough of South Plainfield, Middlesex County.**

These comments are in response to your submission of the following documents, received at the Historic Preservation Office (HPO) on February 15, 2002:

Cultural Resources Investigation, Helen Street Extension Project, prepared by Nancy L. Zerbe Historic Preservation Consulting, Inc. and Richard Grubb and Associates, Inc. and dated April 1999;

Phase IB Archaeological Investigation, Helen Street Extension Project, prepared by Richard Grubb and Associates, Inc. and dated July 2001; and

Effects Assessment Report for Historic Architectural Resources, Helen Street Extension Project, prepared by ARCH2, Inc. (Nancy Zerbe) and dated August 2001.

Summary: Two (2) new historic properties have been identified. The currently proposed project will have **no adverse effect** on historic properties if conditions described in this letter are fulfilled.

800.4 Identification of Historic Properties

Adequate effort to identify archaeological historic properties has been undertaken and the archaeological survey report (MID F 578a) is acceptable as submitted. No further archaeological survey is recommended for this project as currently proposed.

I concur with the submitted cultural resources report that both the **Lehigh Valley Railroad** and the **Port Reading Railroad** are eligible for inclusion in the National Register of Historic Places (NRHP) as linear historic districts under Criteria A and C. Both NRHP eligible resources extend beyond the Area of Potential Effects of the proposed Helen Street Extension Project.

The **Lehigh Valley Railroad** follows a route from Phillipsburg (Warren County) New Jersey east across the state to Jersey City in Hudson County. At South Plainfield, the original main line right of way, now partially active and partially out of service, continues to Perth Amboy.

The **Lehigh Valley Railroad** is eligible for the NRHP at the state level of significance for its role in providing access to New York markets (and the Port of New York and New Jersey) for the Lehigh Valley Railroad, a major inter-state carrier of anthracite coal. The Lehigh Valley Railroad is also eligible for the NRHP at the local level of significance for its contribution to the industrial development of South Plainfield and other Middlesex County communities such as Perth Amboy. The report indicates that the Lehigh Valley Railroad "consists of several components within South Plainfield including portions of the original main line which ran between Phillipsburg and Perth Amboy, portions of the 1888 line which connected the main line in South Plainfield with the Central Railroad of New Jersey in Roselle, the interlocking tracks between these two lines, the South Plainfield freight and coal yard, and numerous industrial sidings." Previous HPO comments for another project identified the Lehigh Valley Oak Island Yard Historic District in the City of Newark, Essex County as a historic property eligible for the NRHP. This historic property would also contribute to and be a part of the Lehigh Valley Railroad Historic District.

The **Port Reading Railroad** is approximately twenty (20) miles long. Historically, the railroad began in Bound Brook (Somerset County) and terminated at Port Reading (Woodbridge Township) where the railroad constructed and maintained substantial facilities on the Arthur Kill.

The **Port Reading Railroad** is eligible for the NRHP at the state and local levels of significance. The construction and operation of the Port Reading Railroad represents the aggressive efforts of railroads, in this case the Reading Railroad, to obtain access to New York markets and the Port of New York and New Jersey. The Port Reading Railroad provided the parent Reading Railroad, another major inter-state carrier of anthracite coal, with access to New York Harbor and was an instrument of railroad expansion, acquisition, and consolidation during the late 19th and early 20th centuries.

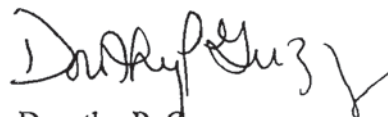
Previous HPO comments for other projects have identified the Port Reading Railroad Terminal (Township of Woodbridge) and the Route 27 Bridge over the Port Reading Railroad (Borough of Metuchen) as historic properties individually eligible for the NRHP. These properties would also contribute to the Port Reading Railroad Historic District.

800.5 Assessment of Adverse Effects

I concur with the submitted effects assessment that the proposed project, the construction of an extension of Helen Street on the preferred alignment (H-3.3) will have **no adverse effect** on historic properties if the design and construction of the roadway extension avoids alterations to the physical and visual integrity of the railroad historic districts and if plans and specifications for the project, especially the proposed railroad grade crossings, are submitted to the HPO for review and comment prior to the finalization of design or the solicitation of project construction bids.

If you have any questions regarding these comments or require additional assistance, please contact Steve Hardegen at (609) 984-0141 or Michael Gregg (regarding archaeology) at (609) 633-2395.

Sincerely,



Dorothy P. Guzzo
Deputy State Historic
Preservation Officer

C: Amy Fox, FHWA
Lauralee Rappleye-Marsett, NJDOT
ACHP
Consulting Parties List

DG/CS C:\NJDOTC2002-151

-

Union County Parks Survey

Park Name

Lenape Park

Township/City

Springfield, Kenilworth, Cranford, Union

photo



Type

☐ recreational ☐ pastoral ☐ picturesque ☐ natural ☐ scenic ☐ other:

Existing Topography

☐ flat ☐ sloped ☐ hilly ☐ mountainous ☐ other:

Spatial Relationships

☐ symmetrical ☐ asymmetrical ☐ axial

Circulation

<input type="checkbox"/> trails	<input type="checkbox"/> asphalt, dirt
<input type="checkbox"/> cycling paths	
<input type="checkbox"/> bridle paths	
<input type="checkbox"/> parking lots	<input type="checkbox"/> crushed stone
<input type="checkbox"/> handicapped ramps	
<input type="checkbox"/> other:	

Parkways

<input type="checkbox"/> Border roads	<input type="checkbox"/> Connecting pleasure roads	<input type="checkbox"/> Other:	<input type="checkbox"/> approach
<input type="checkbox"/> Internal park roads	<input type="checkbox"/> River parkways		
<input type="checkbox"/> Vernacular roads	<input type="checkbox"/> Estate roads		
<input type="checkbox"/> Summit roads	<input type="checkbox"/> Ocean parkways		

Union County Parks Survey

Drainage & Engineering

<input type="checkbox"/> Bridge (road)	(See bridge info)
<input type="checkbox"/> Bridge (pedestrian)	
<input type="checkbox"/> Channel	
<input type="checkbox"/> Culvert	
<input type="checkbox"/> Spillway	
<input type="checkbox"/> Millrace	
<input type="checkbox"/> Retaining wall	
<input type="checkbox"/> Storm sewer	
<input type="checkbox"/> Swale	
<input type="checkbox"/> Other:	concrete
semi-barrier	

Bridge

carries	bridge date	info source	bridge type/design	bridge material
Kenilworth Boulevard	1925/1992	A.G. Lichtenstein	box beam	steel & concrete w/ stone cladding

Land Uses

- | | |
|---|--|
| <input type="checkbox"/> archery | <input type="checkbox"/> skating--sidewalk |
| <input type="checkbox"/> baseball | <input type="checkbox"/> soccer |
| <input type="checkbox"/> basketball | <input type="checkbox"/> softball |
| <input type="checkbox"/> boating/canoeing | <input type="checkbox"/> swimming |
| <input type="checkbox"/> biking | <input type="checkbox"/> tennis |
| <input type="checkbox"/> camping | <input type="checkbox"/> theater/music |
| <input type="checkbox"/> concession/dining | <input type="checkbox"/> track & field |
| <input type="checkbox"/> cricket | <input type="checkbox"/> trap/skeet shooting |
| <input type="checkbox"/> driving | <input type="checkbox"/> trail |
| <input type="checkbox"/> fishing | <input type="checkbox"/> wading |
| <input type="checkbox"/> football | <input type="checkbox"/> other: |
| <input type="checkbox"/> golf | |
| <input type="checkbox"/> gymnastics | |
| <input type="checkbox"/> handball court | |
| <input type="checkbox"/> hockey | |
| <input type="checkbox"/> horseback riding | |
| <input type="checkbox"/> horticulture/gardening | |
| <input type="checkbox"/> lawn bowling | |
| <input type="checkbox"/> model boating | |
| <input type="checkbox"/> picnicking | |
| <input type="checkbox"/> playground | |
| <input type="checkbox"/> shuffleboard | |
| <input type="checkbox"/> skating--rink | |

Landscape Dividers

boxed-wood fence leading into trap shooting range and along Kenilworth Boulevard.
wooden gate

Water Elements

- | | | |
|---------------------------------|-------------------------------------|--|
| <input type="checkbox"/> rivers | <input type="checkbox"/> creeks | <input type="checkbox"/> pools |
| <input type="checkbox"/> lakes | <input type="checkbox"/> ponds | <input type="checkbox"/> fountains |
| <input type="checkbox"/> brooks | <input type="checkbox"/> waterfalls | <input type="checkbox"/> other : swamp |

Buildings

- | | | |
|---|--|---------------------------------|
| <input type="checkbox"/> maintenance | <input type="checkbox"/> stable | <input type="checkbox"/> other: |
| <input type="checkbox"/> recreational | <input type="checkbox"/> house | |
| <input type="checkbox"/> concession | <input type="checkbox"/> rest rooms | |
| <input type="checkbox"/> picnic shelter | <input type="checkbox"/> mill | |
| <input type="checkbox"/> gazebo | <input type="checkbox"/> museum/visitor center | |
| <input type="checkbox"/> bandstand | <input type="checkbox"/> public utility | |

Landscape Furniture

- | | |
|---|-------------------------------------|
| <input type="checkbox"/> benches | <input type="checkbox"/> planters |
| <input type="checkbox"/> bleachers | <input type="checkbox"/> trash cans |
| <input type="checkbox"/> bike racks | <input type="checkbox"/> other: |
| <input type="checkbox"/> bollards | |
| <input type="checkbox"/> drinking fountains | |
| <input type="checkbox"/> fitness trail stations | |
| <input type="checkbox"/> grills | |
| <input type="checkbox"/> lighting fixtures | |
| <input type="checkbox"/> picnic tables | |

Surveyor Gregory Dietrich

Date 10/2/03

Union County Parks Survey

Description

Lenape Park consists of a pastoral landscape, a recreational component, and a largely undeveloped portion. An approach with a landscaped median called Nomahegan Boulevard comprises the entrance to the park, terminating at the park's southwestern border. The areas in this and other portions to the north are largely undeveloped, and a natural buffer of trees and wild vegetation lines the park's southern border.

The pastoral landscape is located within the park's southwestern portion. Lenape Pond is located within this section, and is formed by Nomahegan Brook running through it in an east-west direction. Scattered mature trees are located along a meadow that is south of the pond. This open landscape alternates with dense thickets of woods in other areas bordering the pond, and along the park's southern perimeter. An asphalt footpath runs east-west within the park's southern portion along a berm. Open areas of grass line the berm on both sides, framed by dense wood thickets. A small pond is located south of the trail within the northeastern portion of the park. Scattered trees are located around this pond.

The footpath connects to a crushed stone driveway, terminating at a parking lot within the skeet shooting area. A mid-20th-century field house fronts the parking lot, while two lookout sheds, plywood walls and wooden fencing define the target practice area. Several mature trees immediately surround this building. A branch of the Rahway River runs through the eastern portion of this park, and is augmented by the confluence of Black Brook in the northern region. A large concrete dike (c.1960) acts as a flood control device for the river in the area just north of Kenilworth Boulevard. Informal trails are present within this portion of the park, accessible from Kenilworth Boulevard. A wide, grassy trail surrounded on both sides by wild trees and vegetation follows the western border of the river, continuing north into Black Brook Park. A short segment of this trail is paved with asphalt blocks.



Setting

Lenape Park is roughly bordered by Diamond Road to the north, Black Brook Park and North 8th Street to the east, Kenilworth Boulevard and Country Club Park Drive to the south, and Springfield Avenue to the west. 1½ - 2-story single-family dwellings (c.1960-90) are located along North 8th Street and other small residential roads to the east. Nomahegan Park is located on Kenilworth Boulevard to the south, while 1½ -story single-family dwellings (c.1965) are located along Country Club Drive, also to the south. A mixture of 1½ - 2-story single-family dwellings and commercial buildings (c.1920-90) are located along Springfield Avenue to the west.



Union County Parks Survey

Markers/
monuments/
structure

Signage

wood and plexi-glass

Utilities

Plantings

mature oaks, sycamores, weeping willows, wild trees and vegetation

CRM issues

NJ Site Survey No. 28-Un-12: Springfield; Survey Form reference number (13)
NJ Site Survey No. 28-Un-16: Rahway River Bridge Historic Site South; Survey Form reference number (40)

Character Defining
Features/Resources

Contributing Features

Non-Contributing Features

Configuration of Nomahegan Drive
Lenape Pond
Nomahegan Brook
Rahway River
Black Brook
Field House
Configuration of trapshooting area,
including driveway, parking lot, field
house and shooting area
Scattered mature trees w/in
southwestern portion
Berm formerly housing tracks of Rahway
Valley Railroad

Densely wooded areas along the southern
perimeter, Rahway River and Black Brook, and
within the wilderness areas north of Nomahegan
Brook
Partially paved/grassy trail along the west side of
the Rahway River

Surveyor Gregory Dietrich

Date 10/2/03

Union County Parks Survey

Contributing Resources

1925/1992 bridge carrying Kenilworth Boulevard

Non-Contributing Resources

Concrete semi-barrier
Signage along berm
Asphalt paving along berm

Boundary of eligible portion of park

Eligible

☐ yes
☐ no

All of the property contained within Block 138, Lot 1 and Block 143, Lot 4 of the Township of Springfield; Block 101.01, Lot 1; Block 101.02, Lot 1; Block 102.01, Lot 1; Block 102.02, Lot 1; Block 102.03, Lot 1; Block 102.04, Lot 1; Block 102.05, Lot 1; and Block 102.06, Lot 1 of the Township of Cranford; Block 65, Lot 1 of the Borough of Kenilworth; and Block 3409 Lot 1 of the Township of Union; and Block 480, Lot 20 of the Town of Westfield, Union County, New Jersey.

Rating

☐ Key Contributing ☐ Contributing ☐ Non-Contributing

Areas of Significance

- | | | | |
|---|---|---|--|
| <input type="checkbox"/> Agriculture | <input type="checkbox"/> Economics | <input type="checkbox"/> Industry | <input type="checkbox"/> Performing Arts |
| <input type="checkbox"/> Architecture | <input type="checkbox"/> Education | <input type="checkbox"/> Invention | <input type="checkbox"/> Philosophy |
| <input type="checkbox"/> Art | <input type="checkbox"/> Engineering | <input type="checkbox"/> Landscape Architecture | <input type="checkbox"/> Politics |
| <input type="checkbox"/> Commerce | <input type="checkbox"/> Entertainment/Recreation | <input type="checkbox"/> Law | <input type="checkbox"/> Religion |
| <input type="checkbox"/> Communications | <input type="checkbox"/> Ethnic Heritage | <input type="checkbox"/> Literature | <input type="checkbox"/> Science |
| <input type="checkbox"/> Community Planning | <input type="checkbox"/> Exploration/Settlement | <input type="checkbox"/> Maritime History | <input type="checkbox"/> Social History |
| <input type="checkbox"/> Conservation | <input type="checkbox"/> Health/Medicine | <input type="checkbox"/> Military | <input type="checkbox"/> Transportation |

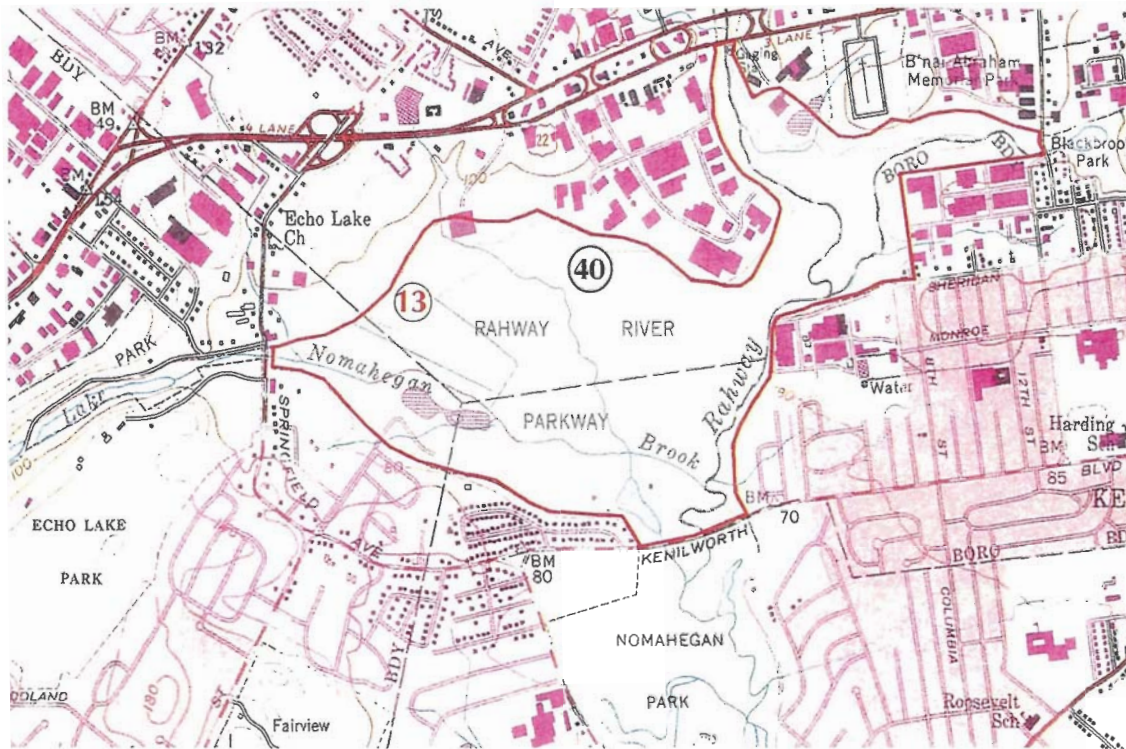
Justification of Eligibility

Acquired and developed within the period of significance (1921-1964), Lenape Park is eligible under Criterion A for having made contributions to the broad patterns of history in the areas of Community Planning, Entertainment/Recreation, and Landscape Architecture on the national level; and in the area of Conservation on the state level. Under Criterion C, it is significant for its association with the nationally renowned Olmsted Brothers Landscape Architecture Firm of Brookline, Massachusetts and its successor firm, Olmsted Associates. With regard to historic integrity, Lenape Park has remained largely undeveloped and therefore retained most of its original location, design, setting, materials, workmanship, feeling and association.

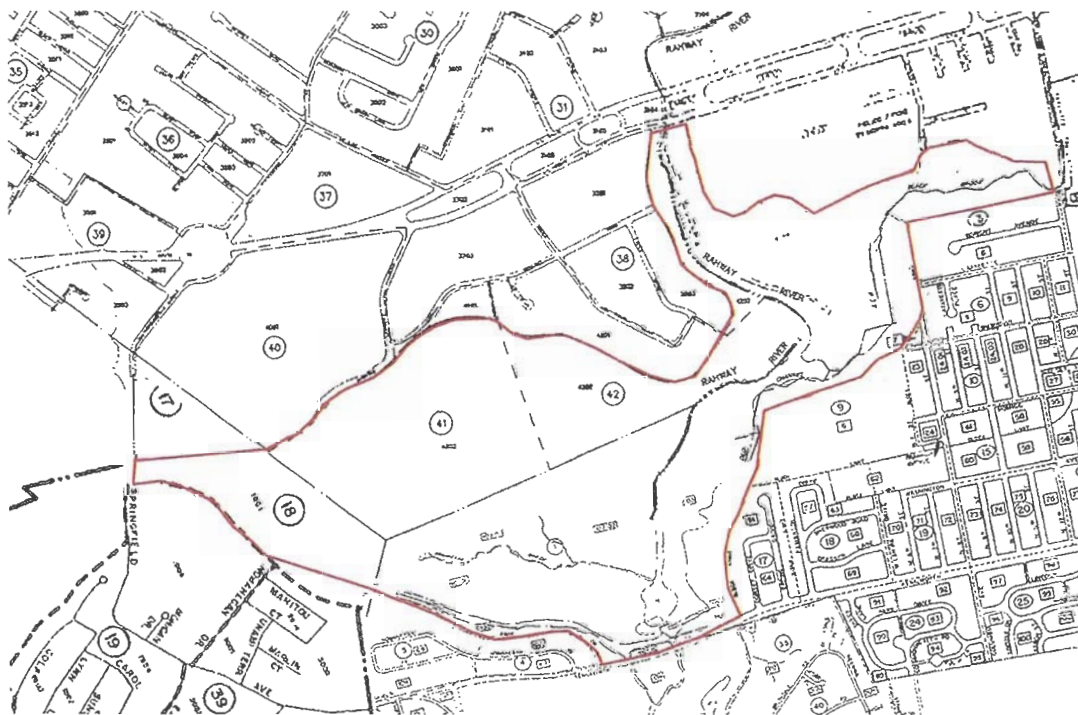
Surveyor Gregory Dietrich

Date 10/2/03

Union County Parks Survey



USGS 7.5' Quadrangle: Roselle, N.J. 1955 (Photorevised 1981).



Block 138, Lot 1; Block 143, Lot 4; of Springfield Township, Blocks 101.01, 101.02, 102.01, 102.02, 102.03, 102.04, 102.05 and 102.06, Lots 1; of Cranford Township, Block 6 & 66, Lots 1; of Kenilworth Borough and Block 3409 Lot 1; of Union Borough and Block 480, Lot 20; of the Town of Westfield.

HISTORIC SITES AND STRUCTURES INVENTORY

Rating <input checked="" type="checkbox"/> Contributing <input type="checkbox"/> Non-Contributing		County Union		Survey No.	
Historic Name Field House		Township Kenilworth/Union/Springfield/Cranford		Surveyor Initials GD	
Address Lenape Park		City		Survey Date 10/2/2003	
Common Name Field House	Use	Present	Past	USGS Quad	
Architect/Builder Unknown	Residence	<input type="checkbox"/>	<input type="checkbox"/>	Category <input checked="" type="checkbox"/> building <input type="checkbox"/> structure <input type="checkbox"/> district <input type="checkbox"/> object <input type="checkbox"/> site <input type="checkbox"/> landscape	
Style(s) Colonial Revival	Commercial	<input type="checkbox"/>	<input type="checkbox"/>		
Time Period(s) 1927/1959	Industrial	<input type="checkbox"/>	<input type="checkbox"/>		
Stories 1½	Institutional	<input type="checkbox"/>	<input type="checkbox"/>		
Plan T	Other	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	Outbuildings Tower	
Foundation CONCRETE	Condition		Areas of Significance:		
Roof ASPHALT	<input type="checkbox"/> Excellent <input checked="" type="checkbox"/> Good <input type="checkbox"/> Fair <input type="checkbox"/> Deteriorated <input type="checkbox"/> Ruins		Agriculture Architecture Art Commerce Communications <input checked="" type="checkbox"/> Community Planning <input checked="" type="checkbox"/> Conservation Economics Education Engineering <input checked="" type="checkbox"/> Entertainment/recreation <input type="checkbox"/> Ethnic Heritage		
Porches N/A	Integrity		Exploration/settlement Health/medicine Industry Invention <input checked="" type="checkbox"/> Landscape Architecture Law Literature Maritime History Military Performing Arts Philosophy Politics		
Interior N/A	<input type="checkbox"/> Unaltered <input checked="" type="checkbox"/> Slightly Altered <input type="checkbox"/> Severely Altered <input type="checkbox"/> Moved				
Walls STUCCO WOOD: Shingle	Date Moved				

Historic Context(s)/Statement of Significance/Additional Description:

The Field House is a 1½-story, rectilinear building, clad in stucco with a concrete foundation. A central door is flanked by window openings with wood infill. The roof is sheathed in asphalt shingles with a rubblestone, slope chimney and vinyl soffits. The gable ends are sheathed in plywood and wood shingles. There is a 1-story rear ell that is also clad in stucco. The Fieldhouse is located within the eastern portion of Lenape Park, between the Rahway River to the east and the Nomahegan Brook to the south. It is oriented northeast toward the target range, and is surrounded by a mature tree and concrete paving, and an asphalt parking lot to the south.



Located within APE on the Rahway River Parkway are six named Union County Parks (from north to south down the Rahway River): Lenape, Nomahegan, McConnell, Sperry, Lincoln, and Driescher's Mill. After the Rahway River Parkway was determined eligible for listing in the National Register of Historic Places by the New Jersey HPO, the entire Union County Park System (ID #4424) was determined eligible on March 19, 2005.⁶ According to the Union County Park System Determination of Eligibility (DOE), Lenape Park, Nomahegan Park, the "McConnell Park Section of the Rahway River Parkway," the "Sperry Section of the Rahway River Parkway," and the Cranford Section of Rahway River Parkway are all eligible for the NRHP under the Park System DOE. Technically, McConnell Park and Sperry Park are located in the Cranford Section of the Parkway (see Figure 5.11); it can only be assumed that the SHPO wanted specifically to name these parks. In addition, Lincoln and Driescher's Mill parks are located in the Cranford Section.

Four named Cranford Township municipal parks are situated within the APE on the Rahway River Parkway. They include, moving north to south down the Rahway River, Memorial Park, Hampton Park, Girl Scout Park, and Hanson Park (see Figure 5.2). According to the DOE for Rahway River Parkway, these municipal parks are eligible as contributing resources to the district for their "recreational nature" (Tingey 2002: Continuation Sheet 1).

Lenape Park. Lenape Park (ID #4424.017 and PRN 124) is eligible for listing in the National Register as a contributing resource to the Rahway River Parkway HD (ID #4079) and the Union County Park System HD (ID #4424) (see Figure 5.14). Lenape Park was originally highlighted, but not specifically named, in the 1921 Olmsted Brothers report on the initial development of a park system for Union County (see Figure 5.10, marked section G4). The firm recommended the acquisition of this "swamp area" near the Rahway River because of the "nature of the ground and the consequent low value" (Olmsted Brothers 1921:29). They believed it could be a "desirable addition to either the cross-county parkway or the parking of the Rahway River" (Olmsted Brothers 1921:29).

Lenape Park has always been one of the least developed of the parks within the system, despite its initial identification by Olmsted Brothers. According to a report drafted by Olmsted Associates, the successor firm to Olmsted Brothers, the lack of development was partly the result of the lack of topographic information provided to the company about this area (Dietrich 2004:103, quoting Olmsted Associates 1963). Nevertheless, by 1930 the Park System had recreational facilities in place in the "Kenilworth Boulevard Section of the Rahway Parkway." These facilities included three traps for clay target shooting, a rifle and pistol range, a one-mile bridle path, a football field, and a baseball field (Union County Park Commission 1930). By this time, the area also had two lakes. However, according to Secretary Tracy, the land now comprising Lenape Park remained largely undeveloped as a result of Commission funding priorities. In 1932, Tracy wrote about the future of Lenape Park: "[it is] destined to remain in its natural state for some time, other projects having priority on the development program when funds became available (Dietrich 2004:104 quoting Tracey 1932:85).

In 1936 during a Works Progress Administration (WPA) project in the park area,⁷ workers uncovered a "three-foot tusk and four four-pound teeth" belonging to a mastodon (*The Daily Princetonian* 1936:1). Several weeks later, other mastodon bones were found creating a sensation.

⁶ A DOE has been made but no formal National Register nomination has been submitted.

⁷ Newspaper articles indicate that the mastodon bones were found in Nomahegan Park "at the lake excavation" (Cranford Chronicle 1936). However, notes from the period on the backs of the mastodon dig photos indicate that the site was near the "trapshooting grounds." Only Lenape Park has ever had traps for shooting clay targets.

Dr. Dorothy Cross, archaeologist and state supervisor of a federal, scientific project sponsored by the New Jersey State Museum, came to the site to gather the teeth, tusks, and skull fragments found. They would be taken back to Trenton where an attempt would be made to reconstruct the head of the mastodon (*Cranford Chronicle* 1936) (Figure 5.17). Professors at Rutgers believed that the mastodon bones were between 25,000 and 35,000 years old, and it was anticipated that further excavations would happen under the direction of Princeton and Rutgers (*The Daily Princetonian* 1936:1). However, by the time the dust settled, the dig seems to have been overseen by Charles Lang, paleontologist at the American Museum of Natural History (Figure 5.18).

The mastodon bones were taken to the New Jersey State Museum and placed on exhibit in the Main Hall of the building in 1937. The bones were placed in a "flat top case near the main entrance of the museum" and attracted much interest (*Cranford Historical Society, Mastodon file, newspaper article with no name, "Exhibit Mastodon Bones"*). In 2011, the Cranford Historical Society and the Union County Park System became interested in what happened to the bones and contacted the museum. It seems that the bones had been used in school programs throughout the years and very few, if any, remained. Nevertheless, the Park System is preparing an exhibition on the finding of the mastodon.

In the 1960s, Olmsted Associates was contracted to create a preliminary plan for the park, which was still not called Lenape. The firm proposed a number of rural uses and pastimes for the park including picnicking, fishing, boating, and other forms of informal recreation utilizing what they thought were the park's best features—the Rahway River, Nomahegan and Black



Figure 5.17. Charles Lang, in suit, Dr. Dorothy Cross, an unidentified man look over the mastodon dig site in Lenape Park in May 1936 (courtesy of Cranford Historical Society).



Figure 5.18. Charles Lang (in white hat, necktie and dress shoes) assisted by his crew makes a plaster cast for transporting the fragile mastodon bone fragments, May 1936 (courtesy of Cranford Historical Society).

brooks. Both the Commission and the company believed that since the area was prone to flooding, it should remain a watershed area, thereby providing a haven for conservation and natural enjoyment. Nevertheless, Olmsted Associates did not see this area as completely wild and to be left alone. It proposed an internal vehicular parkway with connections to Nomahegan Drive, Kenilworth Boulevard, and Springfield Avenue, a boathouse/administration building, native shrub and tree arboretums, and an observation tower (Dietrich 1004:104, quoting Olmsted Associates 1963).

In 1965, the park was officially named "Lenape" after the original Indian inhabitants of New Jersey (Cunningham 1972). Nomahegan Park and Lenape Park were joined across Kenilworth Boulevard (Route 509), and the Olmsted's plans were never implemented. At the time of the current investigation, Lenape Park is entered from Kenilworth Boulevard between two flood-control levees that are topped with paths (see Figure 5.14; see Section 6.0, Inventory of Identified Architectural Resources).⁸ Within the park are a Snack Shop (PRN 12), no longer

⁸ The levees that occur throughout the Union County Park System are not typical large-scale levees like those seen on the Mississippi River. These levees vary in height and width and are generally topped with pathways throughout the system. The average user of these paths likely has no idea that they are on a flood-control device. There is some debate regarding the actual builder of the levees and when they were constructed throughout the system. Since neither the municipalities nor USACE claim them, the Union County Park System or the State of New Jersey seem to be the likely candidates. However, neither of these candidates claims to have information on the levees construction.

used and boarded up, a gravel parking area, remnants of the skeet shooting traps (PRN 73), a dam/spillway (PRN 13), a foot bridge (PRN 102), and eight earthen explosives storage structures (PRN 103), associated with the former American Can Company munitions loading site in Kenilworth (for more information on these resources see Section 5.1.3, the Kenilworth Borough section, of this report).

Between Lenape and Nomahegan Parks. Two resources, PRN 14, and PRN 15, are located between Lenape and Nomahegan parks in the adjoining area of Highway 509, Kenilworth Boulevard. These resources are bridges that are both eligible for the NRHP as contributing resources to the NRE Rahway River Parkway HD (ID #4079) and the NRE Union County Park System HD (ID #4424). Both bridges were constructed by Union County in 1992 to replace earlier bridges in the exact locations. PRN 14 (NJDOT #2003018) crosses the Rahway River and PRN 15 (NJDOT #2003006) crosses a tributary of the Rahway River. Both bridges are concrete with stone faces made to blend with the park environment.

Nomahegan Park. Nomahegan Park (ID #4424.021 and PRN 121; Figure 5.19) is eligible for listing in the National Register as a contributing resource to the Rahway River Parkway HD (ID #4079) and the Union County Park System HD (ID #4424). Although this park was not specifically named in the Olmsted Brothers report of 1921, the park location can be seen on the map accompanying the report (see Figure 5.10). Olmsted Brothers may have included this area in the special park site G4, the swampy site close to the Rahway River. The site, when it was acquired

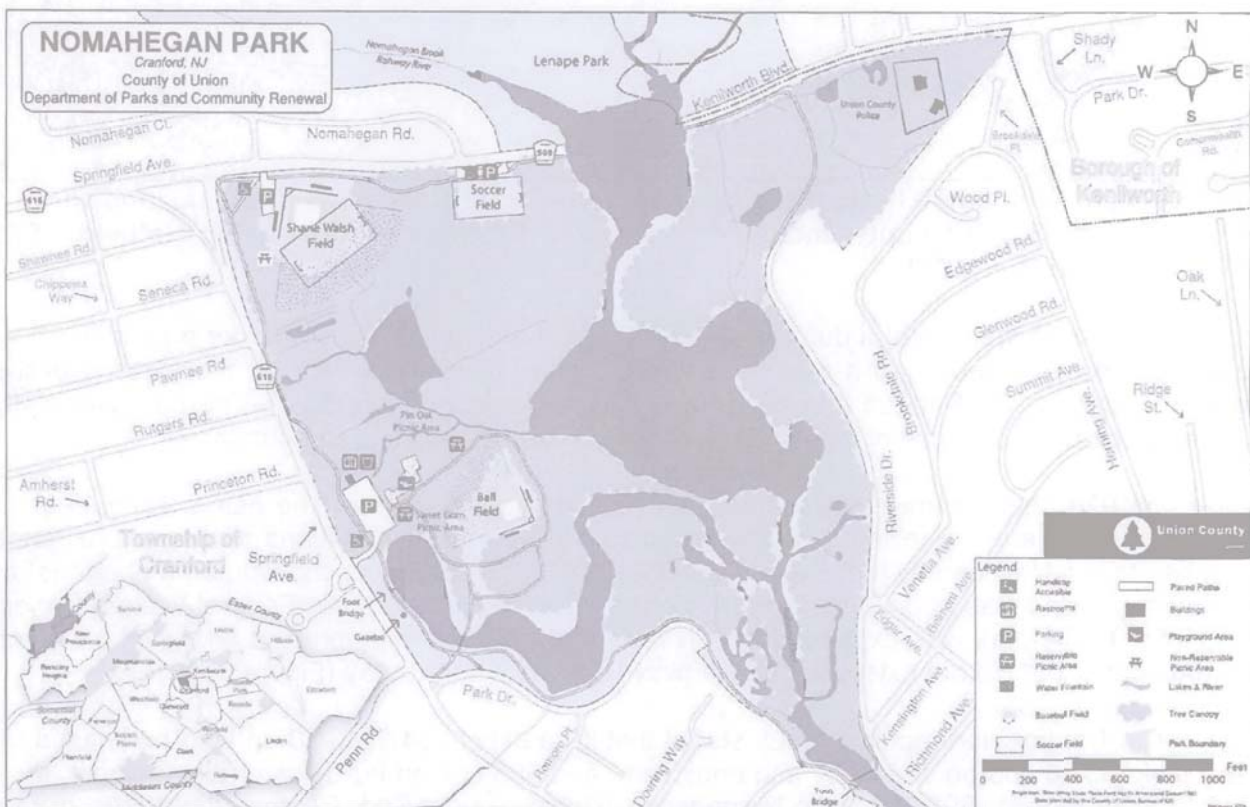


Figure 5.19. Nomahegan Park, Union County Department of Parks and Community Renewal (Union County 2012).

Union County Parks Survey

Park Name **Nomahegan Park**
 Township/City **Cranford**

photo



Type

☐ recreational ☐ pastoral ☐ picturesque ☐ natural ☐ scenic ☐ other:

Existing Topography

☐ flat ☐ sloped ☐ hilly ☐ mountainous ☐ other:

Spatial Relationships

☐ symmetrical ☐ asymmetrical ☐ axial

Circulation

☐ trails ☐ dirt; asphalt

cycling paths

☐ bridle paths

☐ parking lots

asphalt; crushed stone

☐ handicapped ramps

☐ other:

Parkways

☐ Border roads ☐ Connecting pleasure roads ☐ Other:
☐ Internal park roads ☐ River parkways
☐ Vernacular roads ☐ Estate roads
☐ Summit roads ☐ Ocean parkways

Surveyor Gregory Dietrich

Date 10/2/03

Union County Parks Survey

Drainage & Engineering

<input type="checkbox"/> Bridge (road)	(See Bridge info and Kenilworth Boulevard bridge info for Lenape Park)
<input type="checkbox"/> Bridge (pedestrian)	wood
<input type="checkbox"/> Channel	
<input type="checkbox"/> Culvert	concrete
<input type="checkbox"/> Spillway	
<input type="checkbox"/> Millrace	
<input type="checkbox"/> Retaining wall	wood along pond; brownstone wall along Riverside Drive
<input type="checkbox"/> Storm sewer	
<input type="checkbox"/> Swale	
<input type="checkbox"/> Other:	

Bridge

carries	bridge date	info source	bridge type/design	bridge material
Springfield Avenue	2003	Plaque	girder	steel reinforced concrete granite cladding and spiked metal railing

Landscape Dividers

wooden corral fence

Land Uses

<input type="checkbox"/> archery	<input type="checkbox"/> skating--sidewalk
<input type="checkbox"/> baseball	<input type="checkbox"/> soccer
<input type="checkbox"/> basketball	<input type="checkbox"/> softball
<input type="checkbox"/> boating/canoeing	<input type="checkbox"/> swimming
<input type="checkbox"/> biking	<input type="checkbox"/> tennis
<input type="checkbox"/> camping	<input type="checkbox"/> theater/music
<input type="checkbox"/> concession/dining	<input type="checkbox"/> track & field
<input type="checkbox"/> cricket	<input type="checkbox"/> trap/skeet shooting
<input type="checkbox"/> driving	<input type="checkbox"/> trail
<input type="checkbox"/> fishing	<input type="checkbox"/> wading
<input type="checkbox"/> football	<input type="checkbox"/> other:
<input type="checkbox"/> golf	
<input type="checkbox"/> gymnastics	
<input type="checkbox"/> handball court	
<input type="checkbox"/> hockey	
<input type="checkbox"/> horseback riding	
<input type="checkbox"/> horticulture/gardening	
<input type="checkbox"/> lawn bowling	fitness trail
<input type="checkbox"/> model boating	
<input type="checkbox"/> picnicking	
<input type="checkbox"/> playground	
<input type="checkbox"/> shuffleboard	
<input type="checkbox"/> skating--rink	

Water Elements

<input type="checkbox"/> rivers	<input type="checkbox"/> creeks	<input type="checkbox"/> pools
<input type="checkbox"/> lakes	<input type="checkbox"/> ponds	<input type="checkbox"/> fountains
<input type="checkbox"/> brooks	<input type="checkbox"/> waterfalls	<input type="checkbox"/> other : swamp

Buildings

<input type="checkbox"/> maintenance	<input type="checkbox"/> stable	<input type="checkbox"/> other:
<input type="checkbox"/> recreational	<input type="checkbox"/> house	
<input type="checkbox"/> concession	<input type="checkbox"/> rest rooms	
<input type="checkbox"/> picnic shelter	<input type="checkbox"/> mill	
<input type="checkbox"/> gazebo	<input type="checkbox"/> museum/visitor center	
<input type="checkbox"/> bandstand	<input type="checkbox"/> public utility	

Landscape Furniture

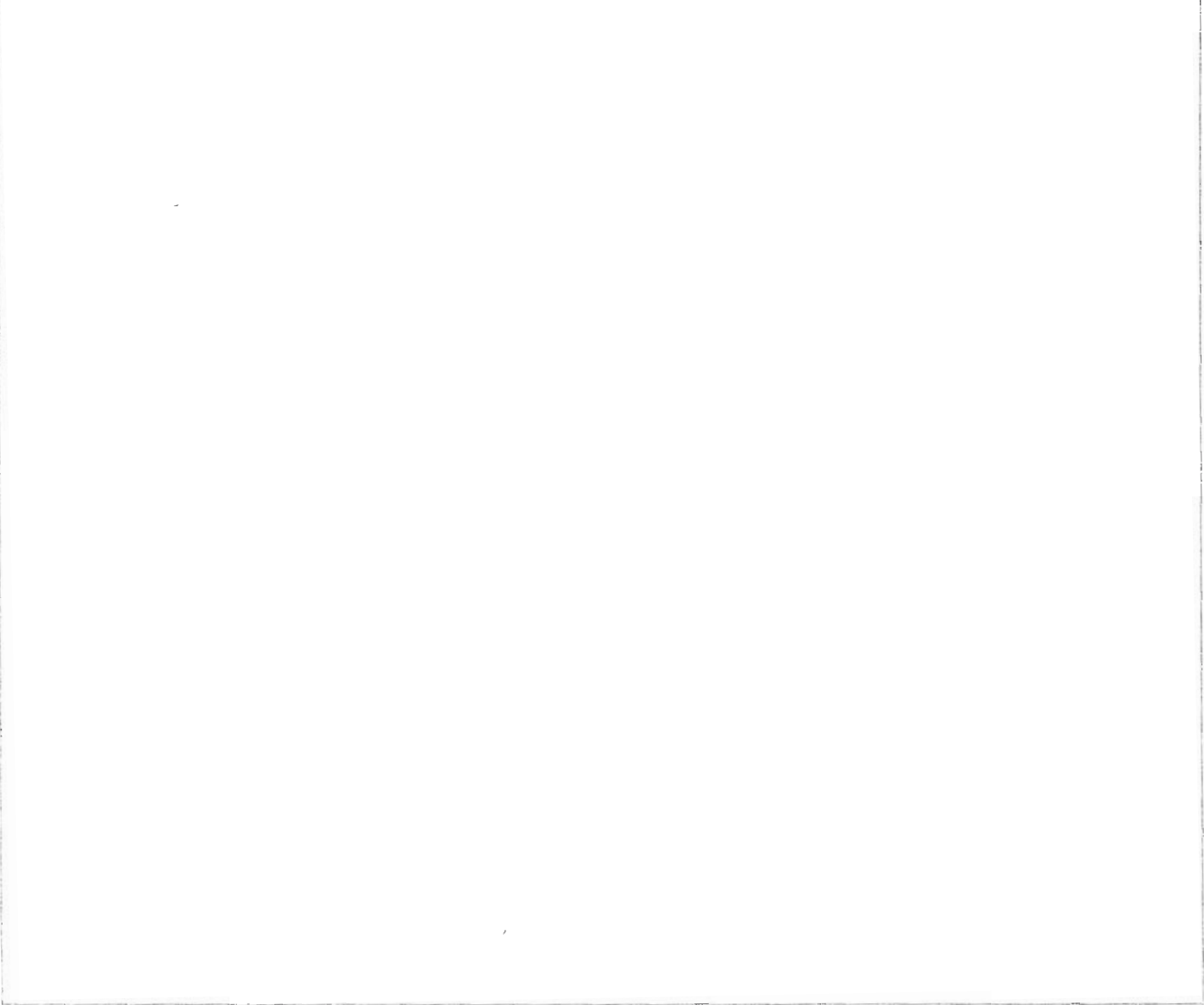
<input type="checkbox"/> benches	<input type="checkbox"/> planters
<input type="checkbox"/> bleachers	<input type="checkbox"/> trash cans
<input type="checkbox"/> bike racks	<input type="checkbox"/> other:
<input type="checkbox"/> bollards	
<input type="checkbox"/> drinking fountains	
<input type="checkbox"/> fitness trail stations	
<input type="checkbox"/> grills	
<input type="checkbox"/> lighting fixtures	
<input type="checkbox"/> picnic tables	

Surveyor Gregory Dietrich

Date 10/2/03

Union County Parks Survey

Description Nomahegan Park consists of several distinct areas that are separated by groves of mature trees: recreational components, a pastoral landscape, and undeveloped areas. Ball fields and a parking lot are located within the northwestern portion of the park, while the northeastern portion is characterized by a branch of Normahegan Brook and the Rahway River that run through undeveloped areas containing wild trees and vegetation set amidst swampland. A modern rest room facility, parking lot, playground and ball field are located south of the ball fields. These are separated by a wooded area known as Pin Oaks Picnic Area, containing a curvilinear fitness trail that runs along Springfield Avenue. Another picnic grove known as Sweet Gum Picnic Area is located east of Pin Oaks. A lake with an island is located south of these components along the same trail, and connects to the Rahway River which runs parallel to Riverside Drive, to the east. The confluence of the lake tributary and the Rahway River occurs in an undeveloped area within the southwestern portion of the park. A wooden footbridge spans the Rahway River south of this confluence.



Setting Nomahegan Park is bordered by Kenilworth Boulevard to the north, Riverside Drive to the east, Normandie Drive to the south, and Springfield Avenue and Park Drive to the west. 1½-story single-family dwellings (c.1960) and Lenape Park are located along Kenilworth Boulevard. 1½ - 2-story single-family dwellings (c.1930-1990) are located along Riverside Drive, while 1½ - 2-story single-family dwellings (c.1946) are located along Park Drive and Springfield Avenue. There is also a college campus along Springfield Avenue.



Union County Parks Survey

Markers/ r s. pture	boulder plaque commemorating Crane's Ford, dedicated in 1929.
Signage	metal
Utilities	
Plantings	mature oaks, sycamores, sweetgums 10 yr. old trees along north side of lake
CRM issues	NJ Site Survey No. 28-Un-7: Cranford Prehistoric; Survey Form reference number (17) NJ-HPO ID No. 3838: North Cranford Historic District; Survey Form reference number (38) NJ-HPO ID No. 4079: Rahway River Parkway Historic District; Survey Form reference number (40) NJ-HPO ID No. 4053: Central Railroad of NJ Bridge over Rahway River; Survey Form reference number (35)

Character Defining Features/Resources	Contributing Features	Non-Contributing Features
Overall configuration of park, excluding modern restroom facility & parking lot Rahway River Normahiggin Brook Lakes Island Pin Oaks Picnic Area Sweet Gum Picnic Area	Swamp areas south of landscaped park area	Young plantings along the west side of the lake below the softball field

Union County Parks Survey

Contributing Resources

Athletic fields
1929 Boulder plaque commemorating Crane's Ford

Non-Contributing Resources

2003 bridge carrying Springfield Avenue
Playground apparatus
Wooden footbridge
Gazebo
Restroom facility
Parking lot fronting restroom facility

Boundary of eligible portion of park

All of the property contained within Block 103, Lot 1; Block 104, Lot 1; and Block 105, Lot 1 of the Township of Cranford, Union County, New Jersey.

Eligible

☐ yes
☐ no

Rating

☐ Key Contributing ☐ Contributing ☐ Non-Contributing

Areas of Significance

<input type="checkbox"/> Agriculture	<input type="checkbox"/> Economics	<input type="checkbox"/> Industry	<input type="checkbox"/> Performing Arts
<input type="checkbox"/> Architecture	<input type="checkbox"/> Education	<input type="checkbox"/> Invention	<input type="checkbox"/> Philosophy
<input type="checkbox"/> Art	<input type="checkbox"/> Engineering	<input type="checkbox"/> Landscape Architecture	<input type="checkbox"/> Politics
<input type="checkbox"/> Commerce	<input type="checkbox"/> Entertainment/Recreation	<input type="checkbox"/> Law	<input type="checkbox"/> Religion
<input type="checkbox"/> Communications	<input type="checkbox"/> Ethnic Heritage	<input type="checkbox"/> Literature	<input type="checkbox"/> Science
<input type="checkbox"/> Community Planning	<input type="checkbox"/> Exploration/Settlement	<input type="checkbox"/> Maritime History	<input type="checkbox"/> Social History
<input type="checkbox"/> Conservation	<input type="checkbox"/> Health/Medicine	<input type="checkbox"/> Military	<input type="checkbox"/> Transportation

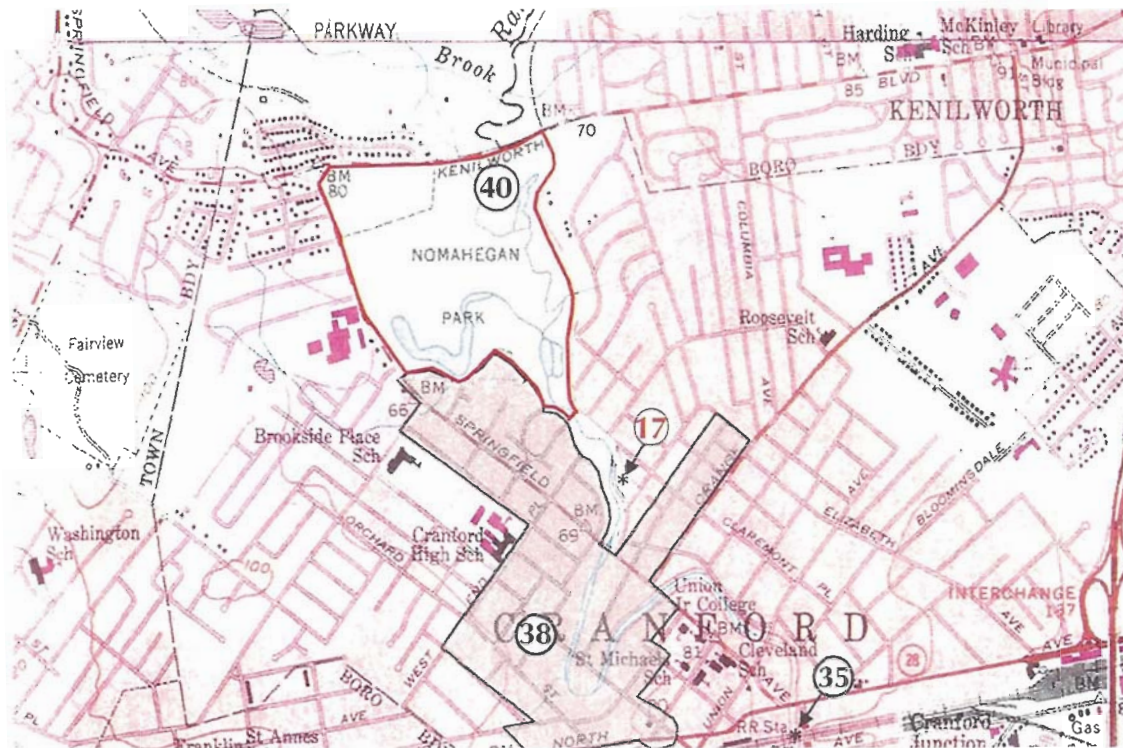
Justification of Eligibility

Acquired and developed within the period of significance (1921-1964), Nomahegan Park is eligible under Criterion A for having made contributions to the broad patterns of history in the areas of Community Planning, Entertainment/Recreation, and Landscape Architecture on the national level; and in the area of Conservation on the state level. Under Criterion C, it is significant for its association with the nationally renowned Olmsted Brothers Landscape Architecture Firm of Brookline, Massachusetts. Under Criterion D, it is eligible for having the capacity to yield important information related to both prehistory and history. With regard to historic integrity, Nomahegan Park has retained most of its original location, design, setting, materials, workmanship, feeling and association.

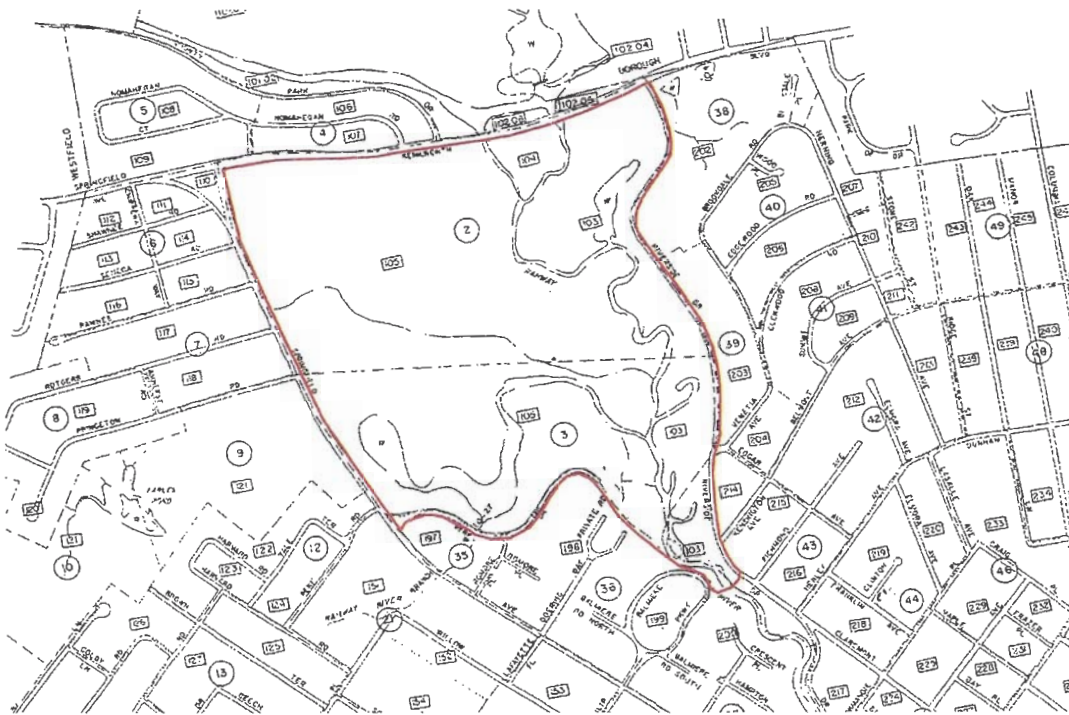
Surveyor Gregory Dietrich

Date 10/2/03

Union County Parks Survey



USGS 7.5' Quadrangle: Roselle, N.J. 1955 (Photorevised 1981).



Blocks 103,104,105, Lots 1; of the Township of Cranford.

Surveyor Gregory Dietrich

Date 10/2/03

used and boarded up, a gravel parking area, remnants of the skeet shooting traps (PRN 73), a dam/spillway (PRN 13), a foot bridge (PRN 102), and eight earthen explosives storage structures (PRN 103), associated with the former American Can Company munitions loading site in Kenilworth (for more information on these resources see Section 5.1.3, the Kenilworth Borough section, of this report).

Between Lenape and Nomahegan Parks. Two resources, PRN 14, and PRN 15, are located between Lenape and Nomahegan parks in the adjoining area of Highway 509, Kenilworth Boulevard. These resources are bridges that are both eligible for the NRHP as contributing resources to the NRE Rahway River Parkway HD (ID #4079) and the NRE Union County Park System HD (ID #4424). Both bridges were constructed by Union County in 1992 to replace earlier bridges in the exact locations. PRN 14 (NJDOT #2003018) crosses the Rahway River and PRN 15 (NJDOT #2003006) crosses a tributary of the Rahway River. Both bridges are concrete with stone faces made to blend with the park environment.

Nomahegan Park. Nomahegan Park (ID #4424.021 and PRN 121; Figure 5.19) is eligible for listing in the National Register as a contributing resource to the Rahway River Parkway HD (ID #4079) and the Union County Park System HD (ID #4424). Although this park was not specifically named in the Olmsted Brothers report of 1921, the park location can be seen on the map accompanying the report (see Figure 5.10). Olmsted Brothers may have included this area in the special park site G4, the swampy site close to the Rahway River. The site, when it was acquired

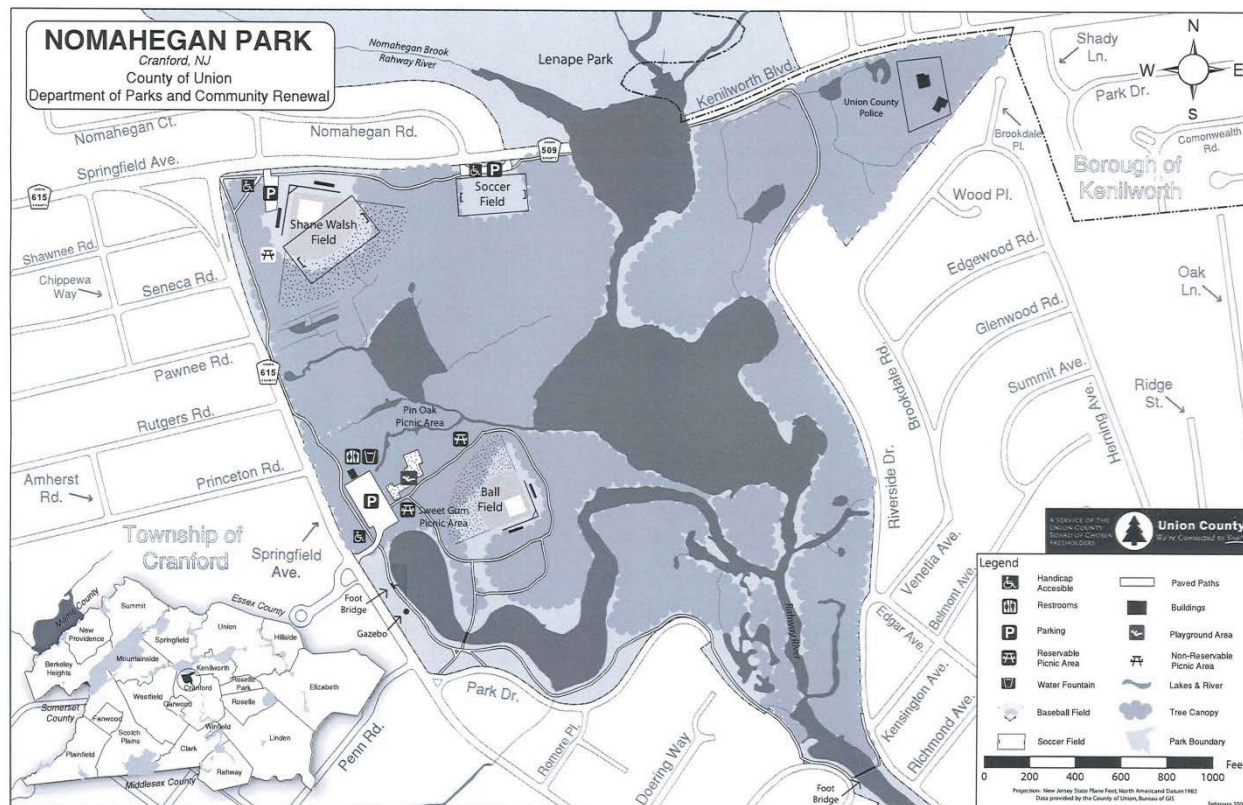


Figure 5.19. Nomahegan Park, Union County Department of Parks and Community Renewal (Union County 2012).

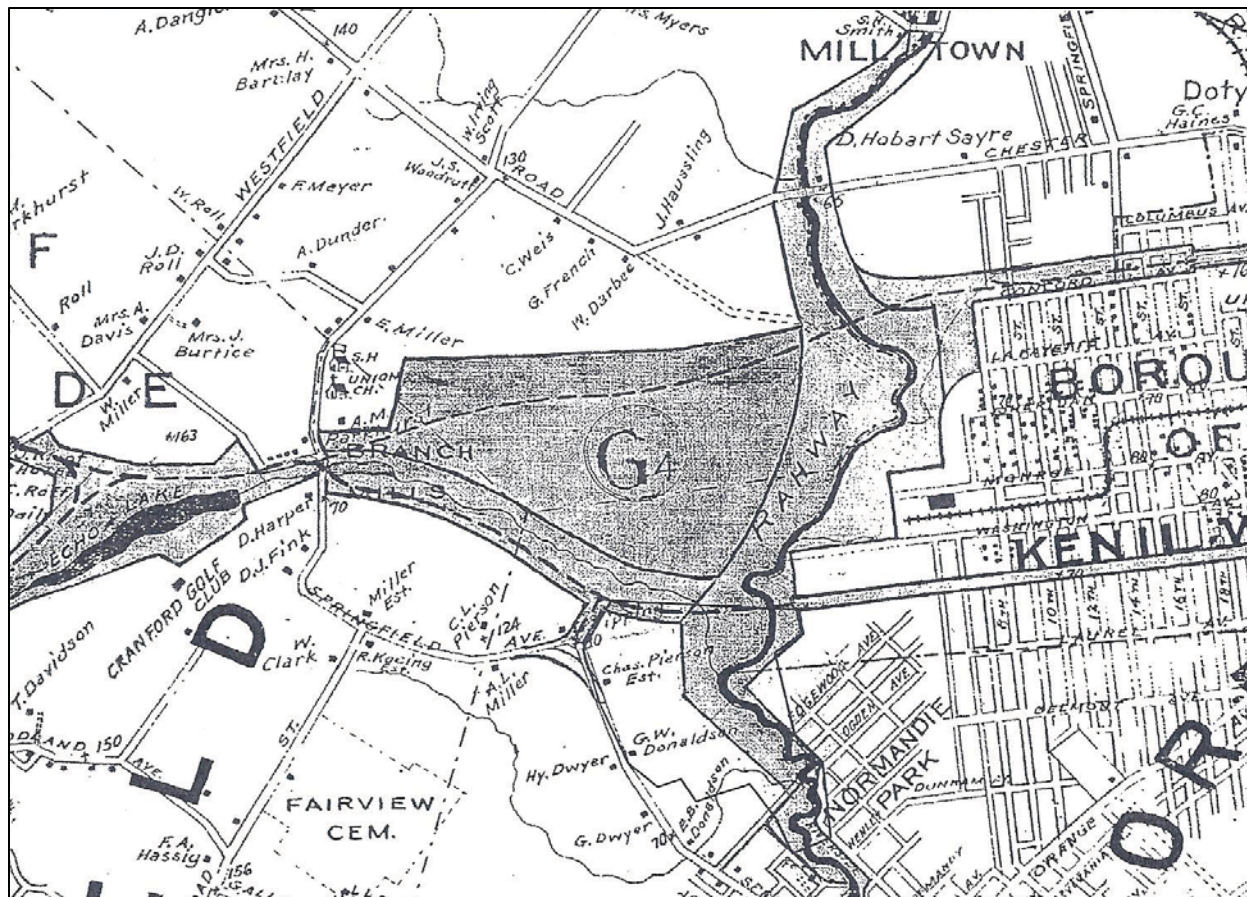


Figure 5.20. Normandie Park, the original incarnation of Nomahegan Park, as shown on a 1913 map of Union County, included in Olmsted Brothers report to H.S. Hatfield, 1921 (Dietrich 2004: Figure 68).

in 1926, was the location of a dump and a swamp, not a very promising start for a park. In 1924, Olmsted Brothers submitted a plan for a modest park, Normandie Park, at the location of the present Nomahegan Park as part of Rahway River Parkway (Figure 5.20). The park was likely called Normandie because of the terminus of Normandie Place at one its boundaries.

In 1926, the Commission renamed the tract Nomahegan Park. The name “Nomahegan,” which has numerous spellings, was believed to be a variation of *Noluns Mohegans*, a name applied to the New Jersey Indians in a treaty of 1758. It is translated as “women Mohegans” or she-wolves, and was applied to them in scorn by the Haudenosaunee (Federal Writers Project 1939:547). The park was expanded by this date and included an enormous lake with an island in the center, but did not extend to the Rahway River as it does today (Figure 5.21).

The Commission report of 1928 stated that it had spent \$47,840.70 on land purchased for the park; \$3,286.83 on architects and engineers; and \$959.54 on buildings and equipment, for a total expenditure of \$90,585.85 at Nomahegan (Union County Park Commission 1928). In that same year, 5,670 people were estimated to have visited to the park, with the highest number of visits at Warinanco Park of 635,301 (Union County Park Commission 1928). The 1930 Commission report indicated that Nomahegan Park had a baseball field, four fireplaces, one lake, one fishing area, one boating area, one wading pool, one marked nature trail, and one hockey

rink, totaling ten recreational facilities. Warinanco park, on the other hand, had 68 recreational facilities and Watchung Reservation had 89 (Union County Park Commission 1930).

A January 1931 visit by an Olmsted Brothers representative revealed a number of problems at Nomahegan Park. The large island, islands being a hallmark of Olmsted design, was experiencing a number of drainage and planting problems. A large number of trees were dying on the island and it was believed the area had become too wet. However, there was much consternation since the island's drainage problems had only recently begun and the company was puzzled. It recommended that the dying trees be replanted with the same species but that a foot of soil be added to the whole island. The cost of the new plantings for the park was estimated at \$950.00 (Olmsted Brothers 1931:6).

The Olmsted inspector was very pleased to find two or more nice patches of sheep laurel (*Kalmia angustiflora*), which is rare in the region, and a number of patches of brook euonymus (*Euonymus americanus*), a low ground cover. The firm wanted Tracey to experiment with transplanting patches of both to "the nursery" (probably Watchung Reservation) to use elsewhere in the system. However, the euonymus had scale, an insect infestation, and the firm was concerned that it might not transplant well and that the nursery attendants might not want to try to eliminate the infestation. Olmsted Brothers thought it was something that the system should try (Olmsted Brothers 1931:6).

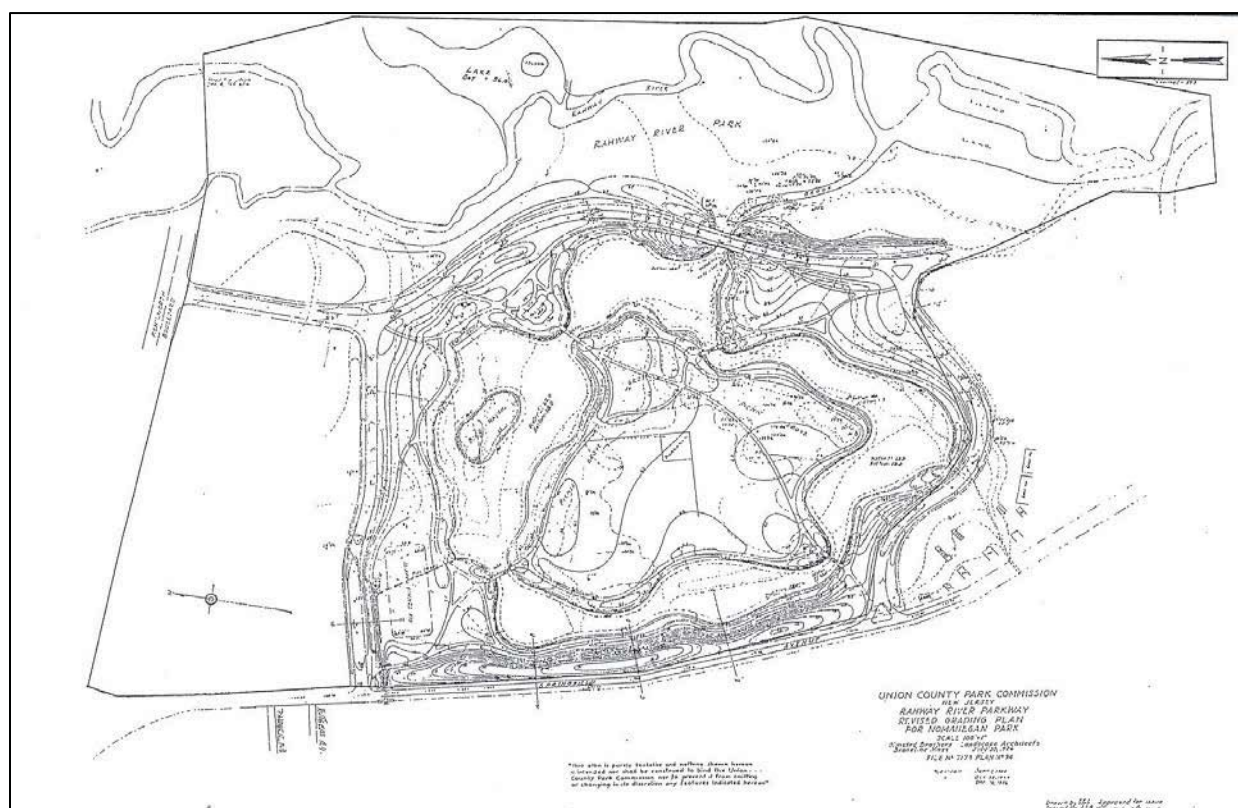


Figure 5.21. Nomahegan Park, 1926, revised grading plan, Olmsted Brothers; note the Rahway River Parkway on the east edge of the park (Dietrich 2004: Figure 68).

While the parks were viewed by many simply as places of recreation, the Commission also wanted to keep them as places of rest and contemplation. In 1935, Arthur R. Wendell, President of the Union County Park Commission, wrote an article for the magazine *Parks and Recreation*, which extolled the esthetic appeal of the Union County Park system. In it he wrote,

Beauty...is everywhere and runs rampant in the parks. Though much of it can be traced to Mother Nature throughout the years, more of it, probably, is due to successful planning and planting, in the conversion of swamps into lakes, planting of trees, shrubs, and flowers, where ash and garbage dumps were before, and the coordination of all phases of park work into a beautiful and usable whole [Wendell 1935:365].

Wendell noted that Nomahegan Park annually received Japanese cherry trees from the Cranford Garden Club that were planted around the lake. In keeping with this tradition, in 1971, the Cranford Centennial Committee donated 100 Japanese cherry trees to the park to be planted along the lake's edge near Springfield Avenue in honor of the township's centennial (Dietrich 2004).

At present, Nomahegan Park's primary entrance is from Springfield Avenue between levees (see Figure 5.19). The park has a paved parking area and bathroom facilities, ball fields, a soccer field, picnic areas and walking trails, none of them historic. The Rahway River Parkway extends south from the park and is accessed by a walking path that tops a levee.

McConnell Park. McConnell Park (ID #4424.025 [McConnell Park Section of Rahway River Parkway]), PRN 123) is eligible for the NRHP as a contributing resource to the Rahway River Parkway HD (ID #4079), the Union County Park System HD (ID #4424), and the North Cranford HD (ID #3838) (Figure 5.22). McConnell Park is located at the end of Central Avenue, enclosed in a "U"-shaped bend in the Rahway River (see fold-out map at the end of the report). This park is not an Olmsted Brothers designed site.

The park is named after Cranford's first physician, Dr. Joseph Kerr MacConnell, who owned the property where the park is located. It is reported that Dr. MacConnell sold the property to the township for use as a park at price below its market value (Cultural Resource Consulting Group 2001:19 quoting *Cranford Citizen* August 12, 1919). It is not known when the "a" dropped out of MacConnell when applied to the park.

Although the current Union County Park System map shows a building in the park, one is no longer extant. The park is simply an open space with a walkway around the outside edge with scattered picnic tables, benches, and trash cans. At the time of the current investigation, McConnell Park was still suffering from the effects of the 2011 flood with many trees downed and much debris still piled on the grounds.

New Jersey Department of Environmental Protection
Historic Preservation Office

Page 1 of 2

BASE SURVEY FORM

Historic Sites #: _____

Property Name: Rahway River Parkway

Street Address: Street #: _____ (Low) _____ (High) Apartment #: _____ (Low) _____ (High)

Prefix: _____ Street Name: _____ Suffix: _____ Type: _____

County(s): Union Zip Code: 07081

Municipality(s): Townships of Springfield, Union, Cranford and Clark; Borough of Kenilworth;

Cities of Rahway and Linden Block(s): _____ Lot(s): _____

Ownership: Public USGS Quad: Roselle and Perth Amboy

Description:

The Rahway River Parkway is a riverine greenway, defined by the dense vegetation bordering the river and designed by the Olmsted Brothers Landscape Architects for the Union County Park Commission. As the first park unit planned by the park commission, the Rahway River Parkway serves several purposes: to restore the Rahway River as a natural aquifer, to preserve fish and wildlife, to provide recreation to county residents, and most importantly, to be the cross county link between the entire Union County Park system. Located nearly in the center of Union County, the Rahway River Parkway forms the spine of an interconnected series of parks planned and developed by the Union County Park Commission, only the second county park commission in the country when it was formed in 1921.

Registration and Status Dates:

National Historic Landmark: _____

SHPO Opinion: _____

National Register: _____

Local Designation: _____

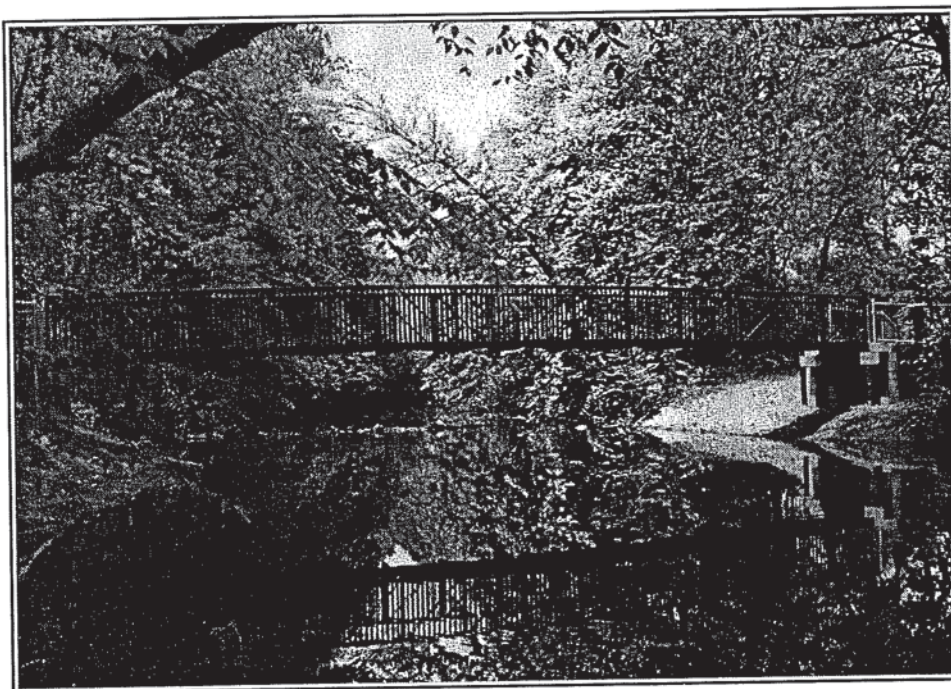
New Jersey Register: _____

Other Designation: _____

Determination of Eligibility: _____

Other Designation Date: _____

Photograph:



Survey Name: NI Route 82 Bridge over Rahway River

Date: January 2002

Surveyor: Glenn R. Modica

Organization: Richard Grubb & Associates

New Jersey Department of Environmental Protection
Historic Preservation Office

Page 2 of 2

BASE SURVEY FORM

Historic Sites #:

Location Map:

2.5" x 3.5"
(Include North Arrow and Bar Scale)

SEE CONTINUATION SHEET

Site Map:**INTENSIVE-LEVEL USE ONLY**

2.5" x 3.5"
(Include North Arrow and Bar Scale)

Bibliography/Sources:

SEE CONTINUATION SHEET

Additional Information:**More Research Needed?** ☐ Yes ☐ No**INTENSIVE-LEVEL USE ONLY:**

Attachments Included: _____ Building/Element _____ Landscape _____ Farm
_____ Bridge _____ Industry

Historic District: ☐ Yes ☐ No **Historic District Name:** _____

Status: _____ Key Contributing _____ Contributing _____ Non Contributing

Associated Archaeological Site/Deposits? ☐ Yes ☐ No

(Known or potential sites - If yes, please describe briefly)

Unknown

Survey Name: NJ Route 82 Bridge over Rahway River

Date: January 2002

Surveyor: Glenn R. Modica

Organization: Richard Grubb & Associates

CONTINUATION SHEET

Historic Sites #:

Survey Name: NJ Route 82 Bridge over Rahway RiverDate: January 2002Surveyor: Glenn R. ModicaOrganization: Richard Grubb & Associates

New Jersey Department of Environmental Protection
Historic Preservation Office

Page 1 of 1

LANDSCAPE ATTACHMENT

Historic Sites #:

Common Name: Rahway River Parkway
Historic Name: Rawhay River Parkway
Present Use: Passive recreation-outdoor
Historic Use: Passive recreation-outdoor
Construction Date: 1922 **Source:** Union County Park Commission 1922
Alteration Date(s): 1922-present **Source:** Union County Park Commission 1922-1963; Ryan 2002
Primary Landscape Architect/Designer: Olmsted Brothers Landscape Architects
Type: Greenway **Physical Condition:** Good
Style: Olmstedian **Remaining Historic Fabric:** Medium
Acreage: 533
Hardscape: _____
Plantings: Mature Hardwoods
Other Features: Ponds; dams

Description:

See Base Form

Setting:

While the setting within the Rahway River Parkway appears largely naturalistic, bordered by a dense growth of trees and vegetation, land use along its outside borders vary from low-density single family houses at its northern extremities, then gradually changing to high-density multi-family dwellings and modern commercial standalone buildings at the parkway's southern reaches.

Survey Name: NJ Route 82 Bridge over Rahway RiverDate: January 2002Surveyor: Glenn R. ModicaOrganization: Richard Grubb & Associates

New Jersey Department of Environmental Protection
Historic Preservation Office

Page 1 of 1

ELIGIBILITY WORKSHEET

Historic Sites #:

History:

SEE CONTINUATION SHEET

Statement of Significance:

SEE CONTINUATION SHEET

**Eligibility for New Jersey
and National Registers:**☒ Yes ☐ No**National****Register Criteria:** ☐ A ☐ B ☒ C ☐ D**Level of Significance:** ☐ Local ☐ State ☒ National**Justification of Eligibility/Ineligibility:**

The Rahway River Parkway retains integrity of setting, location, workmanship, design, materials, feeling and association. Over time, certain sections of the parkway have been altered, sometimes to its detriment, as in the construction of the Garden State Parkway, and other times to meet the needs of the community, such as the addition of ballfields or playgrounds. Nonetheless, the extent and original borders of the Rahway River Parkway have remained intact.

For Historic Districts Only:

Property Count: Key Contributing: _____ Contributing: _____ Non Contributing: _____

For Individual Properties Only:

List the completed Attachments related to the property's significance:

Landscape Attachment _____

_____**Narrative Boundary Description:**

Bordered on the north by the south side of Springfield Avenue, Springfield Township and to the south by the north side of Elizabeth Avenue, City of Rahway, and including, Lenape, Nomahegan, Rahway River, Milton Lake, Black Brook and Wheatena parks.

Survey Name: NJ Route 82 Bridge over Rahway RiverDate: January 2002Surveyor: Glenn R. ModicaOrganization: Richard Grubb & Associates

CONTINUATION SHEET

Historic Sites #:

HISTORY

How the Rahway River Parkway came to be and how it has changed over time represents society's changing perceptions of nature. The desirability of urban parks and their benefits can be traced to the development of garden cemeteries in the 1830s. Generally located in suburban areas such as Cambridge, Massachusetts and Brooklyn, New York, these attractively laid out garden cemeteries were intended to provide moral, religious and intellectual uplift. This combination of moralism and landscape aesthetics reached a wide American audience through the work of Andrew Jackson Downing, who espoused the idea that exposure to rural and pastoral scenery would cleanse the soul and improve moral behavior. According to Downing, "parks would soften and humanize the rude, educate and enlighten the ignorant, and give continual enjoyment to the educated" (Schuyler 1986:66).

Social reform through landscape architecture found its most forceful and influential protagonist in Frederick Law Olmsted. As the foremost designer of parks in the United States, Frederick Law Olmsted's best-known works are Central and Prospect Parks in New York City and the "Emerald Necklace" in Boston. Not only a skilled landscape designer, Olmsted advocated that open public spaces and parkland would provide relief from overcrowded urban conditions. Like other reformers of his day, Olmsted was particularly concerned about the well-being of the working poor, who endured the worst conditions and lacked the means to escape their plight (Newton 1971: 267). Unlike earlier reformers who linked pastoral scenery with moral behavior, Olmsted advocated development of public parks in terms of their therapeutic value: "the enjoyment of scenery employs the mind without fatigue and yet exercises it, tranquilizes it, and yet enlivens it; and thus, through the influence of the mind over body the effect of refreshing rest and reinvigoration of the whole system" (Rybczynski 1999: 258). When Olmsted, along with his partner Calvert Vaux, designed Central Park in New York they ushered in a new era in park planning. Previously, urban parks had been laid out as afterthoughts, on land left over after street openings. Building Central Park set a precedent as it was the first urban park built with public funds and open to all (Newton 1971: 267).

At the same time yet on a larger scale, the national park movement in the American west ushered in a new conservation ethic. As westward settlement encroached on scenic areas and vital watersheds, the federal government stepped in and acquired vast acres of land, beginning with Yellowstone Park in 1872, thereby forming the foundation of the national park system. Conservation efforts in the east focused on reclaiming rivers and streams from years of pollution by industrial and human waste. During the 1880s, Frederick Law Olmsted and the city of Boston embarked on a project to link multiple parks within the city by tree-lined parkways. To connect the city's parks, Olmsted proposed the Muddy River Improvement project, also known as "green fingers." This continuous strip of parkland followed the Muddy River valley and connected a series of parks from Boston Commons to Franklin Park. In later years sections of this riverine parkway would be known as Riverway and Jamaica Way. In 1893, development of the city's parkland was placed under the jurisdiction of the Metropolitan Park Commission (Newton 1971: 294, 300). In 1906, engineers in Westchester County, New York began to clean and beautify the Bronx River, long a receptacle of sewage and industrial waste, by creating parkland on both sides of the river. Although not initially planned, this conservation effort led to the construction of the Bronx River Parkway (Newton 1971: 598).

In the early twentieth century, the forces of conservation and landscape architecture fused with urban planning to create the City Beautiful movement. Originating from Chicago's 1893 World's Columbian Exposition, proponents of the City Beautiful movement were concerned with rescuing cities from their chaos, ugliness, filth and poverty. They called upon landscape architects, artists, planners and businessmen to integrate city planning with a design aesthetic to revive the American city and relieve the maladies of urban life. While the most obvious manifestation of the City Beautiful movement is monumental neoclassical architecture, City Beautiful advocates were also interested in shaping and beautifying the whole urban environment. Many City Beautiful plans for urban areas included extensive systems of outlying parks, parkways and boulevards, such as Daniel Burnham's plans for San Francisco (1906) and Chicago (1909), Charles Mulford Robinson's design for Denver (1906) and Honolulu (1906) and John Nolan's recommendations for San Diego (1909) and Reading, Pennsylvania (1910) (Wilson 1980: 176-83).

In New Jersey, the Park Act of 1895 enabled local governments to create multi-jurisdictional park commissions. As a result, Essex County established the first county park commission in the United States. The Park Act was modeled on the one established in Boston in 1893 creating the Metropolitan Park Commission. Established two years prior to the Essex County Park Commission, Boston's Metropolitan Park Commission has the distinction of being the first *metropolitan* park system (Newton 1971: 323). Although the Park Act of 1895 provided the basis for the establishment of other county and municipal park commissions in New Jersey, it would not be until 1921 that the state's second park commission was established in Union County.

Survey Name: NJ Route 82 Bridge over Rahway RiverSurveyor: Glenn R. ModicaOrganization: Richard Grubb & AssociatesDate: January 2002

CONTINUATION SHEET

Historic Sites #:

HISTORY CONTINUED

The Rahway River supplied Union County residents with fundamental and recreational needs, for bathing, drinking, canoeing, skating and fishing. In Rahway, that city's incipient development as a center of shipping and trade owes much to the fact that large sailing vessels could not navigate the Rahway River beyond the city. From its river landings, Rahway carried on a brisk trade with Perth Amboy, and as steamboats were introduced in the nineteenth century direct service was provided to New Brunswick and Elizabeth (Lane 1939:63 and 214). While the river's narrow channel precluded maritime trade further upstream, it did provide ample flow to power early mills. In 1834, twenty mill seats between Springfield and Rahway were engaged in grinding grain, sawing lumber and producing paper, cotton and wool (Gordon 1834: 222). By the early twentieth century more intensive industries located along the banks of the river, which had become by then a receptacle for human and industrial waste. The pollution of the Rahway River became such a concern that it motivated influential citizens to take action.

In 1919, Union County Sheriff James E. Warner, who in his youth had swam and fished in the Rahway River, decried its polluted condition in a letter to the *Cranford Citizen* (Foley 1990: 3). Warner's letter moved Cranford Township Committeeman D.C.N. Collins to bring further attention to the situation in a letter to the *Elizabeth Daily Journal* and in a privately printed booklet distributed to the Cranford Township Committee. Collins was dismayed at the gradual deterioration of the Rahway River; industrial and human waste that poured into the river poisoned all the fish; new development that encroached along its banks pushed out wildlife; and, private ownership of adjacent land precluded public enjoyment. To safeguard the natural beauty of the Rahway River, Collins, an engineer by trade, sketched a plan for "Union County Memorial Park," a linear greenway dedicated to the veterans of the First World War, that stretched along the banks of the Rahway River from Springfield Avenue in Springfield Township to St. George Avenue in Rahway (Cunningham 1971). The Memorial Park, Collins hoped, would eventually connect with the parks in Essex County. But Collins did not stop there. He also envisioned miles of interconnected drives, recreational areas and camp grounds that linked every municipality in the county, a "connecting chain of recreation scenes," as Collins wrote.

The Union County Park Association was duly formed and charged with the task of establishing a permanent park commission. On April 30, 1921 New Jersey Superior Court Justice James J. Bergen appointed a temporary commission to study the feasibility of a permanent park commission (Union County Park Commission 1922a:5). The temporary commission was given two years and a budget of \$10,000 to prepare their report. In September 1921, after only five months and spending less than two thousand dollars, the temporary commission urged prompt action to establish a county park system. In their report the commission couched their language in the rhetoric of contemporary progressive reformers, asserting that "with no provision for recreation, no breathing places for its increasing population, no spots available for outdoor amusement...the health of the whole community is menaced," and taking a page from Frederick Law Olmsted, stating that only a county park system would "assure proper development of mind and body" (Union County Park Commission 1922a:29-30). Well aware of their county's proximity to the New York metropolitan area, the temporary commission presciently warned that it would be only a matter of time before their county would become more urbanized and densely populated, thereby foreclosing any opportunity to acquire any undeveloped land. The commission was "particularly impressed by large areas of ground either now or wholly waste land or occupied as farm or wood land, admirably adapted for Park purposes, which are now available at reasonable prices but which are in imminent danger of soon being beyond the reach of reasonable acquisition" (Union County Park Commission 1946 Report: 5). The acquisition of such land, urged the commission, would achieve their goals of preserving the water supply and establishing much needed parkland (Union County Park Commission 1922a: 26-30). In the November elections a referendum was put on the ballot to appoint a permanent park commission. Despite opposition from the more populated areas of the county, voters approved the referendum, and on November 19, 1921 Justice Bergen appointed a permanent five-member Union County Park Commission. Its original members were Henry S. Chatfield of Elizabeth, Arthur R. Wendell of Rahway, Charles Hansell of Cranford, Caxton Brown of Summit and Charles A. Reed of Plainfield (Union County Park Commission 1922a:6).

The choice for a landscape architect was easy. Olmsted Brothers Landscape Architects of Brookline, Massachusetts, who had worked so successfully on the Essex County park system, were the obvious choice. The legacy of Frederick Law Olmsted, Sr. had passed to his son, Frederick, Jr. and to his stepson, John Charles Olmsted. Working together under the name Olmsted Brothers, this second generation landscape design firm formed the core of the nation's leading landscape design firm in the early-to mid-twentieth century (Louis Berger & Associates, Inc. 1999:8). Early in 1921, the temporary park commission retained the firm to recommend a feasible approach to the county-wide park system.

The Olmsted Brothers plan for Union County called for series of parks linked by the county's natural features, its "undulating topography and the quiet pastoral character of the county-side." Main elements of the plan were a park in the Watchung Reservation, the "parking" of the Elizabeth and Rahway rivers, which ran north to south through the county, and an east-west parkway linking the whole. Individual parks to serve local communities were also recommended as land became available. The Olmsted's envisioned linking the Rahway and Elizabeth river parkways with the Essex County Parks. This vision never was realized (Olmsted Brothers Landscape Architects 1921).

Survey Name: NI Route 82 Bridge over Rahway River
Surveyor: Glenn R. Modica
Organization: Richard Grubb & Associates

Date: January 2002

CONTINUATION SHEET

Historic Sites #:

HISTORY CONTINUED

The Olmsted Brothers plan for "parking" the Rahway River, the firm acknowledged, was based on D.C. Newman Collins plan for the Union County Memorial Park drawn up two years earlier but with some modifications (Olmsted Brothers Landscape Architects 1921). The Olmsted's reduced the extent of the parkway in areas they believed the land was more suitable for residential purposes, such as in Cranford, the Sperry Farm in Kenilworth and the region south of Morris Avenue in Springfield. The Olmsted's expanded the Collins plan in Rahway, where they proposed extending the parkway along the Robinson's Branch up to Madison Hill Road. They advocated acquiring a minimum amount of land bordering the river- "a narrow margin of bank-" that was too low and marshy for development yet would provide a pleasing environment for future residential development. In all, the Olmsted Brothers envisioned a river park encompassing 1097 acres (Olmsted Brothers Landscape Architects 1921).

While the Olmsted Brothers made their recommendations and prepared general plans for the parkway, it was up to the Union County Park Commission to carry out the work. They immediately got to the task of surveying the Rahway River valley. By August 1922 the park commission had completed a topographical survey of land in Cranford, and by February 1923 another survey had been made from Cranford through Springfield Township to the Essex County border (Union County Park Commission 1922b and 1923). In time, the swamps and low-lying areas bordering the river would either be drained or filled to create lakes; farmland and wood lots would be graded and landscaped. While still in the planning stages, the Union County Park Commission described the Rahway River as a "picturesque and winding stream" that "offers unusual opportunities for a continuous parkway development linking up a series of neighborhood parks and other parks, preserving places of exceptional natural beauty." According to the commission, it was to be "perhaps the most important unit in the Park System" (Union County Park Commission 1925: 18).

To oversee the engineering and to work with the county's landscape architects, the Union County Park Commission hired W. Richmond Tracy. Tracy had been chief bridge builder on the Bronx River Parkway but was lured away to the park commission in 1922 to serve as the newly appointed Engineer and Secretary, positions he held until 1957 (Louis Berger & Associates, Inc. 1999: 10). Tracy was largely responsible for implementing Olmsted's Plan and approving most development plans for the Union County Park system.

The first piece of land obtained for the Rahway River Parkway, and for that matter, the Union County Park System, was donated in 1922 by the Wheatena Company of Rahway. By a unanimous vote, the stockholders of the Wheatena Company donated five acres of improved land in Rahway with a proviso that the land be used solely for park or playground purposes (Union County Park Commission 1922a:16; 1925:11). This property at Elizabeth and Grand Avenue's with a 2300 foot frontage along the river and Main streets is today's Wheatena Park.

In 1925, the City of Rahway donated to the county a 40-acre tract of land occupied by the Rahway Poor Farm. In the next four years the park commission had built baseball and soccer fields and dammed the river to create a lake. By the summer of 1931, the park had become a summer paradise, replete with a bath house, swimming pool and sand beach, the first outdoor bathing complex in the county (Schmidt 1980). Now called Rahway River Park, this section was known until the 1940s simply as the Rahway River Parkway-Rahway Section. Perhaps planned as the gateway to the entire parkway, a vehicular drive enters Rahway River Park at St. Georges Avenue and continues north past Valley Road allowing an uninterrupted scenic drive along the river.

Land acquisition continued apace in the 1920s. Capital expenditures between 1924-1927 exceeded \$800,000 a year, a figure that would never be approached again (Union County Park Commission 1963: 46). In fact, by October 1925 about 35% of lands for the Rahway Parkway had been acquired or were under contract, and by the end of the decade the commission had expended more money for land acquisition for the Rahway River Parkway than any other park (Union County Park Commission 1925: 19). The Park Act of 1895 authorized the commission to acquire land for park purposes either by purchase, donation or, as a last resort, condemnation. Most of the property for the Rahway River Parkway, in particular, and the Union County Park system, in general, had been purchased directly from the property owners. By 1957, when the county parks encompassed 4874 acres, the commission had acquired 3594 acres through purchase, 650 acres through donations and 630 acres by condemnation (Union County Park Commission 1957: 13). By 1930, the county had acquired nearly 90% of the land intended for the entire park system, an impressive achievement. Still, most of the proposed parkway still lay undeveloped. And although the commission had by 1929 completed the first step- acquire the land- with the depression of the 1930s and then the Second World War, it would be up to future generations to shape it.

The Park Commission had produced four annual reports between 1922-1931, yet not another was prepared until 1946. Whereas the earlier reports had proudly listed notable accomplishments and had expressed optimism about the future, the 1946 Report, fifteen years in the making, exudes a far more somber, almost defeated tone. In his Presidents Report, Caxton Brown, now 68 years old and a member of the commission since its inception in 1921, speaks of the troubled period the park commission had experienced in the intervening years. Brown singled out the Rahway River Parkway as a project that was "still in its incipency."

Survey Name: NJ Route 82 Bridge over Rahway River
Surveyor: Glenn R. Modica
Organization: Richard Grubb & Associates

Date: January 2002

New Jersey Department of Environmental Protection
Historic Preservation Office

Page 1 of 1

CONTINUATION SHEET

Historic Sites #:

HISTORY CONTINUED

Both sides of the river still needed to be landscaped, Brown pointed out, with a "protective fringe and the establishment of suitable paths, driveways and bridges to make the area conform in appearance to some of the famous river treatment of the Westchester County Park System in New York" (Union County Park Commission 1946: 2). After nearly two decades of economic uncertainty, the park commission had to keep expenditures down. New land acquisition came to a virtual halt and little was expended for maintenance, allowing the park system to deteriorate. Lawns, shrubs and trees suffered from insects; soils were depleted. Capital expenditures that had exceeded \$5 million in the first ten year period of the park commission (1922-1931) had been reduced in the ensuing fifteen years (1932-1946) to less than \$500,000 (Union County Park Commission 1946: 46). "Hence it was proved," Brown stated in his report, "that there could be no more unwise economy than to jeopardize the existing investment and the beauty and facilities of that which had been developed" (Union County Park Commission 1946:3).

Federal work relief programs, such as the CCC, WPA and PWA, in cooperation with the National Park Service did provide some funding, labor and materials for further development of the county's parks. The CCC program, active between 1933-1939, converted the American Chemical Factory at Meisel Avenue into Camp NJ SP-3 Company 1272 (Union County Park Commission ca. 1935). This complex of 17 buildings was converted to workshops, dormitories, an infirmary, recreation hall and administration buildings (Union County Park Commission 1946:36). Brown certainly had been proud of the previous era's work, commending the park commission's previous effort and achievement, particularly in land acquisition. Yet "that character of work," Brown pointed out, "is not now indispensable." Considering that nearly all of the land for the county park system had been acquired, Brown suggested that its future development and beautification "should be assumed by those who later on will have a real need of them" (Union County Park Commission 1946: 2).

But the next generation had other issues to contend with. New highway construction infringed upon existing parkland, leaving isolated sections of parkland that bore no relation to the county plan. The Garden State Parkway removed part of the western edge of the Rahway River Parkway for about a mile before finally severing it at Raritan Road, taking away twenty-two acres of greenway (Union County Park Commission 1957: 11). In the face of "an even greater problem with the recently announced Federal-State highway program" (the Interstate Highway Act of 1956), the Union County Park Commission conceded that "we see no virtue in stubborn opposition" (Union County Park Commission 1957: 5). By 1958, 58 acres of parkland had been ceded to highway development. Highway acquisition did, however, put much needed revenue into the county coffers- \$709,000 by 1958- thereby allowing the park commission to acquire an additional 404 acres for parkland between 1947-57 (Union County Park Commission 1957:13).

More highways also meant more people, placing even greater demands upon the park system. As the population changed so too did their perceptions of nature. With the rise of suburbs, naturalistic parks became less essential than before when they provided the only means to escape congested urban centers. As communities grew, they demanded more active recreational facilities, more ball fields and playgrounds, more picnic areas and refreshment stands. The public's idea of nature no longer meant a place of pastoral scenery meant for quiet contemplation but rather as the backdrop to large open areas of active play and recreation (Nash 1973). To meet the need, the Park Commission began clearing discrete sections of the Rahway River Parkway for recreational facilities and small playgrounds to serve the local community. These projects, however, have occurred outside the margin of the Rahway River's vegetative fringe.

In 1978, the autonomous Union County Park Commission was abolished and replaced by the Union County Department of Parks and Recreation. Today, the Union County park system contains 26 parks totaling 5,574 acres (Union County Department of Parks and Recreation 1999).

Survey Name: NJ Route 82 Bridge over Rahway RiverDate: January 2002Surveyor: Glenn R. ModicaOrganization: Richard Grubb & Associates

New Jersey Department of Environmental Protection
Historic Preservation Office

Page 1 of 1

CONTINUATION SHEET

Historic Sites #:

STATEMENT OF SIGNIFICANCE

The Rahway River Parkway has a national level of significance as an intact, early and unique example of a river-oriented greenway that is an integral part of one of the first planned regional park systems in the United States. The Rahway River Parkway meets National Register Criterion A for its significant historical association with the conservation movement, a movement that originated with the nineteenth century National Parks system. The creation of the Rahway River Parkway represents an early and successful effort to safeguard the water supply and preserve the scenic qualities of the Rahway River. Furthermore, the Rahway River Parkway, in particular, and the Union County park system, in general, represent a benchmark in an evolving social movement to develop public parkland in the face of growing urbanization. Since the mid-nineteenth century, prominent landscape architects such as Alexander Jackson Downing and Frederick Law Olmsted advocated the creation of public parkland to intellectually, morally and spiritually benefit society. By the early twentieth century, the movement to incorporate landscape architecture with urban land use planning had reached its fullest expression in metropolitan settings with the City Beautiful Movement. Following this tradition the Union County Park Commission was established in 1921, only the second county park commission in the country, with the express mission of developing a county-based park system linked through a series of riverine parkways. Through their continual effort to acquire and develop a system of interconnected of parks using public funds for the benefit of the public, the Union County Park Commission set an example that would be followed by other municipal park commissions later in the century.

The Rahway River Parkway also meets National Register Criterion C for its association with a master landscape architectural firm- Olmsted Brothers Landscape Architects of Brookline, Massachusetts. Carrying on the tradition of Frederick Law Olmsted, Sr., Olmsted Brothers Landscape Architects were the foremost landscape design firm of the early twentieth century, having consulted on major projects throughout the United States, including the Essex County park system in New Jersey. The temporary Union County Park Commission hired the firm in 1921 to recommend a county-wide park system, and in the following decades the firm continued to consult and prepare plans for the development of Rahway River Parkway.

The suggested period of significance is 1921-1968, a period that represents the parkway's extended period of design and development, beginning with the year the Union County Park Commission was formed and ending with the last documented plan for the parkway prepared by Olmsted Brothers Landscape Architects (Union County Park Commission 1930b, revised 1956 and 1968). Boundaries for this historic resource, which are based on a review of original plans for the Rahway River Parkway and an assessment of existing integrity, should include the entirety of the Rahway River Parkway as well as the local parks which it links. The suggested boundaries are shown on Figure 9.4. In general terms, the northern boundary for the Rahway River Parkway is the south side of Springfield Avenue in Springfield Township and the southern boundary is the north side of Elizabeth Avenue in Rahway. The parkway should also include the Robinsons Branch from its confluence with the Rahway River just north of Elizabeth Avenue to the south side of Madison Hill Road. The boundaries should encompass the vegetative buffer that lines both banks of the Rahway River. Within the original boundaries of the Rahway River Parkway- as depicted on early plans (see Figures 9.1-9.3)- are present-day Lenape Park, Nomahegan Park, Rahway River Park, Black Brook Park, Wheatena Park and Milton Lake Park. It is recommended that these individual parks, conceived and developed after the Rahway River Parkway and designated Project Number 7173- the same as the parkway- by the Olmsted Brothers Landscape Architects, should be included within the parkway's boundaries. The smaller municipal parks within the parkway should also be included as they contribute to the recreational nature of the resource.

Additionally, all bridges spanning the Rahway River within the boundaries of the Rahway River Parkway and built prior to 1968 should be considered contributing resources to the parkway.

Not evaluated as part of this investigation is the contributing or non-contributing status of the numerous extended and truncated segments of vehicular roads that parallel the Rahway River Parkway. These roads are an external and secondary feature to the parkway rather than part of an inter-connected circulation system. Within the APE, one such example is Riverside Drive in Springfield Township. This road is an extension of Washington Avenue that parallels the west side of the parkway and continues south beyond Meisel Avenue Park and the boundaries of the parkway.

Survey Name: NI Route 82 Bridge over Rahway RiverDate: January 2002Surveyor: Glenn R. ModicaOrganization: Richard Grubb & Associates

New Jersey Department of Environmental Protection
Historic Preservation Office

Page 1 of 1

CONTINUATION SHEET

Historic Sites #:

BIBLIOGRAPHY

- Collins, D.C.N.
1919 Big County Park Should Be Made. *Elizabeth Journal* May 16, 1919. On file, Union County Department of Parks and Recreation. Elizabeth, New Jersey.
- Cunningham, John T.
1971 *To Benefit the Whole Population*. Union County Park Commission, Elizabeth, New Jersey.
- Foley, Jean L. (Ed.)
1990 The Union County Parks System. Union County Board of Chosen Freeholders, Elizabeth, New Jersey.
- Nash, Roderick
1973 *Wilderness and the American Mind*. Yale University Press, New Haven, Connecticut.
- Newton, Norman T.
1971 *Design on the Land: The Development of Landscape Architecture*. Belknap Press of Harvard University Press, Cambridge, Massachusetts.
- Olmsted Brothers Landscape Architects
1921 Letter to Mr. H.S. Charfield, Chairman, Union County Park Commission, Elizabeth, New Jersey. On file, Historic Preservation Office, Trenton, New Jersey.
- Ryan, Sean
2002 Personal Communication, Sean Ryan, Landscape Architect, Union County Department of Parks and Recreation, Elizabeth, New Jersey.
- Rybczynski, Witold
1999 *A Clearing in the Distance*. Simon & Schuster, New York, New York.
- Schuyler, David
1986 *The New Urban landscape: The Redefinition of City Form in Nineteenth-Century America*. Johns Hopkins University Press, Baltimore, Maryland.
- Union County Department of Parks and Recreation
1999 *The Union County Parks and Recreation Guide*. Union County Department of Parks and Recreation, Elizabeth, New Jersey.
- Union County Office of Cultural and Heritage Affairs
1986 Union County Historic Sites Inventory. Office of Cultural and Heritage Affairs, Westfield, New Jersey. On file, Historic Preservation Office, Trenton, New Jersey.
- Union County Park Commission
1911 Battlefield Park, Springfield, N.J. owned and developed by George F. Treiber. June 28, 1911. J.L. Bauer, Civil Engineer, Elizabeth, NJ. On file, Union County Department of Parks and Recreation, Elizabeth, New Jersey.
- 1922a *Report for 1922*. On file, Union County Department of Parks and Recreation, Elizabeth, New Jersey.
- 1922b Topographical Map of Properties along the Rahway River Through the Township of Cranford. August 1922. N.K.P. On file, Union County Department of Parks and Recreation, Elizabeth, New Jersey.
- 1923 Topographical Map of Properties along the Rahway River from Cranford Township to Essex County, February 1923. Union County Park Commission. On file, Union County Department of Parks and Recreation, Elizabeth, New Jersey.

Survey Name: NJ Route 82 Bridge over Rahway River
Surveyor: Glenn R. Modica
Organization: Richard Grubb & Associates

Date: January 2002

New Jersey Department of Environmental Protection
Historic Preservation Office

Page 1 of 1

CONTINUATION SHEET

Historic Sites #

- 1924 Rahway River Parking, Section from Milltown Road to Morris Avenue, June 1924. Olmsted Brothers Landscape Architects, Brookline, Massachusetts. On file, Union County. Department of Parks and Recreation, Elizabeth, New Jersey.
- 1925 *Report for the Period from January, 1923 to October, 1925.* On file, Union County. Department of Parks and Recreation, Elizabeth, New Jersey.
- 1926a Rahway River Parkway, Sketch Showing Proposed Park Boundary Thru Property of the Elizabethtown Water Co., Springfield, Feb. 2, 1926 (retraced 1946). On file, Union County. Department of Parks and Recreation, Elizabeth, New Jersey.
- 1926b Rahway River Parkway, Survey of Woodruff and Berger Properties Situated in Union Township. August 1926. W.R. Pearson. On file, Union County. Department of Parks and Recreation, Elizabeth, New Jersey.
- 1926c Map Showing Properties from the Township of Cranford to the Township of Springfield Included in the Proposed Rahway River Parkway, Map No. 115, Nov. 1926, E.F.M. On file, Union County. Department of Parks and Recreation, Elizabeth, New Jersey.
- 1927 Survey of Properties of Rich and Elizabethtown Water Co. Situated in Springfield Township Showing Proposed Park Boundary, November 1927, C.E. and Surveyor. On file, Union County. Department of Parks and Recreation, Elizabeth, New Jersey.
- 1930 General Plan Rahway River Parkway, Jan 18, 1930. Olmsted Brothers Landscape Architects, Brookline, Massachusetts. On file, Union County. Department of Parks and Recreation, Elizabeth, New Jersey.
- 1930a General Plan Rahway River Parkway, January 18, 1930 (revised Jan 6, 1956 and July 24, 1968). Olmsted Brothers Landscape Architects, Brookline, Massachusetts. On file, Union County. Department of Parks and Recreation, Elizabeth, New Jersey.
- 1933 Rahway River Parkway, Channel Improvement. Preliminary Plan Showing Tree Location From Rahway Valley Railroad to Morris Ave, Springfield, March 1933. J.E.L. On file, Union County. Department of Parks and Recreation, Elizabeth, New Jersey.
- 1935 Civilian Conservation Corps, Camp NJ SP-3 Company 1272, Springfield, NJ. No date. R.C. Porter Jr. On file, Union County. Department of Parks and Recreation, Elizabeth, New Jersey.
- 1937 Rahway River Parkway Master Plan, Section- Rahway to Cranford, Sept. 30, 1937. George Spicer, Jr. On file, Union County. Department of Parks and Recreation, Elizabeth, New Jersey.
- 1944 Rahway River Parkway, Plan for Improvement of "Battlehill" Section at Springfield, March 1944, O.L.P. On file, Union County. Department of Parks and Recreation, Elizabeth, New Jersey.
- 1946 *Twenty-Five-Year Report, 1921-1946.* On file, Union County. Department of Parks and Recreation, Elizabeth, New Jersey.
- 1957 *Report for 1947-1957.* On file, Union County. Department of Parks and Recreation, Elizabeth, New Jersey.
- 1963 *Report for 1958-1963.* On file, Union County. Department of Parks and Recreation, Elizabeth, New Jersey.

Survey Name: NJ Route 82 Bridge over Rahway RiverDate: January 2002Surveyor: Glenn R. ModicaOrganization: Richard Grubb & Associates



HPO-I2002-189
02-3189
PROD
Page 1 of 3

James E. McGreevey
Governor

State of New Jersey
Department of Environmental Protection

Bradley M. Campbell
Commissioner

Division of Parks & Forestry, Historic Preservation Office
PO Box 404, Trenton, NJ 08625
TEL: (609) 292-2023 FAX: (609) 984-0578
www.state.nj.us/dep/hpo

September 18, 2002

John F. McQuillan, Manager
Bureau of Environmental Services
New Jersey Department of Transportation
1035 Parkway Avenue
P.O. Box 600
Trenton, NJ 08625-0600

Dear Mr. McQuillan:

As Deputy State Historic Preservation Officer for New Jersey, in accordance with 36 CFR Part 800: Protection of Historic Properties, as published in the Federal Register on 18 May 1999 (64 FR 27071-27084), I am providing Consultation Comments for the following project:

**Union County, Springfield and Union Townships
Replacement of the Morris Avenue (Route 82) Bridge over Rahway River
Str. # 2012-150**

These comments were prepared in response to your request for HPO review and comment on the following report:

"Cultural Resources Investigation, Route 82 (Morris Avenue) Bridge over Rahway River, (Structure No. 2012-150), Springfield and Union Townships, Union County, New Jersey" by Richard Grubb and Associates, Inc. (June 2002). HPO accession number UNI F 78b.

SUMMARY: *One new historic property is identified within this letter. The project as proposed will have an adverse effect on historic properties.*

800.4 Identifying Historic Properties

It is my opinion as Deputy State Historic Preservation Officer for New Jersey, in concurrence with the reviewed report, that the **Rahway River Parkway Historic District – located in Union County; Townships of Springfield, Union, Cranford and Clark as well as Kenilworth Borough– is eligible to be listed in the National Register**

of Historic Places. The Rahway River Parkway Historic District is eligible under Criterion A for its significant association with the conservation movement and under Criterion C as the work of a master (Olmsted Brothers Landscape Architects). The period of significance is 1921-1968. The eligible property is bordered on the north by the south side of Springfield Avenue in Springfield Township and to the south by the north side of Elizabeth Avenue in Rahway City. The eligible property includes Lenape, Nomahegan, Rahway River, Milton Lake, Black Brook and Wheatena parks.

Although not individually eligible, I concur with your recommendation that the Route 82 bridge (Str. #2012-150) is a contributing resource within the eligible Rahway River Parkway Historic District.

I concur that the following properties are not eligible to be listed:

- 2822-2824 Morris Avenue, Union Township
- 4-6 Washington Avenue, Springfield Township
- 8-10 Washington Avenue, Springfield Township
- 16 Washington Avenue, Springfield Township
- 18 Washington Avenue, Springfield Township
- 26 Washington Avenue, Springfield Township
- 19 Morris Avenue, Springfield Township
- 34 Morris Avenue, Springfield Township
- 37 Morris Avenue, Springfield Township
- 38 Morris Avenue, Springfield Township

Adequate efforts to identify archaeological historic properties have been made. There are no archaeological historic properties in the area of potential effect for this project. No further archaeological work is recommended.

800.5 Assessing Effects

I concur with your assessment that the project as proposed, demolition of a contributing resource within an eligible historic district, will have an **adverse effect** on identified historic properties. I look forward to additional documentation, such as an alternatives analysis report, demonstrating that you have sought means to avoid or reduce harm to historic properties.

Report Comments

No corrections or additions to the report are recommended.

I applaud the decision to conduct a metal detector survey to identify remains of the Battle of Springfield.



HPO-I2002-189
02-3189
PROD
Page 3 of 3

If you have any questions regarding this letter, please contact Andrea Tingey regarding architecture at (609-984-0539) or Mike Gregg regarding archaeology at (609-633-2395). Thank you.

Sincerely,

Dorothy P. Guzzo
Deputy State Historic
Preservation Officer

cc Randell Prescott, FHWA
 George Hoops, FHWA
 Sean Vroom, NJTPA
 Bruce Connor, Union County Engineer
 Susan Cohen, Director, Union County Cultural & Heritage Commission
 Dan Bernier, Director, Union County Park Planning & Maintenance
 Sean Ryan, Planner, Union County Park Planning & Maintenance
 Dennis Miranda, Project Manager, NJ Conservation Foundation
 Tom Carbone, NJDOT-BPSD
 Al Maiocchi, NJDOT-LGS

fallen servicemen from World War II (Union County Park Commission 1958:21). In 1950, a masonry field house, housing restrooms, sporting equipment and a concession stand, was constructed inside the northeast section of the vehicular parkway. In addition, the four tennis courts were re-surfaced and given new metal nets. New stone fireplace units were also installed in the park's various picnic areas. In 1957, a stone bench and plaque were installed along the lake to honor former commissioner Arthur R. Wendell, while in 1969 a series of Japanese cherry trees were planted along a lakeside grove. In 1980, a fitness trail was installed along and within the parkway loop, and in 1982 a new bathhouse was constructed across from the existing one (See Figure 78) (Union County Board of Chosen Freeholders). Since its completion, Rahway River Park has retained much of its original circulation system and historic buildings, while allowing its original bathhouse to deteriorate and introducing a modern one. Trees that were originally isolated from one another in its more densely wooded areas and lining its allée have now grown to form a rich tree canopy, while others along its internal parkways and surrounding its open meadows and ballfields have matured without interrupting viewsheds to these areas.

4.6.P. RAHWAY RIVER PARKWAY

As the centerpiece of the Union County Park System, the Rahway River Parkway figured prominently in the commission's plans. Having completed topographical surveys of sections in Cranford in 1922, and Springfield in 1923, the commission praised its "picturesque and winding stream" that "offers unusual opportunities for a continuous parkway development linking up a series of neighborhood parks and other parks, preserving places of exception natural beauty" (See Plates 46 and 47) (Union County Park Commission 1925:18). It also considered it "perhaps the most important unit in the Park System" (Union County Park Commission 1925:18). As noted, the Rahway River Parkway was conceived by Sheriff James E. Warner as a means of reclaiming and rehabilitating its polluted waters. In turn, this idea was refined by engineer D.C. Newman Collins, who also envisioned its rehabilitation—albeit through the creation of a memorial park to honor the soldiers of World War I. More importantly, Collins' proposal encompassed having the river part of a larger network of connecting rivers, greenways and parkways for a comprehensive Union County Park System. Utilizing his professional skills as an engineer, Collins drafted a preliminary plan for the Rahway River Parkway that the Olmsted Brothers incorporated into their own proposal (See Figure 5) (Olmsted Brothers to H.S. Chatfield, 5 July 1921). Preferring to capitalize on the potential for parks to increase adjacent area ratables, the firm's main modification of Collins' plan was a greater allowance for residential development within the Cranford section of the parkway. The firm also proposed expanding the parkway along the western branch of the Rahway River, known as Robinson's Branch, up to Madison Hill Road. As per the firm's recommendations, the commission would acquire 1,097 acres in order to create the parkway.

The first park to be developed expressly for the Union County Park System was located in a section of the Rahway River Parkway, in the City of Rahway at the intersection of Elizabeth Avenue and Grand Street (now Grand Avenue). As noted, the Wheatena Company made the

first donation of park land to the county on January 18, 1922, ceding 4 acres of company land along a 2,300-foot stretch of the Rahway River on both sides of the Grand Avenue Bridge (Union County Park Commission 1923:17). Occupying seven parcels that were formerly under private ownership and intended for industrial use, the company not only contributed some of its land, but also commissioned its own team of landscape engineers and architects to grade and landscape it (See Plates 48, 49, and 50) (Union County Park Commission 1923:13). Furthermore, the Wheatena Company specified that the land was to be exclusively used for park purposes and that the commission would require authorization from the company to construct any future building on the site. Its deed to the commission stated, "[T]he intent of this restriction being to prevent any action that would detract from an artistic and harmonious development of land along the banks of the Rahway River" (Union County Park Commission 1923:17-18). Following this land transaction, this section of the riverine parkway was first known as Rahway River Park, and then later as Wheatena Park (See Plate 51). Most recently, it has been known as the Rahway Section, housing the Rahway River Scenic Trail which provides the southern gateway into the Rahway River Parkway to the north.

Following this acquisition, a section of the Rahway River and adjacent land located in Clark Township was donated to the commission between 1923 and 1925. Comprised of 37 acres that was part of the former Bloodgood farm, this land contained a "picturesque lake" constituting 4,060 feet of river frontage, and was donated by the Better Land Realty Company (Union County Park Commission 1926:11). Augmenting this acquisition in Clark Township, four donors in Cranford each gave four acres of land along the Rahway River, totaling 5,265 feet of river frontage, to the commission during this same period (Union County Park Commission 1926:12). One of these donors was S.R. Droescher, whose mill would continue under his ownership, occupying a prominent place on the river north of the Lincoln Avenue Bridge. Also during this time, the commission purchased additional land in Cranford, totaling 1,680 feet of river frontage leading up to Springfield Avenue, with the intent of transforming a swamp and garbage dump into park land (Union County Park Commission 1926:12).

By 1926, only 35% of the land targeted for the Rahway River Parkway had either been obtained or was in the process of being acquired (Union County Park Commission 1926:19). Between 1929 and 1930, a series of vehicular parkways that had been largely proposed by the Olmsted Brothers for the areas bordering the park in Rahway, Clark, Linden, Cranford and Union were only under preliminary development (See Figure 79) (Union County Park Commission 1930:17). Correspondence between Percival Gallagher and Caxton Brown suggests that the commission was unable to decide whether to construct parkways that were isolated from residential fabric or interactive with it. Gallagher wrote, "I had been led to believe that the Commission had become convinced of the soundness of the plan of marginal driveways which would at the same time afford approaches to the abutting private properties" (Percival Gallagher to Caxton Brown, 20 June 1929). Noting his preference for the interactive plan, Gallagher emphasized the economy of land that would result in only having a single lane along the parkway, instead of two-way traffic that masked the residential buildings through the use of supplementary landscaping.

Evidently, the commission was slow to authorize widespread planning and development of vehicular parkways due to an absence of funding, which Gallagher alluded to in his letter. Consequently, in its 1947 report, it referred to the segments of riverine roadways within the Clark section of the Rahway River Parkway as "a foretaste of what is to follow when the parkway is completed" (Union County Park Commission 1947:1-2). Overall, in spite of the commission's good intentions, progress on the parkway was hampered, prompting it to write:

As to development, it should be realized that considerable expense still remains in the future. This will be appreciated by the example of the Rahway River plans, which are practically in their incipency, except as to acquisition of land and occasional partially developed areas, but which eventually include landscaping of both sides of the river by means of a protective fringe and the establishment of suitable paths, driveways and bridges to make that area conform in appearance to some of the famous river treatments of the Westchester County Park System in New York (Union County Park Commission 1947:1-2).

Although work was slow to begin on the parkway, numerous plans executed by the commission's landscape architects, working in conjunction with the National Park Service, during the mid 1930s attest to the commission's interest in its development.

Regarding portions of the Rahway River Parkway that were either developed or in the process of being so, the commission listed Wheatena Park, Rahway River Park, Sweet Gum and Osceola picnic areas and ball fields, Nomahegan Park, Lenape Park and Springfield Park (See Figure 80) (Union County Park Commission 1947:33). This last park unit was spread out along the riverine parkway, along both sides of Meisel Avenue in Springfield and Union Townships, continuing north up to Springfield Avenue. As noted, the section of the park west of Meisel Avenue was the former site of the American Chemical Company, and later served as a base camp for the Civilian Conservation Corps (See Figure 81). Upon conversion into a park, a new athletic field and running track were installed, and a former factory building was converted into a locker room facility. Because of its proximity to Jonathan Dayton Regional High School, this section of the parkway proved to be a welcome addition to its campus.

In 1948, a dike was constructed in the Sperry Farm Section of the parkway, alleviating flooding in the lower areas (Union County Park Commission 1958:21). In addition, a footbridge was also built during this time to assist children on their way to school, along with the replacement of a wooden footbridge in Winfield. In the Meisel Avenue Section, an additional softball field was added, while other parts of the parkway were cleared for hiking such as the area at Pinewood Drive in Union Township, and between Church Street and St. George's Avenue in Rahway. By 1964, the land acquired for the Rahway River Parkway numbered 991 acres. Within the Clark Section, facilities included park shelters, picnic areas, a day camping area, and a softball field (See Figure 82) (Union County Park Commission

1964:n.p.). In 1963, park staff repaired the wall on the west side of Jackson's Dam, located off of Valley Road, along with the water control gates in Jackson's Pond, and those within its counterparts to the north, known as Bloodgood's Pond (Union County Park Commission 1964:n.p.). In addition, the east side of Jackson's Pond was graded and expanded, accommodating more parking than it had before.

To the north in the Springfield Section of the parkway, facilities developed during this time included a park shelter, playground, and running track, along with fields for softball, baseball and football, and areas for fishing, ice skating, and archery. In 1959, a small lake was excavated to facilitate ice skating. One year later, the commission deeded a recreational park unit along Riverside Drive to Springfield Township. Further south, in the Meisel Avenue section, the commission granted permission to the Board of Education overseeing Regional High School, District No. 1, to renovate the park's athletic fields. This work was authorized on the condition that it adhered to commission plans and specifications, prepared in consultation with Olmsted Associates (See Figure 83) (Union County Park Commission 1964:n.p.). The commission's 1964 report noted that development work and ongoing maintenance would be borne by the school board (See Figure 84) (Union County Park Commission 1964:n.p.). In 1969, Japanese cherry trees were planted in the McConnell and Sperry park sections of the parkway (See Figures 85-87) (Cunningham 1972). Starting in the summer of 2000, the Meisel Avenue Section of the parkway was renovated. Since its completion, Rahway River Parkway has retained much of its original circulation system, while ad hoc trails by park users have been introduced throughout its wooded areas.

4.6.Q ROSELLE PARK

In 1930, the commission had begun negotiations to purchase a 12-acre tract of land in Roselle Park for the purposes of developing a small neighborhood park and recreation center (Union County Park Commission 1930:25). Abutting Roselle Park High School's athletic field, located between West Colfax and West Webster Avenues, the park was the smallest unit within the park system, and housed a children's playground and ball fields by 1947 (Union County Park Commission 1947:23). In 1962, the commission sold the park to the Board of Education for \$150,000 (Union County Park Commission 1964:n.p.). In its sale agreement, the board agreed to preserve the parcel as park land and allow the public continued access. The commission resolved that proceeds from the sale of this land would go toward the acquisition of new park land (Union County Park Commission 1964:n.p.).

4.6.R. UNAMI PARK

Like Cedar and Green Brook parks, Unami Park was obtained through a municipal donation. Originally known as Garwood Park, it was renamed Unami Park after the indigenous sub-tribe of the Lenape who had flourished in the region before the settlement of European immigrants. Twelve and one-quarter acres of its thirty-six-acre tract was given to the commission by the Borough of Garwood ca. 1930 (Union County Park Commission 1930:13). Containing

still in ponds or flowing in brooks or in rivers. Such scenes enriched by flowering plants are the crowning glory of the park. They create a scene of freedom or of escape from the urban condition of everyday life of the city dweller that is beyond estimate in its value upon the minds of those beholding them even when only relatively conscious of their actual charm and beauty. It is well established that landscape scenery, particularly of a quiet, pastoral character, has a profoundly soothing effect, and is highly re-creational [sic] to the spiritual nature of man...Our general idea in the development of design for the park would be to seek every opportunity to enhance whatever landscape quality the land affords and to locate the special feature necessary to the use and enjoyment of the park in such relation to one another and the landscapes as will form agreeable compositions [Olmsted Brothers 1922:6-7].

Although Olmsted Brothers clearly favored a less organized and more passive approach to recreation pursuits within the confines of a park, the Union County Park Commission felt that the county parks should have a strong recreational core. Therefore, they rejected the sixth governing principle as outlined by the Olmsted's. There would be "playgrounds" or organized recreation at the parks. The commission also chose to create a generalized plan of park locations that could be changed based on future needs, rather than opting for a formal arrangement where specific parks were fixed at specific locations. If the specific location of all the parks was announced, real estate speculation would cause land prices to soar and might cause land donations to dwindle. By not announcing specific locations, the commission was ensuring that planning would not be dictated by the volatile forces of the real estate market or that philanthropy would cease. In addition, by planning the park system in increments across time, the commission would be better able to address the needs of its citizens which it could not have foreseen in the system's infancy (Dietrich 2004).

The Commission's plans worked and Olmsted Brothers adapted the Olmsted ideas of what made parks work to satisfy the needs of its clients, retaining many of the ideals and concepts regarding the contemplation of beauty and the mix of passive recreation with active recreation. The Union County parks were designed with passive and active activity segments; the designers taking their cues from the landscape. In many cases the largest landscape element was a river—the Elizabeth River or Rahway River; this is particularly true for the parks located within the APE.

5.1.4.3 Rahway River Parkway Historic District. The Rahway River Parkway (ID #4079) is in actuality a system of parks and open spaces along the banks of the Rahway River. This system has been quite fluid during the course of the twentieth century, as the parks and green spaces have changed names, grown larger, become parks independent of the parkway, or have joined with municipal parks. The parkway itself is so big that it has traditionally been divided into sections and is often discussed that way. Unfortunately, the number of sections, the name of the sections, and the geographic boundaries of those sections have not been standard across the history of the parkway. At present, according to the Union County Department of Parks, the Rahway River Parkway is divided into three sections (north to south)—Cranford, Clark/Linden, and Rahway (Figures 5.11, 5.12, 5.13). Nevertheless, the parkway historically stretches north from the Cranford section passing through what is now Nomahegan Park (the park originally did not extend east over to the river) and continuing north through Lenape Park and out the northeast edge of Lenape Park up to Springfield Avenue in Springfield Township (Figure 5.14; see Figure 5.10).

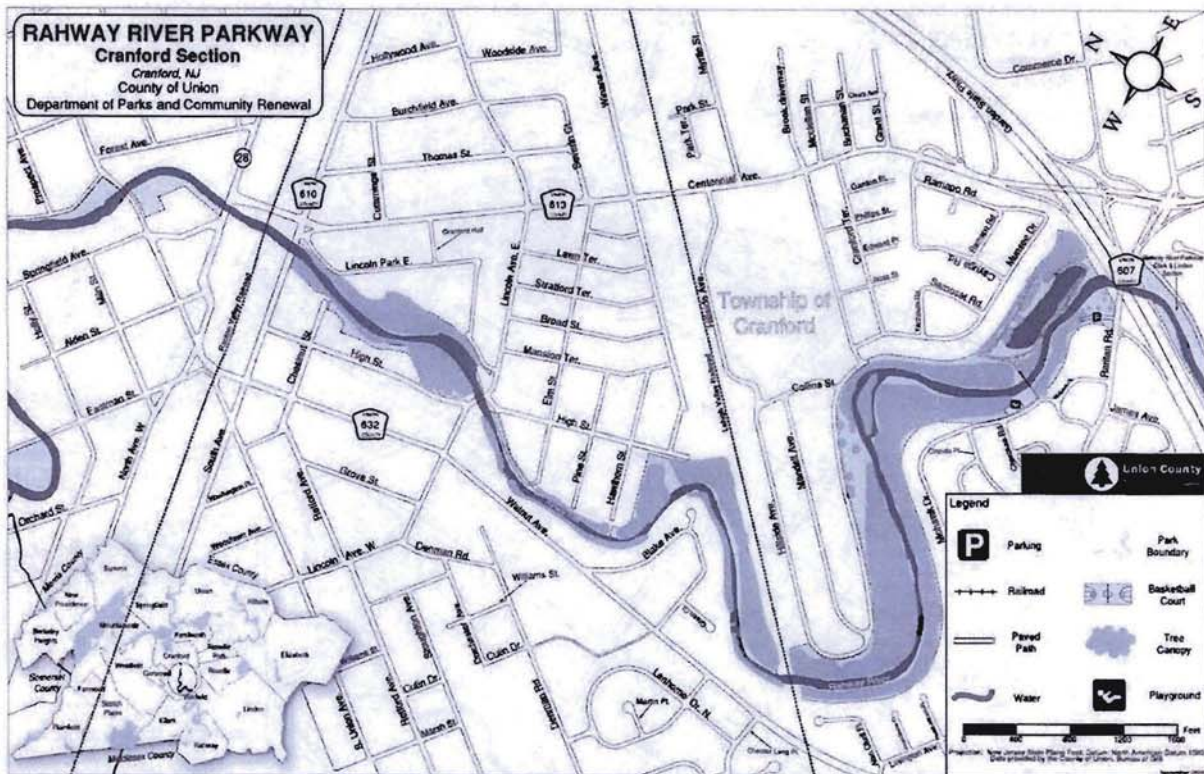


Figure 5.11. Rahway River Parkway, Cranford Section, Union County Department of Parks and Community Renewal (Union County 2012).

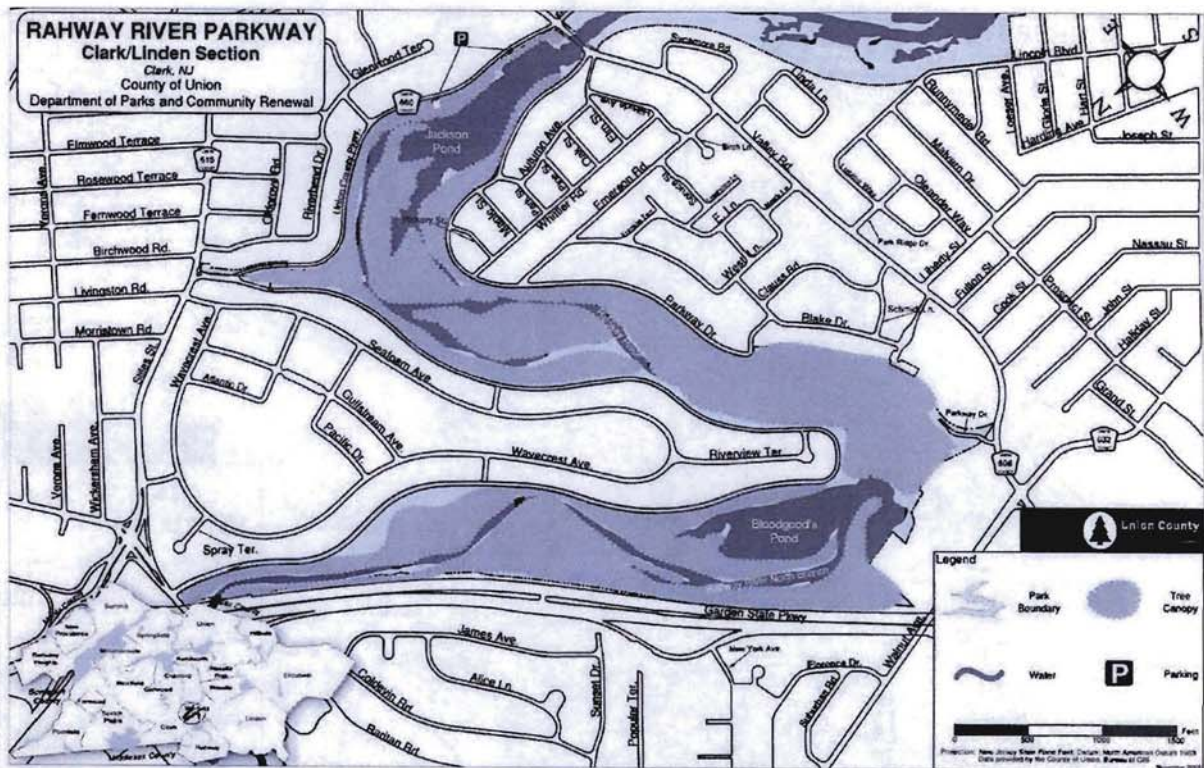


Figure 5.12. Rahway River Parkway, Clark/Linden Section, Union County Department of Parks and Community Renewal (Union County 2012).

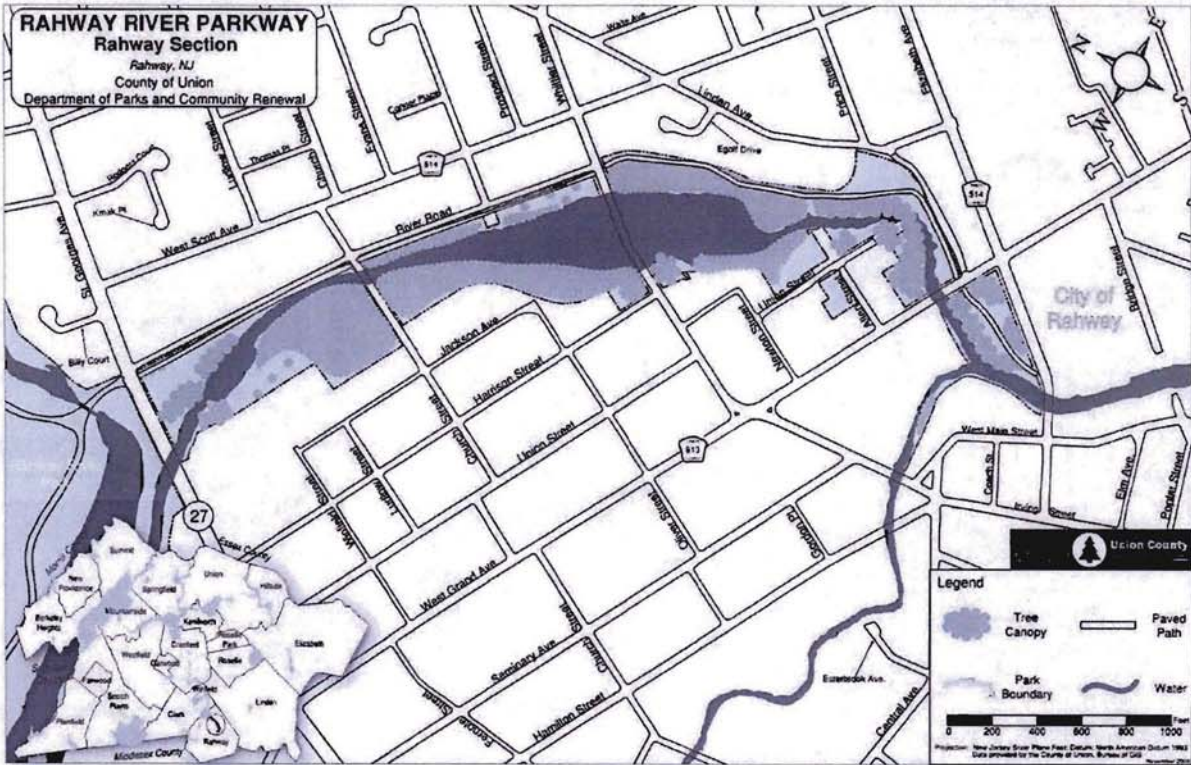


Figure 5.13. Rahway River Parkway, Rahway Section, Union County Department of Parks and Community Renewal (Union County 2012).

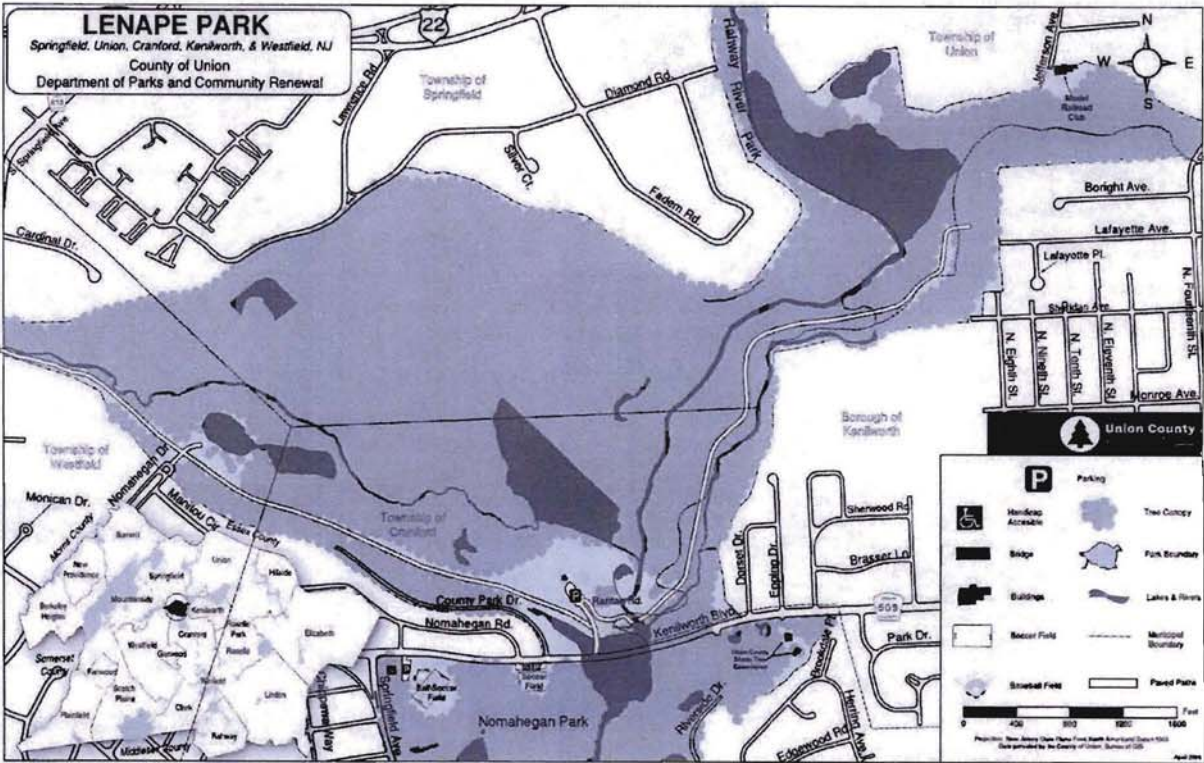


Figure 5.14. Lenape Park with Rahway River Park mislabeled on the northeast, Union County Department of Parks and Community Renewal (Union County 2012).

The New Jersey HPO Determination of Eligibility (Tingey 2002) found the Rahway River Parkway eligible for the NRHP under Criteria A and C.⁴ It was eligible under Criterion A for its association with the conservation movement, "a movement that originated with the nineteenth century National Parks system. The Rahway River Parkway represents an early and successful effort to safeguard the water supply and preserve the scenic qualities of the Rahway River" (Tingey 2002: Continuation Sheet 1). In addition, "the Rahway River Parkway in particular, and the Union County park system, in general, represent a benchmark in an evolving social movement to develop public parkland in the face of growing urbanization" (Tingey 2002: Continuation Sheet 1).

It was eligible under Criterion C for its association with master landscape architectural firm, Olmsted Brothers Landscape Architects, Brookline, Massachusetts. "Carrying on the tradition of Frederick Law Olmsted, Sr. [their father], Olmsted Brothers Landscape Architects were the foremost landscape design firm of the early twentieth century..." (Tingey 2002: Continuation Sheet 1).

The DOE narrative boundary description of the Rahway River Parkway reads: "Bordered on the north by the south side of Springfield Avenue, Springfield Township and to the south by the north side of Elizabeth Avenue, City of Rahway, and including Lenape, Nomahegan, Rahway River, Milton Lake, Black Brook and Wheatena Parks" (Tingey 2002: Eligibility Worksheet 1). Additionally, "the smaller municipal parks within the parkway should also be included as they contribute to the recreational nature of the resource...all bridges spanning the Rahway River within the boundaries of the Rahway River Parkway and built prior to 1968 [the period of significance of the Rahway River Parkway is 1921-1968] should be considered contributing resources to the parkway" (Tingey 2002: Continuation Sheet 1). These boundaries comport with the original boundaries as conceived by Olmsted Brothers and supported by the Union County Park Commission.

One of the earliest planned parks, but not the earliest, created within the Union County park system was the Rahway River Parkway. Olmsted Brothers' first recommendation for a park unit within the system was for "parking the Elizabeth and Rahway Rivers" (Olmsted Brothers 1921). Its plan for the Rahway River Parkway was actually based on an even earlier plan for a Union County Memorial Park for the Great War proposed by Edward Mosher and championed by D.C.N. Collins, township engineer,⁵ in 1919 (Figure 5.15). Collins made a thorough set of surveys and decided that the idea was bigger than Cranford and, therefore, enlisted the aid of A.W. Wendell of the Rahway Park Commission. Wendell was interested, and the plan finally made it to the Cranford Civic Board whose president, Charles Hansel, was a nationally known consulting engineer for railroad construction and valuation. Everyone liked the plan, but the timing was not right and the plan was shelved. When the Union County Parks Commission was formed in 1921 and Hansel was appointed vice-chairman, the plan was revived and made its way to Olmsted Brothers for review (Hall 1964).

Olmsted Brothers acknowledged that they used the Memorial Park plan but made some distinct changes to it (see Figure 5.10). Its plan greatly reduced the amount of land required for purchase along the river. The Olmsted's believed that it was possible to omit land around the northerly portion of Kenilworth, in the region south of Morris Avenue in Springfield, and in

⁴ A Determination of Eligibility has been made for the Rahway River Parkway, but a formal National Register nomination has not been submitted.

⁵ See footnote 3.

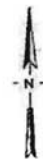
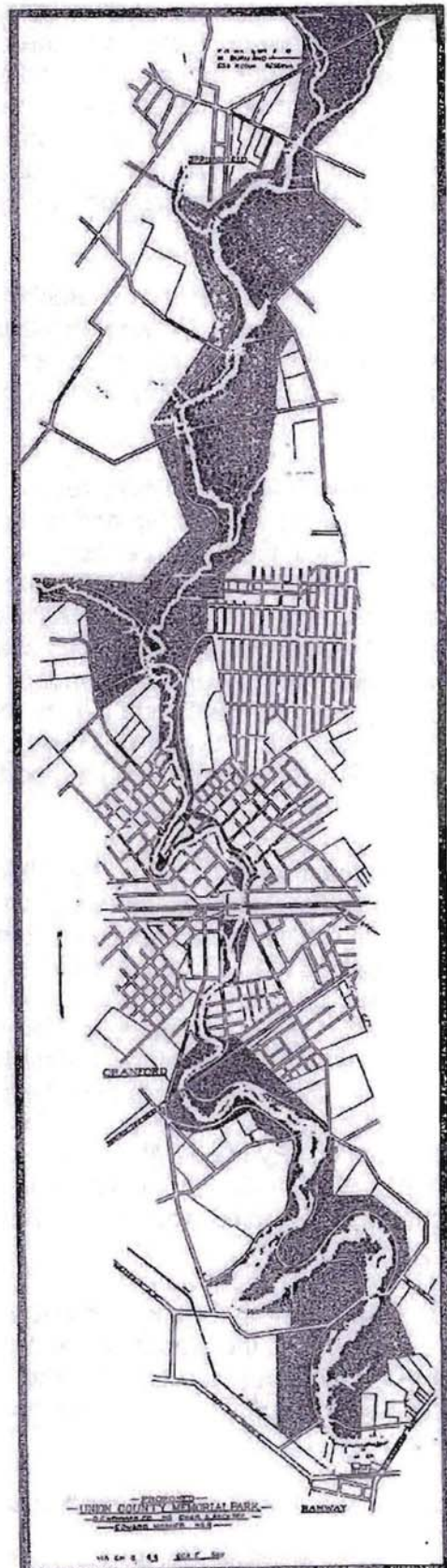


Figure 5.15. 1919 Collins and Mosher Proposed Plan of Union County Memorial Park, New Jersey (Dietrich 2004: Figure 5 [Original at Union County Historical Society, Hanson House, Cranford, New Jersey]).

Cranford around Sperry Farm because these areas were "well fitted for residence purposes, and as such would be in harmony with the park scheme" (Olmsted Brothers 1921:11). They thought that carefully controlling the development of streets in neighborhoods around the "parking" would accomplish what the commission desired. It recommended that the commission "make a special study of the street system throughout the undeveloped sections bordering the river north and south of Cranford because of the value of these districts for residential development" (Olmsted 1921). Olmsted Brothers were sure that the park would add real estate value. It further recommended that the "Memorial Park Project," as they were still calling it, "be extended through the City of Rahway and include Robinson's branch from Madison Hall Road to its junction with the Rahway River" (Olmsted Brothers 1921:12).

Olmsted Brothers also recommended, number 6, the acquisition of special or secondary parks "which for one reason or another should be acquired" (Olmsted 1921:27). One of the areas to be purchased was G-4, "swamp area near the Rahway River" at what is now Lenape Park. This area "was noted separately because of the nature of the ground and the consequent low value. The area would be a desirable addition to either the cross-county parkway [a recommendation that Olmsted Brothers' made for the Union County system so that it could connect to the Essex County system] or the parking of the Rahway River" (Olmsted Brothers 1921:29).

Olmsted Brothers estimated that its plan for parking the Rahway River would require 1,097 acres of land. The land required would be in the following municipalities:

Springfield	141 acres
Union	181 acres
Kenilworth (Twn)	40 acres
Cranford	260 acres
Clark	290 acres
Rahway	177 acres
Linden (Twn)	3 acres

(Olmsted Brothers 1921: accompanying Estimate of Areas in Suggest Park Cities)

It estimated that the swamp area site was 225 acres. Interestingly, it also recommended in this same estimate that 43 acres should be obtained in Cranford (Olmsted Brothers 1921: accompanying Estimate of Areas in Suggest Park Cities). However, in the accompanying report, no special parks were mentioned in Cranford. It is tempting to believe that perhaps these recommended 43 acres might have been the beginning of Nomahegan Park.

In 1922, the commission began estimating the costs of purchasing land for specific park units. The parking of the Rahway River was so large that the project was divided into several sections, and at this period the commission was considering purchasing the Rahway Poor Farm for inclusion in the Rahway River Parkway. The Rahway Poor Farm offered 135 acres for \$700 per acre for a total of \$108,000. It was estimated that another 900 acres could be purchased for \$1,000 per acre for a total of \$900,000 (Union County Park Commission 1922). The commission's preliminary estimate suggested spending more than a \$1,000,000 on the Rahway River Parkway, almost twice what it was estimating as the costs to be spent on the Elizabeth River Parkway.

The Rahway River Parkway was the centerpiece of the Union County Park plan and was considered "perhaps the most important unit in the Park System" (Dietrich 2004:113, quoting Union

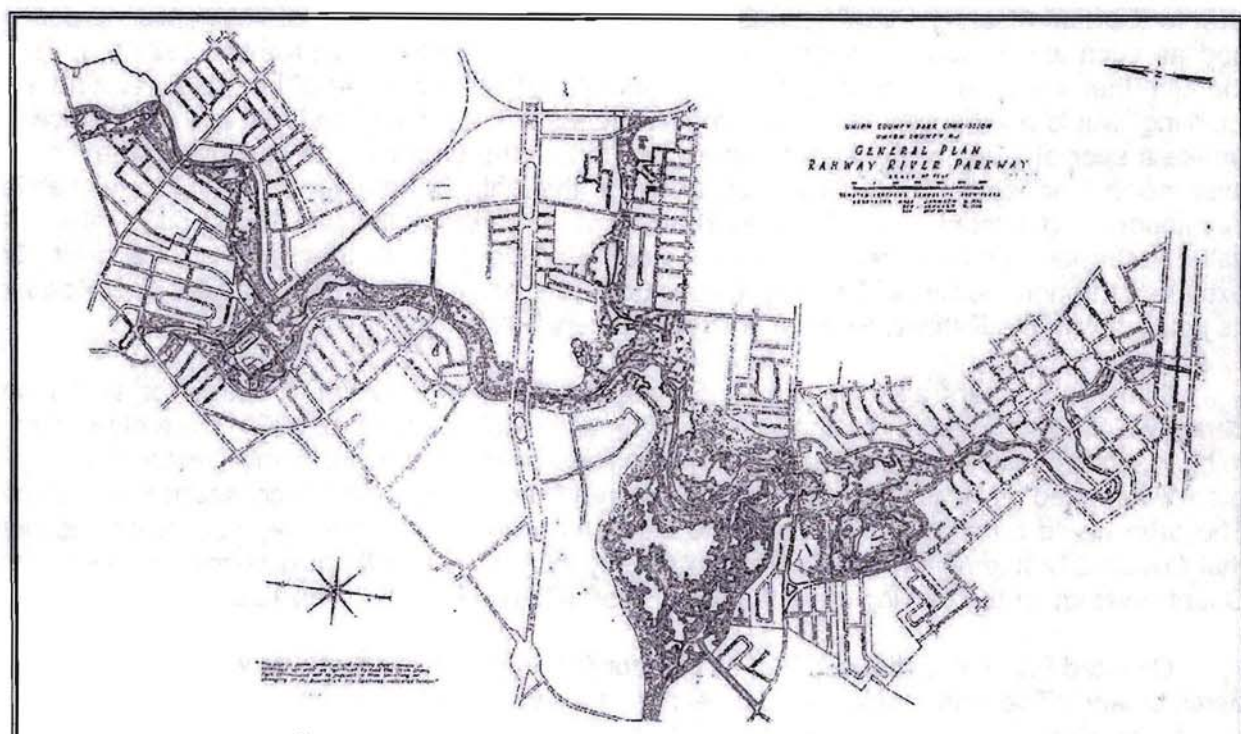


Figure 5.16. 1930 (revised 1956 and 1968) Olmsted Brothers plan of Rahway River Parkway from its northern-most point in Springfield Township to the Central Railroad of New Jersey (Dietrich 2004: Figure 79).

County Parks Commission 1925:18). In 1925, Olmsted Brothers first drew a plan for the Rahway River Parkway and then altered it again in 1930 (Dietrich 2004). The 1930 plan was the plan that was used for the parkway and altered continuously including in 1956 and 1968 (Figure 5.16).

The first park to be developed for the Union County Park System was in 1922 and was located in a section of the Rahway River Parkway—Wheatena Park. The Wheatena Company, makers of boxed cereal, created land specifically for the park and donated services for its creation. The park was originally referred to as Rahway River Park (not to be confused with present-day Rahway River Park), and then later called Wheatena Park. Wheatena Park is located in what is now the Rahway Section of the parkway and is the site of the Rahway River Scenic Trail (Dietrich 2004; see Figure 5.13).

Following the Wheatena acquisition, land was donated between 1923 and 1925 in Clark Township at the former Bloodgood farm, which contained a picturesque landscape and significant Rahway River frontage. Donors in Cranford then contributed land for the project: S.R. Driescher donated land along the river between South Avenue and Lincoln Street, including the mill site on Lincoln Street; the mill continued in operation. Driescher's donation is now Lincoln Park, which is at its southernmost end of the current APE. Also during this period the Commission purchased river frontage off Springfield Avenue that comprised a swamp and a dump with the intent of creating a park (Dietrich 2004). This site became Nomahegan Park and is also in the current APE.

In 1926, W.R. Tracy, Engineer and Secretary, Union County Parks, notified Olmsted Brothers that the Rahway River Parkway project had been subdivided into 16 sections:

The Wheatena Tract;
The Poor Farm Tract;
The Nomahegan Park Tract; and (moving from south to north up the river)
Section 1—Elizabeth Street to St. George Avenue;
Section 2—St. George Avenue to Valley Road;
Section 3—Valley Road to Raritan Road;
Section 4—Raritan Road to Lehigh Valley Railroad;
Section 5—Lehigh Valley Railroad to Central Railroad of New Jersey;
Section 6—Central Railroad of New Jersey to Springfield Avenue at Normandie Place;
Section 7—Springfield Avenue to Kenilworth Boulevard;
Section 8—Kenilworth Boulevard to Chester Avenue;
Section 9—Rahway River to Echo Lake;
Section 10—Chester Avenue to Milltown Road;
Section 11—Milltown Road to Rahway Valley Railroad;
Section 12—Rahway Valley Railroad to Morris Avenue; and
Section 13—Morris Avenue to Seven Bridges Road.

The reason for the division of the project into so many sections is not exactly clear, but Tracy asked that the Olmsted's to submit their bills for payment according to the sections he designated. By this date only 35 percent of the land targeted for the parkway had been obtained or was in the process of being acquired (Dietrich 2004:114, quoting Union County Park Commission 1926:12).

In the Commission's 1928-1930 report, the Rahway River Parkway's recreational facilities were listed in four sections of the parkway: Rahway, Kenilworth Boulevard, Cranford, and Springfield. In analyzing the recreational activities available in each section, the Kenilworth Boulevard section is actually what is now Lenape Park. This observation is evident based on the fact that this area offered trap shooting and rifle and pistol ranges (Union County Park Commission 1930). The only part of the Union County Park system to offer those types of recreational activities was Lenape Park (Hidden Valley Park, a much latter addition, now has a pistol range). It is interesting to note that the Kenilworth Section also offered a one-mile bridle path. Nomahegan Park, which had been included in the parkway by Secretary Tracy in 1926, was listed in the Commission's 1930 report as a park separate from the Rahway River Parkway with 26 of its own recreational facilities (Union County Park Commission 1930).

In 1931, when an Olmsted Brothers representative inspected the park system, he was pleased to note that "the whole system generally, as far as can be judged at this time of year [January], looked very well indeed..." (Olmsted Brothers 1931). Several suggestions were made and some specific problems pointed out, particularly relating to Nomahegan Park, as a result of the inspection. Cost estimates for plants, new plantings, and replacements were included in the report. Section 2 of the Rahway River Parkway (St. George Avenue to Valley Road) needed new plantings under plan #7173-121 at a cost of \$1,006.00 (Olmsted Brothers 1931).

For the next thirty years, the Park Commission slowly made additions to the parkway and improvements to parks within it. The entire park system continued to grow as new parks and recreational facilities were regularly added and upgraded as demand required. At the time of the current investigation, portions of the parkway, particularly those areas not immediately associated with large parks, seem to be less conscientiously maintained and are not clearly delineated to the public as the Rahway River Parkway.

Located within APE on the Rahway River Parkway are six named Union County Parks (from north to south down the Rahway River): Lenape, Nomahegan, McConnell, Sperry, Lincoln, and Driescher's Mill. After the Rahway River Parkway was determined eligible for listing in the National Register of Historic Places by the New Jersey HPO, the entire Union County Park System (ID #4424) was determined eligible on March 19, 2005.⁶ According to the Union County Park System Determination of Eligibility (DOE), Lenape Park, Nomahegan Park, the "McConnell Park Section of the Rahway River Parkway," the "Sperry Section of the Rahway River Parkway," and the Cranford Section of Rahway River Parkway are all eligible for the NRHP under the Park System DOE. Technically, McConnell Park and Sperry Park are located in the Cranford Section of the Parkway (see Figure 5.11); it can only be assumed that the SHPO wanted specifically to name these parks. In addition, Lincoln and Driescher's Mill parks are located in the Cranford Section.

Four named Cranford Township municipal parks are situated within the APE on the Rahway River Parkway. They include, moving north to south down the Rahway River, Memorial Park, Hampton Park, Girl Scout Park, and Hanson Park (see Figure 5.2). According to the DOE for Rahway River Parkway, these municipal parks are eligible as contributing resources to the district for their "recreational nature" (Tingey 2002: Continuation Sheet 1).

Lenape Park. Lenape Park (ID #4424.017 and PRN 124) is eligible for listing in the National Register as a contributing resource to the Rahway River Parkway HD (ID #4079) and the Union County Park System HD (ID #4424) (see Figure 5.14). Lenape Park was originally highlighted, but not specifically named, in the 1921 Olmsted Brothers report on the initial development of a park system for Union County (see Figure 5.10, marked section G4). The firm recommended the acquisition of this "swamp area" near the Rahway River because of the "nature of the ground and the consequent low value" (Olmsted Brothers 1921:29). They believed it could be a "desirable addition to either the cross-county parkway or the parking of the Rahway River" (Olmsted Brothers 1921:29).

Lenape Park has always been one of the least developed of the parks within the system, despite its initial identification by Olmsted Brothers. According to a report drafted by Olmsted Associates, the successor firm to Olmsted Brothers, the lack of development was partly the result of the lack of topographic information provided to the company about this area (Dietrich 2004:103, quoting Olmsted Associates 1963). Nevertheless, by 1930 the Park System had recreational facilities in place in the "Kenilworth Boulevard Section of the Rahway Parkway." These facilities included three traps for clay target shooting, a rifle and pistol range, a one-mile bridle path, a football field, and a baseball field (Union County Park Commission 1930). By this time, the area also had two lakes. However, according to Secretary Tracy, the land now comprising Lenape Park remained largely undeveloped as a result of Commission funding priorities. In 1932, Tracy wrote about the future of Lenape Park: "[it is] destined to remain in its natural state for some time, other projects having priority on the development program when funds became available (Dietrich 2004:104 quoting Tracey 1932:85).

In 1936 during a Works Progress Administration (WPA) project in the park area,⁷ workers uncovered a "three-foot tusk and four four-pound teeth" belonging to a mastodon (*The Daily Princetonian* 1936:1). Several weeks later, other mastodon bones were found creating a sensation.

⁶ A DOE has been made but no formal National Register nomination has been submitted.

⁷ Newspaper articles indicate that the mastodon bones were found in Nomahegan Park "at the lake excavation" (Cranford Chronicle 1936). However, notes from the period on the backs of the mastodon dig photos indicate that the site was near the "trapshooting grounds." Only Lenape Park has ever had traps for shooting clay targets.

OFFICE OF CULTURAL AND ENVIRONMENTAL SERVICES

HISTORIC PRESERVATION SECTION
HISTORIC DISTRICT SURVEY FORM

HISTORIC SITES INVENTORY NO.

DISTRICT NAME:	Staten Island Railroad	UTM REFERENCES:	Zone/Northing/Easting
MUNICIPALITY:	Elizabeth, Linden, Roselle, Cranford		A
COUNTY:	Union		B
TYPE OF DISTRICT:	Linear		C
USGS QUAD:	Roselle, Elizabeth, Arthur Kill, Jersey City, The Narrows		D

DESCRIPTION: (General description of district as a whole and boundaries)

The Staten Island Railroad runs between Cranford Junction, New Jersey and the St. George section of Staten Island, New York (see Figure 1). The entire railroad system is 26.5 miles in length. In New Jersey, the rail line runs for approximately 6.5 miles from a vertical lift bridge over the Arthur Kill west to Cranford Junction. Approximately 3,400 feet (1 kilometer) of the rail line, including the Arthur Kill lift bridge, is located within the New Jersey study area for the Staten Island Bridges Project (see Figure 2). The New Jersey portion of the right-of-way includes the rail line itself, rail bridges, a clerical office/communications shed, a diner, and a concrete telephone booth.

Goethals Bridge Study Area

The portion of the rail line within the Goethals Bridge study area includes the 1959 vertical lift bridge over the Arthur Kill, the world's longest lift bridge; an elevated concrete bridge that proceeds west from the Arthur Kill; an on-grade portion of the line that continues west; a small bridge crossing South Front Street joined at either end by an elevated section of the rail line; a long timber trestle bridge that stretches from approximately South Front Street underneath the Goethals Bridge to the Conrail Chemical Coast rail line; and a metal bridge that crosses the Conrail line and the New Jersey Turnpike (see Photograph 1). This stretch of the rail line -- originally known as the Baltimore & New York Railroad -- was constructed between 1884 and 1889 as part of the Baltimore and Ohio Railroad (B&O) system. Parts of the rail line have been reconstructed.

Staten Island Railroad West from the Goethals Bridge Study Area to Cranford Junction

West of the Goethals Bridge Study Area, the rail line contains two bridges, both determined eligible for listing on the New Jersey and National Registers of Historic Places. These bridges are located in Linden, New Jersey near the Amtrak main line; the 1889 through truss bridge is west of Linden Avenue and the 1905 three-plate girder bridge is near Elizabeth Avenue and Cranford Junction Street.

The rail line also contains a clerical office/communications shed and diner near Brunswick Avenue in Linden, a concrete phone booth approximately ¼ quarter mile east of the shed, and concrete culverts. All appear to be at least 50 years old. The shed is roughly 20' by 16' and served as freight and switchman's offices. According to SHPO's 6/11/91 letter, the diner appears to retain many of its original characteristics and may be considered an eligible resource. Train crews used the phone booth to communicate with the bridge engineer at the Arthur Kill Bridge until radios were installed in the engines in 1982. The exact location of a 1904 concrete arch culvert is unknown, but according to SHPO, it may be individually eligible as an early example of bridge technology.

APPROXIMATE NUMBER OF BUILDINGS: N/A

PHYSICAL CONDITION OF STRUCTURES: Excellent ____% Good ____% Fair ____% Poor ____%

REGISTER ELIGIBILITY: ☐ Yes ☒ Possible ☐ No

THREATS TO DISTRICT/LOCAL ATTITUDES:

COMMENTS:

Historical Overview

In the 1880's, the B&O, which had primarily served Baltimore, its eastern terminal, began a major northern expansion. The railroad built a Baltimore-to-Philadelphia connection, giving it a through line from Baltimore to Bound Brook, New Jersey, via the Reading Railroad. While the B&O could also use the facilities of the Central Railroad of New Jersey, it sought to obtain its own access to Manhattan/New York City, then among the nation's most important freight terminals. From a waterfront terminal within New York Harbor's "lighterage limits," a railroad could serve the entire New York area without having to acquire/build track in developed and costly locations. Freight could be loaded onto railroad-owned barges (known as "lighters"), carfloats, etc., for distribution by water. Although most of the waterfront on the west shore of the Hudson River in New Jersey had already been purchased by other railroads, the north shore of Staten Island was available.

The B&O purchased the Staten Island Railroad, an independent railway constructed circa 1860 by Cornelius Vanderbilt that originally ran down the east shore of Staten Island from Vanderbilt's Landing (as the ferry landing in Clifton was known) to Tottenville, at the southern tip of Staten Island. The railroad was expanded in 1886 between St. George and South Beach (along Staten Island's east shore) and Elm Park (along Staten Island's north shore.) After the B&O purchased the Staten Island Railroad, it extended the line west along the north shore to the Arthur Kill and on to Cranford, New Jersey, where the line intersected with the main line of the Central New Jersey Railroad.

Shortly after acquiring the Staten Island Railroad, the B&O opened a new railroad/ferry terminal in St. George. The following year (1889), the B&O built a railroad lift bridge over the Arthur Kill. Service on the B&O's extension to Staten Island started in 1890. The New Jersey portion of the railroad was originally known as the Baltimore & New York Railroad, but was later merged with the Staten Island Rapid Transit (SIRT), as the Staten Island portion of the line had come to be known. The St. George terminal was originally intended to serve passengers traveling from New Jersey who would then proceed by ferry to Manhattan, but this plan was never implemented; passenger trains used the Central New Jersey station at Jersey City instead. Local passenger service from the ferry terminal at St. George was limited to Staten Island locations. During this period, B&O opened a Hudson River railyard near West 26th Street in Manhattan that connected to the Staten Island rail facilities via a transfer bridge at West 26th Street.

(Continued on following page)

REFERENCES: (Include representation in existing surveys)

ONJH-F91-48—Letter dated June 11, 1991 from Nancy Zerbe, Deputy State Historic Preservation Officer, to the Interstate Commerce Commission
SIRR files at New Jersey Historic Preservation Office, Trenton, New Jersey.
Thomas Flagg, "Overview of Staten Island Rapid Transit Railroad," December 16, 1990; prepared for Jonathan Gell (on file at NJSHPO).
Richard Travis, "From Chessie to Suzie Q," in *Railpace Newsmagazine*, March 1986, pages 18-26.

ATTACHMENTS: (Indicate number)

MAPS: 2 PHOTOS: 1 SLIDES: _____

OTHER: (Specify)

RECORDED BY: Christina Plattner

DATE: 12/6/94

ORGANIZATION: Allee King Rosen & Fleming, Inc.

SIGNIFICANCE (Continued)

In 1925, the entire line was electrified in anticipation of a subway extension from Brooklyn to Staten Island. The tunnel beneath the Narrows was never completed, but the Staten Island Railroad was busy during this period with both passengers and freight.

In 1948, the City of New York's Board of Transportation took over competing bus lines on Staten Island, lowering the fare from a dime to a nickel, and permitting unlimited free transfers on Staten Island. SIRT's revenues fell dramatically and the railroad reduced its service. The B&O unsuccessfully offered to sell the SIRT to the Board of Transportation. By 1953, the SIRT had discontinued all passenger service on the North Shore line and completely abandoned the South Beach line, but retained service on the Tottenville line. By 1971, passenger operations were taken over by the Staten Island Rapid Transit Operating Authority, a subsidiary of the Metropolitan Transportation Authority (the Board of Transportation's successor) to prevent abandonment. To provide freight service, the B&O retained trackage rights on the Tottenville line and kept the North Shore line. It was renamed the Staten Island Railroad Corporation (SIRC) and continued to serve businesses along the north shore and in New Jersey.

After Conrail ended all other main line railroad marine service in 1976, the B&O maintained carfloat service from St. George to Manhattan until about 1980, when this service was eliminated and freight traffic on Staten Island was greatly diminished. In 1985, the B&O (now part of the Chessie System) turned the line over to the Delaware Otsego system, a "designated operator" of short-line railroads. The Delaware Otsego system was able to operate rail lines with only 5 employees instead of the 35 required by a regular railroad. The railroad's customers then included Procter & Gamble (in Port Ivory, Staten Island), the Howland Hook Marine Terminal, Laminated Paper Company at Bayway, and the Federal Plastics Corporation in Cranford, New Jersey. In 1990-1991, the Staten Island Railway Corporation (SIRY), a subsidiary of Delaware & Otsego, sought approval of the Interstate Commerce Commission to abandon the railroad. Since the files at NJSHPO do not contain any subsequent correspondence, it is assumed that this application did not proceed.

In 1994, the the New Jersey Department of Transportation acquired the New Jersey section of the Staten Island Railroad and the New York City Economic Development Corporation acquired the Staten Island section. Although both New York City and NJDOT have plans to reactivate the line, no timetable has been announced.

Conclusion

The New Jersey portion of the Staten Island Railroad is historically important as a critical link in the Baltimore & Ohio Railroad's freight service between Baltimore and Manhattan via Staten Island. During the late 19th century, Manhattan was one of the nation's most important freight destinations, but it was separated from the major rail lines to the west by the Hudson River, which was too wide to be spanned by a tunnel or bridge. Freight was carried to the Port of New York by a complex network of rail lines, lift bridges (e.g., the B&O's bridge over the Arthur Kill), barges and lighter ships, and railroad transfer bridges. For a century, the Staten Island Railroad provided service from New Jersey to Staten Island. While its role in carrying freight diminished in recent years, it may be reactivated in the foreseeable future.

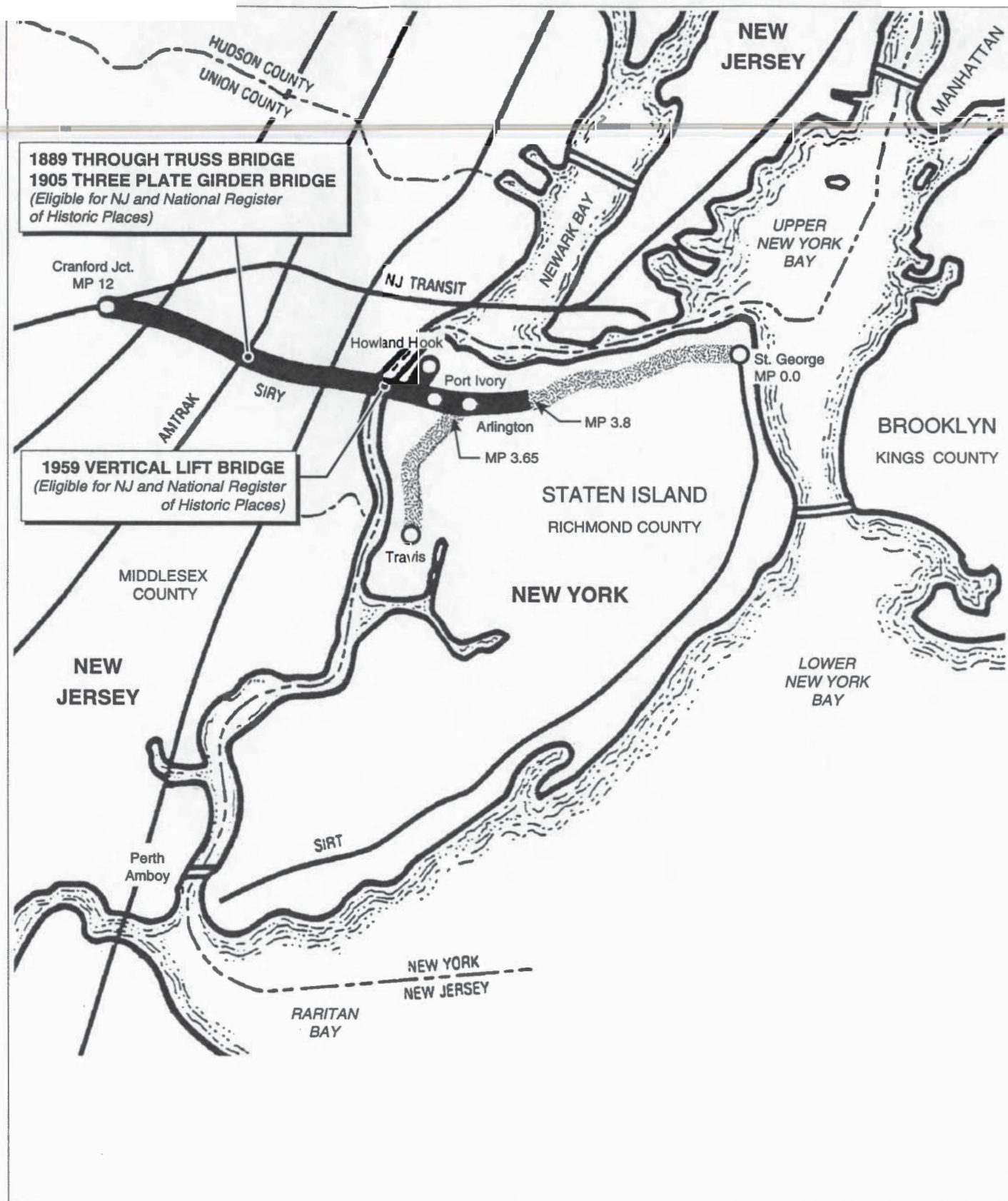
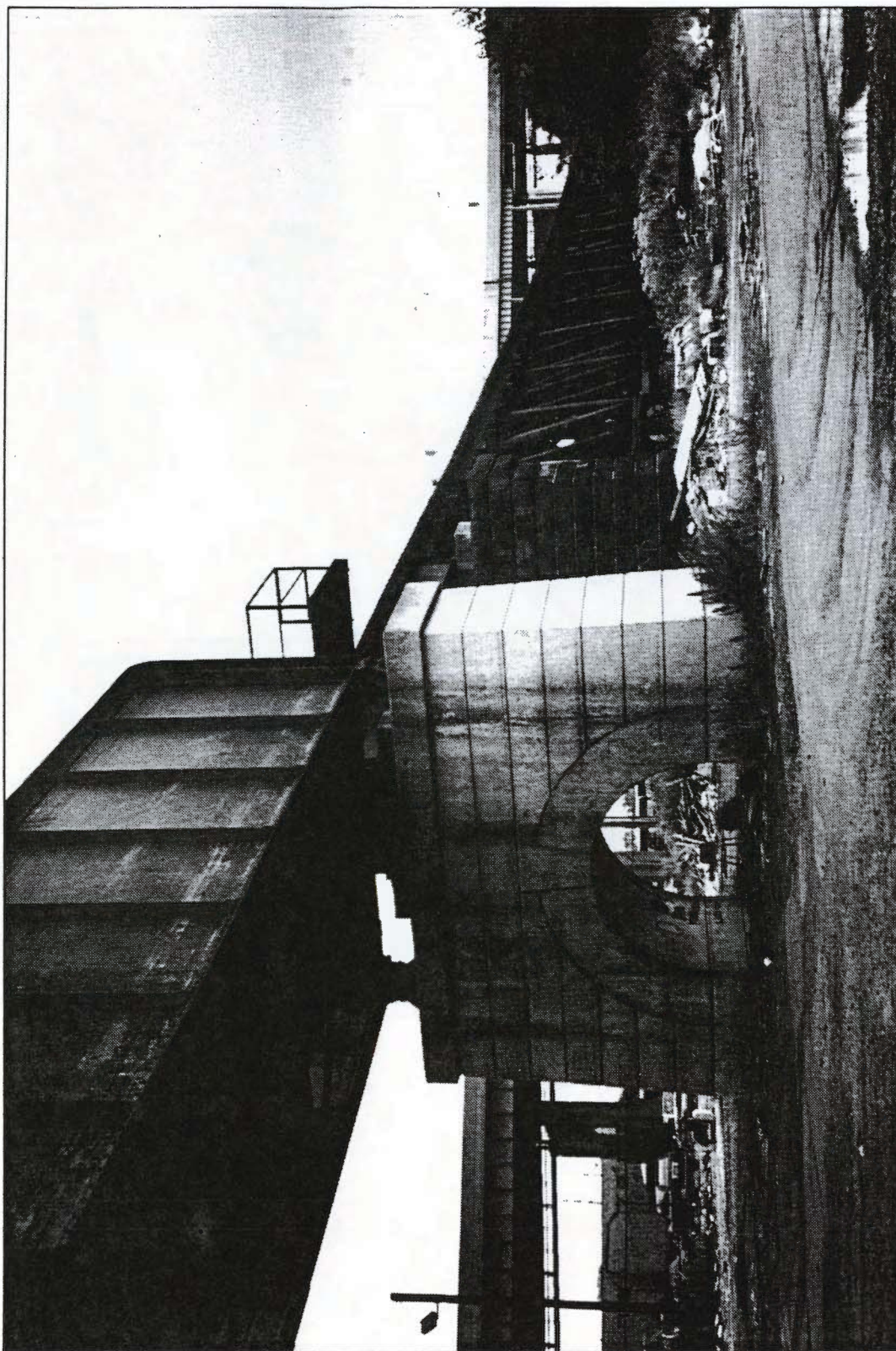


Figure 1
Staten Island Railroad System Diagram
circa 1990





Photograph 1
**View of Staten Island Railroad
Looking Northeast from Bay Way
near Conrail (Chemical Coast) RR Line**



State of New Jersey

Christine Todd Whitman
Governor

Department of Environmental Protection
DIVISION OF PARKS AND FORESTRY
HISTORIC PRESERVATION OFFICE
CN-404
TRENTON, N.J. 08625-0404
TEL: (609) 292-2023
FAX: (609) 984-0578

Robert C. Shinn, Jr.
Commissioner

February 27, 1995
HPO-B95-103

Ms. Christina Plattner
Senior Planner
Allee King Rosen & Fleming, Inc.
117 East 29th Street
New York, NY 10016

Dear Ms. Plattner:

As Deputy State Historic Preservation Officer for New Jersey, in accordance with 36 CFR Part 800: Protection of Historic Properties, as published in the Federal Register on September 2, 1986 (51, 169, 31115-31125), I am providing additional Consultation Comments for the following project:

**Union County, Elizabeth City
Staten Island Bridge Project**

These comments are in response to your letter requesting Historic Preservation Office (HPO) review and comments on the architectural report for this project. It is understood that consultation regarding the identification of archaeological resources and project effects on historic properties will be initiated in the future. The reviewed report is:

"Staten Island Bridges Program, Modernization and Capacity Enhancement Project, Historic Resources Survey - New Jersey," by Allee King Rosen & Fleming, Inc. (December 1994).

800.4 Identifying Historic Properties

The Goethals Bridge was previously identified as eligible for listing in the National Register of Historic Places by my opinion of February 14, 1995.

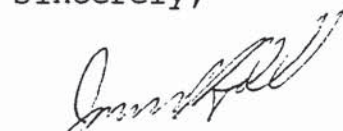
As Deputy State Historic Preservation Officer, it is my opinion that the portion of the Staten Island Railroad which lies in New Jersey is eligible for listing in the National Register of Historic Places under Criterion A as a historic district. This opinion is based on the information in the reviewed report, which recommended this resource as potentially eligible. The Staten

Island Railroad is significant as a critical link in the Baltimore & Ohio Railroad's freight service between Baltimore and Manhattan via Staten Island. The Staten Island Railroad runs from Cranford Junction, New Jersey to the Saint George section of Staten Island, New York (for a total length of 26.5 miles). In New Jersey, the line runs for approximately 6.5 miles from Cranford Junction east to a vertical lift bridge over the Arthur Kill in Elizabeth. The boundaries of this historic district are the right-of-way, including: the rail line itself, rail bridges, a clerical office/communications shed, a diner, and a concrete telephone booth as well as any other associated features of structures. The Staten Island Railroad Historic District lies within the Union County municipalities of: Cranford Township, Roselle Borough, Roselle Park Borough, and Elizabeth City.

As Deputy Historic Preservation Officer for New Jersey, it is my opinion, in concurrence with the reviewed report, that the following properties, located in Elizabeth City, are ineligible for listing in the National Register of Historic Places: 167 Bayway, 147 Bayway, 145 Bayway, 137-143 Bayway, 135 Bayway, 133 Bayway, 123 Bayway, 119 Bayway, 117 Bayway, 109 Bayway, 100-103 Bayway, 93-95 Bayway, 89 Bayway, 663 Amboy Avenue, 100-106 Krakow Street, 112 Krakow Street, 114 Krakow Street, 118 Krakow Street, 120 Krakow Street, 66 South Front Street (Bayway Terminal Storage Warehouses 16 A & B), Phelps Dodge Complex, and 632 South Front Street.

Should you have any questions regarding this project, please contact Andrea Tingey of my staff as (609-292-2023).

Sincerely,



James F. Hall
Deputy State Historic
Preservation Officer

c Gary Kasoff, USCG
Timothy Ng, Port Authority
Jim Warren, NYSHPO
Advisory Council on Historic Preservation
Andras Fekete, NJDOT-BEA

JH:AT
C:\wpwin\95-473

Union County Parks Survey

Park Name

Township/City

Unami Park

Garwood, Westfield

photo



Type

Existing Topography

Spatial Relationships

Circulation

☐ recreational

☐ pastoral

☐ picturesque

☐ natural

☐ scenic

☐ other:

☐ flat

☐ sloped

☐ hilly

☐ mountainous

☐ other:

☐ symmetrical

☐ asymmetrical

☐ axial

trails

cycling paths

bridle paths

parking lots

handicapped ramps

other:

asphalt

asphalt

asphalt

Parkways

☐ Border roads

☐ Internal park roads

☐ Vernacular roads

☐ Summit roads

☐ Connecting pleasure roads

☐ River parkways

☐ Estate roads

☐ Ocean parkways

☐ Other:

Union County Parks Survey

Drainage & Engineering

<input type="checkbox"/> Bridge (road)	
<input type="checkbox"/> Bridge (pedestrian)	
<input type="checkbox"/> Channel	
<input type="checkbox"/> Culvert	
<input type="checkbox"/> Spillway	
<input type="checkbox"/> Millrace	
<input type="checkbox"/> Retaining wall	rubble-stone
<input type="checkbox"/> Storm sewer	
<input type="checkbox"/> Swale	
<input type="checkbox"/> Other:	rubble-stone
staircase	

Bridge

carries	bridge date	info source	bridge type/design	bridge material

Land Uses

<input type="checkbox"/> archery	<input type="checkbox"/> skating--sidewalk
<input type="checkbox"/> baseball	<input type="checkbox"/> soccer
<input type="checkbox"/> basketball	<input type="checkbox"/> softball
<input type="checkbox"/> boating/canoeing	<input type="checkbox"/> swimming
<input type="checkbox"/> biking	<input type="checkbox"/> tennis
<input type="checkbox"/> camping	<input type="checkbox"/> theater/music
<input type="checkbox"/> concession/dining	<input type="checkbox"/> track & field
<input type="checkbox"/> cricket	<input type="checkbox"/> trap/skeet shooting
<input type="checkbox"/> driving	<input type="checkbox"/> trail
<input type="checkbox"/> fishing	<input type="checkbox"/> wading
<input type="checkbox"/> football	<input type="checkbox"/> other:
<input type="checkbox"/> golf	
<input type="checkbox"/> gymnastics	
<input type="checkbox"/> handball court	
<input type="checkbox"/> hockey	
<input type="checkbox"/> horseback riding	
<input type="checkbox"/> horticulture/gardening	
<input type="checkbox"/> lawn bowling	
<input type="checkbox"/> model boating	
<input type="checkbox"/> picnicking	
<input type="checkbox"/> playground	
<input type="checkbox"/> shuffleboard	
<input type="checkbox"/> skating--rink	

Landscape Dividers

chain link fence

Water Elements

<input type="checkbox"/> rivers	<input type="checkbox"/> creeks	<input type="checkbox"/> pools
<input type="checkbox"/> lakes	<input type="checkbox"/> ponds	<input type="checkbox"/> fountains
<input type="checkbox"/> brooks	<input type="checkbox"/> waterfalls	<input type="checkbox"/> other :

Buildings

<input type="checkbox"/> maintenance	<input type="checkbox"/> stable	<input type="checkbox"/> other:
<input type="checkbox"/> recreational	<input type="checkbox"/> house	
<input type="checkbox"/> concession	<input type="checkbox"/> rest rooms	
<input type="checkbox"/> picnic shelter	<input type="checkbox"/> mill	
<input type="checkbox"/> gazebo	<input type="checkbox"/> museum/visitor center	
<input type="checkbox"/> bandstand	<input type="checkbox"/> public utility	

Landscape Furniture

<input type="checkbox"/> benches	<input type="checkbox"/> planters
<input type="checkbox"/> bleachers	<input type="checkbox"/> trash cans
<input type="checkbox"/> bike racks	<input type="checkbox"/> other:
<input type="checkbox"/> bollards	
<input type="checkbox"/> drinking fountains	payphone
<input type="checkbox"/> fitness trail stations	
<input type="checkbox"/> grills	
<input type="checkbox"/> lighting fixtures	
<input type="checkbox"/> picnic tables	

Union County Parks Survey

Description	<p>Unami Park is a triangular park that contains pastoral landscapes, a mall, and various recreational components. Mature trees line the park's borders along Lexington and Union avenues, as well as encompassing a parking lot that is located on Lexington Avenue. An elevated mall runs perpendicular to Union Avenue in the park's center, and connects to East Street to the north, and several tennis courts to the northeast, via asphalt cycling trails. The mall becomes a loop within the park's center, encircling a smaller paved ring with radiating paths. There is a flagpole within the center of this inner ring and a WWII memorial boulder at its northwest border. A picnic grove, playground and modern rest room facility are located north of the loop. A playing field on the same level as the mall is located west of it, and other fields make up the bulk of the park's eastern portion. The tennis courts are located atop a rubble-stone base adjoining a rubble-stone staircase. Trees line the base's eastern edge.</p>
Setting	<p>Unami Park is bordered by Hickory Avenue to the north, Lexington and Union avenues to the east, and Grandview and Columbus avenues and Center Street to the west. All three sides of the park are bordered by 1, 1½, 2 and 2½ -story single-family dwellings.</p>
Surveyor	Gregory Dietrich
Date	10/21/03

Union County Parks Survey

Markers/ uments/ apture	flagpole boulder plaque memorializing WWII veterans, dedicated 1948
Signage	metal
Utilities	
Plantings	mature sycamores, locusts, oaks, sweetgums, maple young evergreens and maples sycamore-lined mall
CRM issues	

Character Defining Features/Resources	Contributing Features	Non-Contributing Features
Overall configuration of park Mall Sycamore trees lining parking lot Picnic groves Dense wooded area	Scattered mature trees along Union Avenue Scattered mature tree clusters demarcating different athletic fields Mature trees along tennis courts	Young tree plantings along Union Avenue

Union County Parks Survey

Contributing Resources

Tennis courts
Flagpole
WWII boulder plaque memorial
Parking lot
Athletic fields
Fieldstone steps and base

Non-Contributing Resources

Playground apparatus
Restroom facility
Picnic tables

Boundary of
eligible portion
of park

All of the property contained within Block 91, Lot 1.A; Block 92, Lot 14 of the Borough of Garwood; and Block 546, Lots 2, 3, 4, 5 and 6 of the Town of Westfield, Union County, New Jersey.

Eligible

☐ yes
☐ no

Rating

☐ Key Contributing ☐ Contributing ☐ Non-Contributing

Areas of Significance

<input type="checkbox"/> Agriculture	<input type="checkbox"/> Economics	<input type="checkbox"/> Industry	<input type="checkbox"/> Performing Arts
<input type="checkbox"/> Architecture	<input type="checkbox"/> Education	<input type="checkbox"/> Invention	<input type="checkbox"/> Philosophy
<input type="checkbox"/> Art	<input type="checkbox"/> Engineering	<input type="checkbox"/> Landscape Architecture	<input type="checkbox"/> Politics
<input type="checkbox"/> Commerce	<input type="checkbox"/> Entertainment/Recreation	<input type="checkbox"/> Law	<input type="checkbox"/> Religion
<input type="checkbox"/> Communications	<input type="checkbox"/> Ethnic Heritage	<input type="checkbox"/> Literature	<input type="checkbox"/> Science
<input type="checkbox"/> Community Planning	<input type="checkbox"/> Exploration/Settlement	<input type="checkbox"/> Maritime History	<input type="checkbox"/> Social History
<input type="checkbox"/> Conservation	<input type="checkbox"/> Health/Medicine	<input type="checkbox"/> Military	<input type="checkbox"/> Transportation

Justification of Eligibility

Acquired and developed within the period of significance (1921-1964), Unami Park is eligible under Criterion A for having made contributions to the broad patterns of history in the areas of Community Planning, Entertainment/Recreation, and Landscape Architecture on the national level; and in the area of Conservation on the state level. With regard to historic integrity, Unami Park has retained most of its original location, design, setting, materials, workmanship, feeling and association.

Union County Parks Survey



USGS 7.5' Quadrangle: Roselle, N.J. 1955 (Photorevised 1981).



Block 91, Lot 1A; Block 92, 14; of the Borough of Garwood, Block 546, Lots 2,3,4,5 & 6; of the Town of Westfield.

Surveyor Gregory Dietrich

Date 10/21/03

5.0 Description of Historic Cultural Landscapes and Evaluation of Eligibility

5.1 UNION COUNTY PARK SYSTEM

The historic designed landscapes within the Union County Park System comprise 5,260 of its total 5,574 acres. These parks consist of urban park models, as originally pioneered by the Olmsted firm, such as Rahway River and Warinanco parks; specialized parks, such as Ash Brook Golf Course and Reservation, and Galloping Hill Golf Course; riverine greenways, such as the Rahway, Elizabeth and Passaic river parkways; a scenic reservation, such as Watchung Reservation; intensive recreational parks set within naturalistic settings, such as Cedar Brook, Nomahegan, Unami, and Wheeler parks; natural and naturalistic parks with little to moderate recreational components, such as Briant, Echo Lake, Green Brook, Lenape, and Milton Lake parks; and neighborhood parks, such as Black Brook, Mattano, and Madison Avenue parks.

Lenape, Nomahegan, Rahway River Park, and Milton Lake Park are linked via the Rahway River Parkway, while Mattano Park is linked via the river to sections contained within the Elizabeth River Parkway. Other park units, such as Echo Lake, Nomahegan, Black Brook, and Galloping Hill Park are linked by land and smaller tributaries; while the rest of parks under consideration, such as Ash Brook Golf Course and Reservation, Briant Park, Cedar Brook Park, Green Brook Park, Madison Avenue Park, Unami Park, Warinanco Park, Watchung Reservation, and Wheeler Park, are largely isolated park units lacking connections to the other listed park units and to one another.

Designed in part by the Olmsted Brothers Landscape Architecture Firm of Brookline, Massachusetts, the Union County Park System was initially planned by a judicially appointed commission of volunteers in 1921 to counteract the detrimental effects of pollution upon the region's natural resources and foster a greater appreciation of nature, while also contributing to the public's health and well-being. These objectives were accomplished through the creation of a diversified collection of park units containing natural—and naturalistic—landscapes, as well as recreational facilities. The Union County Park System, like its neighboring counterparts of the time, was intended to meet the needs of New York City's expanding metropolis into the New Jersey suburbs by providing a regional network of interconnected outdoor leisure and recreational areas. Moreover, this system was meant to eventually link to other regional park systems through a complex system of greenways, riverine greenways, and vehicular parkways. Although the many of these connections were never implemented, the park system and its multifaceted components stand as a testament to the commission's resolve to not only provide leisure and recreation areas for the county's existing populations, but moreover, to anticipate the needs of its future generations.

Adopting an unconventional approach to the planning and design process of the park system, the commission succeeded in marrying two objectives: preserving and enhancing the area's resources to foster a greater appreciation of nature, and developing a highly diversified collection of recreational facilities. Unlike neighboring Essex County, which relied exclusively

on the expertise of Frederick Law Olmsted, Sr. and his successor firm to plan its system and design its individual units, the Union County Park Commission chose to utilize the expertise of successor firm, Olmsted Brothers Landscape Architecture Firm of Brookline, Massachusetts in a consulting landscape architect capacity, while also employing the skills and experience of its own team comprised of commissioners, county engineers, and landscape architects. This produced an unusual organizational arrangement that was subsequently highlighted in a nationwide survey of the time as an alternative means of comprehensively developing a regional park system.

Evaluation of Eligibility

Many of the significant aspects of this park system are largely attributable to the Olmsted Brothers, which not only made preliminary recommendations for the system as a whole, but also contributed designs for many of its individual units that were later realized to a greater or lesser extent. The firm's involvement between 1921 and 1946 and the involvement of its successor firm, Olmsted Associates, between 1962 and 1965, have resulted in a period of significance lasting from 1921 to 1964. 1921 marks the time when the Olmsted Brothers submitted its recommendations to the Union County Park Commission for a comprehensive park system, while 1964 marks the date in which the last park (Passaic River Parkway) known to be affiliated with an Olmsted firm (Olmsted Associates) was annexed into the system.

As a potential historic district, the identified resources within the Union County Park System conform to statewide historic contexts of Metropolitan New Jersey (A.D. 1910 – A.D. 1945) and Modern New Jersey (A.D. 1945 – present), owing to their ensemble role as a publicly-owned and developed regional entity contributing to the development of the state. More specifically, the proposed historic district possesses statewide significance under National Register Criterion A in the category of Conservation for its substantial accomplishments in preserving New Jersey's natural resources, both within the region of Union County and in cooperation with adjacent counties, such as Essex, Somerset, Morris, and Passaic counties.

Moreover, the proposed Union County Park System historic district possesses national significance under National Register Criterion A for its contributions to the broad patterns of development in the areas of Community Planning, Entertainment/Recreation, and Landscape Architecture. With regard to community planning, the Union County Park Commission adopted an unconventional approach to the planning of its system that built upon the strengths of America's most renowned landscape architecture firm as a means of furthering its agenda for the conservation of its natural resources and the provision for widespread and multifaceted recreation. In the area of Entertainment/Recreation, the commission's success in providing a multitude of recreational facilities through both natural and naturalistic venues and man-made facilities was so exemplary it was considered a national model for county park planning during the Union County Park System's first decade of development.

In the area of Landscape Architecture, the system is significant for its fusion of natural and naturalistic landscapes, and man-made recreation facilities, into cohesively designed units throughout the system, primarily through an Olmstedian aesthetic of planning and design. In actuality, eleven parks were planned and designed by the Olmsted Brothers, working in conjunction with the Union County Park Commission and its engineers, landscape architects, and construction crews. Beyond the system's associations with the Olmsted Brothers, there are associations with other masters as well, such as renowned golf course architects Alfred H. Tull and Robert Trent Jones. Some of these and other parks within the system are eligible under Criterion D for their capacity to yield future information pertaining to prehistory and history.

5.2 ASH BROOK GOLF COURSE AND RESERVATION

Ash Brook Golf Course and Reservation consists of a pastoral landscape containing an 18-hole golf course and pitch-and-putt course that is surrounded by a reservation containing densely wooded swampland (See Figure 13) (See Plate 84). The entrance to the golf course is accessed via a formal mall, consisting of a central green lined with trees, flanked by twin parking lots that are separated from neighboring residences by a spiked metal fence (See Plate 85). An American Revolutionary War monument, honoring the soldiers who fought in the Battle of Short Hills in 1777 within the southern portion of the reservation, is located at the entrance to the mall, while the clubhouse and golf cart garage are located at the rear (See Plate 86). The clubhouse is a modern, A-frame building with a landscaped front yard consisting of a few mature trees, two concrete planters, a lawn and a large boulder. To the right of the clubhouse is a modern 3-door, gable-front golf cart garage with doors on its front and back sides (See Plate 87). Behind the golf cart garage to the south is a pitch-and-putt course are two sheds: one with a small broken-gabled shed and the other with a gable roof.

The main course radiates out from the clubhouse into two large undulating sections that together form a giant claw. The northeastern half contains the first nine holes and the southwestern half contains the second nine holes. The fairways are separated from one another in select locations by mature tree clusters. Curvilinear golf cart roads weave around the fairways and are occasionally lined with corral fencing and young and mature evergreens (See Plate 88). A concrete substation is built into the side of a mound containing the Hole 13 of the course. Robinson's Branch of the Rahway River traverses the course and reservation and forms its southeastern border.

A service yard is located at the junction of Raritan Road and Terrill Road (See Plate 89). A slab bridge (1956) carries Raritan Road in front of the service yard (See Plate 90). This complex consists of two long buildings: one is a modern garage, and the other is an historic garage-office building. A brook running north to south forks within the vicinity of this complex, with one tributary running alongside of it. This tributary continues within the course and eventually links to Robinson's Branch.



State of New Jersey

Richard J. Codey
Acting Governor

Department of Environmental Protection
Natural and Historic Resources, Historic Preservation Office
PO Box 404, Trenton, NJ 08625-0404
TEL: (609) 292-2023 FAX: (609) 984-0578
www.state.nj.us/dep/hpo

Bradley M. Campbell
Commissioner

March 18, 2005
HPO-C2005-198 PROD
04-0835-5

Ms. Eileen Harvey
G.C. Environmental, Inc.
410 Saw Mill River Road
Ardsley, NY 10502

Dear Ms. Harvey:

In accordance with 36 CFR Part 800: Protection of Historic Properties, as published with amendments in the Federal Register on 6 July 2004 (69 FR 40544-40555), I am providing Consultation Comments for the following proposed undertaking:

**Union County, Hillside Township
1104-1132 Liberty Avenue Monopole
T-Mobile
Federal Communications Commission**

Summary: The Elizabeth River Parkway is eligible for listing on the National Register of Historic Places as a contributing part of the Union County Park System. This is a **new SHPO opinion** of eligibility.

My comments on the effect of this project are based on the *Visual Representations of the Proposed Hillside Telecommunications Monopole Project* prepared by G.C. Environmental, Inc. dated January 25, 2005. My opinion of eligibility is based on the *Cultural Landscape and Resource Survey, Union County Park System, Union County, New Jersey*, October 29, 2004, prepared by Cultural Resource Consulting Group for the Union County Division of Engineering.

800.4 Identification of Historic Properties

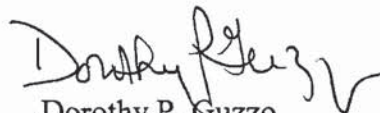
The Elizabeth River Parkway is eligible for listing on the New Jersey and National Register of Historic Places under Criteria A, C, and D as a contributing part of a larger Union County Park System Historic District.

800.5 Applying the Criteria of Adverse Effect

Based on the results of the Visual Representations of the project, it is my assessment that the project will have **no adverse effect** on the Elizabeth River Parkway.

Thank you for providing the opportunity to comment on the potential for the proposed wireless communications equipment to affect historic properties. If you have any questions, please contact Dan Saunders of my staff at (609) 633-2397.

Sincerely,

A handwritten signature in dark ink, appearing to read 'Dorothy P. Guzzo', with a stylized flourish at the end.

Dorothy P. Guzzo
Deputy State Historic
Preservation Officer



Phase II Historic Architectural Investigation
Springfield Avenue Bridge
No. 2003014 Over Rahway River
Township of Cranford, Union County, New Jersey

stretch from Springfield Avenue in Springfield Township at the north to Elizabeth Avenue in the City of Rahway to the south (RGA 2002: 4-14). Its period of significance is 1921-1968. Bridges built prior to 1968 are also contributing resources to the historic district and include the Springfield Avenue Bridge (RGA 2002: 9-19).

Three (3) historic architectural resources within the physical and visual A.P.E. contribute to the Rahway River Parkway Historic District:

- Springfield Avenue Bridge
- Cranford Canoe Club, 250 Springfield Avenue
- Cranford Clay Courts and Girl Scout Park, 301 Springfield Avenue

Union County Park System Historic District

The Union County Park System Historic District is located within the subject property; therefore, the district falls within the physical A.P.E. of the proposed project. The portion of the Union County Park System Historic District that extends into the A.P.E. mirrors the boundaries and features of the Rahway River Parkway. Since the Rahway River Parkway is a contributing resource to the Union County Park System, those resources contributing to it are also parts of the larger district. Accordingly, the Springfield Avenue Bridge is a contributing resource to the Union County Park System Historic District. On a larger scale the Union County Park System is characterized by the following description:

The historic designed landscapes within the Union County Park System comprise 5260 of its total 5574 acres. These parks consist of urban park models, as originally pioneered by the Olmsted firm, such as Rahway River and Warinanco parks; specialized parks, such as Ash Brook Golf Course and Reservation, and Galloping Hill Golf Course; riverine greenways, such as the Rahway, Elizabeth and Passaic river parkways; a scenic reservation, such as Watchung Reservation; intensive recreational parks set within naturalistic parks with little to moderate recreational components, such as Briant Park, Echo Lake, Green Brook, Lenape, and Milton Lake parks; and neighborhood parks, such as Black Brook, Mattano, and Madison Avenue parks (CRCG 2004: 133).

Three (3) historic architectural resources within the physical and visual A.P.E. contribute to the Union County Park System Historic District:

- Springfield Avenue Bridge
- Cranford Canoe Club, 250 Springfield Avenue
- Cranford Clay Courts and Girl Scout Park, 301 Springfield Avenue



Phase II Historic Architectural Investigation
Springfield Avenue Bridge
No. 2003014 Over Rahway River
Township of Cranford, Union County, New Jersey

The Rahway River Parkway Historic District

Overall this naturalistic riverine greenway retains integrity of materials, workmanship, design, feeling, association, setting, and location. According to the RGA survey, the Rahway River Parkway “retains integrity of materials and workmanship exemplified by the presence of mature trees and other plantings” (RGA 2002: 9-15). It retains its integrity of design and setting despite its exposure to improvements on the municipal level. The district retains its boundaries and therefore, its location. Given the above, the parkway still maintains its integrity of feeling as well.

On 9/18/02 the Rahway River Parkway Historic District was determined eligible for listing on the National Register of Historic Places by NJDEP-HPO. The district meets Criterion A “for its significant historical association with the conservation movement, a movement that originated with the nineteenth century national parks system” and for its role “in an evolving social movement to develop public parkland in the face of growing urbanization” (RGA 2002: 9-16). The district also meets Criterion C for “its association with a master landscape architectural firm – Olmsted Brothers Landscape Architects of Brookline, Massachusetts” (RGA 2002: 9-17). The district’s period of significance is 1921-1968. Since the district retains its integrity, and is able to convey its historical significance, it remains eligible for listing on the National Register.

Union County Park System Historic District

According to CRCG’s 2004 report on the Union County Park System, the 20 parks within the historic district retain a moderate to high level of integrity. In October 2004, CRCG determined the Rahway River Parkway was a contributing resource to the Union County Park System Historic District and on 3/18/05 the district was determined eligible by NJDEP-HPO. The district meets statewide significance under Criterion A “in the category of Conservation for its substantial accomplishments in preserving New Jersey’s natural resources, both within the region of Union County and in cooperation with adjacent counties, such as Essex, Somerset, Morris, and Passaic Counties.” It also possesses national significance under Criterion A “for its contributions to the broad patterns of development in the areas of Community Planning, Entertainment/Recreation, and Landscape Architecture” (CRCG 2004: 134). In addition, some of the parks within the system were considered eligible under Criterion D for their capacity to yield future information pertaining to prehistory and history. No evidence was found to suggest that the district is eligible under Criterion B for association with significant individuals. The period of significance is 1921-1964.

- PRN 94, Sperry Park (ID #4424.026), Riverside Drive/North Union Avenue/Springfield Avenue, eligible as a contributing element to the NRE Rahway River Parkway HD (ID #4079), NRE Union County Park System HD (ID#4424), and NCHD
- PRN 96, Flood Control Dam, Rahway River, Sperry Park, Riverside Drive, not eligible and non-contributing

Springfield Avenue

- PRN 30, Bridge (NJDOT #2003072), Springfield Avenue at Riverside Drive over Rahway River, eligible as a contributing element to the NRE Rahway River Parkway HD (Tingey 2002) and recommended eligible as a contributing element to the NCHD (NJDOT 2001)
- PRN 31, Crane's Ford Monument, Riverside Drive at Springfield Avenue, east bank of Rahway River, eligible as a contributing element to the NRE Rahway River Parkway HD (Tingey 2002) and recommended eligible as a contributing element to the NCHD
- PRN 83, Cranford Canoe Club, 8 Springfield Avenue (various), eligible as a contributing element (Bary 2012)
- PRN 84, Bridge (NJDOT #2003014) on Springfield Avenue at Orange Street crossing Rahway River, eligible as a contributing element to the NRE Rahway River Parkway HD (Tingey 2002) and recommended eligible as a contributing element to the NCHD (NJDOT 2001)
- PRN 122, Girl Scout Park, Springfield Avenue, eligible as a contributing element to the NRE Rahway River Parkway HD (Tingey 2002) (see Section 5.1.4.3 for the discussion of the Rahway River Parkway) and recommended eligible as a contributing element to the NRE NCHD
- PRN 120, Hanson Park, Hanson House, Springfield Avenue, eligible as a contributing element to the NRE Rahway River Parkway HD (Tingey 2002) (Section 5.1.4.3 for the discussion of the Rahway River Parkway HD) and recommended eligible as a contributing element to the NRE NCHD

Union Avenue

- PRN 97, Bridge (1916), Union Avenue crossing the Rahway River at Sperry Park, eligible as a contributing element to the NRE Rahway River Parkway HD, the NRE Union County Park System HD (NJDOT 2001: Union County), and the NCHD

5.1.4.2 Union County Park System Historic District. The Union County Park System HD (ID #4424) was determined eligible for National Register by the New Jersey HPO on March 18, 2005; it has never been formally nominated. The exact number of parks within the Union County Park System HD is unclear as is the exact number of parks within the entire system. The website of the Union County Department of Parks and Community Renewal (<http://ucnj.org/community/parks-community-renewal/parks-facilities/>) does not list all the parks in the county park system nor does the Dietrich report (2004)² on the park system. One of the confusing aspects of the Union County Park System HD is the parkways. Riverine parkways

² The New Jersey HPO (2/19/2010) suggested that this report should be used carefully and with independent confirmation of all information.

comprise a series of green spaces and parks along the banks of a particular river. In the APE, all the small county parks and green spaces that make up a parkway are named according to onsite signage. However, county park maps and county park literature rarely or ever delineate or discuss these small parks (i.e., Sperry, Droesch's Mill, and Lincoln in the case of this report).

Within the current APE, there are seven Union County Parks: Lenape (ID #4424.017); Nomahegan (ID #4424.021); McConnell (ID #4424.025—section of the Rahway River Parkway); Sperry (ID #4424.026—section of the Rahway River Parkway); Lincoln; Droesch's Mill; and the Rahway River Parkway (ID #4079). All of these parks are eligible for listing in the National Register as contributing elements to the NRE Union County Park System HD. Further, the Rahway River Parkway is individually eligible for the NRHP as a historic district. The period of significance of the Union County Park System HD is 1921-1968, the system's extended period of design and development. The year 2011 marked the 90th anniversary of the Union County Park system, making it one of the oldest park systems in the United States (*Suburban News* 2011).

The system's creation in 1921 is near legendary (and recounted in many documents), involving the support of more than 400 men and women and crusading Union County Sheriff James E. Warner and Cranford Township Committeeman D.C.N. Collins (*Suburban News* 2011; Union County Board of Chosen Freeholders 2010; Tingey 2002).³ When the Union County Park Commission was formed in 1921, it moved immediately to hire a landscape architect. Taking a cue from its neighbor, Essex County, the commission hired the Olmsted Brothers Landscape Architects from Brookline, Massachusetts. The Olmsted brothers were scions of the famous landscape architect Fredrick Law Olmsted (FLO). Frederick "Rick" Law Olmsted, Jr., his son, and John Charles "JC" Olmsted, his stepson, formed the firm after their father's death and the company was the nation's leading landscape design firm during the early to mid-twentieth century.

Although FLO did not die until 1903, he had been seriously ill since 1895 and had been committed to McLean Hospital in Belmont, Massachusetts in 1898 when his illness and periods of paranoia and derangement became too much for the family to manage. In 1883, JC became a partner in the firm, and in 1898, when Rick also became a partner, the firm was renamed Olmsted Brothers. The firm retained the name until 1961, well after the death of both brothers. JC kept the firm on an even keel through the last years of his stepfather's life and firmly established business practices that set the standards for other architectural practices (Pray 1922). After Rick, a Harvard graduate, entered the firm, the company grew exponentially. Rick complemented JC as JC seemed more inclined to office oversight than Rick. In general, Rick and Percival Gallagher, a senior assistant, oversaw projects in the eastern portion of the United States, while JC and Frederick Dawson worked the western portion, which included the Pacific Coast, the South, and Midwest (Klaus 1997). Rick traveled extensively and was a member of numerous far-flung boards and organizations. During the height of his city-planning activities in February 1910, his schedule for one week included stops in New York City, Newport (Rhode Island), Washington D.C., Annapolis (Maryland), and Pittsburgh, an incredible itinerary for a traveler dependent on trains (Klaus 1997).

³ D.C.N. (Newman) Collins is credited in various sources as being a Cranford Township Committeeman (*Suburban News* 2011; Union County Board of Chosen Freeholders 2010; Tingey 2002) and in others as being the township engineer (Hall 1964). Whatever his position, he influenced the decision-making process involved in the creation of the parks.

JC traveled but was by necessity not absent from the office for long periods since he was responsible for the company's general operation. When FLO died, he comprised his father's entire work force, but by 1917 the staff included the four principals, 31 draftsmen and assistants, and 16 secretarial and support staff (Klaus 1997:86). During the first six months of 1911, the firm recorded over 4,000 outgoing calls (Klaus 1997:89). Olmsted Brothers was a huge firm that continued to practice the precepts of the profession as laid down by their father, the founder of the company.

Because of its size, the company could complete a plethora of projects. The brothers designed expositions including: the Lewis and Clark, Portland, Oregon (1906), the Seattle Exposition (1900), and the San Diego Exposition (1915). They planned new towns and communities, including Vandergrift, Pennsylvania (1895), Kohler, Wisconsin (1913), Roland Park, Baltimore (which pioneered the use of protective covenants), Forest Glenn Gardens, Long Island, and Palos Verdes Estates on the coast of California. The firm also continued to plan parks for cities (e.g., Boston, Hartford, Brooklyn, Buffalo, Chicago, Milwaukee, Detroit, Seattle, Spokane, Portland, Louisville, Charleston, Atlanta, New Orleans) and for counties (Essex and Union in New Jersey). The firm designed college campuses for Smith, Mount Holyoke, Ohio State, and the U.S. Military Academy at West Point, New York, as well as smaller gardens and estates across the country (Boyd 1918).

While these works alone are impressive, the work that JC and Rick did during their non-company time is almost more impressive. They were founding members of the American Society of Landscape Architects in 1899. JC served as its first president (1899-1901) and again from 1903 to 1905; Rick also served as president twice (1907-1909 and 1918-1922). Rick in particular exerted a great civic influence. In 1900, he was hired by Harvard University to develop a curriculum for teaching landscape architecture at that university. It was the world's first comprehensive course in landscape architecture (Mann 1993). He also taught in this program from 1900-1914.

Rick was particularly interested in city planning and the specific problems of conserving natural scenery. In 1901, he was appointed by President Theodore Roosevelt to the four-man McMillan Park Commission along with Daniel Burnham, Charles McKim, and Augustus Saint-Gaudens. They were charged with restoring and developing L'Enfant's plans for Washington D.C. The work of this commission initiated the "Cities Beautiful" movement that spread to all parts of the country by the mid-1920s. As a consequence of Rick's participation, Olmsted Brothers was hired to complete many specific jobs related to the development of the McMillan plan. These included: the White House grounds, Lafayette Park, the Washington Monument Gardens, the Jefferson Memorial, the National Arboretum, Washington Cathedral, Rock Creek Park, the National Zoo, and the Shrine of the Immaculate Conception. This work also led to Rick's chairmanship (1910-1919) of the National Conference on City Planning during the same time he headed the town planning division of the United States Housing Corporation.

One of Rick's lasting legacies was in the field of conservation. He crystallized the National Park idea by framing the formula in the Congressional Act of 1916 which established the National Park Service: "To conserve the scenery and national and historic objects in such a manner and by such means as will save them unimpaired for the enjoyment of future generations" (Whiting and Phillips 1958:149). From 1929 until 1956 he served as a member of the Committee of Experts that advised on plans and policies related to Yosemite National Park. In 1945 he was employed to study and report on recreational resources of the Colorado River basin in connection with the plans of the Bureau of Reclamation for conservation of water resources in the area.

Rick was also a prolific author, publishing main articles in *Atlantic Monthly*, *Landscape Architecture*, *American City*, *Park and Recreation*, and the *Journal of American Civic Associations*. Unfortunately, a large portion of Rick's work is often attributed to his father since he dropped the use of "Jr." after his father's death and his ideas and attitudes were much the same as his father's. In 1922, the centennial year of his father's birth, Rick and Theodora Kimball (Hubbard) wrote the book, *Frederick Law Olmsted, 1822-1903*.

When Olmsted Brothers was hired in 1921 by Union County to design their park system, JC had just died after a protracted illness. The transition, however, was seamless; Rick operated the business by relying on his seasoned senior assistants, Frederick Dawson, Percival Gallagher, and Henry V. Hubbard. During the 1930s, the practice of landscape architecture changed significantly with state and federal governments replacing private clients as major clients. While the Olmsted Brothers firm was experienced at designing and managing these large-scale projects, a number of major competitors, many of whom were staffed with Olmsted Brothers alumni, entered the field. The staff at Olmsted Brothers declined from a high of 80 professionals in 1930 to just 17 in 1941 (Klaus 1997:94). Olmsted Brothers ceased operation in 1964 when only one partner, Artemas P. Richardson, remained. Richardson continued to practice privately from his office in Freeman, New Hampshire, primarily consulting on conservation-related issues throughout the 1990s although he was well past retirement age (Richardson 1996).

For 100 years an Olmsted was actively engaged in shaping the American landscape (Klaus 1997). Father and sons were extremely prolific. From the time of FLO's appointment as superintendent of Central Park in 1857 to the retirement of Rick in 1950, the firm logged over 6,000 projects with more than 3,000 of them completed (Klaus 1997:30). Olmsted Brothers was the largest landscape architecture firm in the world and they dominated the practice for nearly a century. In addition, people trained in the Olmsted office formed the influential nucleus of the profession up to World War II (Klaus 1997). The enduring legacy of the Olmsted's, father and sons, cannot be overstated.

Designing the Park System. In 1921, H.S. Chatfield, Chairman of the Union County Park Commission, solicited the services of the Olmsted Brothers for the creation of a system of public parks throughout the county. This process began with a site visit to the county and the gathering of statistical data and other information about the county and the growth of the New Jersey and New York region. In the report of its findings and recommendations, Olmsted Brothers noted, "the attractiveness of much of the County...is to be found in its pleasantly undulating topography and the quite pastoral character of the countryside, in which the chief detail of lively interest are the many streams of water and ponds along their way" (Olmsted Brothers 1921).

After studying the possibilities of a county park system, Olmsted Brothers believed that the system should be governed by six considerations:

1. The topographical character of the land.
2. The tendencies of the growth of several cities and towns.
3. The fact that the county is part of the metropolis of New York.
4. The acquirement of lands in an open country where the land is the cheapest price.
5. The choice of locations which would be shared by two or more communities.
6. The omission of playgrounds as such, because they are local in character and are associated more clearly with the school than with parks (Olmsted Brothers 1921).

With these guiding principles in mind, Olmsted Brothers assessed the physical conditions of the county. Once that was completed, the firm recommended:

1. The parking of the Elizabeth and Rahway Rivers.
2. A cross county parkway.
3. A Reservation on the Hills (the western part of the county).
4. A park in the northeast section of the county.
5. A park in the southeast section of the county.
6. A park in the southwest section of the county.
7. Special Parks, local in character, called for by consideration of either population or special natural conditions or both (Olmsted Brothers 1921) (Figure 5.10).

None of the recommendations to the county were out of line with traditional Olmsted thinking.

In 1925, Olmsted Brothers articulated in a letter to W.R. Tracy, Secretary of the Union County Park System, its view of the design of parks:

The design of a public park should be developed with three primary objects in mind. (a) It should effectively meet the requirements of its use and thus best serve its adjacent communities. (b) It should enhance the natural beauty and character of the landscape. (c) It should preferably convert unsightly or inferior land into highly improved land thus increasing its value and the value of all surrounding properties [Olmsted Brothers 1925:1].

The letter further expounds, “There appears to be no reason why a public park should not be as thoroughly and as efficiently planned and prepared for the functions it is intended to performed as a building that is to be used for the purpose of a public library or for the purposes of a school (Olmsted Brothers 1925:1). The company believed that there were certain fundamental qualities —“now generally accepted”—that best served the community: landscape beauty and recreational fitness.

There is no doubt that landscape beauty is one such quality and yet this should be an inherent one rather than one introduced through arrangements of an exotic nature in its planting: and its structures should be, as a rule, unobtrusive and harmonious rather than conspicuous although, when advisable, architectural forms of refinement in line and proportion strike a note of beauty in their formal contrast with the surrounding naturalistic landscape forms...To man recreation means active sports and games out-of-doors, but there are equally valuable forms of recreation of a passive sort, such as the quiet contemplation of landscape scenes and the enjoyment of the freedom of open green spaces away from the noise and grime of the urban streets [Olmsted Brothers 1925:2].

In 1922, Olmsted Brothers wrote to Plainfield, New Jersey, Mayor Charles Loizeaux about its municipal park, Green Brook Park, which would shortly become a part of the Union County system:

The dominant element of a park is its landscape, affording varied views of broad expanse of open turf fringed about with trees, of sun-flecked glades reaching away among shaded groves, of glistening water lying

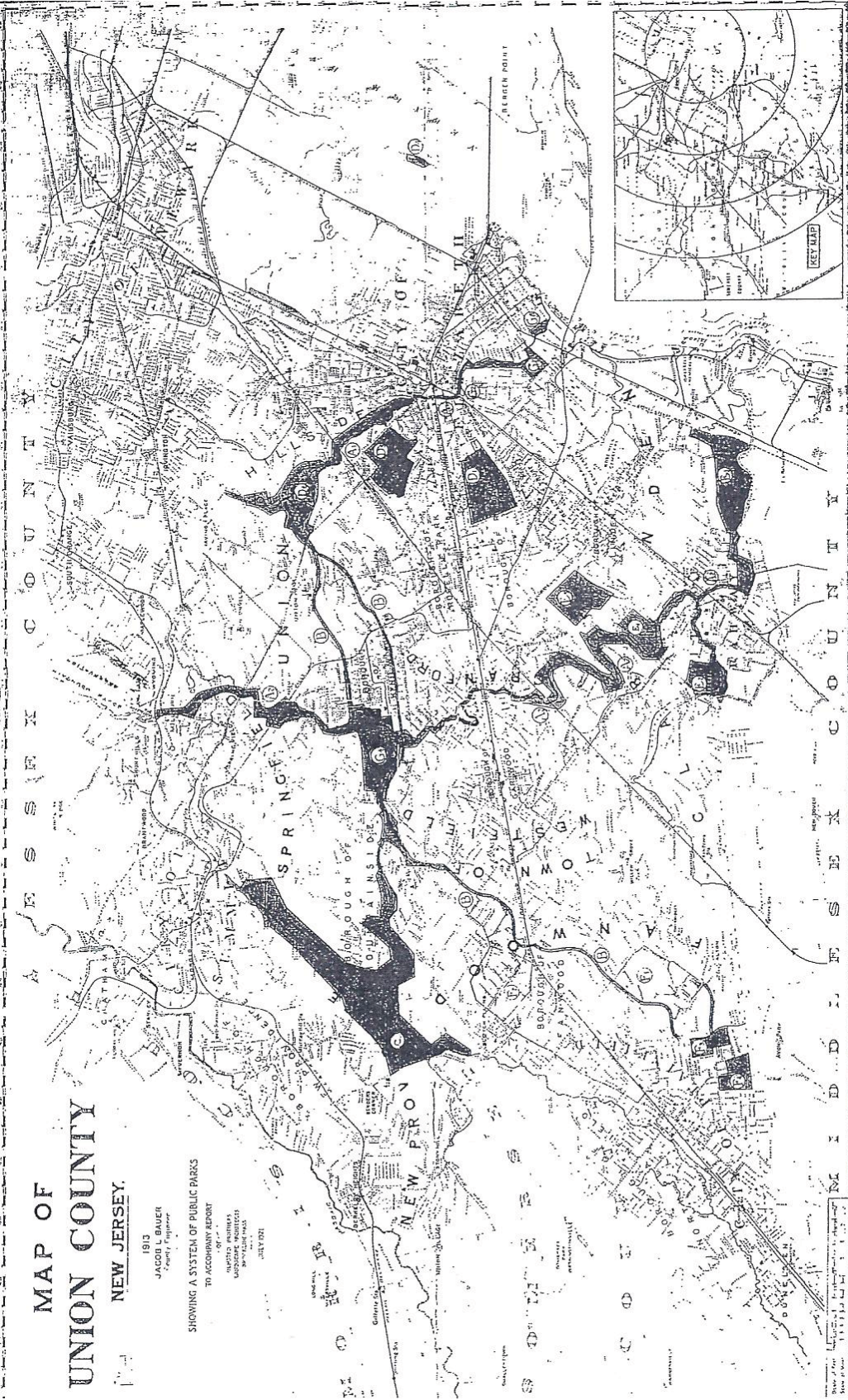


Figure 5.10. Map of Union County showing Olmsted Brothers proposed system of public parks, 1921 (Dietrich 2004: Figure 9).

still in ponds or flowing in brooks or in rivers. Such scenes enriched by flowering plants are the crowning glory of the park. They create a scene of freedom or of escape from the urban condition of everyday life of the city dweller that is beyond estimate in its value upon the minds of those beholding them even when only relatively conscious of their actual charm and beauty. It is well established that landscape scenery, particularly of a quiet, pastoral character, has a profoundly soothing effect, and is highly re-creational [sic] to the spiritual nature of man...Our general idea in the development of design for the park would be to seek every opportunity to enhance whatever landscape quality the land affords and to locate the special feature necessary to the use and enjoyment of the park in such relation to one another and the landscapes as will form agreeable compositions [Olmsted Brothers 1922:6-7].

Although Olmsted Brothers clearly favored a less organized and more passive approach to recreation pursuits within the confines of a park, the Union County Park Commission felt that the county parks should have a strong recreational core. Therefore, they rejected the sixth governing principle as outlined by the Olmsted's. There would be "playgrounds" or organized recreation at the parks. The commission also chose to create a generalized plan of park locations that could be changed based on future needs, rather than opting for a formal arrangement where specific parks were fixed at specific locations. If the specific location of all the parks was announced, real estate speculation would cause land prices to soar and might cause land donations to dwindle. By not announcing specific locations, the commission was ensuring that planning would not be dictated by the volatile forces of the real estate market or that philanthropy would cease. In addition, by planning the park system in increments across time, the commission would be better able to address the needs of its citizens which it could not have foreseen in the system's infancy (Dietrich 2004).

The Commission's plans worked and Olmsted Brothers adapted the Olmsted ideas of what made parks work to satisfy the needs of its clients, retaining many of the ideals and concepts regarding the contemplation of beauty and the mix of passive recreation with active recreation. The Union County parks were designed with passive and active activity segments; the designers taking their cues from the landscape. In many cases the largest landscape element was a river—the Elizabeth River or Rahway River; this is particularly true for the parks located within the APE.

5.1.4.3 Rahway River Parkway Historic District. The Rahway River Parkway (ID #4079) is in actuality a system of parks and open spaces along the banks of the Rahway River. This system has been quite fluid during the course of the twentieth century, as the parks and green spaces have changed names, grown larger, become parks independent of the parkway, or have joined with municipal parks. The parkway itself is so big that it has traditionally been divided into sections and is often discussed that way. Unfortunately, the number of sections, the name of the sections, and the geographic boundaries of those sections have not been standard across the history of the parkway. At present, according to the Union County Department of Parks, the Rahway River Parkway is divided into three sections (north to south)—Cranford, Clark/Linden, and Rahway (Figures 5.11, 5.12, 5.13). Nevertheless, the parkway historically stretches north from the Cranford section passing through what is now Nomahegan Park (the park originally did not extend east over to the river) and continuing north through Lenape Park and out the northeast edge of Lenape Park up to Springfield Avenue in Springfield Township (Figure 5.14; see Figure 5.10).

APPENDIX C: TILE STREET SIGN INVENTORY

HPAB began an inventory of Cranford's unique tile street sign's in 2010 in cooperation with Cranford Girl Scout Troop 779. HPAB created a database, and the scouts recorded the condition and location of 77 of the surviving street signs. Photographs of selected signs were collected by HPAB and the scouts. The inventory of recorded signs is below in Table 10, and the locations are depicted in Fig. 30. Ninety-six of the original posts still stand. HPAB recommends finalizing the condition assessment of the surviving signs at Phase 2, and developing conservation treatments in conjunction with the Township.

Tile Sign History

CRANFORD STREET POSTS

By Maureen Strazdon, Cranford Historic Preservation Advisory Board
May 2012

Even before they were all installed, Cranford's unique street name signs were the subject of much comment and controversy. An article from The Cranford Citizen and Chronicle of April 25, 1929 reported on the complaints the Township Committee had heard about the difficulty of reading the street sign posts. In spite of those concerns and others expressed over the years, Cranford still boasts the signs, and the Girl Scouts of Troop 779 are documenting them.

You might have noticed that there are two types of posts. The ones with the blue letters on yellow tiles are the originals. The ones with yellow letters on the blue background are newer and are the posts the township is using to replace the originals.

Street signs were first installed in 1906 when the Township decided that the streets of the growing town needed name signs. The Township Committee discussed the signs in May of that year and 190 signs were installed by Christmas. These weren't the posts we know today. They were the usual street signs that you see in virtually every other town – plates with the letters running horizontally.

In early 1929 the town decided to install a new style of markers, a concrete post with the street name in blue on yellow-colored tiles set vertically. The tiles were manufactured by Mueller Tile in Trenton and the original 200 signs, which were installed by the end of the year, cost \$1,021. One of the signs is even pictured in one of the company's catalogs (Fig 28).



Fig. 28: Mueller Tile Catalog, 1924.
(Image: Internet Archive)

The Mueller Mosaic Company was established in 1908 and produced tiles for many important buildings and structures, including many New York subway stations. They also made the Cranford Rotary sign that still stands at the corner of North and Springfield Avenues (Fig. 29). The Mueller Company closed in 1941 with the death of the owner.

As the original posts have been damaged or destroyed, they have been replaced by signs with yellow letters on a blue background.

Six Girl Scouts from Troop 779 at Hillside School have found and documented 77 of the remaining posts. The Scouts, Sarah Cuprewich, Jenna Ellenbacher, Julianne Hodgkins, Nikki Pascual, Kylie Remley, Phoebe Weiman, have chosen to work with the Historic Preservation Advisory Board on this and other projects. The Scouts are working toward their Girl Scout Silver Award, which represents a girl's accomplishments in Girl Scouting and her community. The Scouts have also aided in creating a database of the historical structures in town, with the Historic Cranford Scavenger Hunt, and with the Memorial Day Parade float featuring the North Cranford Historic District.



Fig. 29: Mueller Tile Catalog, 1924. (Image: Internet Archive)

Table 10: Tile Street Sign Inventory

ID	Street1	Street2	Corner	Status	Condition	PhotoNum
1	South Union Ave	Marsh St	W	Exists	Good	need photo
2	Benjamin St	Williams St	E	Exists	Good	need photo
3	Cranford Ter.	Rose St.	S	Exists	Fair	1
4	Cranford Ter.	Edward Pl.	S	Exists	Fair	2
5	Cranford Ter.	Garden Pl.	S	Exists	Good	3
6	High St.	Bluff St.	SW	Exists	Good	4
7	High St.	Chesnut St.	NE	Exists	Good	5
8	Centenial Ave.	Cummings St.	N	Exists	Fair	6
9	Elm St.	Stratford Ter.	NE	Exists	Good	7
10	Elm St.	Broad St.	NE	Exists	Good	8
11	N Lehigh Ave.	Stratford Ter.	N	Exists	Good	9
12	N Lehigh Ave.	Lawn Ter.	E	Exists	Fair	10
13	Hawthorne St.	Mansion Ter.	W	Exists	Good	11
14	Walnut Ave.	Blake Ave.	E	Exists	Good	12
15	Meeker Ave.	Quine St.	NE	Exists	Good	13
16	Meeker Ave.	Heinrich St.	NE	Exists	Good	14
17	Meeker Ave.	Hale St.	SE	Exists	Good	15
18	Burchfield Ave.	Cummings St.	SW	Exists	Good	16
19	Thomas St.	Cummings St.	SE	Exists	Fair	17
20	Burchfield Ave.	South Ave. E	E	Exists	Good	18
21	Berkeley Place	Pittsfield Street	S	Exists	Fair	need photo
22	Pawnee Road	Springfield Avenue	SW	Exists	Fair	19
23	Burnside Ave	Besler Ave	N	Exists	Good	need photo
25	Burnside Ave	Roosevelt Ave	N	Exists	Good	need photo
26	Burnside Ave	Lincoln Ave	W	Exists	Fair	need photo
24	West Holly Street	Spring Garden	NE	Exists	Good	need photo
27	Christopher St	Union Ave	W	Exists	Good	need photo
28	Greaves Place	West Holly Street	NE	Exists	Good	need photo
29	Hillcrest Avenue	Besler Avenue	W	Exists		need photo
31	Hillcrest Avenue	James Ave	W	Exists		need photo
32	Hillcrest Avenue	Marsh Street	E	Exists		need photo
33	Hillcrest Avenue	New Street	W	Exists		need photo
34	Hillcrest Avenue	Roosevelt Avenue	N	Exists		need photo
35	Hillcrest Ave	Williams St	E	Exists		need photo
30	Springfield Ave	Hampton Road	N	Exists	Good	need photo
36	James Ave	Burnside Ave	N	Exists		need photo
37	James Ave	Union Ave	N	Exists		need photo
38	Norman Place	Pittsfield Street	N	Exists		need photo
39	Orchard Street	Hampton Street	N	Exists		need photo
40	Orchard St	Pittsfield St	E	Exists		need photo
43	Beech Street	Tulip Street	W	Exists		need photo
44	West End Place	Springfield Avenue	E	Exists		06 57
42	Preston Ave	Hillcrest Ave	N	Exists		need photo
45	Williams Street	Retford Avenue	N	Exists		need photo
46	Williams Street	Union Avenue	N	Exists		need photo

Table 10: Tile Street Sign Inventory

ID	Street1	Street2	Corner	Status	Condition	PhotoNum
47	Woodlawn Avenue	Union Avenue	N	Exists		need photo
48	Bargos Place	Central Avenue	W	Exists		need photo
49	Central Ave	Claremont Pl	W	Exists		06 08
50	Central Avenue	Day Place	S	Exists		06 10
51	Central Avenue	Maple Place	S	Exists		06 12
52	Central Avenue	Frazer Place	E	Exists		06 14
53	Central Avenue	Craig Place	S	Exists		06 16
54	Normandie Place	LaSalle Avenue	W	Exists		06 19
55	Normandie Place	Elmora Avenue	N	Exists		06 22
56	Normandie Place	Maple Place	E	Exists		06 25
57	Normandie Place	Franklin Avenue	N	Exists		06 27
58	Normandie Place	Claremont Place	W	Exists		06 28
59	Normandie Place	Riverside Drive	N	Exists		06 29
60	Riverside Drive	Richmond Avenue	E	Exists		06 30
61	Elmora Avenue	Dunham Avenue	E	Exists		06 34
62	LaSalle Avenue	Dunham Avenue	E	Exists		06 35
63	Manor Avenue	Dunham Avenue	SE	Exists		06 36
64	Columbia Avenue	Dunham Avenue	SE	Exists		06 38
65	Orange Avenue	Raleigh Avenue	E	Exists		06 41
66	Orange Avenue	Pacific Avenue	W	Exists		06 42
67	Orange Avenue	Columbia Avenue	N	Exists		06 44
68	Orange Avenue	Parker Avenue	E	Exists		06 46
69	Orange Avenue	Craig Place	W	Exists		06 47
70	Orange Avenue	Linden Avenue	E	Exists		06 48
71	Orange Avenue	Frazer Place	W	Exists		06 50
72	Orange Avenue	Maple Place	N	Exists		06 51
73	Orange Avenue	Day Place	N	Exists		06 52
74	Balmiere Place	Springfield Avenue	E	Exists		06 58
75	brookside place	arnet	S	Replaced	Fair	need photo
76	Holly Street	Alden Street	S	Exists	Good	need photo
77	Orchard	Brookside	S	Exists		need photo
78	Brookside Place	Hillside Place	E	Exists		need photo
79	Cedar	5th	NE	Exists		need photo
41	Orchard St	Tulip St	S	Exists		need photo
80	Hamilton	Sylvester	E	Replaced		need photo
81	Hamilton	Sylvester	N	Exists		need photo
82	Forest	Cranford	E	Exists		need photo
83	N Union Ave.	Riverside Drive	NW	Exists		need photo
84	Prospect	Riverside Drive	N	Exists		need photo
85	Prospect	Claremont	E	Exists		need photo
86	Prospect	Maple Place	N	Exists		need photo
87	Prospect	Manor	S	Exists		need photo
88	N Union Ave.	Lenox	W	Exists		need photo
89	Tuxedo	Lenox	W	Exists		need photo
90	Casino	Maple Place	E	Exists		need photo
91	Casino	Claremont	S	Exists		need photo

Table 10: Tile Street Sign Inventory

ID	Street1	Street2	Corner	Status	Condition	PhotoNum
92	Rankin	Roosevelt	S	Exists		need photo
93	Rankin	Preston	E	Exists		need photo
94	Rankin	Besler	S	Exists		need photo
95	Burnside Ave	New Street	N	Exists		need photo
96	Burnside Ave	Elise	W	Exists		need photo
98	North Avenue E	Elizabeth Avenue	NW	Exists		need photo
97	North Avenue E	Carpenter Place	NW	Exists		need photo

Table 10: Tile Street Sign Inventory

(Page intentionally left blank)

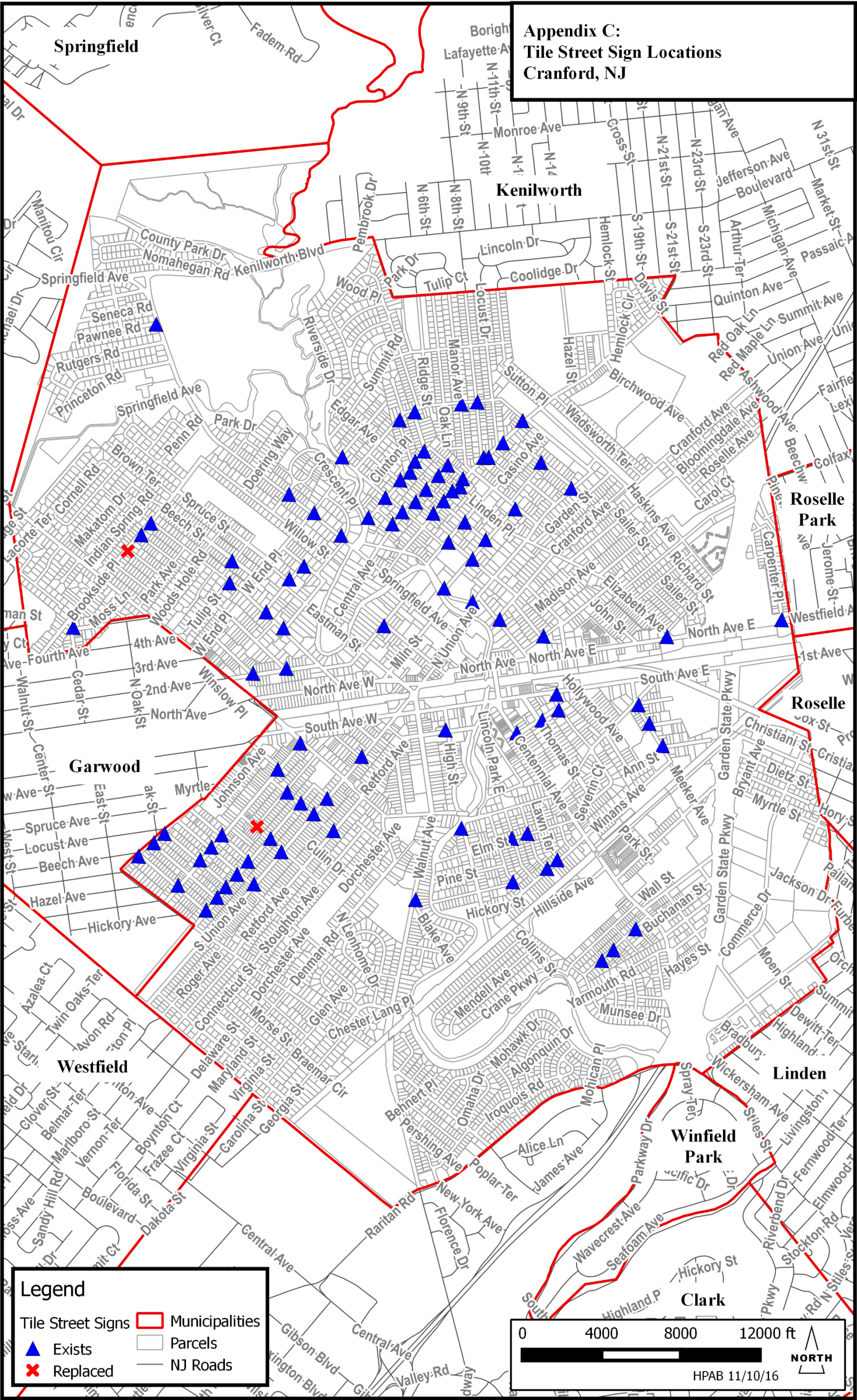


Figure 30: Tile Street Sign Locations

(Page intentionally left blank)

APPENDIX D: CRANFORD HISTORIC PRESERVATION ORDINANCE

The Cranford Historic Preservation Ordinance (Cranford New Jersey Municipal Code, §6-40.1 – §6-40.24) was originally passed in 1993 (Ord. No. 93-71), and most recently amended in 2013 (Ord. No. 2013-01). The ordinance is codified with the municipal code and available from:
<http://www.ecode360.com/11246912>

(Page intentionally left blank)

ARTICLE XIII B

Historic Preservation Advisory Board

[Added 12-28-1993 by Ord. No. 93-71; amended 7-20-2004 by Ord. No. 2004-30; 6-13-2006 by Ord. No. 2006-22; 5-11-2010 by Ord. No. 2010-25; 3-26-2013 by Ord. No. 2013-01]

§ 6-40.10. Findings, purposes and objectives.

- A. The historical, cultural, architectural and social heritage of the Township of Cranford is given in trust from generation to generation to be used, enriched and then passed on.
- B. The character, lifestyle and very quality of life in the Township of Cranford depend in great measure on the protection of this heritage.
- C. The presence of historic landmarks and districts as an essential element of the Township character and identity is an important factor in the economy of the Township and the property values therein.
- D. Such landmarks and districts are vital to the education and civic-mindedness of the residents of the Township of Cranford.
- E. The welfare of the Township is dependent on the preservation of its historic heritage for the reasons set forth above.
- F. This article will achieve these purposes and objectives and will promote the preservation of the environment, promote visual harmony through creative development techniques and good civic design and arrangements, prevent the degradation of the environment through improper use of land and promote the most appropriate use of land in the Township.

§ 6-40.11. Definitions.

Unless specifically defined below, words or phrases used in this article shall be interpreted so as to give them the meanings they have in common usage and to give this article its most reasonable application.

ADMINISTRATIVE OFFICER — The Construction Code Official, who is the person designated by the Township to handle the administration of building permit applications, shall be the administrative officer.

BUILDING — A structure created to shelter any form of human activity.

CONTRIBUTING — Building, site, structure or object that adds to the historic architectural qualities, historic associations or archaeological values for which an historic district is significant because: it was present during the period of significance and possesses historic integrity reflecting its character at that time or is capable of yielding important information about the period; or it independently meets the criteria for significance as set forth in § 6-40.24B(1) through (8).

DESIGNATED LANDMARK OR DISTRICT — An individual building, structure, site, object or district which has been designated as having historical, architectural, cultural, aesthetic or other significance pursuant to the provisions of this chapter.

HISTORIC DISTRICT — A geographic area with distinctly definable boundaries composed of multiple buildings, sites, structures, or objects which has acquired a unity of character through the interrelationships of the components; and has been designated as having historical, archeological, cultural, scenic, architectural or other significance pursuant to the provisions of this chapter.

HISTORIC SITE — See "site."

INTEGRITY — The authenticity of a landmark or district's historic identity, evidenced by the survival of physical characteristics that existed during the period of significance.

LANDMARK — A building, structure, site or object which has a special character or special historical or aesthetic interest as part of the development, heritage or cultural characteristics of the Township, state or nation and which has been designated as a landmark pursuant to the provisions of this chapter.

LANDMARK DESIGNATION — See "designated landmark or district."

MASTER PLAN — The Master Plan of the Township of Cranford, as amended from time to time, compiled pursuant to the Municipal Land Use Law.

MUNICIPAL LAND USE LAW — The Municipal Land Use Law of the State of New Jersey, P.L. 1975, c. 291 (N.J.S.A. 40:55D-1 et seq.), as amended from time to time.

NONCONTRIBUTING — A building, site, structure or object that does not add to the historic architectural qualities, historic associations or archaeological values for which a district is significant because: it was not present during the period of significance; alterations, disturbances, additions or other changes cause it no longer to possess historic integrity, or it is incapable of yielding important information about the period; nor does it independently meet the National Register criteria.

OBJECT — Construction that is primarily artistic in nature or is relatively small in scale and simply constructed, as distinguished from a building or structure; although it may be movable by nature or design, an object is associated with a specific setting or environment such as statuary in a designed landscape; objects should be located in a setting appropriate to their significant historic use, role or character.

OFFICIAL NEWSPAPER — The official newspapers as designated by the Township governing body.

SITE — The location of a significant event, a prehistoric or historic occupation or activity, or a ruin, where the location itself maintains historic or archaeological value regardless of the value of any existing buildings, structures or objects.

STRUCTURE — As distinguished from buildings, those functional constructions made usually for purposes other than creating shelter.

§ 6-40.12. Creation.

There is hereby created in and for the Township of Cranford a Board to be known as the "Historic Preservation Advisory Board of the Township of Cranford."

§ 6-40.13. Composition.

The Historic Preservation Advisory Board shall be comprised of eight members, including a liaison from the Township Committee, with two alternates, and associate members as warranted.

A. Of the eight regular Board members:

- (1) At least one less than a majority shall be designated as "Class A" and "Class B," said classes defined as:
 - (a) Class A: a person who is knowledgeable in building and construction or architectural history and who may reside outside the municipality.
 - (b) Class B: a person who is knowledgeable in or has a demonstrated interest in local history and who may reside outside the municipality.
- (2) Four shall be Class C members, said class being defined as citizens of the municipality who shall hold no other municipal office, position or employment except for membership on the Planning Board or Board of Adjustment. One Class C member shall be a Cranford High School student recommended by the high school history faculty.
- (3) To the extent available from within the Township of Cranford, the Mayor shall appoint, with regard to Class A, trained individuals from the discipline of architecture or architectural history who meet the requirements as set forth in the 1980 amendments to the National Historic Preservation Act.
- (4) A Township Committee member who serves as liaison with the Board.

B. Of the two alternate Board members, both shall be Class C members, as Class C is defined above. The two alternate Board members shall be respectively designated "Alternate No.1" and "Alternate No. 2."

C. Alternate members may participate in discussion of the proceedings but may not vote except in the absence or disqualification of a regular member. A vote shall not be delayed in order that a regular member may vote instead of an alternate member. In the event that a choice must be made as to which alternate member is to vote, Alternate No.1 shall vote.

D. Interested members of the community may participate on the Board as associate members. Associate members may participate in discussion of the proceedings, but may not vote.

E. The Township Committee liaison shall vote on all questions.

§ 6-40.14. Appointment of members.

Regular members shall be appointed by the Mayor and shall serve three-year terms, except that Municipal Planning Board or Zoning Board of Adjustment members shall serve during their incumbency in office. The terms of the members first appointed under this article shall be so determined that, to the greatest practicable extent, the expiration of such terms shall be distributed, in the case of the regular members, evenly over the first three years after their appointment, provided that the initial term of no regular member shall exceed three years and that the initial term of no alternate members shall exceed two years. Associate members shall be appointed by the Mayor and shall serve for one-year terms. If any member of the Board misses three consecutive meetings without a legitimate reason, he or she shall be removed from the Board and so notified by the Mayor. Vacancies shall be filled, within 60 days, in the same manner in which the previous incumbent was appointed and for such incumbent's unexpired term.

§ 6-40.15. Appointment of Chairman and Vice Chairman.

The Mayor shall appoint a Chairman and Vice Chairman.

§ 6-40.16. Rules and regulations.

The Historic Preservation Advisory Board shall create rules and procedures for the transaction of its business, subject to the following:

- A. A quorum for the transaction of business shall consist of five of the Historic Preservation Advisory Board members, including the Chairman or, in his absence, the Vice Chairman.
- B. The Historic Preservation Advisory Board shall appoint a Secretary, who need not be a member of the Board. The Secretary shall keep minutes and records of all meetings and proceedings, including voting records, attendance, resolutions, findings, determinations and decisions. All such material shall be public record.
- C. The Historic Preservation Advisory Board shall prepare and adopt rules of procedure which will be made available to the public and which include a specific section prohibiting conflicts of interest.

§ 6-40.17. Compensation.

The members of the Historic Preservation Advisory Board shall serve without compensation.

§ 6-40.18. Appropriating funds; retaining professional assistance.

The governing body may make provision in its budget and appropriate funds for the expenses of the Historic Preservation Advisory Board. The Historic Preservation

Advisory Board may employ, contract for and fix the compensation of experts and other staff and services as it shall deem necessary, subject to the availability of appropriated funds. The Board shall obtain its legal counsel from the Municipal Attorney at the rate of compensation determined by the governing body. Expenditures pursuant to this section shall not exceed, exclusive of gifts or grants, the amount appropriated by the governing body for the Board's use.

§ 6-40.19. Powers and duties.

The Historic Preservation Advisory Board shall have the following powers and duties:

- A. To identify, record and maintain a system for survey and inventory of all building sites, places and landmarks and structures of historical or architectural significance based on the Secretary of the Interior's Standards and Guidelines for Archaeology and Historic Preservation (Standards and Guidelines for Identification) and to aid the public in understanding their worth, methods of preservation, techniques of gathering documentation and related matters.
- B. To prepare and adopt, pursuant to § 6-40.24 of this article, a public landmark and historic district designation list and official map which shall then be referred to the Planning Board for consideration of inclusion in the Master Plan and to the governing body of the Township of Cranford for consideration of inclusion in the Zoning Ordinance.¹
- C. To propose to the Township Committee an historic river district including several public facilities along the Rahway River.
- D. To amend, from time to time, as circumstances warrant, the landmark historic district designation list and landmark historic district map in the manner set forth in § 6-40.24.
- E. To make recommendations to the Planning Board on the historic preservation plan element of the Master Plan and on the implications for preservation of historic sites and historic districts of any other Master Plan elements.
- F. To advise the Planning Board on the inclusion of historic sites in the recommended capital building, site, structure, object program.
- G. To advise the Planning Board and Board of Adjustment on applications for development pursuant to § 6-40.23 of this article.
- H. To provide written reports on the application of the Zoning Ordinance provisions concerning historic preservation.
- I. To report at least annually to the Planning Board and the governing body of the Township of Cranford on the status of historic preservation in the Township and to recommend measures to improve the same.

1. Editor's Note: See Ch. 136, Land Development, Art. V, Zoning.

- J. To collect and disseminate material on the importance of historic preservation and specific techniques for achieving the same.
- K. To advise all municipal agencies regarding goals and techniques of historic preservation and advise the Township Committee and Planning Board of the relative merits of proposals involving public funds to restore, preserve and protect historic buildings, places and structures, including the preparation of long-range plans therefor, securing state, federal and other grants and aid to assist therein and monitoring such projects once underway.
- L. To adopt and promulgate such regulations and procedures not inconsistent with this article as are necessary and proper for the effective performance of the duties herein assigned.
- M. To review all proposed National Register nominations for properties that come within the jurisdiction of the Board, as established by the criteria of the 1980 amendments of the National Historic Preservation Act of 1966.

§ 6-40.20. Meetings.

The Historic Preservation Advisory Board shall meet on a regular basis, but at a minimum of four times a year. All meetings shall comply with the Open Public Meetings Act (N.J.S.A. 10:4-7 et seq.).

§ 6-40.21. Conflicts of interest.

No member of the Historic Preservation Advisory Board of Cranford Township shall be permitted to act on any matter coming before the Board in which he or she has, either directly or indirectly, any personal or financial conflict of interest.

§ 6-40.22. Emergency meetings.

- A. In the event that there arises a need for an emergency meeting of the Historic Preservation Advisory Board, the Chairman shall convene a meeting of the Board having given due and appropriate notice in accordance with the Open Public Meetings Act.²The Chairman shall instruct the Secretary to contact the members to inform them of the time, date, location and purpose of the emergency meeting.
- B. Emergency meetings shall be convened for such instances as pending demolition permits for designated historic sites or contributing building, site, structure, or object within an historic district in the Township or such other circumstances that warrant the immediate action of the Historic Preservation Advisory Board.
- C. In the case of any emergency meeting, all of the requirements for the transaction of business during a regularly scheduled meeting shall apply.

2. Editor's Note: See N.J.S.A. 10:4-7 et seq.

§ 6-40.23. Demolition permit applications.

(Reserved)

§ 6-40.24. Designation of landmarks and historic districts.

- A. Survey. The HPAB shall maintain a comprehensive survey of the Township of Cranford to identify historic landmarks and historic districts that are worthy of protection and preservation.
- B. Criteria for designation. The criteria for evaluating and designating historic landmarks and historic districts shall be guided by the National Register criteria. The Historic Preservation Advisory Board or any person may recommend designation of historic landmarks or historic districts that are in accordance with the National Register criteria or that possess one or more of the following attributes:
 - (1) Character, interest, or value as part of the development, heritage or cultural characteristics of the Township, state or nation; or
 - (2) Association with events that have made a significant contribution to the broad patterns of our history; or
 - (3) Association with the lives of persons significant in our past; or
 - (4) Embodiment of the distinctive characteristics of a type, period or method of construction, architecture, or engineering; or
 - (5) Identification with the work of a builder, designer, artist, architect or landscape architect whose work has influenced the development of the Township, state or nation; or
 - (6) Embodiment of elements of design, detail, material or craftsmanship that render a building, site, structure, object architecturally significant or structurally innovative; or
 - (7) Unique location or singular physical characteristics that make a district or landmark an established or familiar visual feature; or
 - (8) Ability or potential ability to yield information important in prehistory or history.
- C. Procedures for designation.
 - (1) Persons wishing to make a nomination shall contact the HPAB regarding consideration of a proposed historic landmark or historic district. The HPAB may also initiate the consideration of an historic landmark or historic district.
 - (2) A nomination to propose an historic landmark shall include the following information which addresses the criteria for designation as set forth herein:

- (a) A photograph of the proposed landmark; and
 - (b) A copy of the municipal tax map showing the property on which the proposed landmark is located; and
 - (c) A physical description of the proposed landmark; and
 - (d) A statement of significance.
- (3) A nomination to propose an historic district shall include the following information which addresses the criteria for designation as set forth herein:
- (a) A building-by-building inventory of all properties within the district; and
 - (b) A photograph of all properties within the district; and
 - (c) A copy of the municipal tax map of the district showing boundaries; and
 - (d) A physical description of the proposed district; and
 - (e) A statement of significance.
- (4) Following receipt of a nomination to propose an historic landmark or historic district, the HPAB shall evaluate the proposal and, if fitting the criteria, schedule a public meeting on the proposed designation which may coincide with the regularly scheduled meetings of the HPAB.
- (5) At least 20 days prior to the public meeting, the HPAB shall, by personal service or regular mail, perform the following:
- (a) Notify the owner(s) of record of a property that has been proposed for historic landmark designation, or the owner(s) of record of all properties located within a district that has been proposed for historic district designation, that the property or district, as applicable, is being considered for such designation and the reasons therefor;
 - (b) Advise the owner(s) of record of the significance and consequences of such designation, and of the rights of the owner(s) of record to contest such designation under the provisions of this article;
 - (c) Notify the owner(s) of record of the date, time and location of the meeting concerning the proposed designation of the property or district; and
 - (d) Serve any further notices as may be required under the provisions of the Municipal Land Use Law.
- (6) At least 20 days prior to the public meeting, the HPAB shall also cause public notice of the meeting to be published in the official newspaper of the Township.

- (7) At least 20 days prior to the public meeting, a copy of the nomination report shall be made available for public inspection in the municipal offices of the Township.
- (8) At the public meeting scheduled in accordance with this article, the HPAB shall review the nomination report and accompanying documents. Comments from interested persons, both in person and in writing, citing specific reasons in favor of or against the proposal, shall be heard and discussed. The HPAB may schedule subsequent public meetings on the nomination to ensure maximum community input and address objections.
- (9) If the proposed nomination is approved by the HPAB, then the HPAB shall forward a report concerning the proposed historic landmark or historic district to the Township Committee, which shall contain a statement of the HPAB's recommendations and the reasons therefor. Township Committee action on designating an historic landmark or an historic district shall then be otherwise subject to those procedures and statutes which apply to a change of a zoning designation and the adoption, revision or amendment of any development regulation.
- (10) All buildings, structures, sites or objects within a designated historic district will be considered part of the district.
- (11) All other requirements of the Municipal Land Use Law regarding adoption of development regulations shall be followed.
- (12) Upon adoption of an article by the Township Committee designating an historic landmark or an historic district, the designation shall supplement, rather than supersede, the existing zoning district in which the affected historic landmark or historic district is located. At that time, the designation list and map shall be incorporated into the Master Plan and Zoning Ordinance of the Township as required by the Municipal Land Use Law. Designated properties and districts shall also be noted as such on the records for those properties as maintained by the Planning and Zoning offices, as well as the offices of the Construction Official, the Township Tax Assessor and the Township Clerk.
- (13) Historic districts may be enlarged or reduced in accordance with the provisions of this article.